



U.S. Department of Transportation
Federal Railroad Administration



BALTIMORE-WASHINGTON
SUPERCONDUCTING
MAGLEV PROJECT

FINAL
Preliminary Alternatives Screening Report

January 2018

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Table of Contents

Chapter 1.	Introduction.....	1
Chapter 2.	Purpose and Need Summary	4
Chapter 3.	SCMAGLEV Alternatives Development Process	5
Chapter 4.	Screening Level 1	10
Chapter 5.	Screening Level 2	17
Chapter 6.	Agency and Public Coordination.....	31
Chapter 7.	Recommended Preliminary Alternatives for Detailed Analysis	37
Chapter 8.	Conclusions and Next Steps.....	42

Figures

Figure 1:	Alignments Screened in the PASR	3
Figure 2:	SCMAGLEV Alternatives Development Process	5
Figure 3:	Initial Alignments.....	9
Figure 4:	Baltimore Station Zones	13
Figure 5:	BWI Marshall Airport Station Zone.....	13
Figure 6:	Washington, DC Station Zones	14
Figure 7:	Preliminary Alignments.....	18
Figure 8:	Typical Viaduct Cross Section for the Preliminary Alignments	19
Figure 9:	Typical Tunnel Cross Section for the Preliminary Alignments	20
Figure 10:	Guideway Analysis Zones for Parks/Federal Lands	26
Figure 11:	Guideway Analysis Zones for Residential/Cultural	26
Figure 12:	Preliminary Alignment Presented at the October 2017 Public Open House Meetings	30
Figure 13:	Public Open House Meeting Locations.....	33
Figure 14:	Alignments Recommended for Detailed Analysis	41

Tables

Table 1:	Initial Alignments and Summary Descriptions.....	7
Table 2:	Acceptable Geometric Design Criteria.....	10
Table 3:	Screening Level 1 Results – Initial Alignments	11
Table 4:	Screening Level 1 Results - Station Zones.....	16
Table 5:	Segment Length by Construction Method	21
Table 6:	Screening Level 2 Evaluation Criteria Summary	22
Table 7:	Screening Level 2 Notes and Threshold Ranges.....	23
Table 8:	Screening Level 2 Results Presented at the October 2017 Public Open House Meetings.....	29
Table 9:	Summary of Public Comments.....	34
Table 10:	Final Screening Level 2 Results.....	40

Appendices

Appendix A	Previous Maglev Study Alternatives
Appendix B	Additional Screening Details
Appendix C	Agency and Public Outreach
Appendix D	Baltimore-Washington SCMAGLEV Technical Memorandum: Speed as a Screening Criterion

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Chapter 1. Introduction

The Federal Railroad Administration (FRA), in coordination with the Maryland Department of Transportation (MDOT), is preparing an Environmental Impact Statement (EIS) for the proposed Baltimore-Washington Superconducting Magnetic Levitation (SCMAGLEV) Project (the Project¹) between Baltimore, Maryland and Washington, DC. As part of the EIS development process, FRA and MDOT have prepared this *Preliminary Alternatives Screening Report* (PASR) that documents the screening of a reasonable range of alignments and possible station locations using a broad area of influence for this first phase evaluation. The results from this PASR will advance into the next phase where the alignments will be refined with more detailed analysis, based on the actual anticipated Limits of Disturbance (LOD), station locations and more specificity of the required support facilities which will be documented in the Alternatives Report.

Congress has expressed its intent that the Safe Accountable Flexible Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) funding for this project be used to directly advance and result in construction of a maglev project.² In March 2015, FRA issued a Notice of Funding Availability (NOFA) under the Maglev Deployment Program³ (MDP), established in the Transportation Equity Act for the 21st Century⁴ (TEA-21) with the purpose of demonstrating the feasibility of Maglev technology, to solicit applications for construction of high speed rail. In April 2015, acting on Baltimore Washington Rapid Rail's (BWRR) behalf, MDOT submitted an application to FRA for the SAFETEA-LU funds to perform preliminary engineering (PE) and National Environmental Policy Act (NEPA) work related to BWRR's SCMAGLEV proposal.

In November 2015, the Maryland Public Service Commission approved BWRR's application to acquire a passenger railroad franchise to deploy a SCMAGLEV system between Baltimore and Washington, DC. BWRR is a private corporation and is the project sponsor and developer of the proposed SCMAGLEV service. The SCMAGLEV Project would be capable of 311 mph (500 km/h) operating speed, with state of the art safety, signaling and automated train control systems. In 2016, FRA awarded a \$27.8 million Maglev grant to MDOT for PE and a NEPA study of the SCMAGLEV Project. BWRR will provide a 20 percent fund match for the NEPA study and PE work and will provide preliminary engineering and technical assistance.

¹ For purposes of this study, magnetic levitation (maglev) is defined as an advanced transportation technology in which magnetic forces lift, propel, and guide a vehicle over a specially designed guideway. This study proposes to implement superconducting maglev (SCMAGLEV) technology, which differs from other maglev systems (such as the German Transrapid system) in that SCMAGLEV accelerates and decelerates through an electromagnetic force generated between superconducting magnets on the vehicle and reaction coils on the guideway sidewalls. The superconducting magnetism is much stronger than ordinary normal conducting electromagnets. Additionally, SCMAGLEV uses inductive magnetic reactions with no active control and rides in a U-shaped guideway; whereas, the German Transrapid system uses attractive reactions that need active controls and rides in a T-shaped guideway.

² Notice of Funding Availability and Solicitation of Applications for Magnetic Levitation Projects. Federal Register 80, no. 54 (March 20, 2015): 15053-15057 (hereafter referred to as NOFA), Section 3.2, Project Eligibility. BWRR's "Response to the NOFA", dated April 17, 2015, states "The Project involves the Baltimore, MD – Washington, DC segment of the New York, NY – Washington, DC federally designated high-speed ground transportation corridor."

³ The Maglev Deployment Program, as authorized by Congress under TEA-21, encourages the development and construction of an operating transportation system employing magnetic levitation.

⁴ TEA-21 is Pub. L. 105-178 (June 9, 1998). The TEA-21/SAFETEA-LU-maglev related provisions are codified (23 USC 322) and (49 USC 309).

This *Preliminary Alternatives Screening Report* outlines the screening methodology and results for the first phase of evaluation of the SCMAGLEV Project. FRA and MDOT identified a broad and reasonable range of alignments by examining previous magnetic levitation (maglev) studies conducted in the Baltimore-Washington region (see Appendix A), input from the agency and public outreach process conducted to date, and through coordination with the private project sponsor, BWRR.

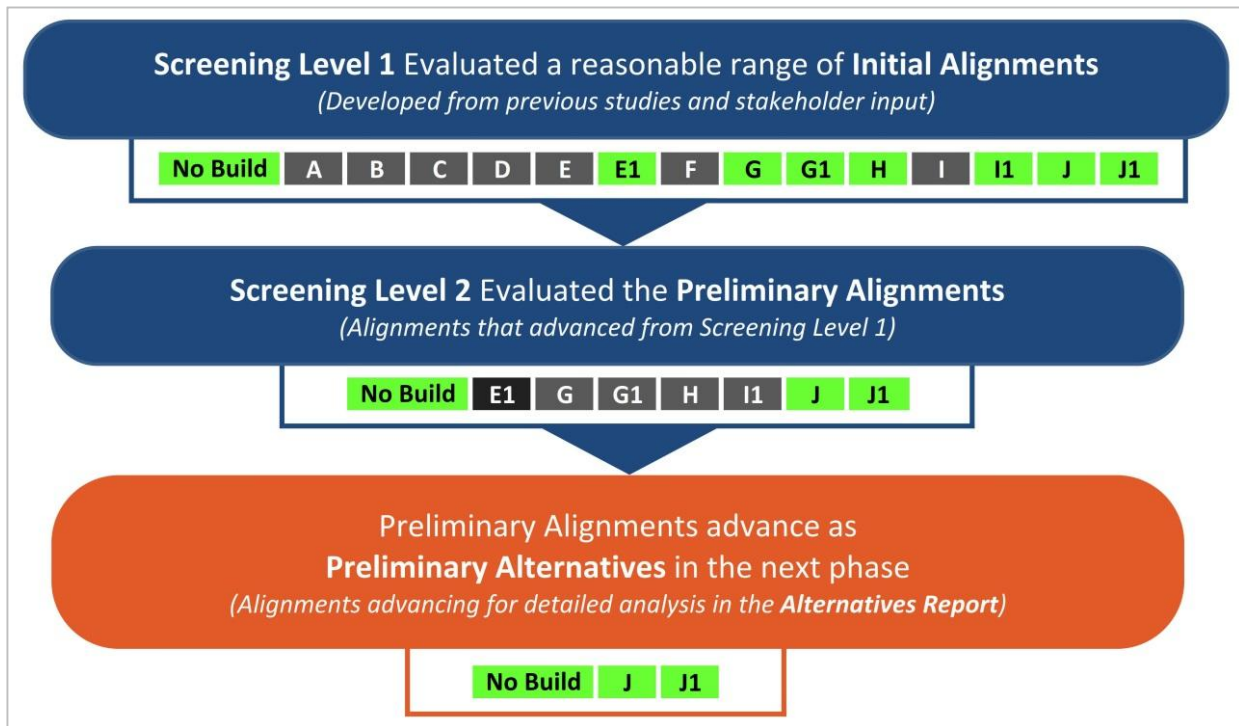
FRA and MDOT used a two-level screening approach to identify alignments meeting project technical specifications and the project Purpose and Need, summarized in Chapter 2, to advance for further development and detailed analysis during the next step in the alternative development process, the Alternatives Report. The initial screening began in early 2017, utilizing design criteria for the SCMAGLEV and desktop analysis of environmental resources in the study area (see Appendix B), and continued into the summer of 2017 based on refinements to the Purpose and Need and Project objectives, field visit findings, additional engineering, agency input, and public feedback. FRA and MDOT revised and finalized this PASR after the five October 2017 public open house meetings (see Appendix C for details on outreach and meetings).

This report details how FRA and MDOT selected preliminary alignments to be studied further in the Alternatives Report by way of a rigorous two-level screening process. FRA and MDOT analyzed 14 initial alignments for fatal flaws, via a pass/fail analysis that compared conceptual level geometric design of each alignment to SCMAGLEV design criteria, during Screening Level 1. Of the initial 14 alignments, FRA and MDOT advanced seven to Screening Level 2 which included an analysis for construction feasibility and potential environmental impacts. FRA and MDOT presented the draft Screening Level 2 results at the five October 2017 public open house meetings, showing three remaining preliminary alternative alignments plus the No Build Alternative, draft possible station zones, and the possible locations of the primary support facilities. FRA and MDOT have determined that two alignments plus the No Build Alternative be advanced for further study in the upcoming Alternatives Report.

Figure 1 graphically outlines the two-level screening process using alignments that were analyzed in the PASR. The two remaining alignments will be refined and presented in the Alternatives Report, which will conclude with the alternatives recommended for detailed study in the DEIS.

Although the No Build Alternative was not formally evaluated in this PASR, analysis of a No Build Alternative is required pursuant to Council on Environmental Quality (CEQ) regulations for implementing NEPA (40 CFR 1502.14). FRA and MDOT will utilize the No Build Alternative to serve as a baseline for comparing the feasibility, profitability, impact evaluation, and other such factors to the alternative(s) studied in the DEIS.

Figure 1: Alignments Screened in the PASR



Note: Please refer to Table 1 for the descriptions of Alignments A through J1 and the No Build Alternative

Chapter 2. Purpose and Need Summary

The purpose of the SCMAGLEV Project is to evaluate and ultimately construct and operate a safe, revenue-producing, high-speed ground transportation system that achieves the optimum operating speed of the SCMAGLEV technology to significantly reduce travel time in order to meet the capacity and ridership needs of the Baltimore-Washington region. To achieve the operational and safety metrics needed for a SCMAGLEV system, the Project must include:

- Infrastructure, vehicles, and operating procedures required for the SCMAGLEV system.
- An alignment which allows the highest practical speed that can be attained by SCMAGLEV technology at a given location and which avoids the need for reduction in speed other than that imposed by the normal acceleration and braking curves into and out of passenger stations.

The objectives of the SCMAGLEV project are to:

- Improve redundancy and mobility options for transportation between the metropolitan areas of Baltimore and Washington, DC.
- Provide connectivity to existing transportation modes in the region (e.g., heavy rail, light rail, bus, air).
- Provide a complementary alternative to future rail expansion opportunities on adjacent corridors.
- Support local and regional economic growth.

The project is needed to address increasing population and employment; growing demands on the existing transportation network; inadequate capacity of the existing transportation network; increasing travel times; decreasing mobility; and maintaining economic viability.

The Baltimore-Washington region is one of the largest and densest population centers in the United States. Over the next 25 years, the population in the region is projected to increase by approximately 20 percent with employment workforce increasing approximately 25 percent⁵. Similarly, the number of visitors to the region is also projected to increase with tourism serving as a significant driver of the economy in both the City of Baltimore and Washington, DC. As the population, workforce, and tourism continue to grow, the demand on the transportation infrastructure between Baltimore and Washington, DC will continue to increase along major roadways and railways including I-95, the Baltimore-Washington Parkway, MD 295, US 29, US 1, and the Northeast Corridor (NEC).

The conditions above translate into the need to evaluate and implement an improved mobility option of travel between the Baltimore and Washington, DC metropolitan areas utilizing SCMAGLEV technology that achieves optimal operating speed and minimizes impacts to the human and natural environment.

⁵ 2015 to 2040 population and employment forecasts are based on the Baltimore Metropolitan Council (BMC) Round 8A Forecast and Metropolitan Washington Council of Governments (COG) Round 9.0 Cooperative Forecasts. Additional information is found in the Project's Purpose and Need Report, available on the project website (<http://www.bwmaglev.info>).

Chapter 3. SCMAGLEV Alternatives Development Process

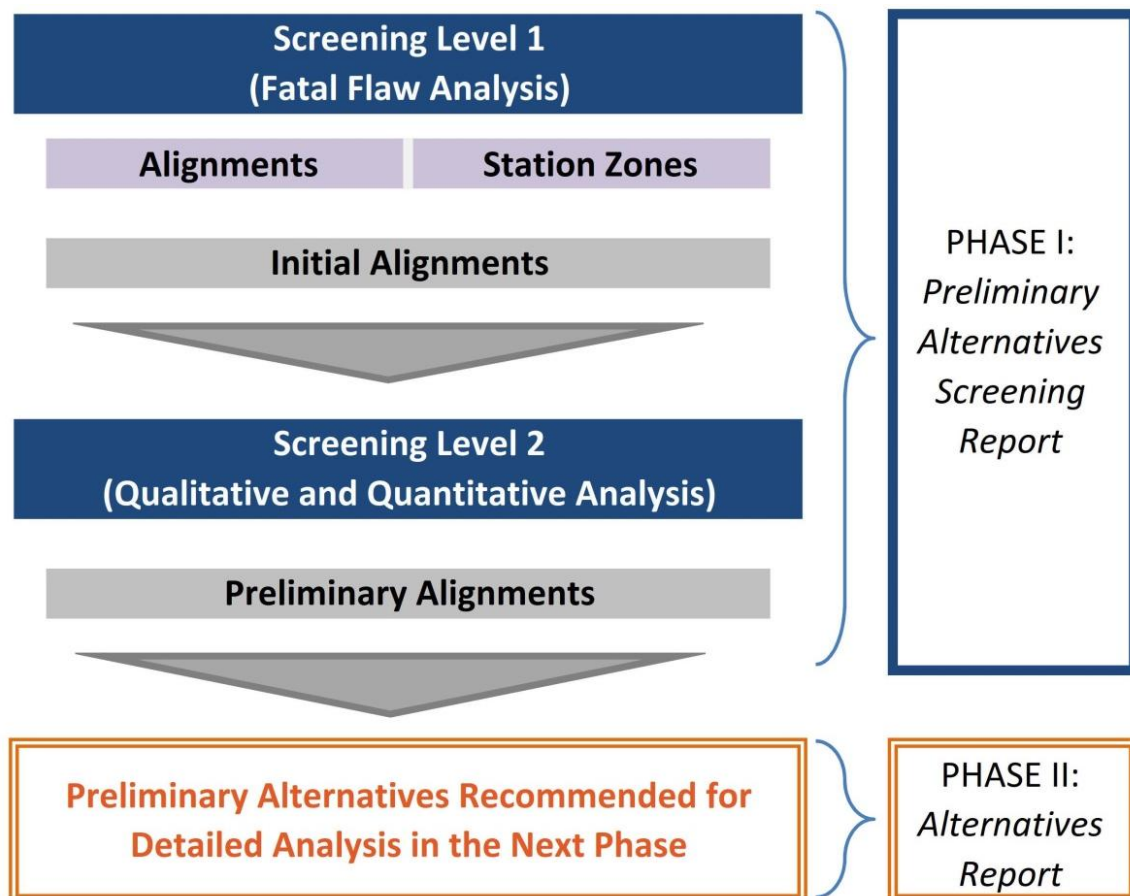
The SCMAGLEV alternatives development process encompasses two key phases. The first phase, Phase I, which develops, analyzes and advances preliminary alignments is the subject of this Preliminary Alternatives Screening Report and corresponding activities described below. During the second phase, Phase II, FRA and MDOT will document further development of alignments, including ancillary facilities, consistent with the level of engineering performed at that time, as part of the future Alternatives Report.

Figure 2 illustrates the Alternatives Development Process conducted by FRA and MDOT.

During Phase I, FRA and MDOT performed the preliminary screening of alignments utilizing a rigorous two-level screening process:

- Screening Level 1 included the screening of the initial alignments and station zones using a fatal flaw analysis.
- Screening Level 2 included the preliminary impact evaluation of retained alignments from Screening Level 1, using a quantitative and qualitative analysis that yielded the alignments to be studied in detail in the upcoming Alternatives Report.

Figure 2: SCMAGLEV Alternatives Development Process



During Phase II, the development of the Alternatives Report, FRA and MDOT will guide BWRR to develop “complete” alternatives from retained preliminary alignments to include stations and support facilities (Rolling Stock Depot (RSD), wayside maintenance facilities, substations, ventilation plants, operations control center (likely at RSD location), and other miscellaneous facilities). These complete alternatives will be subject to a more detailed environmental analysis, including refinements by the NEPA team. Results will be documented in the Alternatives Report, which will identify the alternative(s) recommended for detailed study in the EIS.

The EIS will include a comparative environmental and mitigation analyses, cost comparisons, and operations plans followed by selection of the Agency's Preferred Alternative.

3.1 Initial Alignments

Phase I began with FRA, MDOT and BWRR identifying a reasonable range of initial alignments and station zones between Baltimore and Washington, DC, that traverse the Baltimore/Washington International Thurgood Marshall Airport (BWI Marshall), based on the project's Purpose and Need and comments from initial scoping meetings. Reasonable alignments include those that are practical or feasible from the technical, environmental and constructability standpoint rather than simply desirable from the standpoint of the applicant. Adverse environmental impacts of reasonable alignments can be mitigated.

Given the complexity of the study area, FRA and MDOT considered initial alignments that contain a balance of residential/commercial, natural, federal, historic, and infrastructure resources that would avoid unreasonable impacts to any one single resource. They also developed a comprehensive list of alignments that included a RSD site, so as to possibly avoid needing to identify more alignments or increasing size of footprint later.

FRA and MDOT initially considered nearly straight alignments between Washington, DC and Baltimore, since the Purpose and Need states that the SCMAGLEV alignment must achieve the optimum operating speed. However, these alignments bisected the Patuxent Research Refuge (PRR), a large track federally protected environmental land between the Baltimore Washington Parkway and Amtrak corridors. In early discussions with representatives from the U.S. Fish and Wildlife Service (USFWS), they cautioned that any SCMAGLEV route that bisects the PRR is a non-starter. Therefore, FRA and MDOT directed BWRR to develop initial alignments that avoided or minimized impacts to PRR.

Initial alignments generally follow existing transportation corridors between Washington, DC and Baltimore. Table 1 contains descriptions of the alignments (shown in Figure 3) and the No Build Alternative.

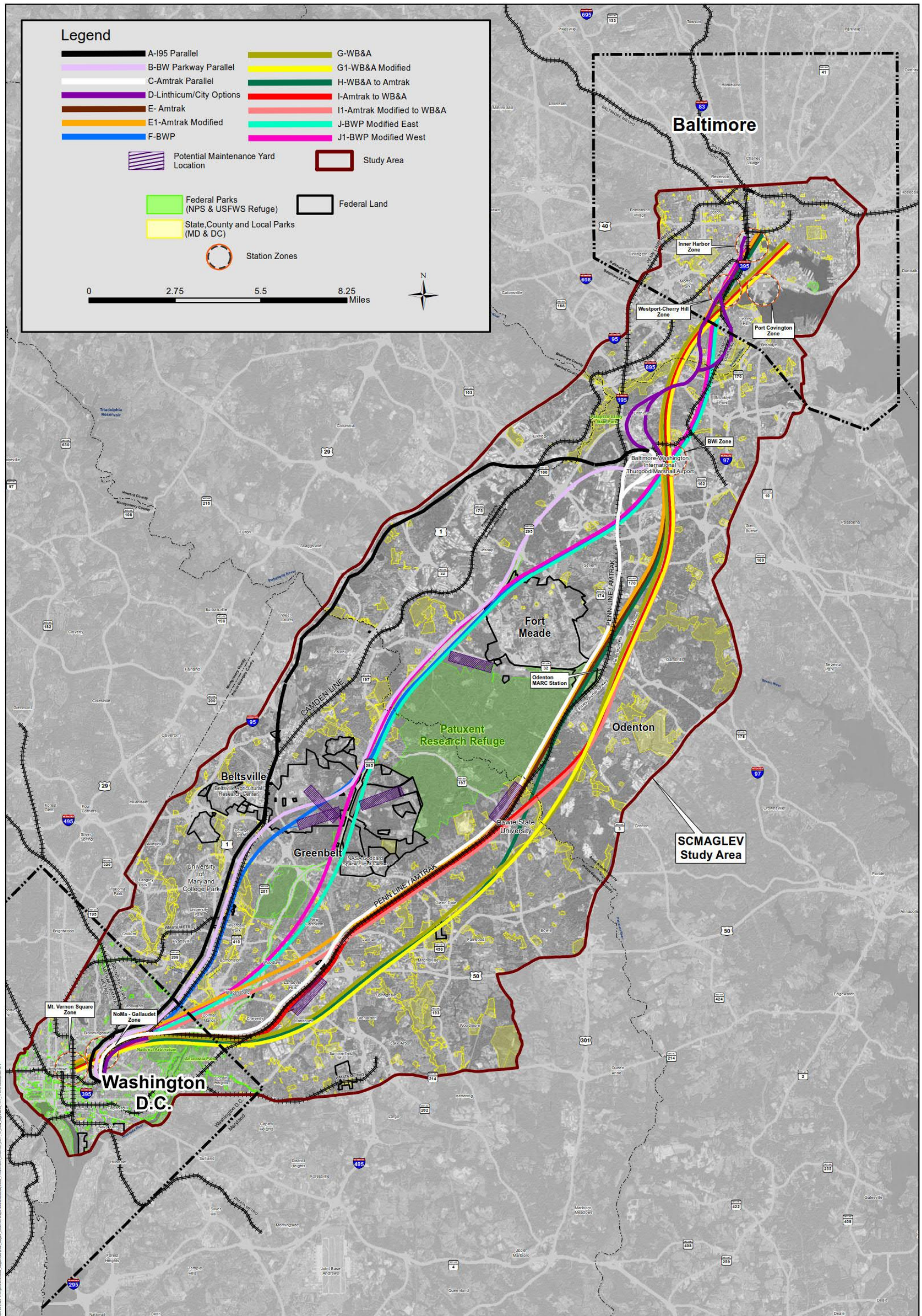
Table 1: Initial Alignments and Summary Descriptions

ID	Alignment Name	Description
A	I-95 Parallel	Generally follows the Amtrak railroad right-of-way and CSXT Camden Line right-of-way out of Washington to I-495, then parallels the I-95 corridor before turning easterly for the BWI Marshall Airport station, and then approximately follows MD 295 to Baltimore.
B	Baltimore-Washington (BW) Parkway Parallel	Generally follows the Amtrak railroad right-of-way and CSXT Camden Line right-of-way out of Washington to MD 193 where it turns northeasterly and crosses the Beltsville Agricultural Research Center (BARC) property to the BW Parkway, then runs parallel to the west side of the BW Parkway before veering east toward the BWI Marshall Airport station, and then approximately follows MD 295 to Baltimore.
C	Amtrak Parallel	Generally follows the Amtrak railroad right-of-way out of Washington and then continues north through portions of the Patuxent Research Refuge (PRR) and Fort George G. Meade to BWI Marshall Airport station, and then approximately follows MD 295 to Baltimore.
D	Linthicum/ City Options	Small segments that provide different options to connect the middle segments of any of the three previous alignments (A, B, and C above) between Baltimore, MD and BWI Marshall Airport and also a different option to the terminus in Washington.
E	Amtrak Corridor	Generally follows the Northeast Corridor/Amtrak railroad right-of-way out of Washington through Odenton, and then continues in tunnel to BWI Marshall Airport and beyond to Baltimore.
E1	Amtrak Modified	Extended tunnel out of Washington to a transition portal north of the Capital Beltway, and then joining the previous alignment E (which runs on elevated structure following the Northeast Corridor/Amtrak railroad right-of-way through Odenton, and then transitions back to tunnel toward BWI Marshall Airport and continues underground to Baltimore).
F	BW Parkway Corridor	Generally follows WMATA and MARC out of Washington through College Park, then crosses eastward to the BW Parkway corridor between the interchanges of MD 197 and MD 32, and then continues in tunnel to BWI Marshall Airport and continuing to Baltimore.
G	Washington Baltimore & Annapolis (WB&A) Corridor	Generally follows US 50 out of Washington in tunnel, transitions to elevated structure over the Anacostia River, then transitions back to tunnel under Landover Road before transitioning back to elevated structure over the Capital Beltway along MD 704, then continues elevated along WB&A Trail and WB&A Road, then enters a tunnel towards BWI Marshall Airport and continues underground to Baltimore.
G1	WB&A Modified	Like G, G1 generally follows US 50 out of Washington, then transfers to MD 704, then continues along WB&A Trail and WB&A Road, and then enters a tunnel to BWI Marshall Airport and continues to Baltimore. Slight horizontal refinements and an additional tunnel section under Odenton (from approximately Patuxent Road to just north of MD 32) were the added modifications to the WB&A corridor.
H	WB&A to Amtrak	This hybrid alignment follows the WB&A alignment (G) to Bowie and then transitions westward to run alongside Amtrak (E) through Odenton, before continuing in a tunnel to BWI Marshall Airport and beyond to Baltimore.

I	Amtrak to WB&A	This hybrid alignment utilizes the Amtrak alignment (E) in the south out of Washington, before shifting to the northeast near Bowie State University to then match the WB&A alignment (G) to the north of Odenton.
I1	Amtrak Modified to WB&A	This hybrid alignment utilizes the Amtrak Modified (E1) to a transition portal north of the Capital Beltway, then turns eastward on elevated structure to the WB&A alignment (G) south of Odenton, then transitions into tunnel near Severn to continue to the BWI Marshall Airport and then Baltimore underground.
J	BWP Modified East	This modification to the BW Parkway alignment includes an extended tunnel under Washington until after the Capital Beltway before transitioning to the elevated guideway. The modified alignment then generally follows the BW Parkway on the east side through BARC, the PRR, and Fort George G. Meade before returning to tunnel towards BWI Marshall Airport station, then continuing in tunnel to Baltimore.
J1	BWP Modified West	This modification to the BW Parkway alignment includes an extended tunnel under Washington until after the Capital Beltway before transitioning to the elevated guideway. The modified alignment then generally follows the BW Parkway on the west side through BARC then turns to the east in tunnel to BWI Marshall Airport station, then continuing in tunnel to Baltimore. This alignment avoids Patuxent Research Refuge and would be in tunnel under Fort George G. Meade.
--	No Build	Continuation of existing transportation options between Baltimore, MD and Washington, DC, via I-95, US 1, US 29, MD 295, MARC service, and Amtrak service (including the high-speed Acela service). The No Build Alternative would include transportation improvements adopted in the Regional Constrained Long-Range Plan (CLRP) for the Baltimore and Washington, DC areas, such as: US 1 / MD 175 interchange (coordinated with I-95 / MD 175 improvements), I-95/I-495 interchange at Greenbelt Metro Station (2020), Baltimore-Washington Parkway (MD 295)/Greenbelt Rd (MD 193) intersection improvement (2020), and MD 295 widening from 4 to 6 lanes between I-195 and MD 100. The No Build also includes selective planned major rail improvements identified in the NEC FUTURE Final EIS such as the Baltimore & Potomac (B&P) Tunnel replacement, Union Station Development, and preferred infrastructure elements between Baltimore, MD and Washington, DC. Infrastructure elements include chokepoint relief at New Carrollton Station, Odenton Station, and BWI Thurgood Marshall Rail Station; new track from New Carrollton to Halethorpe; and curve modifications in the City of Baltimore, east of Penn Station continuing east of I-895.

- Notes:
1. Alignments are described from south to north (Washington, DC to Baltimore).
 2. Alignments A through D are based on the FRA/MDOT/MTA Baltimore-Washington Maglev Project Draft EIS (2003).
 3. Alignments E through J1 are based on stakeholder input and comments received during project scoping, including three routes based on 2012 studies by the private sponsor, BWRR. BWRR engineers are responsible for the design of the system, including the horizontal and vertical geometry (whether the guideways [“tracks”] are elevated or in a tunnel), based on conceptual and preliminary engineering considerations (including suitability to achieve and maintain optimum operating speed of the SCMAGLEV technology, passenger comfort, construction feasibility, and anticipated construction costs, as well avoiding environmental impacts). FRA, MDOT, and other agencies represented in the Project Team suggested refinements to the alignments based on factors such as interference with federal lands, sensitive communities, and public comments.
 4. The FRA/MDOT/MTA NEPA team provides an independent and professional evaluation of the potential environmental impacts of the various alignments, and made recommendations to BWRR’s engineers regarding the alignments to avoid, minimize, and mitigate impacts so that the SCMAGLEV system, if built, does the least possible harm to the natural and human environment.

Figure 3: Initial Alignments



Spring 2017

BALTIMORE-WASHINGTON SCMAGLEV PROJECT
Initial Alignments

Important Note: Station zones (general areas where a station may be located) and alternative alignments are approximate and are subject to change during the NEPA process based on: additional environmental analysis by the NEPA team; preliminary engineering by the project sponsor, Baltimore Washington Rapid Rail; input from government agencies; and public input.

Chapter 4. Screening Level 1

4.1 Initial Alignments

In Screening Level 1, FRA and MDOT evaluated a reasonable range of initial alignments and station zones. Refer to Table 1 for descriptions of the alignments and Figure 3 for an illustration of the initial alignments.

FRA and MDOT considered ancillary facilities (most notably the RSD's) during screening, but only on a qualitative level for site selection. The RSD stores and maintains the trains at night and during off-peak periods. The site will have several buildings, the largest being where a rigorous maintenance and repair program is implemented. The facility will employ engineers, technicians, and other personnel at a site that has an area of approximately 160 acres. During the initial screening of each alignment, an accessible site of sufficient size was conceptually located for an RSD and included on the project mapping. If a site could not be located, or access could not be achieved, then the alignment was not pursued further or it was revised as necessary for inclusion of the RSD.

Methodology

Screening Level 1 consisted of a fatal flaw analysis to identify alignments that meet the geometric requirements necessary to achieve and maintain optimum operating speed of the SCMAGLEV technology. If an alignment was found to have inadequate geometry, FRA and MDOT eliminated it from further consideration in the screening process. The ability of an alignment to meet acceptable horizontal and vertical geometry was determined by geometric design criteria details listed in Table 2.

Table 2: Acceptable Geometric Design Criteria

Element	Design Criteria
Preferred radius	16,000m (10 mi)
Minimum radius for top speed operation	8000m (5mi)
Minimum radius for slow speeds	800m (2600 ft.)
Minimum tangent section length at stations	1000m (3300 ft.)
Maximum grade	4%
Minimum vertical curve radius for top speed operation	40,000m (25 mi)
Minimum vertical curve radius at slow speeds	3000m (1.9 mi)
Maximum super elevation	10 degrees
Center-to-center spacing of guideways	5.8m (19 ft.)
Out-to-out dimension of elevated guideway (approximate)	14m (46 ft.)
Internal tunnel diameter for two guideways (approximate)	13m (43 ft.)
Minimum internal tunnel cross-sectional area (governed by aerodynamics)	74m ² (800 sf)
ROW limits for elevated structure	22m (72 ft.)
















Source: Design Criteria (2017) provided by BWRR, the private project sponsor, are among the specifications for commercial deployment of SCMAGLEV developed from decades of research and testing by the Central Japan Railway Company (JR Central). See the Technical Memorandum on Speed as a Screening Criterion in Appendix D for further explanation.

Results and Findings

Since this initial screening was a fatal flaw analysis, FRA and MDOT assigned a "yes" or "no" result for an acceptable curve radius geometry. Table 3 presents the alignments that advanced from Screening Level 1 and were evaluated in Screening Level 2. Alignments that did not advance are A, B, C, and D (from the

2003 Draft EIS) and E, F, and I. Each of these alignments have fatal design flaws and do not meet the minimum radius for top speed operation.

Table 3: Screening Level 1 Results – Initial Alignments

Initial Alignments \ Criteria:	Engineering	Recommendation		Comment
	Acceptable Geometry ¹			
No Build Alternative	N/A	Retain		Retain throughout evaluation
Alignment A (I-95)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment B (BW Parkway)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment C (Amtrak)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment D (Linthicum/others)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment E (Amtrak)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment E1 (Amtrak modified)	Yes	Retain		Potential construction issues with existing railroad track and potential impacts to federal properties to be studied further
Alignment F (BW Parkway)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment G (WB&A)	Yes	Retain		Utilizes a former rail corridor, but potential impacts to residential and other properties (the WB&A Trail) are issues to study further
Alignment G1 (WB&A Modified)	Yes	Retain		Utilizes a former rail corridor, but potential impacts to residential and other properties (the WB&A Trail) are issues to study further
Alignment H (WB&A to Amtrak)	Yes	Retain		Potential construction issues with existing railroad track and potential impacts to federal properties to be studied further
Alignment I (Amtrak to WBA)	No	Do Not Retain		Does not meet geometry requirements because of curve radius restrictions
Alignment I1 (Amtrak Modified to WBA)	Yes	Retain		Does not impact Federal Lands, but crossing existing Amtrak rails to be analyzed further in Screening 2
Alignment J (BWP Modified–East)	Yes	Retain		Extended tunnel & potential impacts to Federal lands (BARC, PRR, NPS, DOD) are issues to study further
Alignment J1 (BWP Modified–West)	Yes	Retain		Extended tunnel & potential impacts to Federal land (BARC and NPS) are issues to study further

(1) Refer to Table 2 for the geometric design criteria table.

4.2 Station Zones

The Project Sponsor, BWRR, provided FRA and MDOT potential station locations. All three planned SCMAGLEV stations will be underground, with entrances from the street or within buildings, similar to a subway. The platforms will be approximately 1,000 ft long and 80 to 130 ft deep. Escalators, elevators and stairs will provide vertical circulation. The stations will be located convenient to multimodal connections. Since the actual station platform will ultimately depend on the recommended alignment in the EIS, the PASR presents the evaluation of station zones to identify general areas where it would be feasible to locate a station. The station zones are based on a circular buffer covering an approximately one mile (diameter).

FRA and MDOT initially evaluated five station zones at the northern terminus in Baltimore (Harbor East, Inner Harbor, Port Covington, Westport, and Penn Station). The evaluation resulted in FRA and MDOT identifying as feasible three station areas for downtown Baltimore, MD - Inner Harbor, Westport, and Port Covington (shown in Figure 4).

In addition to the terminus stations, FRA and MDOT proposed an intermediate stop at BWI Marshall Airport, which is subject to ongoing coordination with the Maryland Aviation Administration (MAA) and Federal Aviation Administration (FAA) (see Figure 5).

For the southern terminus in Washington, DC, FRA and MDOT initially evaluated four station zones (Union Station, NoMa-Gallaudet, Farragut Square, and Mount Vernon Square). The evaluation revealed two zones (NoMa-Gallaudet and Mount Vernon Square) as the most feasible station areas in Washington, DC (shown in Figure 6).

In the following figures, the circular buffers represent the general station zones, which are approximately one mile in diameter. Similar to the Screening Level 1 evaluation of the alignments, FRA and MDOT utilized a fatal flaw analysis on the station zone as the exact station platforms were assumed to be underground and the surface footprint would not be a discriminating factor. However, similar to the RSD facility, stations will be analyzed in more detail for the Alternatives Report and incorporated into the refined LOD for a quantitative analysis at that time. The Project Team can geometrically refine all remaining alignments to approach any retained station zone; each proposed route will be in deep tunnel leading up to the station zones. Although each station zone can be accessed by all alignments, ultimately, there will be only one station at each terminus.

Figure 4: Baltimore Station Zones

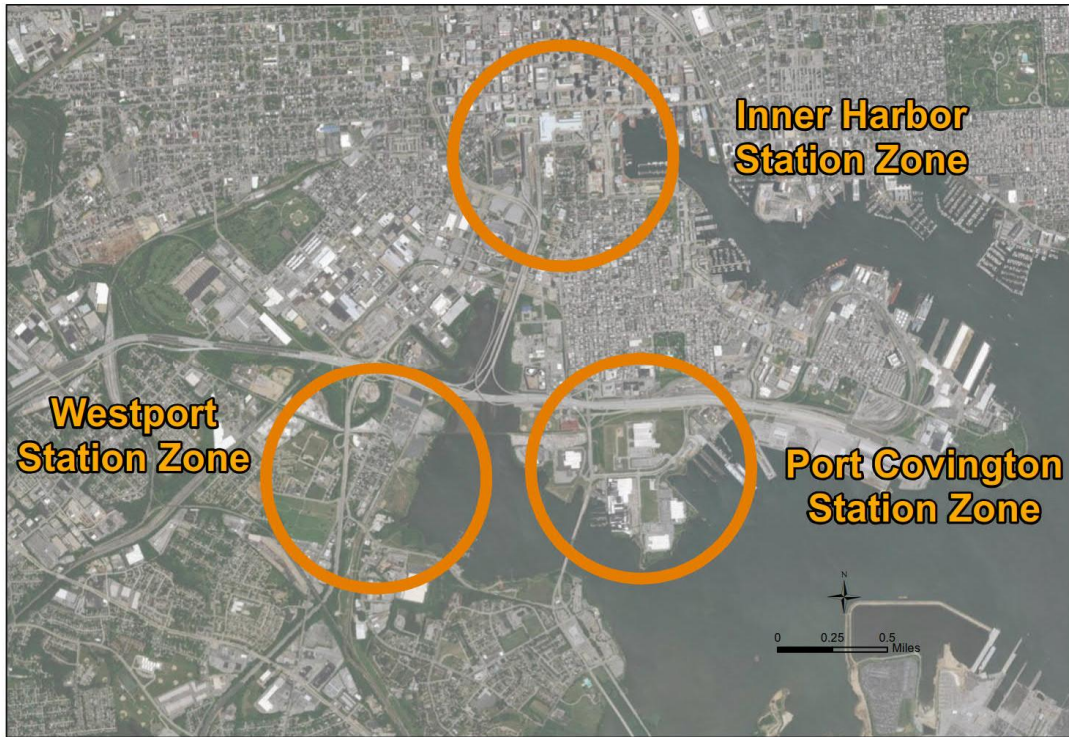


Figure 5: BWI Marshall Airport Station Zone

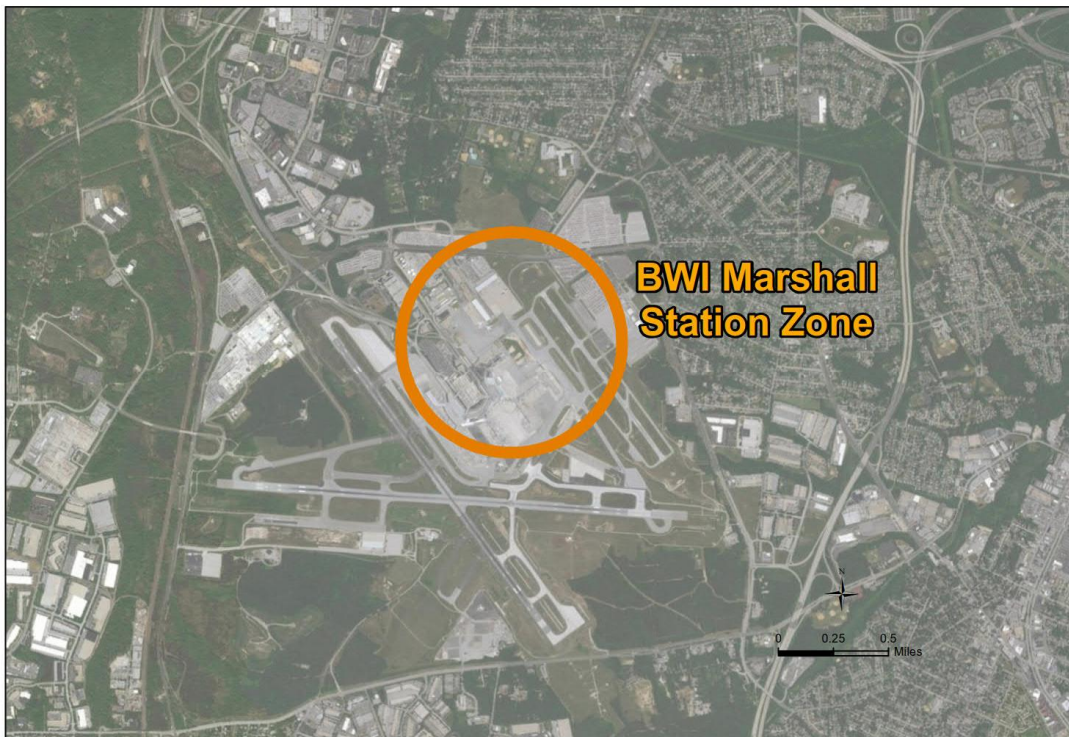
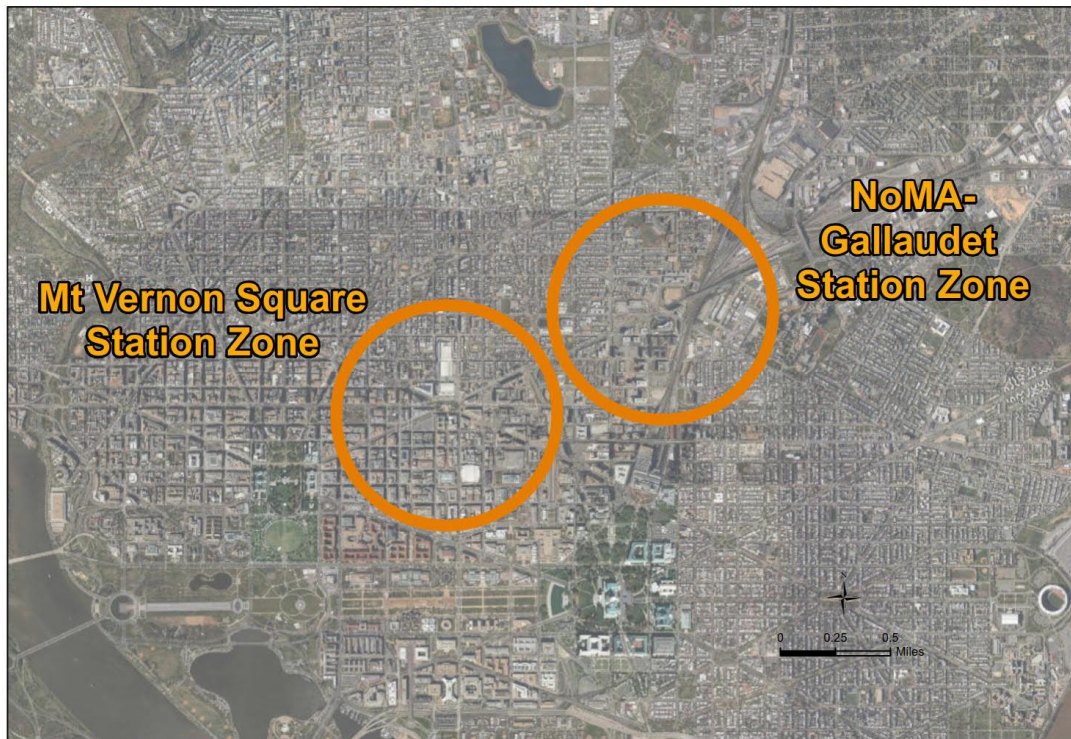


Figure 6: Washington, DC Station Zones



Methodology

FRA and MDOT qualitatively assessed the potential station zones using engineering constraints, information from previous maglev studies and local area plans, aerial mapping, and field observations at selected sites.

FRA and MDOT organized the station zone criteria considered in the screening process by engineering and operational constraints. For reference, a Yes/No metric was used, corresponding to conditions described in this section. Site conditions were visually assessed via aerial mapping, studies, and field visits to selected locations.

a. Engineering

The first engineering evaluation element, geometric feasibility criterion, that FRA and MDOT considered was the geographic location of the station zone with respect to compatibility with the alignments. As stated in the Project Purpose and Need, the station zone must allow the SCMAGLEV system to complete the trip between potential Baltimore and Washington, DC stations along an alignment which allows the highest practical speed attainable by SCMAGLEV technology. Please refer to Appendix D for the Technical Memo regarding practical operating speed of the SCMAGLEV technology. For the EIS study, evaluation of a SCMAGLEV system is being completed for a route between only Washington, DC and Baltimore. The project sponsor intends to continue the SCMAGLEV system northeastward. This results in the geometric constraint that each station zone must not preclude an alignment from continuing on a favorable geometric path to the northeast. A "yes" rating means a station zone is geographically located to allow an alignment to meet the highest practical speed requirement and/or does not preclude future expansion. A

“no” rating means the station zone precluded an alignment from maintaining the highest practical speed and/or precludes future expansion.

The second engineering evaluation element, construction feasibility criterion, evaluates station zones from a construction standpoint to minimize impacts to the human and natural environment, as stated in the Project Purpose and Need. A “yes” rating corresponds to a perceived straightforward construction process that would be achievable in a short timeframe and/or with a single stage of construction. A “no” rating corresponds to a perceived complex construction process that would take more than a single construction stage to complete. For example, the opportunity to utilize top down construction, the availability of staging areas, maintenance of traffic, geotechnical engineering, existing/proposed development, and existing or proposed critical infrastructure within the station zone were taken into consideration while evaluating this criterion.

b. Operational Requirement

The Purpose and Need states one of the objectives is to provide connectivity to existing transportation modes in the region. Thus, FRA and MDOT used the operational criterion of intermodal connectivity (opportunity for passengers to utilize multiple options/modes of transportation during a single trip) to evaluate station zones based on existing or proposed transportation connectivity conditions. A “yes” rating indicates multiple transportation options/modes are available within the station zone; whereas a “no” rating corresponds to a low amount and/or no other access options within the station zone. FRA and MDOT considered mass transit options, vehicular/major highway access, and proximity to all other modes of transportation while evaluating this criterion.

Results and Findings











Table 4 presents the station zones recommended to be retained for further detailed study. The recommended station zones are:

- Inner Harbor, Port Covington, and Westport in Baltimore;
- BWI Marshall Airport; and
- NoMa-Gallaudet and Mount Vernon Square in Washington, DC.

In Baltimore, the Harbor East zone was dropped because it would not provide sufficient intermodal connectivity. The Penn Station zone was dropped primarily because the geographic location precludes the potential extension of the alignments to the northeast. In addition, the construction feasibility for the Penn Station zone did not include sufficient staging areas and the zone contains multiple existing infrastructure features that would not allow for simple top down station construction.

In Washington, DC the Farragut Square zone was dropped because of potential geometric and construction constraints. These included insufficient staging areas, conflicts with existing infrastructure facilities, and limited surface/street access. The Union Station zone was dropped from further consideration due to a combination of the geometry (vertical curve/grade/future expansion to the south) and potential construction challenges (anticipated depth of station and limited street/surface access, restrictions relating to the existing surface and subsurface infrastructure, and current/future development plans for the existing property by others). In addition, the location of both the Farragut Square and Union Station zones did not provide an optimal geometry with respect to the alignments in order to realize the highest practical speed.

Table 4: Screening Level 1 Results - Station Zones

Station Zone/ Criteria		Engineering		Operational	Recommendation	Comment
		Geometric Feasibility	Construction Feasibility	Intermodal Connectivity		
Baltimore	Harbor East Zone	Yes	No	No	Do Not Retain 	Construction and intermodal connectivity constraints
	Inner Harbor Zone	Yes	Yes	Yes	Retain 	Retain for further study
	Port Covington Zone	Yes	Yes	No	Retain 	Retain for further study (but future intermodal connectivity needs to improve & potential new development needs to incorporate a station)
	Westport Zone	Yes	Yes	Yes	Retain 	Retain for further study
	Penn Station Zone	No	No	Yes	Do Not Retain 	Geometry precludes a feasible route to the northeast, complex construction challenges
BWI Marshall	BWI Marshall Airport	Yes	Yes	Yes	Retain 	Retain for further study
Washington, DC	Union Station Zone	No	No	Yes	Do Not Retain 	Construction and geometry constraints
	NoMa-Gallaudet Zone	Yes	Yes	Yes	Retain 	Retain for further study
	Farragut Square Zone	No	No	Yes	Do Not Retain 	Construction and geometry constraints
	Mt. Vernon Square Zone	Yes	Yes	Yes	Retain 	Retain for further study

Chapter 5. Screening Level 2

FRA and MDOT advanced initial alignments that met the minimum requirements from Screening Level 1 and identified them as preliminary alignments in Screening Level 2. Preliminary alignments are routes between Washington, DC and Baltimore that include station zones and potential tunnel portal locations. Preliminary alignments do not include quantitative measurements of the RSD or other ancillary facility sites, though these ancillary site locations have been conceptually identified and qualitatively considered for each alignment. Quantitative evaluation of the RSD and other ancillary facilities will be included in the Alternatives Report.

In Screening Level 2, FRA and MDOT applied the screening criteria based on feedback from governmental agencies and the public pertaining to environmental and socioeconomic factors, as well as the construction feasibility of an alignment. The results of this analysis provided FRA and MDOT a preliminary look into the amount of residential and environmental resources present within broad buffer zones around the alignments for an order of magnitude comparison.

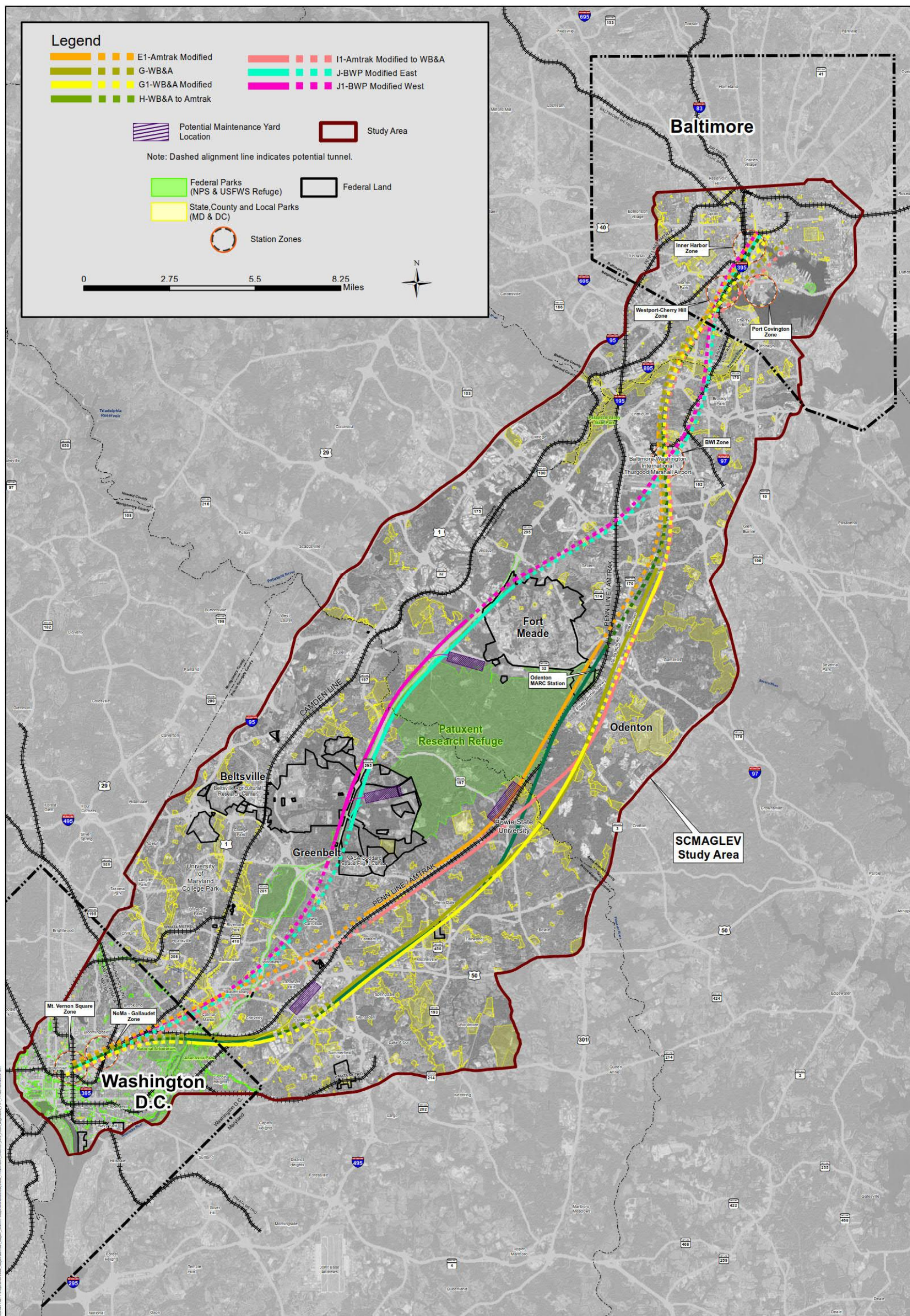
In addition to the No Build Alternative, FRA and MDOT advanced seven of the initial alignments to Screening Level 2. They are:

- Alignment E1 (Amtrak Modified);
- Alignment G (WB&A);
- Alignment G1 (WB&A Modified);
- Alignment H (WB&A to Amtrak);
- Alignment I1 (Amtrak Modified to WB&A);
- Alignment J (BWP Modified-East); and
- Alignment J1 (BWP Modified-West)

Refer back to Table 1 for text descriptions and see Figure 7 for a map of the preliminary alignments.

Please note that for Screening Level 2, the linework was updated to distinguish between the potential tunnel sections (dashed lines) and the potential elevated/viaduct sections (solid lines) for the preliminary alignments map. The typical cross sections are shown in Figure 8 and Figure 9 for the viaduct and tunnel illustrations, respectively.

Figure 7: Preliminary Alignments

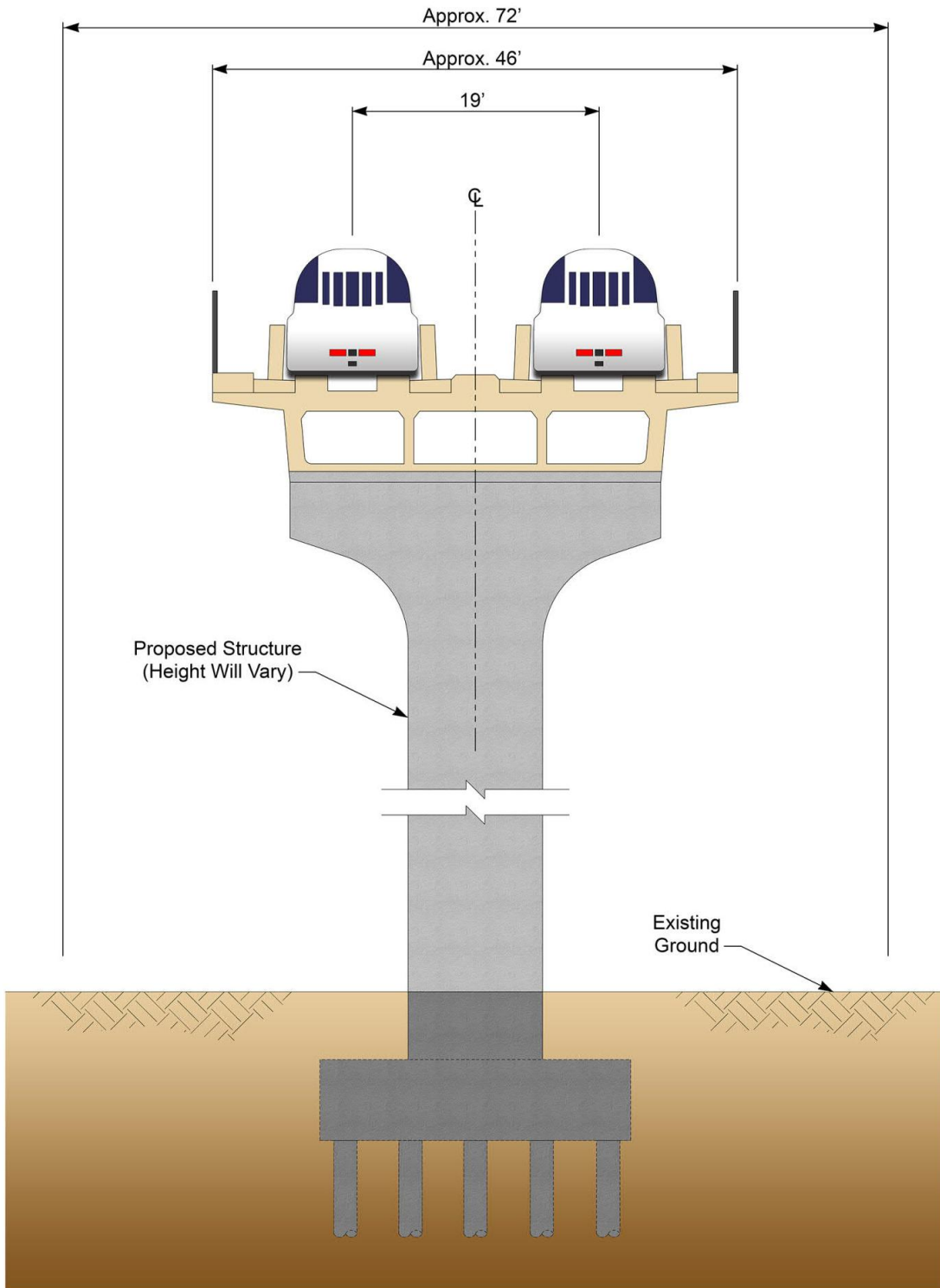


Summer 2017

BALTIMORE-WASHINGTON SCMAGLEV PROJECT
Preliminary Alignments

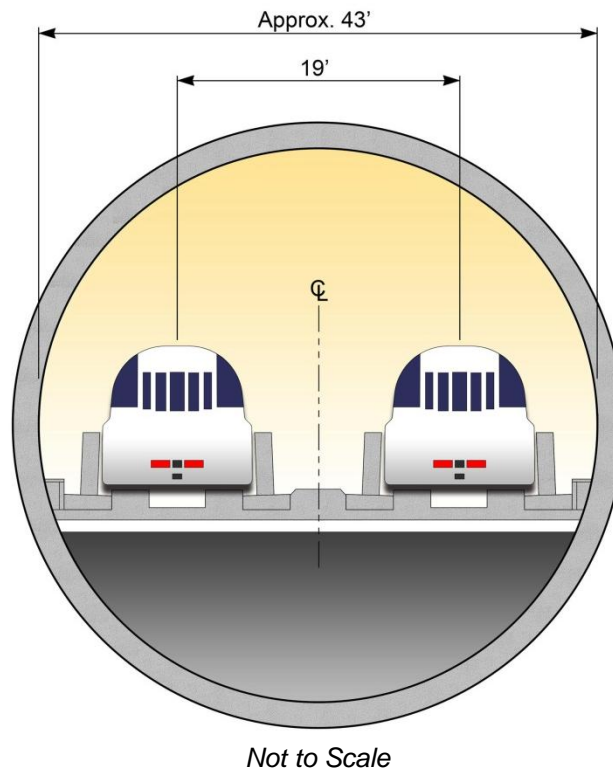
Important Note: Station zones (general areas where a station may be located) and alternative alignments are approximate and are subject to change during the NEPA process based on: additional environmental analysis by the NEPA team; preliminary engineering by the project sponsor, Baltimore Washington Rapid Rail; input from government agencies; and public input.

Figure 8: Typical Viaduct Cross Section for the Preliminary Alignments



Not to Scale

Figure 9: Typical Tunnel Cross Section for the Preliminary Alignments



Segment Length by Construction Method

The construction method is an important element regarding the assessment of potential impacts. For Screening Level 2, it is assumed that the elevated guideway segments will incur potential surface impacts along the alignments. At least one portion of the alignment is planned to be on an elevated viaduct above ground. As illustrated in Figure 8, the viaduct carries two guideways with a width of approximately 46 ft and a typical height above ground of at least 18 ft. The area below the viaduct can be used for roadways, cycle and walking paths, ecological restoration and more. During construction, the contractor occupies a width of around 72 ft.

For the purposes of this document, a tunnel transition/portal is defined as where tunneling operations end and cut-and-cover (excavation and fill) operations transition into the elevated guideway portions. Cut-and-cover may incur potential environmental impacts from excavation for the entire length of the proposed cut-and-cover construction area.

It is assumed that a bored tunnel would only incur potential environmental impacts at the tunnel transition/portal locations. As illustrated in Figure 9, tunnel segments are assumed to be a single tunnel with an interior diameter of approximately 43 ft carrying two guideways. The tunnel sections will be constructed using a tunnel boring machine at a typical depth of 80 to 170 ft.

It is assumed the tunnel sections do not present significant surface impacts, therefore were not included in the surface footprint for this preliminary screening. Table 5 indicates the proposed lengths for the various segments.

Table 5: Segment Length by Construction Method

	Elevated Length (miles)	Deep Tunnel Length (miles)	Cut-Cover Length (miles)	Transition Length (miles)	Total Length (miles)
Alignment E1	11	24.5	0.8	0.6	36.9
Alignment G	19.3	16.8	0.9	1.6	38.6
Alignment G1	13.1	21.8	1.2	2.3	38.5
Alignment H	12.4	21.9	1.2	2.6	38.1
Alignment I1	13.9	22.1	0.5	0.9	37.5
Alignment J	8.9	26.1	0.6	0.7	36.3
Alignment J1	7.5	27.6	0.4	0.8	36.3

Screening Level 2 Methodology

The purpose of Screening Level 2 was to identify the most reasonable alignments from a wide range of seven diverse alignments. Reasonable alignments are those that would achieve the basic objectives of the project but would also avoid or substantially lessen any of the significant negative effects of the project. NEPA does not require consideration of every conceivable alignment to a project; rather it must consider a reasonable range of potentially feasible alignments that will foster informed decision making and public participation. In Screening Level 2, each alignment was evaluated with respect to its ability to address project-specific challenges and objectives in the decision-making process: construction feasibility, environmental factors, and public and political preference. Based on this evaluation, alignments were retained for further study, eliminated or combined with other alignments.

This analysis is based on desktop level research and data collection using readily available data and public and agency engagement. Table 6 summarizes the Screening Level 2 evaluation criteria. Table 7 provides additional notes and the threshold ranges assumed for the results and determinations (presented in Table 8).

Table 6: Screening Level 2 Evaluation Criteria Summary

Criteria		Description
Construction Feasibility	Crossing Existing NEC Tracks	Does the proposed alignment require crossing the existing Amtrak NEC rail operations?
	Residential Properties and Community Resources	Number of residential properties within the primary/secondary analysis zones. Number of community resources (churches, schools, cemeteries, health care/emergency facilities, etc.) within the analysis zone. Number of commercial properties (includes office buildings, retail stores, warehouses, and heavy commercial and industrial uses) within the analysis zone.
Environmental	Cultural Resources	Number of Historic Landmarks and Eligible National Register Sites & Districts within the analysis zone, including properties potentially subject to the Section 4(f) and/or Section 106 process.
	Parks and Federal Lands	Acreage of State/County/Local Parks and acreage of properties noted as Federal Land [Includes Federal Parks/Refuges] within the analysis zone, including properties potentially subject to Section 4(f). Examples include the Baltimore-Washington Parkway (BWP), Patuxent Research Refuge (PRR), National Arboretum, Anacostia Park, Beltsville Agricultural Research Center (BARC), and Fort George G. Meade.
	Natural Resources	Acreage of Wetlands, wetlands of special state concern, and 100-Year floodplains within the analysis zone.
Notes	1. Quantitative screening only applies to the proposed guideway surface footprint for each alignment. Tunnel sections of guideway, the RSD site, and other ancillary facilities are not included in the potential quantitative impact calculations at this time (these will be evaluated during further stages of the EIS process as engineering is refined). 2. Table 7 provides information on the various screening criteria thresholds. 3. Figures 10 and 11 illustrate the analysis zones; Appendix B provides additional details on Screening Level 2.	

The construction feasibility criteria received a yes/no result if the respective preliminary alignment crossed the existing NEC rail tracks. The potential environmental impacts were given a high/medium/low designation derived from a limited desktop level analysis using Geographic Information Systems (GIS) data. The threshold ranges for the high/medium/low designations are described in Table 7. The actual values are contained in Appendix B. FRA and MDOT sourced the data from Baltimore City, the Counties, Washington, DC, and any state or federal readily available information databases (See Appendix B).

Table 7: Screening Level 2 Notes and Threshold Ranges

Descriptions for Screening Level 2 Results Table
<p>A. Total Length: The approximate length of the respective preliminary alignment from downtown Washington, DC to downtown Baltimore, MD (See Table 5 for detailed information).</p>
<p>B. Elevated: The approximate percent of length the guideway would be elevated at least 18' (typical) above the ground surface with clearance below and columns spaced about 120' apart.</p>
<p>C. Tunnel: The approximate percent of length the guideway would be in tunnels, which are typically 80' to 170' below the surface of the ground, except where they emerge through the transitional portals (which will vary in length based on existing ground conditions and the proposed vertical geometry of the SCMAGLEV guideway at the portal location, but typically range from 300 yards to 500 yards long).</p>
<p>D. Construction Feasibility "Crossing Existing NEC Track": This criterion considered proximity to the existing Amtrak NEC rail operations. An alignment that crossed the existing NEC rail tracks presents issues for both the existing NEC operations and the new SCMAGLEV system. The background criteria for evaluating crossing the NEC or constructing SCMAGLEV in the vicinity of the NEC with respect to the SCMAGLEV Project Purpose include the following system characteristics:</p> <ul style="list-style-type: none"> § An alignment which allows the highest practical speed that can be attained by SCMAGLEV technology at a given location as further defined by geometric design criteria. § A system that complies with federal safety requirements including those pertaining to intrusion hazards. § Avoidance, minimization, and mitigation of impacts to the human and natural environment. § Provide a complementary alternative to future rail expansion opportunities on adjacent corridors by minimizing disruption of existing rail operations and minimizing potential conflicts with future planned expansion opportunities. § Support local and regional economic growth by being consistent with past, present, and future economic development in the vicinity of stations along the NEC. § Revenue producing, i.e., commercially viable, consistent with Congress's intent under 1307 of the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU). <p>The construction feasibility column displays a yes/no result if the respective preliminary alignment crossed existing NEC rail tracks.</p>
<p>E. Residential Properties: The presence of concrete elevated guideways in a residential landscape could affect the appearance of the community, and the sound of trains passing could alter neighborhood acoustics.</p> <p>Given that the alignments are 36 to 38 miles long and are going through the Baltimore-Washington corridor, primary impacts on 99 properties or less are considered low, 100 to 349 are considered medium, and 350 or more are considered high.</p> <p>Secondary impacts at this stage are moderate aesthetic impacts, such as the viaduct being visible from neighboring residential properties. Potential secondary impacts of 0 to 1000 properties or less are considered low, 1001 to 2000 are considered medium, and 2001 or more are high.</p> <p>This evaluation only considered the number of potential residential properties within the analysis zone for an order of magnitude comparison. Specific property impacts and type of impact will be studied further in the Draft EIS.</p>

Descriptions for Screening Level 2 Results Table

F. Community Resources: Potential impacts that are "primary" may involve acquisition of the property or major aesthetic impacts on the property. Potential impacts eliminate or significantly reduce operations of the community resource by removing all or part of the building, parking lot, or access to the facility. For example, the resource would potentially be subject to adverse effects of access, visibility, or operation of the facility because of the proximity of the SCMAGLEV system or the impact of infrastructure (e.g. view of signage is partially blocked; pedestrian access becomes more difficult; or recreational/outdoor aesthetics change because of new elevated guideways shadows on neighboring property).

Potential impacts of 4 community resources or less are considered low, 5 to 11 are considered medium, and 12 or more are considered high.

This evaluation only considered the number of potential community resources within the analysis zone for an order of magnitude comparison. Specific resource impacts and type of impact will be studied further in the Draft EIS.

G. Commercial Properties: Potential impacts eliminate or significantly reduce operations of the commercial property by removing all or a part of the building, parking lot, or access to the facility.

Potential impacts on 100 or less commercial properties are considered low, 101 to 200 properties are considered medium, and 201 or more are considered high.

This evaluation only considered the number of potential commercial properties within the analysis zone for an order of magnitude comparison. Specific property impacts and type of impact will be studied further in the Draft EIS.

H. Historic Properties: Alignments should avoid or minimize impacts on historic properties, which are potentially subject to the Section 4(f) and/or Section 106 process. Alignments with fewer historic properties in the surface disturbance footprint are preferable to alignments that could affect more properties that are historic.

Primary potential impacts on five sites or less are considered low, 6 to 14 are considered medium, and 15 or more are considered high. Secondary potential impacts of 0 to 50 sites are considered low, 51 to 150 are considered medium, and 151 or more are considered high.

This evaluation only considered the number of potential historic resources within the analysis zone for an order of magnitude comparison. Specific resource impacts and type of impact will be studied further in the Draft EIS.

I. Parks (state, county & local): This criterion considers the number and acreage of state, local, and county-owned parks within the areas of surface disturbance based on parks identified in the GIS Land Use database. The protection and preservation of parks is required by Section 4(f) of the Department of Transportation Act of 1966 (49 U.S.C. §303).

Primary potential impacts on 12 or less acres are considered low, 13 to 24 acres are considered medium, and 25 acres or more are considered high. Secondary potential impacts on 24 acres or less are considered low, 25 to 49 acres are considered medium, and 50 acres or more are high.

J. Federal Lands: Preservation of federal lands is important; managers of those lands consider the loss of a single acre to be significant. The high/medium/low determination in this evaluation compares the *relative impact of the alignments*; certain alignments would require use of federal land to avoid potential impacts on residential communities.

Primary potential impacts on 49 acres or less are considered low, 50 to 99 acres are considered medium, and 100 acres or more are considered high. Secondary potential impacts on 99 acres or less are considered low, 100 to 199 acres are considered medium, and 200 acres or more are high.

The table also identifies if an alignment crosses PRR with elevated viaduct structure. PRR is a resource directly in the middle of the study area that FRA and MDOT tried to minimize and/or avoid.

Descriptions for Screening Level 2 Results Table

K. Wetlands of Special State Concern (WSSC): Includes wetlands with rare, threatened, or endangered species or unique habitats. The Code of Maryland Regulations (COMAR) Title 26, Subtitle 23, Chapter 06, Sections 01 & 02 identifies these WSSC.

Potential impacts on 5 acres or less are considered low, 6 to 12 acres are considered medium, and 13 acres or more are considered high.

L. Other Wetlands: Includes wetlands that are not identified as WSSC. Wetlands that are not identified as WSSC still have an important function to the natural environment, with countless benefits including increased water quality, groundwater recharge, flood reduction, and habitat.

Potential impacts on 10 acres or less are considered low, 11 to 25 acres are considered medium, and 26 acres or more are considered high.

M. 100-Year Floodplain: Floodplains, like wetlands, are hydrologically important to the natural environment, providing a host of benefits including flood protection, habitat, and pollutant filters. They are defined as the area subject to a one-percent or greater chance of flooding in a given year.

Potential impacts on 19 acres or less are considered low, 20 to 49 acres are considered medium, and 50 acres or more are considered high.

Primary and Secondary Analysis Zone Descriptions

FRA and MDOT identified and measured the environmental features within wide investigative limit of disturbance (LOD) buffers⁶, separated into primary and secondary analysis zones, where applicable. The primary analysis zone represents the potential construction limit (most likely surface disturbance area for the respective alignments). The secondary analysis zone category is likely to be aesthetic based on the sight and sound and general close proximity to the SCMAGLEV guideway and trains.

- The primary zone along the elevated guideway extends 50 feet out from the centerline on each side (100 feet total width) while the primary zone around the transition/portal areas extended 75 feet out from the centerline on each side (150 feet total width) as shown in Figures 10 and 11.
- The secondary zone along the elevated guideway extends out 200 feet beyond the primary zone on each side while the secondary zone around the transition/portal areas extends out 175 feet beyond the primary zone for Parks and Federal Lands as shown in Figure 10.
- The secondary zone for the Residential Properties and Cultural Resources (historic landmarks/sites/districts) was based on an expanded LOD. The secondary zone along aerial structure extends out 450' beyond the primary zone while the secondary zone around the transition portals extends out 425' beyond the primary zone as shown in Figure 11.

⁶ A 72-foot wide buffer along the linear bi-directional SCMAGLEV system is proposed by BWRR to be the eventual LOD; however, the actual LOD for ancillary facilities increases the area of surface disturbance in specific locations. Since this is the first preliminary screening, the Screening Level 2 buffer zones were established to be more generous to allow for further refinement of the alignments as the study progresses. The 72-foot wide guideway buffer would fall with the primary zone of the PASR evaluation.

Figure 10 and Figure 11 are both from the perspective of a “bird’s eye view” looking down on the proposed SCMAGLEV guideway centerline to illustrate the horizontal width away from the centerline that each respective zone category represents.

Figure 10: Guideway Analysis Zones for Parks/Federal Lands

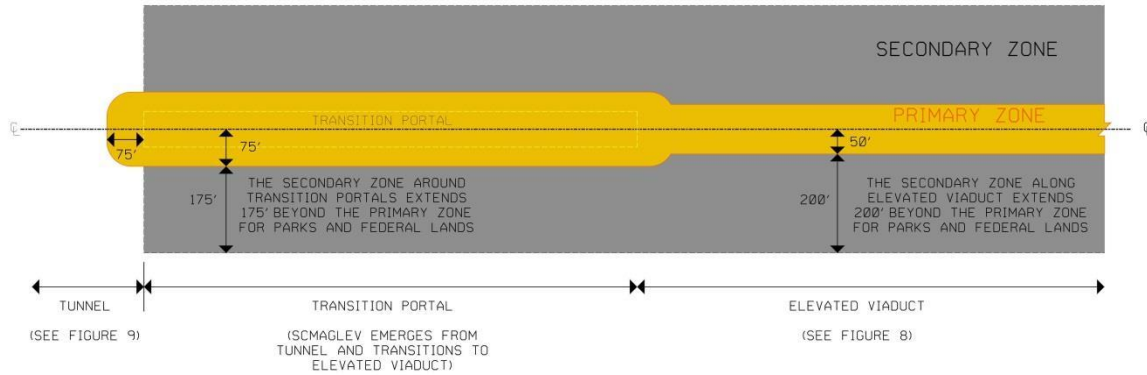
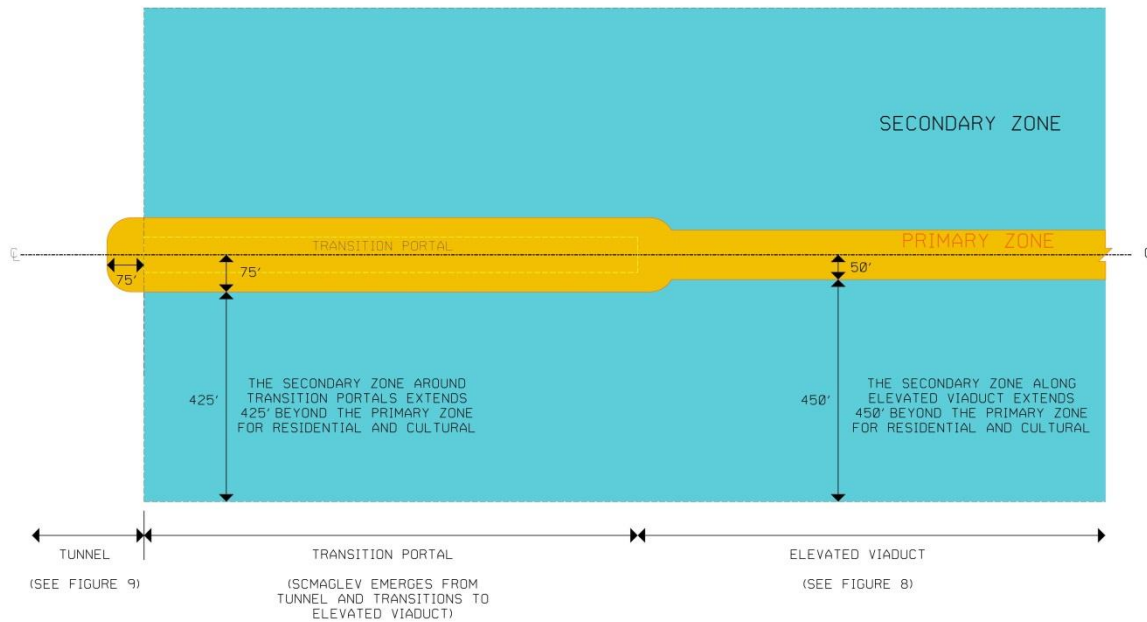


Figure 11: Guideway Analysis Zones for Residential/Cultural



The potential surface disturbance area does not include portions of the alignment which are estimated to be a deep bored tunnel below the surface. FRA and MDOT will analyze the environmental impacts of the alignments retained for further study in greater detail during the Alternatives Report. The Draft EIS will present options to mitigate potential impacts during the detailed evaluation of retained alternatives.

Screening Level 2 Station Zones

The Screening Level 1 process indicated that multiple station zones are feasible for each of the terminus stations (Baltimore and Washington, DC) and FRA and MDOT are coordinating with the Federal Aviation Administration (FAA) and Maryland Aviation Administration (MAA) regarding a potential station at BWI Marshall Airport. General station zones were not evaluated again for Screening Level 2. The station locations and their possible impacts will be studied in detail in the Draft EIS.

Screening Level 2 Results and Findings

FRA and MDOT presented the results of Screening Level 2 at the five October 2017 public open house meetings (see Table 8). Alignments that did not advance are G, G1, H, and I1.

Alignments G and G1 presented the highest potential for residential property impacts of any of the alignments. Potential impacts would involve substantial relocation of residents and disruptions on communities during construction. Alignment G also has the longest section of elevated guideway resulting in the highest potential of visual and sound impacts on 2,000 or more residential properties and communities. Alignment G1 has the third longest section of elevated guideway with potential impacts of visual and sound on over 1,000 residents. Both Alignments G and G1 would also potentially impact some of the most sensitive areas of the Anacostia Park and the National Arboretum. Similarly, Alignment G exhibits the highest potential impacts on state, county and local parks, totaling 50 or more acres in addition to direct and indirect impacts to the WB&A Trail in both Prince George's and Anne Arundel Counties. The Purpose and Need requires that the SCMAGLEV Project minimize impacts to the human environment; yet, the potential impacts to residents and communities presented the highest of any of the alignments. Therefore, for the above reasons Alignments G and G1 were dropped from further consideration.

Alignment H would be one of the most challenging to build since it would require a gradual aerial crossing of the existing NEC rail tracks and catenary system on a very sharp angle. In order to accomplish the crossing, the SCMAGLEV guideway would closely parallel the NEC for a considerable distance on the east side before ultimately crossing over to the west for another considerable stretch of side by side operation. This presents issues for existing NEC operations and constructability for the new SCMAGLEV project. More importantly, it raises safety issues by potentially creating catastrophic incidents (derailments) with its close proximity to Amtrak. Mitigation strategies using crash walls would be very costly, lowering the financial feasibility of the project. Alignment H would also potentially limit opportunities for rail expansion on the adjacent NEC and impact Patuxent Research Refuge and some of the most sensitive areas of the Anacostia Park and the National Arboretum. The purpose of the SCMAGLEV Project is to construct and operate a safe and revenue producing high-speed ground transportation system; yet, the potential safety issues and limits on expansion of the NEC do not meet this requirement. Therefore, Alignment H was dropped from further consideration.

Alignment I1 presented the second highest potential for residential property impacts. Similar to Alignments G and G1, potential impacts would involve substantial relocation of residents and disruptions during construction on communities and historic Bowie State University. Alignment I1 also crosses the NEC presenting safety, operational and constructability issues described above. The Purpose and Need requires that the SCMAGLEV Project minimize impacts to the human environment; yet, the potential impacts to residents and communities and conditions described above do not meet these requirements. Therefore, Alignment I1 was dropped from further consideration.

In addition to a No Build Alternative, FRA and MDOT carried the following preliminary alignments (shown on Figure 12) to the October 2017 public open house.

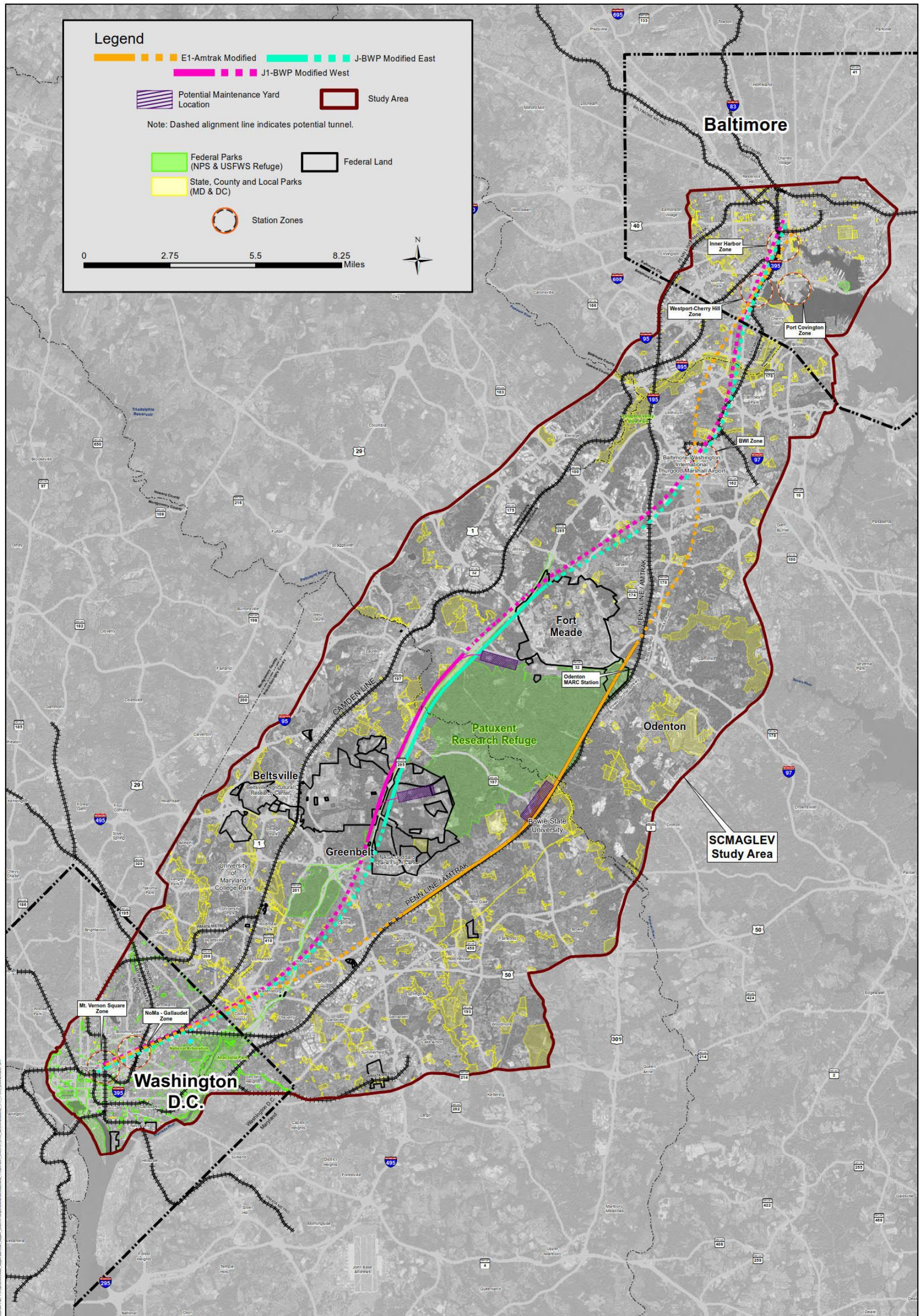
- Alignment E1 (Amtrak Modified);
- Alignment J (BWP Modified East); and
- Alignment J1 (BWP Modified West)

Table 8: Screening Level 2 Results Presented at the October 2017 Public Open House Meetings

Preliminary Alternatives Screening Results Screening Level 2 (Presented at Oct. 2017 Open House Meetings)	Guideway			Construction Feasibility	Environmental														Recommendation	
	Total Length (miles)	Elevated (%)	Tunnel (%)		Residential / Community Resources			Cultural Resources		Parks and Federal Lands				Natural Resources						
				Crossing Existing NEC track (Yes/No)	Potential Impact on Residential Properties		Potential Impact on Community Resources	Potential Impact on Commercial Properties	Historic Landmarks and Eligible National Resister Sites & Districts		Potential Impact on Parks (State, County, and Local)		Potential Impact on Federal Lands & Federal Parks		Crosses Patuxent Research Refuge (Yes/No)	Wetlands of Special State Concern	Other Wetlands	100-Year Floodplain		
					Primary	Scndry			Primary	Scndry	Primary	Scndry	Primary	Scndry						
No - Build Alternative	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	RETAIN
Alignment E1 (Amtrak Modified)	37	32%	68%	NO	Med	Low	Med	Med	Med	Low	Low	Med	Med	High	YES	Med	Med	Med	RETAIN	
Alignment G (WB&A)	38	53%	47%	NO	High	High	Med	Low	Med	Low	High	High	Low	Low	NO	Low	Med	High	Do Not Retain	
Alignment G1 (WB&A Modified)	38	39%	61%	NO	High	Med	Low	Low	Med	Low	Low	Low	Low	Low	NO	Low	Med	High	Do Not Retain	
Alignment H (WBA-Amtrak)	38	37%	63%	YES	Low	Low	Low	Low	Med	Low	Low	Med	Low	Med	YES	Low	Med	Med	Do Not Retain	
Alignment I1 (Amtrak Modified to WBA)	37	38%	62%	YES	High	High	Med	Med	Med	Low	Low	Low	Low	Low	NO	Low	Low	Low	Do Not Retain	
Alignment J (BWP Modified-East)	36	28%	72%	NO	Low	Low	Low	Low	Low	Low	Low	Low	High	High	YES	Med	Med	Low	RETAIN	
Alignment J1 (BWP Modified-West)	36	25%	75%	NO	Low	Low	Low	Low	Med	Low	Med	High	Low	High	NO	Low	Med	Med	RETAIN	

Note: Please refer to Table 7 for the criteria descriptions and the threshold ranges. See Appendix B for additional details.

Figure 12: Preliminary Alignment Presented at the October 2017 Public Open House Meetings



BALTIMORE-WASHINGTON SCMAGLEV PROJECT
Preliminary Alignments Recommended for Detailed Analysis
(Presented at the five October 2017 Public Open House Meetings)

Summer 2017

Important Note: Station zones (general areas where a station may be located) and alternative alignments are approximate and are subject to change during the NEPA process based on: additional environmental analysis by the NEPA team; preliminary engineering by the project sponsor, Baltimore Washington Rapid Rail; input from government agencies; and public input.

Chapter 6. Agency and Public Coordination

FRA and MDOT encouraged agency and public input throughout the development and refinement of the preliminary alignments. FRA and MDOT facilitated interagency meetings, numerous agency-specific meetings, and several public meetings and maintained a Project website and Project e-mail account. Input from agency meetings, the April 2017 and the October 2017 open houses, in addition to comments received during the scoping period, are being noted and considered, as applicable, in the Screening Level 2 evaluation and will continue to be used for further research in the Draft EIS phase of the Project.

Agency Coordination

FRA and MDOT engaged federal, state, and local agencies in the preliminary alignments development phase of SCMAGLEV Project through two interagency meetings in March 2017 (one in Baltimore and one in Washington, DC) followed by the presentation of the draft PASR results at the October 3, 2017 interagency meeting in Greenbelt, MD. FRA and MDOT also held an interagency webinar meeting on December 7, 2017 to provide an update on the PASR status. Representatives from the following agencies typically attend interagency meetings:

- Anne Arundel County Transportation Department
- Baltimore City Department of Transportation (BCDOT)
- Baltimore Metropolitan Council (BMC)
- District Department of Energy and Environment (DOEE)
- District Department of Transportation (DDOT)
- District of Columbia Historic Preservation Office (DC SHPO)
- Federal Aviation Administration (FAA)
- Federal Emergency Management Agency (FEMA)
- Federal Highway Administration (FHWA)
- Federal Transit Administration (FTA)
- Howard County Office of Transportation
- Maryland Department of Natural Resources (MD DNR)
- Maryland Department of Planning (MDP)
- Maryland Historical Trust (MHT)
- Maryland-National Capital Park and Planning Commission (M-NCPPC)
- Maryland State Highway Administration (SHA)
- Metropolitan Washington Council of Governments (MWCOG)
- National Aeronautics and Space Administration (NASA)
- National Capital Planning Commission (NCPC)
- National Park Service (NPS)
- U.S. Army Corps of Engineers (USACE)
- U.S. Commission of Fine Arts (USCFA)
- U.S. Department of Agriculture (USDA)
- U.S. Environmental Protection Agency (USEPA)
- U.S. Fish and Wildlife Service (USFWS)
- Surface Transportation Board (STB)
- Washington Metropolitan Area Transit Authority (WMATA)
- Others (as appropriate/depending on projects discussed)

Additionally, FRA and MDOT met individually with representatives from the FAA and MAA on April 5, 2017 to discuss a potential SCMAGLEV station at BWI Marshall Airport. FRA and MDOT also met with representatives from USFWS and NPS on April 19, 2017 at the Patuxent Research Refuge Visitor Center to discuss policies, regulations, and concerns related to USFWS and NPS properties within the Project study area. FRA and MDOT met with NPS again on November 20, 2017 at the National Capital Region office in Washington, DC.

On multiple occasions (May 3, 2017; June 7, 2017; and August 29, 2017) FRA, MDOT, Maryland Department of the Environment (MDE), and the USACE discussed permitting, scheduling, and other logistics. FRA and MDOT also made presentations to multiple agencies during the Joint Evaluation meetings on June 28, 2017; August 30, 2017; and December 20, 2017.

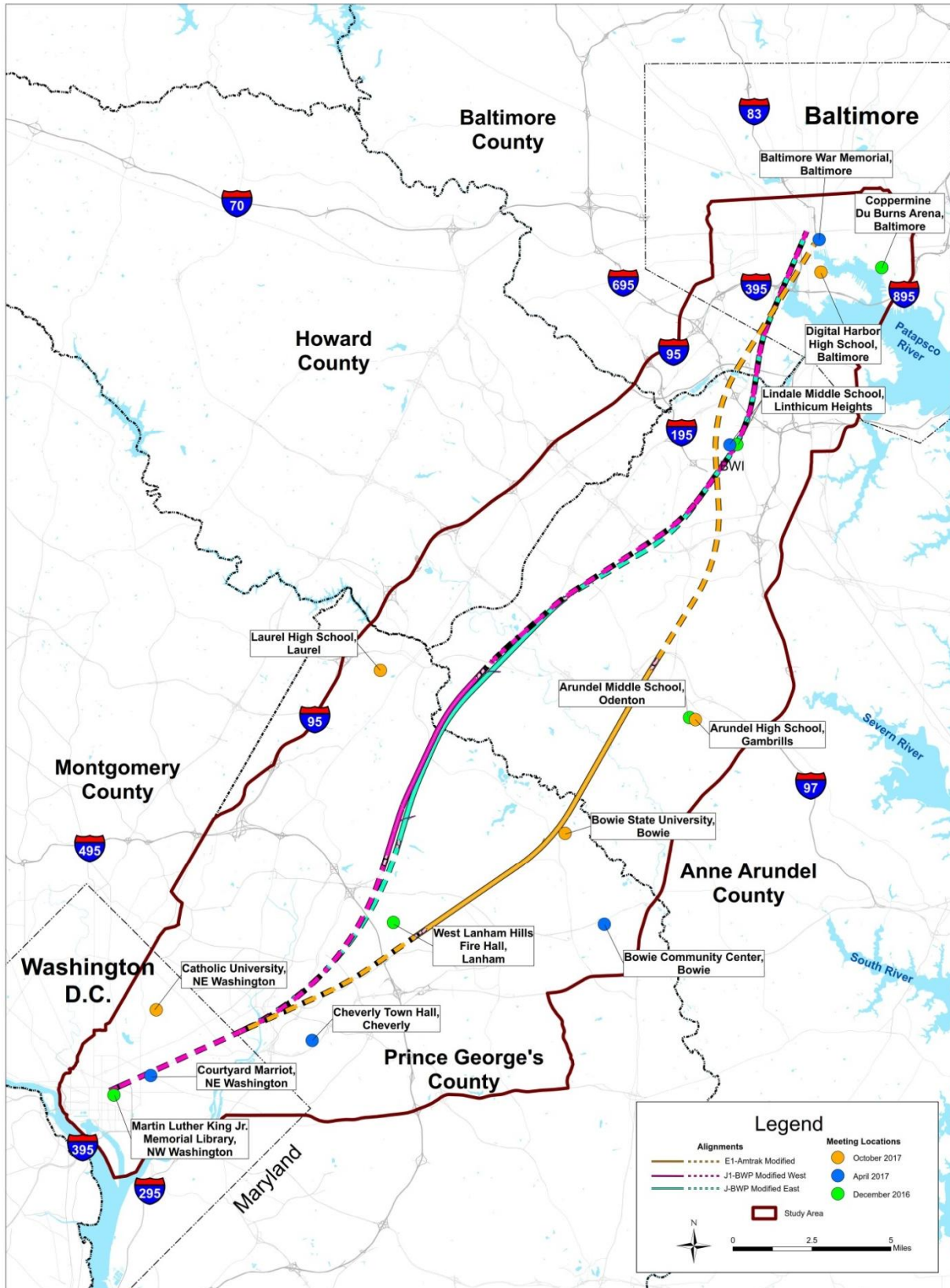
Agency field meetings occurred on July 19, 2017 and July 26, 2017 to provide agencies with an overview of the potential above ground portions of the preliminary alignments. Additional field review meetings will be scheduled as the study progresses.

FRA and MDOT also met with representatives from Anne Arundel County, NASA, the National Security Agency (NSA), Prince George's County, USDA [including the Beltsville Agricultural Research Center (BARC) and the U.S. National Arboretum], and the U.S. Secret Service. Common discussion points included the following: the potential visual and physical impacts to the various resources and agency properties along the preliminary alignments; potential effect or interference from magnetic signals/fields; noise and vibration effects; direct line of sight from the SCMAGLEV guideway into the respective facilities; as well as security of the guideway (from the standpoint of both the elevated viaduct and the proposed tunnel sections with respect to trespassers or potential terror threats). See Appendix C – Attachment A for more information on the agency meetings.

Public Input

FRA and MDOT informed the public of the Draft Project Purpose and Need and preliminary alignments during outreach via the Project website (<http://www.bwmaglev.info>) and notices posted in local and major newspapers; on-line social media and advertisements; notices posted at community and neighborhood organizations; and notices sent to federal, state, county, and local officials. Flyers were also distributed to community centers. FRA and MDOT held and will continue to hold public open houses and a hearing throughout the EIS process. For the NEPA Scoping phase of the project in mid-December 2016, five public open houses were held at different locations throughout the project corridor. A second round of five public open houses, to discuss the preliminary alternatives screening process and the Purpose and Need, were held in April 2017, and a third round of five public open houses presented the findings of the draft *Preliminary Alternatives Screening Report* in October 2017. Please see Figure 13 for a map illustrating the locations of the public open house meetings.

Figure 13: Public Open House Meeting Locations



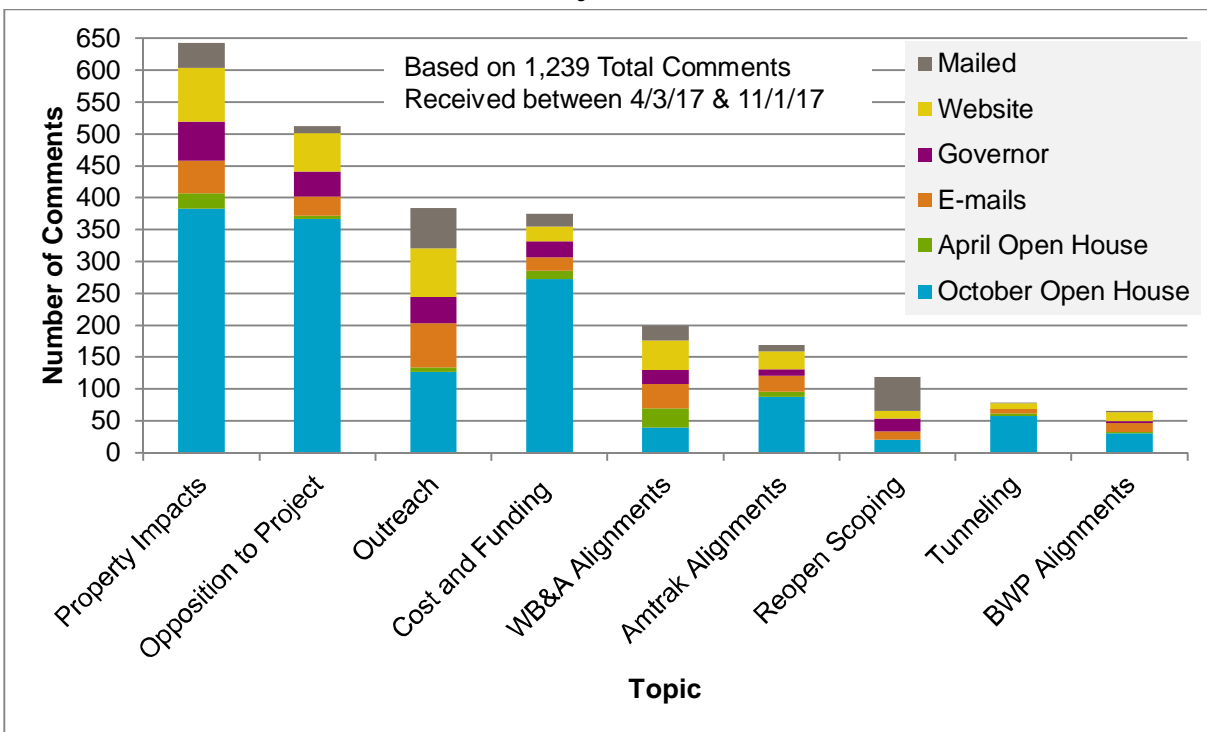
As noted on the project website (<http://www.bwmaglev.info>), where copies of reports and meeting materials can also be found, the December 2016 open houses focused on scoping. Comments from those meetings were reviewed and those pertaining to specific alignments are included in this report.

In April 2017, the open houses focused on the Project Purpose and Need and the preliminary alternatives screening process with conceptual maps of the preliminary alignments available on boards for viewing. Members of the Project Team were present to explain the boards, answer questions, and also encourage the public to comment on the Project. A total of 154 people signed in at the five April 2017 open houses and submitted 52 comments.

In October 2017, the open houses focused on the draft PASR results, with large (1" = 600' scale) maps of the three alignments recommended for further study on tables for viewing. Project Team members were present to explain the maps, boards, answer questions, and also encourage the public to comment on the Project. A total of 1,526 people signed in at the five October 2017 open houses and submitted 653 comments.

In addition to comments received at the open house meetings, the Project Team also received 210 comments via the Project website comment form; 161 comments via the Project e-mail account (info@bwmaglev.info) or e-mails accounts of individual Project Team members; 99 comments via the Governor's Office e-mail account; and 64 comments via mail; for a subtotal of 1,239 comments. Table 9 provides a summary of public comments and topics.

Table 9: Summary of Public Comments



FRA and MDOT analyzed written comments leading up to and including the second round of public meetings in mid-April 2017 through the completion of the third round of public meetings in late October, 2017. This period coincided with the development and screening of preliminary alignments. The top comment types are noted below:

- 1) Property Impacts – 643 or 52% of comments addressed property impacts, including *property devaluation* and use of *eminent domain*. Property impacts are the Public's top concern, and this sentiment has grown, particularly in the Bowie area since the April meetings.
- 2) Opposition to the Project – 512 or 41% of comments expressed *direct opposition* to the project (not just specific alignments).
- 3) Outreach – 384 or 31% of comments addressed public outreach, including 119 or 10% specifically requesting *re-opening the scoping process* due to "insufficient notification."
- 4) Cost and Funding – 375 or 30% of comments addressed project cost and funding, including *ticket price, taxes, and overall cost of the project*.
- 5) WB&A Alignments – 199 or 16% of comments addressed the WB&A Alignments, including *opposition to the alignments* and questions or comments about *how resources (particularly homes) would be impacted* by the alignments.
- 6) Amtrak Alignments – 169 or 14% of comments addressed the Amtrak Alignments. Further analysis of the Amtrak Alignments shows 24 or 2% of comments are in support of the Amtrak Alignments, while 87 or 7% of comments are in opposition.
- 7) Tunneling – 79 or 6% of comments addressed tunneling, including potential impacts of construction and vibration.
- 8) BWP Alignments – 66 or 5% of comments addressed the BWP Alignments. Further analysis of BWP Alignments shows 48 or 4% of comments are in support of the BWP Alignments while 13 or 1% of comments are in opposition.

Other comments are:

- Large numbers of attendees at the October meetings in Bowie and Gambrills (approx. 1,160 of the total 1,526 attendees that signed in) expressed concerns citing direct impacts to historic "old town" Bowie, Odenton, and surrounding areas.
- Although 6% of comments expressed concern with the impacts of tunneling, review agencies and some members of the public appear to favor alignments with greater underground (tunneling) lengths as compared to alignments that are above ground (elevated).
- Some meeting attendees and review agencies expressed concerns regarding impacts to natural and environmentally sensitive areas including Patuxent Research Refuge, Fran Uhler Natural Area, Saw Hill Creek, and Midland Park.
- Meeting attendees also raised concerns regarding an elevated viaduct structure and RSD facility altering the landscape surrounding Bowie State University, which is a historically black university and on the National Register of Historic Places.
- Meeting attendees also noted that Alignment E1 would impact Odenton Volunteer Fire Company, the only fire station in Odenton, and Bowie Assisted Living, Inc., the only proximate

facility of its kind according to residents. Some meeting attendees and review agencies noted that Fort George G. Meade gun range and a closed sanitary landfill would be traversed with alignment E1.

Appendix C – Attachment B contains the seven (7) comments on specific alignments during the previous phase of Project outreach, plus the 1,239 comments received during the development and screening of preliminary alignments for a total of 1,246 comments (as of November 2, 2017). Comments not received or compiled in time for the PASR will continue to be accepted and recorded/considered for future documents/phases. The project website (<http://www.bwmaglev.info>) includes responses to the most common questions under the Frequently Asked Questions (FAQs) page, as well as meeting materials, interactive maps and reports.

Chapter 7. Recommended Preliminary Alternatives for Detailed Analysis

Screening criteria in earlier chapters were not weighted but were given equal consideration during analysis. However, in light of agency and public comments received before and after October Public Meetings, FRA and MDOT revisited screening criteria and recognized that more consideration should be given to human, natural areas and wildlife, and constructability factors during Screening Level 2 results.

Human Factors

FRA and MDOT decided that human factors should be given more consideration in light of the overwhelming comments received from residents in Anne Arundel and Prince George's Counties. Property impacts are noted as the Public's top concern, and this sentiment has grown, particularly in the Bowie area since the April 2017 meetings. Preliminary alignments E1 (Amtrak Modified), G (WB&A), G1 (WB&A Modified), and I1 (Amtrak Modified to WBA) would involve a significant number of residential property acquisitions and elimination or reduction in operation of community facilities during and after construction of SCMAGLEV. These impacts would result in losses to a large number of well-established neighborhoods, community and transportation facilities, and historic properties and districts, which are not easily mitigated and are inconsistent with State requirements to minimally impact those who live and work along proposed alignments. Consequently, in a letter signed by ten State Senators and Delegates from Anne Arundel and Prince Georges Counties in December 2017, they indicated their support of dropping the WB&A Modified alignment and are urging the Maryland Secretary of Transportation to drop the Amtrak Modified alignment, citing impacts to the surrounding communities that would result from that alignment. Refer to Appendix C – Attachment B for the comments received.

Natural Areas and Wildlife Factors

Natural areas and wildlife resources are associated with federally protected environmental lands located within the in the study area. As stated earlier, FRA and MDOT's intent was to avoid or minimize potential impacts to these resources, particularly PRR. Preliminary alignments E1, H, and J (BWP Modified East) attempted to minimize PRR impacts by clipping southern and northern boundaries respectively. However, the Alignment J offers more opportunities to minimize impacts to PRR, which will be explored in Alternatives Report.

Constructability Factors

Constructability factors also play into the feasibility of any alignment. FRA and MDOT also gave additional consideration to any alignment that would be in close proximity or cross the existing NEC rail tracks. This applied to alignment E1, H and I1.

By crossing the NEC, alignment H is inconsistent with the SCMAGLEV Project Purpose as the crossing would not be complementary to existing rail operations or future rail expansion in the NEC. An SCMAGLEV crossing would entail constructing almost 1km (0.6 mi) of foundations, piers and superstructure decking within the 150-foot crash zone on each side of the railroad tracks, representing over 200m (656 ft) of linear viaduct construction directly over active tracks requiring Amtrak, MARC and freight rail service disruptions. Designing and constructing 1km (0.6 mi) of viaduct along and over the NEC, including realignment of catenaries and supporting infrastructure, is unprecedented and may take up to ten years to coordinate.

Crossing the NEC with alignment H or I1 is unreasonable in light of the SCMAGLEV Project Purpose because “crossing this congested segment of the NEC would be more costly and would more directly impact NEC operations...” [Precedent language from Extension of PATH over NEC project].

In addition, the NEC right-of-way is not available for SCMAGLEV because of plans to increase the number of tracks in this part of the NEC as outlined in the NEC Future Tier I EIS. Constructing SCMAGLEV in or very near the NEC right-of-way would conflict with the SCMAGLEV Project Purpose of being compatible with planned future rail expansion in the NEC.

Even though outside the NEC right-of-way, alignments E1, H, and I1 in the vicinity of the NEC would require intrusion protection of SCMAGLEV from a derailment on the NEC, either through distance separation of 150 feet (or greater) or through construction of a crash wall. The needed intrusion protection measures make placing SCMAGLEV in the vicinity of the NEC inconsistent with the Project Purpose. Moving a SCMAGLEV alignment at least 150 feet west of NEC right-of-way, while maintaining a geometry that accomplishes the SCMAGLEV operating speed would increase (not minimize) impacts of SCMAGLEV on surrounding residential communities and businesses. A crash wall could allow for SCMAGLEV to be placed closer to the NEC right-of-way. However, at over 10 miles in length, the crash wall, would be prohibitively expensive and undermine the SCMAGLEV’s commercial viability.

The location of the transition portal into deep tunnel section for alignments E1 and H would directly conflict with future Odenton Town Center Transit-Oriented Development (TOD) at the MARC Odenton station. This would be inconsistent with the Project Purpose of supporting local and regional economic growth.

To be operationally viable, SCMAGLEV requires a RSD adjacent to the alignment. The relatively dense development along the NEC presents difficulties in locating a RSD. The location of a RSD on alignments E1 or I1 would require placement of the tunnel portal at the MARC Seabrook station and nearby development, resulting in a displacement of the MARC Seabrook station. As such, the alignment would not meet the Project’s Purpose in that it would not minimize impact on the human environment, would not be compatible with other rail corridors, and would not support local economic growth. A RSD opposite Bowie State University would severely impact MD 197 during and after construction.

Additional Alignments Eliminated from Further Study

After including these factors into the analysis and applying the requirements of the Purpose and Need for a safe, revenue-producing SCMAGLEV system that minimizes impacts to the human and natural environment, FRA and MDOT added alignment E1 to the list of alignments that did not advance.

Table 10 presents the final results of the preliminary screening that FRA and MDOT updated after the October 2017 open house version to note Alignment E1 (Amtrak Modified) has been eliminated from further study (along with alignments G, G1, H, and I1 originally eliminated in Screening Level 2).

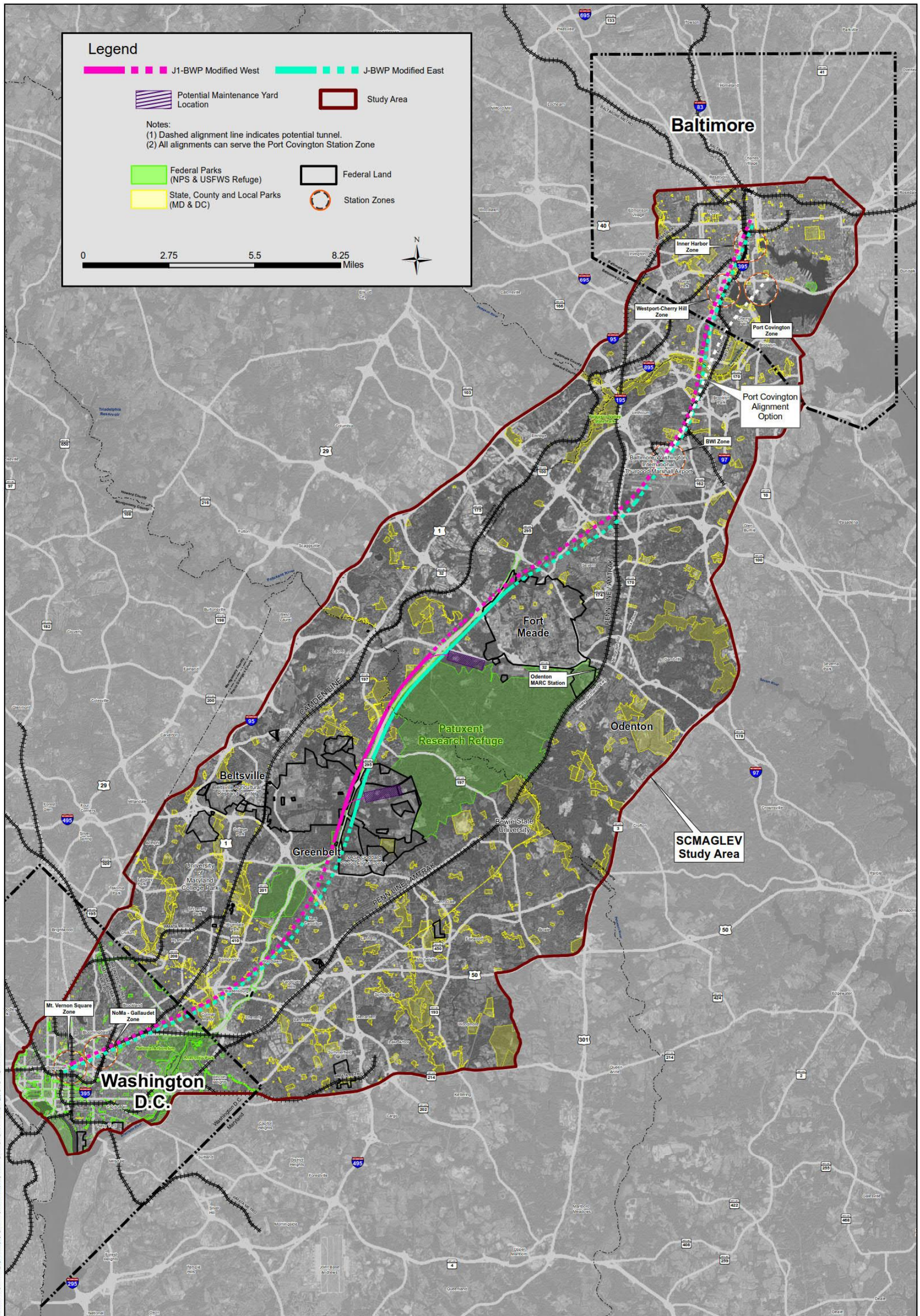
Alignments that advanced to the next phase are referred to as preliminary alternatives recommended for detailed analysis. Based on the results of Screening Level 2 and the review of public comments, FRA and MDOT recommend that alignments J and J1 be carried forward for further engineering development and environmental evaluation, as illustrated in Figure 14.

Table 10: Final Screening Level 2 Results

Preliminary Alternatives Screening Results Screening Level 2 <i>(Updated after the October 2017 open house meetings)</i>	Guideway			Construction Feasibility	Environmental														Recommendation	
	Total Length (miles)	Elevated (%)	Tunnel (%)		Residential / Community Resources			Cultural Resources		Parks and Federal Lands				Natural Resources						
				Crossing Existing NEC track (Yes/No)	Potential Impact on Residential Properties		Potential Impact on Community Resources	Potential Impact on Commercial Properties	Historic Landmarks and Eligible National Register Sites & Districts		Potential Impact on Parks (State, County, and Local)		Potential Impact on Federal Lands & Federal Parks		Crosses Patuxent Research Refuge (Yes/No)	Wetlands of Special State Concern	Other Wetlands	100-Year Floodplain		
					Primary	Scndry			Primary	Scndry	Primary	Scndry	Primary	Scndry						
No - Build Alternative	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	RETAIN
Alignment E1 (Amtrak Modified)	37	32%	68%	NO	Med	Low	Med	Med	Med	Low	Low	Med	Med	High	YES	Med	Med	Med	Do Not Retain	
Alignment G (WB&A)	38	53%	47%	NO	High	High	Med	Low	Med	Low	High	High	Low	Low	NO	Low	Med	High	Do Not Retain	
Alignment G1 (WB&A Modified)	38	39%	61%	NO	High	Med	Low	Low	Med	Low	Low	Low	Low	Low	NO	Low	Med	High	Do Not Retain	
Alignment H (WBA-Amtrak)	38	37%	63%	YES	Low	Low	Low	Low	Med	Low	Low	Med	Low	Med	YES	Low	Med	Med	Do Not Retain	
Alignment I1 (Amtrak Modified to WBA)	37	38%	62%	YES	High	High	Med	Med	Med	Low	Low	Low	Low	Low	NO	Low	Low	Low	Do Not Retain	
Alignment J (BWP Modified-East)	36	28%	72%	NO	Low	Low	Low	Low	Low	Low	Low	Low	High	High	YES	Med	Med	Low	RETAIN	
Alignment J1 (BWP Modified-West)	36	25%	75%	NO	Low	Low	Low	Low	Med	Low	Med	High	Low	High	NO	Low	Med	Med	RETAIN	

Note: Please refer to Table 7 for the criteria descriptions and the threshold ranges. See Appendix B for additional details.

Figure 14: Alignments Recommended for Detailed Analysis



BALTIMORE-WASHINGTON SCMAGLEV PROJECT
Alignments Recommended for Detailed Analysis
(Updated after the October 2017 Public Open Houses)

Updated Fall 2017

Important Note: Station zones (general areas where a station may be located) and alternative alignments are approximate and are subject to change during the NEPA process based on: additional environmental analysis by the NEPA team; preliminary engineering by the project sponsor, Baltimore Washington Rapid Rail; input from government agencies; and public input.

Chapter 8. Conclusions and Next Steps

Alignments J and J1 presented the least potential for residential property impacts of any of the alignments. Alignment J1 has the longest tunnel section, followed by Alignment J which has the second longest tunnel section. Review agencies and some members of the public appear to favor alignments with greater underground tunneling than those above ground. Constructability and safety were not concerns with Alignments J and J1, since they do not conflict with NEC.

Alignments J and J1 would be in tunnel under Anacostia Park and both avoid the National Arboretum. Alignment J1 also avoids PRR. However, both Alignments J and J1 impact the Baltimore Washington Parkway. Potential impacts to this resource are expected to occur mainly near interchanges and crossings. FRA and MDOT are coordinating with NPS on topics including avoidance and least harm options. Coordination will continue throughout the alternatives development process.

FRA and MDOT are recommending that Alignments J and J1 be retained for detailed study in the Alternatives Report given their ability to minimize impacts on the human environment, to avoid potential safety and operational issues with nearby rail lines, not preclude the expansion of NEC, and to offer strategies to minimize impacts to NPS property.

The Alternatives Report will document the refinement of the retained alignments to include the footprint of the various ancillary facilities required for the SCMAGLEV system in the quantitative analysis, utilize the refined LOD to analyze the potential impacts, and identify the alternative(s) retained for the Draft EIS.

The No Build Alternative will also be carried forward and will serve as a baseline for comparison to these remaining Build alternatives, as well as a tool to evaluate feasibility, impacts, and cost effectiveness, pursuant to NEPA. Results of the additional analysis will be documented in the Alternatives Report, which will conclude with the selection of the alternative(s) that advance to the Draft EIS.

As the NEPA process continues, FRA and MDOT will improve the alignments as additional potential environmental impacts are identified and/or to minimize or avoid resources where possible. Coordination with the public and resource agencies regarding the evaluation of alignments will continue throughout the Project and be documented in the forthcoming Alternatives Report.



Appendices

- Appendix A Previous Maglev Study Alternatives
- Appendix B Additional Screening Details
- Appendix C Agency and Public Outreach
- Appendix D Baltimore-Washington SCMAGLEV Technical Memorandum: Speed as a Screening Criterion.

Appendix A. Previous Maglev Study Alternatives

In a 1994 feasibility study, MTA studied both German and Japanese magnetic levitation (maglev) technologies.¹ The study noted a maximum cruising speed of 300 mph (483 km/h), but a total travel time was not stated. The alternatives considered included: I-95 Parallel; Baltimore-Washington (BW) Parkway Parallel; Parkway Independent; and an Amtrak Parallel alignment. The 1994 analysis considered potential stations in Baltimore, BWI Marshall Airport, Greenbelt, New Carrollton, and Washington, DC. The study declared that maglev was feasible between Baltimore and Washington, DC, but did not select a preferred alternative.

FRA completed a Programmatic EIS in 2001 that studied the same technologies studied in MTA's 1994 Feasibility Study. The document identified a maximum cruising speed of 240 mph (386 km/h), but did not state a total travel time. The 2001 FRA study enumerated potential stations at Baltimore, BWI Marshall Airport Terminal, BWI Aviation Boulevard, Greenbelt, New Carrollton, and Washington, DC. The 2001 FRA Programmatic EIS identified Maryland as one of the corridors to advance a Draft EIS with three preferred alternatives retained for detailed study (I-95 Parallel; BW Parkway Parallel, and Amtrak Parallel).

In 2003, FRA and MTA prepared a site-specific Draft EIS, which studied German Transrapid TR07 technology. The 2003 analysis identified a maximum cruising speed of 260 mph (420 km/h), but did not state a total travel time. The alternatives considered included: I-95 Parallel; BW Parkway Parallel; and the Amtrak Parallel alignment that were retained and refined from the 2001 Programmatic EIS. The 2003 Project Team considered potential stations in Baltimore, BWI Marshall Airport Terminal, BWI Aviation Boulevard, Greenbelt, New Carrollton, and Washington, DC. The 2003 Draft EIS study selected the Amtrak Parallel alignment as the preferred alternative.

In 2007, FRA and MTA prepared a Final EIS prepared by FRA/MTA using the German Transrapid TR07 technology. This analysis targeted a total travel time of 18.5 minutes between Baltimore and Washington, with a maximum cruising speed of 260 mph. The alternatives considered included: No Build Alternative and the Amtrak Parallel alignment that was retained and refined from the 2003 Draft EIS. As with earlier analyses, this study included potential stations at Baltimore, BWI Marshall Airport Terminal, and Washington, DC. The study concluded that the construction of a maglev system would have substantial transportation benefits for the area. However, the project was suspended before the Final EIS was released. FRA did not issue a Record of Decision and the project was not advanced further at the time.

In 2012, BWRR, a private proponent, prepared an Alternatives Study using the Japanese SCMAGLEV technology.² This internal study identified a maximum cruising speed of 311 mph (500 km/h) with a total target travel time of approximately 15 minutes. The analysis included an Amtrak Alternative, BW Parkway Alternative, Washington-Baltimore & Annapolis (WB&A) Base Alternative, and an additional WB&A

¹ For purposes of this study, magnetic levitation (Maglev) is defined as an advanced transportation technology in which magnetic forces lift, propel, and guide a vehicle over a specially designed guideway.

² This Project proposes to implement superconducting maglev (SCMAGLEV) technology, which differs from other maglev systems (such as the German Transrapid system) in that SCMAGLEV accelerates and decelerates through an electromagnetic force generated between superconducting magnets on the vehicle and reaction coils on the guideway sidewalls. The superconducting magnetism is much stronger than ordinary normal conducting electromagnets. Additionally, SCMAGLEV uses inductive magnetic reactions with no active control and rides in a U-shaped guideway; whereas, the German Transrapid system uses attractive reactions that need active controls and rides in a T-shaped guideway.

Alternative. The BWRR study identified potential stations in Baltimore (either Camden Yards or Westport), BWI Marshall Airport Terminal, and Mount Vernon Square (Washington, DC).

Table A-1 summarizes the previous Baltimore to Washington Maglev studies, including alternatives considered. Figure A-1 depicts general transportation corridor routes many of the earlier studies utilized.

For purposes of this EIS, FRA and MDOT evaluated three additional alignment alternatives based on revisions to the 2012 BWRR study alignments. The study team evaluated each of these three alternatives to determine if it meets the Project Purpose and Need and how it meets the other high level screening criteria. These 2012 alternatives are shown on Figure A-2 and were utilized as the basis of Alternatives E, F, and G as evaluated in this PASR.

Table A-1: Previous Baltimore to Washington Maglev Studies

Criterion	Previous Baltimore to Washington Maglev Studies				
	1994 Feasibility Study (MTA)	PEIS Development (1999-2001)	DEIS and ARDS (2001-2002)	FEIS Study (2003-2007)	BWRR Alternatives Study (2012)
Technologies Considered	German Transrapid TR07 Japanese MLU-002 4 Concept Systems	German Transrapid TR07 Japanese MLU-002 4 Concept Systems	German Transrapid	German Transrapid	Japanese SCMAGLEV
Maximum Cruising Speed	483 km/h 300 mph	386 km/h 240+ mph	420 km/h 260 mph	420 km/h 260 mph	500 km/h 311 mph
Total Travel Time	Not Given	Not Given	Not Given	18.5 Minutes	15 Minutes
Alternatives Considered	1-95 Parallel Alternative B-W Pkwy .Parallel Alt. Pkwy. Independent Alt. Amtrak Parallel Alt.	1-95 Parallel Alt. B-W Pkwy. Parallel Alt. Pkwy. Independent Alt. Amtrak Parallel Alt. Several options considered and dismissed	I-95 Parallel Alt. B-W Pkwy. Parallel Alt. Amtrak Parallel Alt.	Amtrak Parallel Alt. No Build Alt.	Amtrak Alignment B-W Pkwy. Alt. WB&A Base Case Align. WB&A Option 1 Align.
Preferred Alignment	Not Selected	3 Alignments Retained for Further Study (DEIS Alts. Considered)	Amtrak Parallel Alternative	Amtrak Parallel Alternative	Not given
Potential Station	DC, New Carrollton, Greenbelt, BWI, Baltimore	DC, New Carrollton, Greenbelt, BWI Aviation Blvd., BWI Airport Terminal, Baltimore	DC, New Carrollton, Greenbelt, BWI Aviation Blvd., BWI Airport Terminal, Baltimore	Union Station, BWI Option A & C, Camden Station	Mt. Vernon Station, BWI Station, Camden Station, Westport Station
Other Conclusions	Maglev Declared Feasible, Station Options and Alignments Identified	Maryland Selected by FRA for DEIS Stage. Refinements made to the alternatives retained for Draft EIS	Amtrak Parallel Alt. selected for further study based on environmental /engineering analysis, public and agency comments. No Build Alt. also retained for further study.	Detailed study completed. MTA concluded that construction of Maglev project would have substantial transportation benefits for the area.	Further studies are needed.

Figure A-1: Previous Study Alignments (2003 DEIS Scoping Alternatives)

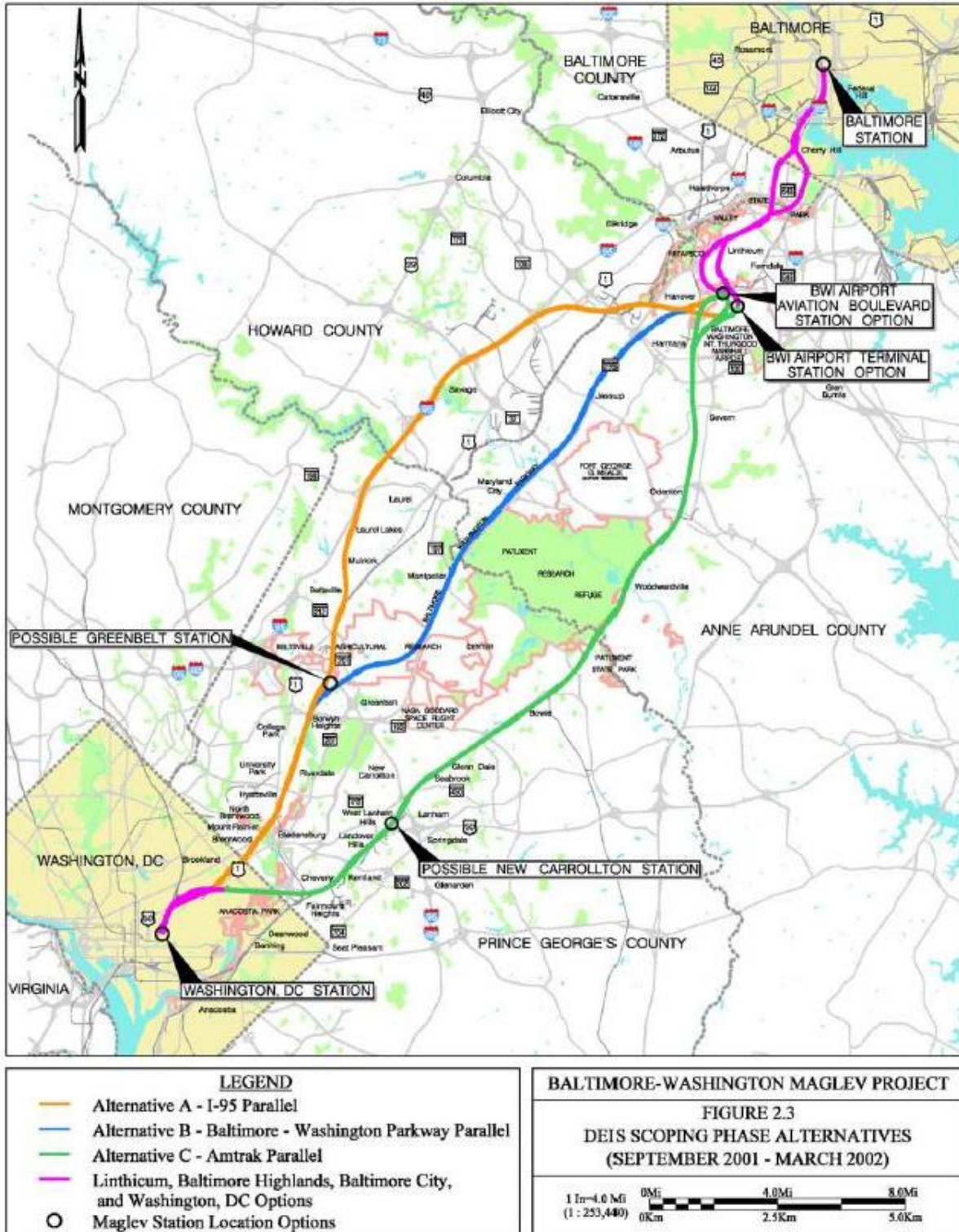
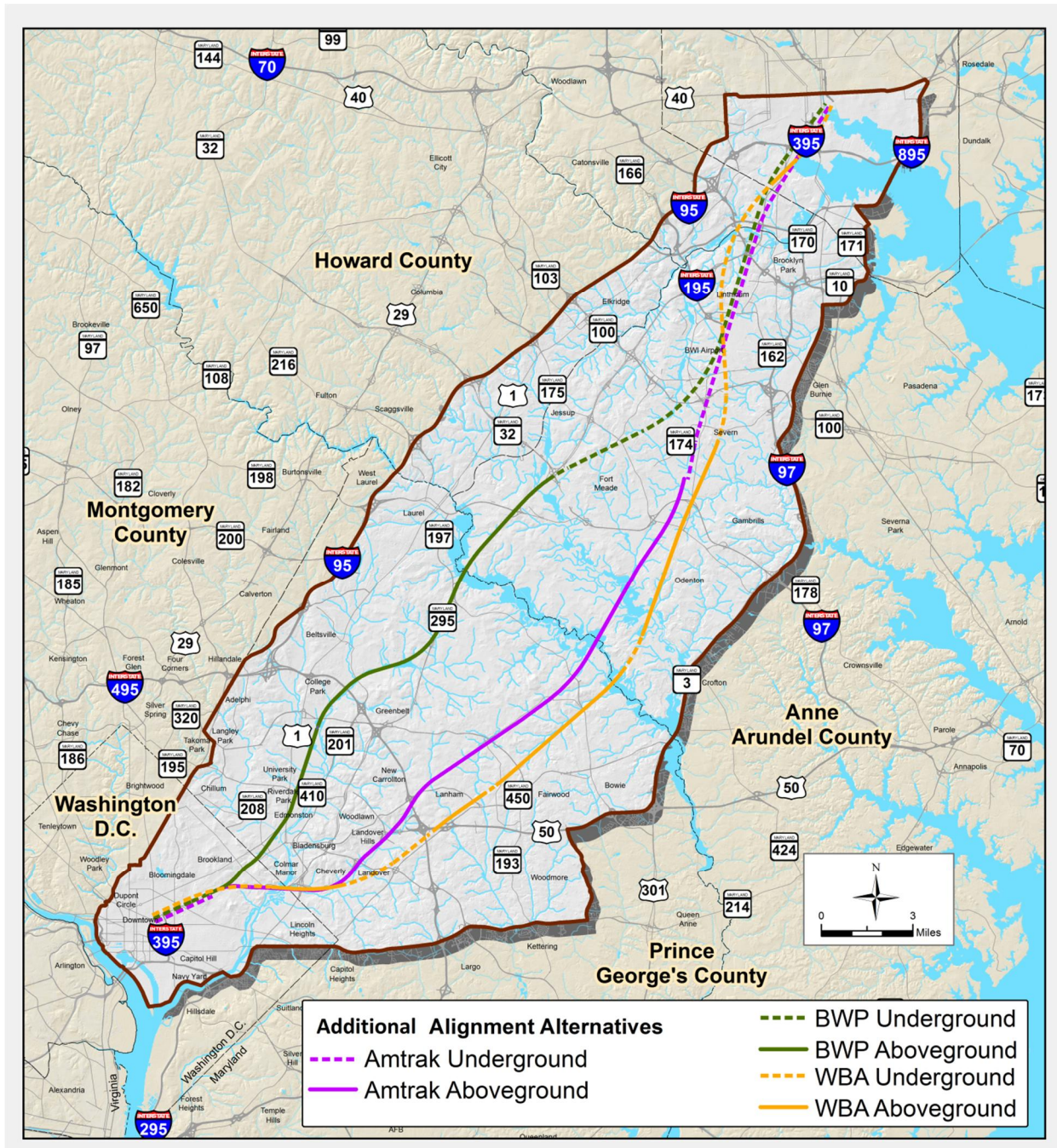


Figure A-2: Additional Alignments (2012 BWRR Alternatives Study)



Appendix B. Additional Screening Details

B.1. Screening Level 2 – Data Sources utilized for the GIS desktop evaluation

Residential Properties: GIS analysts overlaid County and City property parcel boundary data with Maryland Planning 2010 and DC Office of the Chief Technology Officer (OCTO, 2004) residential land use polygons. Residential properties consist largely of owner-occupied single-family homes, townhomes, and condominiums.

Community Resources: Community resource points include schools, day care, hospitals, health clinics, churches, cemeteries, synagogues, and mosques. The GIS analyst applied a radial buffer around the community resources center point locations, using 25 ft. and 100 ft. radius, to identify those community resources that may be within areas of the alignments surface disturbance. The primary data source for this information is the Maryland State Highway Administration's (SHA) 2015 Points of Interest, which was supplemented by individual county GIS resources. These community resources are important to determine the alignments' potential to divide communities and affect community cohesion, or alter response times for emergency response services, or access to services and facilities.

Commercial Properties: Commercial properties include office buildings, retail stores, warehouses, and parcels with heavy commercial and industrial uses. The GIS analysts overlaid County and City property parcel boundary data with Maryland Planning 2010 and DC OCTO (2004) commercial land use polygons.

Historic Properties: This category includes Landmarks and Eligible National Register Sites and Districts. The GIS analysts utilized Maryland Historical Trust (MHT) and the District of Columbia Historic Preservation Office (HPO) GIS databases for this analysis.

Parks (state, county & local): State parkland information was derived from Maryland Department of Natural Resources (MDNR) Statewide Parks 2015 GIS data and DC GIS 2015 Parks. County and local parkland information was retrieved from county level GIS databases, as well as Maryland – National Capital Park and Planning Commission data.

Federal Lands [Includes Federal Parks/Refuges (NPS/Patuxent)]: Federal lands data was compiled from National Park Service 2017 Shapefiles, Department of Defense 2017 Military Installations data, and MDNR Statewide Federal Lands 2016 Shapefile. Figure B-1 illustrates various Parks and Federal Lands based on GIS desktop information. For this evaluation, Parks and Federal Lands were categorized in the following order of priority: (1) Federally Protected Environmental Land {Parks/Refuges}; (2) Federal Land; (3) State Protected Environmental Land/Parks; (4) County Protected Environmental Land/Parks; and (5) Local Parks.

Wetlands of Special State Concern (WSSC): are wetlands with rare, threatened, or endangered species or unique habitats. The MDNR wetlands GIS data has been utilized to determine where Wetlands of Special State Concern (WSSC) are within areas of the alignments surface disturbance. In Maryland, certain wetlands with rare, threatened, or endangered species or unique habitat receive special attention. The Code of Maryland Regulations (COMAR) Title 26, Subtitle 23, Chapter 06, Sections 01 & 02 identifies these WSSC.

Other Wetlands: These include wetlands that are not identified as WSSC. MDNR GIS wetland data was utilized for Maryland counties, and the U.S. Fish and Wildlife Services National Wetlands Inventory wetland data has been utilized to illustrate the locations of wetlands within Washington, DC areas of the alignments surface disturbance. Figure B-2 illustrates various surface waters and wetlands based on GIS desktop information.

100-Year Floodplain: Includes the floodplain as shown on Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMs). The Federal Emergency Management Agency (FEMA) 100-Year floodplain mapping was utilized to determine where floodplain falls within areas of the alignments surface disturbance.

Table B-1: Source of GIS Information Summary Table

Criterion	Data Source	Description
Residential Properties	MP (2010), OCTO (2004), City and County GIS	Residential land uses within areas of surface disturbance (<i>Number of Properties</i>)
Community Resources	SHA (2015), City and County GIS	Churches, schools, cemeteries, and emergency/health services within the areas of surface disturbance (<i>Number of Properties within 25' and 100'</i>)
Commercial Properties	MP (2010), OCTO (2004), City and County GIS	Commercial land uses within areas of surface disturbance (<i>Number of Properties</i>)
NHL and NRHP Listed or Eligible Historic Districts and Individual Properties	MHT, HPO	NHL and NRHP properties and districts within the area of surface disturbance (<i>Number of Sites and/or Districts</i>)
Parks	MDNR, M-NCPPC; DC GIS, City and County GIS	State, local, and county-owned parks within the area of surface disturbance (<i>Number of Parks; Acres</i>)
Federal Lands	DOD, MDNR, NPS	Properties potentially subject to Section 4(f) evaluation within area of surface disturbance (<i>Acres</i>)
Wetlands of Special State Concern	MDNR WSSC	Wetlands with rare, threatened, or endangered species or unique habitat identified by the State of Maryland within the area of surface disturbance (<i>Acres</i>)
Other Wetlands	MDNR, USFWS NWI	Wetlands within the area of surface disturbance (<i>Acres</i>)
100-Year Floodplain	FEMA FIRMs, USFWS	100-year floodplain within the area of surface disturbance (<i>Acres</i>)

Figure B-1: Protected Lands and Environmental Easements

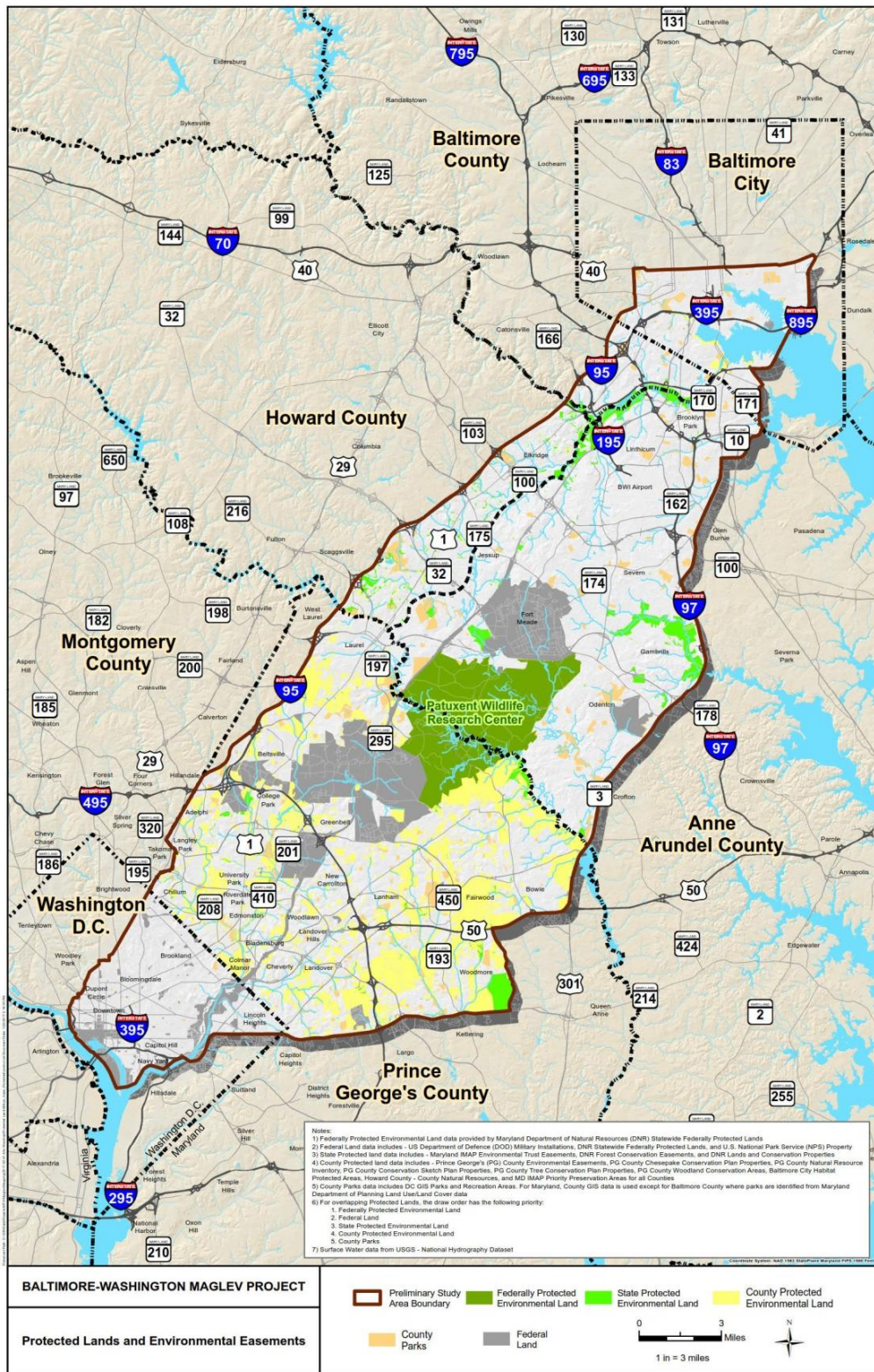
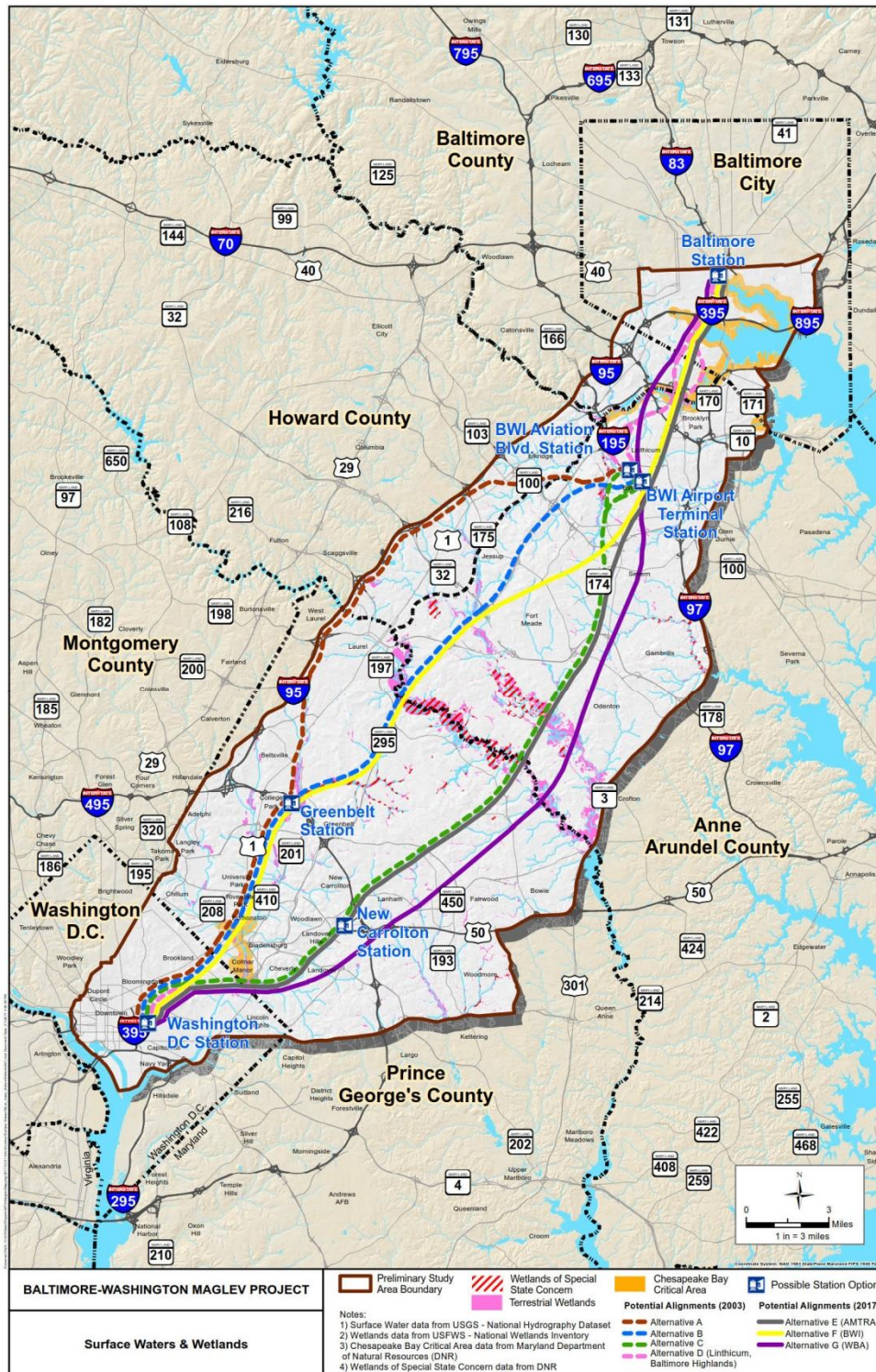


Figure B-2: Surface Waters and Wetlands



B.2. Screening Level 2 – Other Categories considered, but found to be similar between Alignments

The NEPA Team considered the following factors, but potential impacts were similar for each alignment, and therefore the categories were not discriminating factors in comparison for the PASR. These categories will likely be revisited in the Alternatives Report and/or the EIS for further analysis:

Low-income and Minority Areas: A low-income area is defined as those with a median household income at or below the Department of Health and Human Services or Census Bureau poverty guidelines. Minority populations are defined as those where the minority (Black, Hispanic, Asian-American, or American Indian and Alaskan Native) population of the affected area exceeds 50-percent or is meaningfully greater (10 percent) than the minority population percentage in comparison to the respective County average. Low-income and minority information has been derived at the block group level from the latest 2014 Census Block Group American Community Survey data, identifying the location of low-income areas, and minority areas, as well as those locations in which these areas overlap. Considering the potential impacts of the preliminary alternatives to these communities is important in light of Environmental Justice considerations (Executive Order 12898, Department of Transportation Order 5610.2a).

Ecological Resources: Ecological resources consider a wide range of natural areas and species including forests; migration corridors; rare, threatened and endangered species; critical habitat and sensitive species protection areas; wildlife refuges; and conservation easements. For the future analysis, review includes the Maryland Department of Natural Resources (MDNR) datasets including Forest Interior Dwelling Species (FIDS), Forest Conservation Easements (FCE), and Sensitive Species Project Review Areas (SSPRA) provided by both state and county GIS.

Wooded Areas: Wooded areas have been derived from the SHA GIS tree canopy polygons for the counties, vegetated area data in Baltimore City, and wooded area data from DC GIS. Wooded areas can include large tracts of forest as well as roadside vegetated tree lines. These have been utilized to illustrate the locations of wooded areas within areas of the alignments surface disturbance.

B.3. Screening Level 2 – Quantities utilized for the high/medium/low Summary Table

FRA and MDOT utilized the quantities presented in Table B-2 of this appendix for the Screening Level 2 analysis as discussed in Chapter 5. These quantities represent the number and/or acres of the various resources present within the respective primary or secondary analysis zone buffers.

Table B-2: Quantities Data Table

Screening Level 2 Data Table	Construction Feasibility	Environmental																								
	Crossing existing NEC tracks	1		Not used in Summary		2		3		4			5		6		Not used in Summary		Not used	7		8		9		
		Number of Residential Properties in analysis buffer zone		Number of Low Income Areas (L) and Minority Areas (M) in analysis buffer zone		Number of Community Resources in analysis buffer zone		Number of Commercial Properties in analysis buffer zone		Number of Historic Landmarks & Eligible National Register Sites & Districts in analysis buffer zone			Number (Acres) of Parks (State, County, Local) in analysis buffer zone		Crosses Patuxent Research Refuge		Acres of Federal Lands (NPS, Patuxent, Ft. Meade, BARC, etc.) in analysis buffer zone		Acres of Ecological Resources (SSPRA, critical habitat, protective forests easements) in analysis buffer zone		Acres of Wooded Areas in analysis buffer zone	Acres of Wetlands of Special State Concern in analysis buffer zone		Acres of Other Wetlands in analysis buffer zone		Acres of 100 YR Flood-plains in analysis buffer zone
		Yes/No	Primary	Scndry*	Primary	Scndry*	Primary	Scndry*	Primary	Primary	Scndry*	Primary	Scndry	(Yes/No)	Primary	Scndry	Primary	Scndry	Primary	Primary	Primary	Primary	Primary			
No - Build Alternative	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A			
Alignment E1 (Amtrak Modified)	No	126	915	0 LI; 7 M	1 LI; 14 M	4 to 6	13 to 14	124	10	+9	2 (10 AC)	+3 (33 AC)	YES	54 AC	+211 AC	136 AC	337 AC (FIDS ONLY)	103 AC	9 AC	13 AC	21 AC					
Alignment G (WB&A)	No	429	2794	2 LI; 22 M	3 LI; 27 M	4 to 5	22 to 25	16	12	+6	12 (39 AC)	+4 (121 AC)	NO	13 AC	+41 AC	137 AC	371 AC (FIDS ONLY)	145 AC	<1 AC	24 AC	57 AC					
Alignment G1 (WB&A Modified)	No	408	1997	2 LI; 19 M	3 LI; 26 M	0	12 to 14	9	8	+7	1 (2 AC)	+1 (7 AC)	NO	11 AC	+33 AC	119 AC	318 AC (FIDS ONLY)	126 AC	<1 AC	25 AC	60 AC					
Alignment H (WBA to Amtrak)	Yes	87	617	2 LI; 18 M	3 LI; 22 M	1	6 to 8	31	13	+4	9 (12 AC)	+3 (37 AC)	YES	48 AC	+166 AC	154 AC	94 AC (FIDS ONLY)	121 AC	4 AC	23 AC	49 AC					
Alignment I1 (Amtrak Modified to WBA)	Yes	418	2629	0 LI; 8 M	1 LI; 16 M	3 to 6	25 to 26	119	8	+11	3 (3 AC)	+0 (12 AC)	NO	0 AC	0 AC	94 AC	282 AC (FIDS ONLY)	101 AC	0	10 AC	14 AC					
Alignment J (BWP Modified-East)	No	0	112	0 LI; 5 M	1 LI; 9 M	0	0 to 6	0	3	+1	0	0	YES	102 AC	+387 AC	134 AC	317 AC (FIDS ONLY)	101 AC	8 AC	11 AC	16 AC					
Alignment J1 (BWP Modified-West)	No	77	382	0 LI; 7 M	0 LI; 11 M	1	3 to 4	7	10	+1	3 (16 AC)	+0 (63 AC)	NO	45 AC	+203 AC	108 AC	202 AC (FIDS ONLY)	82 AC	2 AC	14 AC	20 AC					

Note: Quantities in Table B-2 were estimated based on assumptions described in Chapter 5 and data sources in Table B-1.

Appendix C. Agency and Public Outreach

FRA and MDOT met with representatives from various agencies, as summarized in Chapter 7. This appendix includes summaries of the meetings as Attachment A.

This appendix also includes the public comments received (as of November 1, 2017), plus a letter signed by ten State Senators and Delegates from Anne Arundel and Prince Georges Counties (dated December 19, 2017) as Attachment B.

Appendix C - Attachment A: Agency Coordination

Agency Preliminary Alternatives Screening Presentation MDOT Interagency Review Meeting

DATE: March 24, 2017 11:00am-11:20am

LOCATION: Presentation given at monthly MDOT IRM
Training Room 2, 211 Madison Street, Baltimore, MD 21202

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- A brief presentation on the preliminary alternatives screening process for the SCMAGLEV project was given at the monthly Maryland Department of Transportation Interagency Review Meeting. The status of the project was presented, and agency representatives were given the opportunity to submit comments and ask questions about the project description, potential impacts, possible alternatives, and the Environmental Impact Statement (EIS) process.
- It was noted that another meeting will be held on March 30th, but will cover the same items. The team will be coordinating with permitting agencies during the NEPA process, but permits would not be expected until after FRA releases its Record of Decision.

Agency Preliminary Alternatives Screening Meeting

DATE: March 30, 2017 10:00am-11:15am

LOCATION: Meeting hosted at National Park Service Headquarters
1100 Ohio Drive, SW, Washington, DC 20024

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- An Agency Meeting was held in person on Thursday, March 30, beginning at 10:00 am. The status of the Baltimore-Washington Superconducting Maglev (SCMAGLEV) Project was presented and agency representatives were given the opportunity to submit comments and ask questions about the project description, potential impacts, possible alternatives, and the Environmental Impact Statement (EIS) process.
- NPS commented that it is building a pedestrian bridge and recently re-did the Anacostia Park area. NPS also highlighted the fact that preliminary alignments pass through some of the wildest and most natural areas along the Anacostia River.
- Other comments and questions ranged from clarification on technical details to ownership of the facility and also notification time required before access is granted for field work on the various agency resources/properties. Question/complaint regarding the use of Union Station, as well as further process and permitting discussions occurred.

SCMAGLEV Follow-Up Meeting with MAA and FAA

DATE: April 5, 2017 2:00 – 3:00 pm
LOCATION: FDE Conference Room
991 Corporate Blvd, Linthicum, Maryland 21090

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- Representatives from the SCMAGLEV Project Team, Maryland Aviation Administration (MAA), and the Federal Aviation Administration (FAA) met on the afternoon of April 5, 2017 to discuss the a potential SCMAGLEV station at the BWI Marshall Airport as well as other concerns related to the SCMAGLEV Project and aviation.
- MAA noted they would prefer the SCMAGLEV station to be near or attached to the terminal, most likely at the site of the current hourly garage. As part of the BWI Marshall Airport growth and transportation plan, the garage could be reconstructed as a multi-modal center combining the airport and SCMAGLEV station (probably connecting to E concourse). It was noted that the station location at BWI Marshall Airport should focus on simple and direct connectivity, so patrons and passengers do not get confused.
- FAA noted some issues that would need to be resolved as part of the EIS, and that the airport needs to retain the right to build over in the future. Many can be worked in parallel and would require coordination within FAA outside of our immediate office, they include: Property issues (ownership/easements/lease), connected actions and temporary elements, actual SCMAGLEV limits (to review guidance requirements and permit airspace evaluation), and also requested any data available on the electro-magnetic propulsion system (to evaluate potential impacts to electronic based navigational aids at the airport).
- Regarding the ancillary facilities needed at BWI Marshall Airport (vent stacks) and potential elevated guideway structures, anything within 100:1 slope must be coordinated with FTA. Any penetrations of the surface at BWI Marshall Airport will require coordination. It was noted that the airport receives redundant power supplied from both the north and south feeder, but they are currently maxed out on power supply.
- The Project Team will provide FAA and MAA with NEPA documents and electronic files as they become available. The Project Team will collaborate with MAA and FAA by addressing issues and receiving FAA/MAA input on NEPA documents. The Project Team will work towards consistency with the BWI Marshall Airport Master Plan and with FAA's requirements for their environmental finding.

SCMAGLEV Follow-Up Meeting with USFWS and NPS

DATE: April 19, 2017 10:00 – 11:30 am

LOCATION: Patuxent Research Refuge Visitor Center
10901 Scarlet Tanager Loop, Laurel, MD

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

Representatives from the SCMAGLEV Project Team, the U.S. Fish and Wildlife Service (USFWS), and the National Park Service (NPS) met at the Patuxent Research Refuge Visitor Center on the morning of April 19, 2017 to discuss issues and concerns related to NPS and USFWS properties and the SCMAGLEV Project as a follow up to the March 30, 2017 Interagency Meeting held at the National Park Service headquarters.

· USFWS Concerns

- Any SCMAGLEV route that overlays the Patuxent Research Refuge (PRR) is not viable because (1) there is a refuge system wide policy not to allow any new ROW on refuge land; and (2) incompatibility with the refuge's purpose and mission (wildlife research and wildlife conservation).
- Any route that would have direct or indirect impacts (even adjacent structures) on wildlife habitat, riparian areas, etc. would be problematic. USFWS is currently evaluating the refuge for presence of the endangered northern long eared bat.
- Consideration of a land exchange is off the table; however, viewsheds are not a major issue for PRR.
- Alternatives along the east side of PRR would require congressional action due to the impacts to NSA gun range on Refuge-managed land.

· NPS Concerns

- NPS stated the southern alternatives pass through some of the wildest and most natural areas of the Anacostia River. Use of land, whether tunneled or above ground, is incompatible and would be considered a taking regardless.
- NPS asked what will happen to infrastructure after maglev technology is obsolete.
- BW Parkway has maintained full integrity since it was originally built. It is NPS' mission to preserve the parkway.
- NPS' position on the project is to stay off NPS property and stay out of the Parkway viewshed. Viewshed impacts to the BW Parkway are a concern.

· Section 4(f)

- NPS asked if the project is privately owned, how FRA's Section 4(f) determination would stand.
 - § Project Team responded there is likely no Section 4(f) avoidance alternative, so a least overall harm analysis will be required.
- NPS indicated the significance of their resources would weigh high in a least overall harm analysis.

SCMAGLEV Follow-Up Meeting with USACE and MDE**DATE:** May 3, 2017

1:00 – 3:00 pm

LOCATION: U.S. Army Corps of Engineers (USACE), Baltimore District
10 South Howard Street, 8th Floor, Room 8510, Baltimore MD 21201

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

Representatives from the Project Team, U.S. Army Corps of Engineers (USACE), and Maryland Department of the Environment (MDE) met on the afternoon of May 3, 2017 at USACE's Baltimore District office.

- It was noted that the Purpose and Need cannot be too specific as that is sacrosanct to the USACE program. Attendees all agreed that the USACE and MDE need to review the Purpose and Need prior to discussing potential alternatives specifics.
- USACE asked the team if they planned to follow the “highway process” or separate NEPA and Section 404 process. The difference between the two approaches:
 - Integrated NEPA-Section 404 Permit (Maryland “highway process” typically applied to FHWA-funded State Highway Administration projects requiring a Section 404 permit)
 - i. USACE concurrence is required at each milestone:
 - § Purpose and Need
 - § Alternatives Retained for Detailed Study
 - § Preferred Alternative and Conceptual Mitigation
 - ii. Other items to note with the “highway process” include:
 - § The Draft EIS will constitute the Section 404 application.
 - § BWRR would need to submit a Joint Permit Application (JPA) to MDE to officially initiate the MDE/USACE Section 404 application review process.
 - § The JPA is submitted before or concurrently with the DEIS. The advantage of submitting the JPA early is to trigger the state regulatory process and consolidate public hearings. Otherwise, there would need to be separate public hearings for the USACE and MDE from the FRA and MTA NEPA public hearings.
 - Separate NEPA and Section 404 processes with a Section 404 permit application to follow after the Record of Decision (ROD)
 - § In addition to the time required to process a Section 404 permit after the ROD, there is also a risk that an agency may not issue a permit for the Preferred Alternative given the multiple permits required for this Project. USACE noted that this was the result of the U.S. Route 301 project in southern Maryland several years ago.
 - All agreed to follow the integrated “highway process” to reduce risk.
- Baltimore Washington Rapid Rail (BWRR) noted that, as the private project proponent and SCMAGLEV system owner and operator, it would be the permit applicant and that BWRR desires that permits will be issued shortly after the ROD is signed.
- USACE and MDE said that from their agencies' perspectives, the DEIS should not identify a Preferred Alternative because their permits' public interest reviews are predicated on public input before identification of a Preferred Alternative. They noted the Intercounty Connector (ICC) DEIS was submitted to the USACE and MDE without completion of Chapter 7 (Pref. Alt).
 - The Project Team is concerned that requiring more than one Build Alternative in the DEIS is premature. The project has been studied previously as documented in the 2003 Baltimore-Washington Maglev DEIS, and the Amtrak Parallel Alternative was the only Build Alternative retained for detailed study in the DEIS. The appropriate regulatory agencies concurred (in

- writing) with the decision to retain the Amtrak Parallel Alternative as the Preferred Alternative.
- USACE suggested that the Project either moves from the IRM (which are more highway-based) to the Joint Evaluation (JE) meetings once a month so that the project is in front of all the necessary agencies, or plan to attend both the IRM and JE. The DC agencies would be welcome to attend the JE meetings as well. USACE is also open to separate meetings, as needed.
 - Section 404 is a federal law, so MDE typically acts as a commenting agency. However, MDE served as a concurring agency for the ICC (Inter County Connector/MD 200) project. USACE and EPA are the Section 404 concurring agencies, while U.S. Fish and Wildlife Service treat concurring vs. commenting as their choice.
 - It was noted that spanning a wetland on structure is not considered a wetland impact. In areas where access roads or other filling of wetlands is required, USACE expects these areas to be identified prior to finalizing the alternatives retained recommendation.
 - USACE asked about the difference between 2003 DEIS and this study.
 - BWRR explained that, in addition to technological differences and higher potential SCMAGLEV speeds, the 2003 DEIS was for a Demonstration Project that was not required to sustain Maglev speed and, hence, had relatively flexible geometry.
 - The current effort is driven by BWRR's plan to build a system with as a commercially viable enterprise. That system would need to sustain cruising speed to attract riders.
 - BWRR has secured a railroad license from the Maryland Public Service Commission, and has the backing of the Central Japan Railway Company (JRC), which is constructing SCMAGLEV in Japan. BWRR also explained why the alignments converge to common stations in Baltimore (because of through service geometry to NYC) and in DC (to be in or near the heart of DC where there is good access to Metro and a logical entry via New York Avenue-Route 50).
 - USACE asked the cost of tunneling. BWRR noted that tunneling is approximately two times more expensive than a guideway on an elevated structure.
 - USACE asked about the review schedule, Project Team replied that the schedule shows the alternatives retained recommendation finalized in September with DEIS in January-February. USACE noted MTA needs to get the resource agencies involved and engaged very soon so that the agencies can adhere to that schedule.
 - A schedule needs to be put together for agency review and that the schedule should also include the major USACE/MDE concurrence milestones. It was also suggested agency field review meetings occur for all alternatives retained recommended. In addition, all alternatives retained will need to have the wetlands field delineated and that the USACE and MDE will need to issue jurisdictional determinations on the field delineated wetland lines.
 - MDE and the USACE have different reviews and the USACE factors do not typically influence MDE. Even with a JPA and concurrence on a Preferred Alternative, MDE will focus mainly on the impacts to resources they regulate (wetlands, waterways, and 100-year floodplains). Having the JPA simply means the reviews become concurrent and you can consolidate the public hearings into one.
 - The next step, following Purpose and Need approval/concurrence, is the preparation of an Alternatives Report that includes documentation of a full range of alternatives; results of Screening Levels 1 and 2; conceptual design information for retained alternatives; and recommendation of reasonable and feasible alternative(s) for analysis in the DEIS. At that time, the Project Team can determine which Build Alternative(s) survive(s) and circulate findings to regulatory agencies for review.
 - The schedule should include a joint public hearing with FRA, MDE, and the USACE all making presentations. After a schedule is presented, further meetings will be necessary to discuss impacts and specific areas of concern. The field review meetings are essential to the process as well.

SCMAGLEV NEPA Meeting with Anne Arundel County

DATE: May 25, 2017 9:30 – 10:30 am

LOCATION: Heritage Complex – 2664 Riva Road

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- AA County asked how this project relates to the FRA NEC Future.
 - The Project Team noted that FRA depicts the project as a complementary service adding additional capacity to the existing constrained transportation corridor.
 - It was noted that the Japanese Government may invest or loan approximately half the cost in order to expand the technology.
 - It was noted that the SCMAGLEV could potentially be constructed sooner than the NEC Future improvements based on the SCMAGLEV's aggressive schedule.
- AA County asked if this is the first phase of a longer system to New York, and does the Baltimore to Washington portion stand alone?
 - The Project Team responded that the P&N and EIS will be for Baltimore to Washington only and that it does have independent utility even if the system is never expanded north of Baltimore.
- The current Baltimore to Washington station design will not preclude future expansion but can stand independently as well.
- AA Co. is due to update the General Development Plan (GDP) starting in 2018, and it is expected to be completed in 2019.
- The County has GIS layers for existing land use and current development plans that it can share with the Project Team.
- AA County noted that this level of detail appears appropriate for this stage of the project and believes the public will appreciate the opportunity to get involved and provide comments versus just being presented with pre-determined decision/results.
- The County cautioned that the Odenton Master Plan has teeth, and there is a TOD for the Odenton MARC station.
- The WB&A alignment could potentially impact the road and existing trail.
- It was also noted that there is a heavy bike contingent in AA Co. that could turn into a supportive group if the project offers continuous bike connectivity.
 - The project is planning to include a bike trail.
 - In addition to restoring the bike trail and expanding connectivity, there may be opportunities for new/additional signage regarding the historical electric railway.
- BWRR has been looking at the WB&A trail since about 2011, and acknowledges how much growth and expansion has occurred in the corridor since that time.
- MTA noted that this SCMAGLEV may be one of the heaviest lifts since the interstate system and is political as well as potentially impactful to residents (especially WB&A alignment).
- It was noted there are height restrictions in the County Code, so this may need to factor that into the design (possibly a standard height once clear of a transition portal), or potentially discuss a waiver if needed.
- It was noted that the Odenton and Severn areas may be sensitive communities.
- Tipton Airport (small craft / hobbyist airport) has a master plan the team should investigate. The County will send contact info.
- Fort Meade:
 - Team should contact a Fort Meade representative to schedule a meeting.

- Fort Meade Alliance Transportation Committee
- Other thoughts:
 - What kind of congestion relief on 295 expected?
 - Potential security risk for Ft. Meade
 - Safety and functionality
 - How will this help Ft. Meade?
 - How many jobs will be created?
 - Physical positives versus social negatives?
- BWI Marshall Airport – possibly advertise or brand this as an Anne Arundel Station versus strictly an airport station.
- Possible future public meeting site or contact suggestions:
 - Severn
 - Jessup
 - Maryland City
 - Laurel
 - Odenton
 - Waugh Chapel community
 - Bike AAA

SCMAGLEV NEPA Meeting with National Security Agency**DATE:** May 30, 2017 11:00 am – 12:00 pm**LOCATION:** 9800 Savage Road, Fort George G. Meade, MD, 20755

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- NSA asked if there is any service like this currently in the U.S. The Project Team responded that there are high speed projects in Texas and California ongoing but no SCMAGLEV technology.
- NSA asked about the economics and about the fare/ticket price?
- NSA is optimistic about the possibility of the project
- NSA wants to help, recognizes the transportation problem, and wants to improve transportation options in the corridor.
- NSA asked the team what is in it for NSA?
- NSA would not be as concerned about the gun range, but would be more concerned about their operational facilities and buildings.
- NSA noted that there are multiple facilities on their campus and the potential impact of magnetic fields would need to be studied by a separate group of NSA experts. They noted that the magnetic fields are a sensitive issue.
 - BWRR is working on a report regarding electromagnetism and will send to NSA when ready, as well as any data from Japan that can be provided.
- In addition to the facilities on NSA and Fort Meade, there are also NSA contracted sites north of MD 32 (west side of BWP) and potentially other locations in the study area.
- Any time infrastructure is planned/built near one of the NSA facilities it is a potential security issue. NSA has various concerns, for either a tunnel or viaduct, including:
 - magnetic field/interference,
 - noise and vibration, and
 - security of guideway (especially non-revenue hours when train not running).
 - § NSA would not want people to have line of sight or proximity access to the NSA facilities or for the guideway to present an opportunity for trespassers or terrorist to plant devices in off hours.
- Maintenance and people accessing near/on/above/below their facilities, as well as direct line of sight from the viaduct would also be a security concern
- NSA would not accept a tunnel directly under any of their facilities.

SCMAGLEV EIS Team Meeting with USDA/ARS/USNA**DATE:** June 01, 2017 9:00 am – 10:00 am**LOCATION:** United States National Arboretum (3501 NY Ave, NE; Washington, DC)

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- A few opening questions were raised:
 - Has a feasibility study been done?
 - Is this project taking funds away from Amtrak?
 - Traveling on 295 and 95 is heavy, but many people do not go downtown to downtown, so how does this benefit regular people?
 - Is Amtrak a competitor?
 - § The Project Team noted that FRA depicts the project as a complimentary service adding additional capacity to existing constrained transportation corridor.
 - Are the Amtrak alignments still on the table, given the complication of RR history & ownership?
- Field work will require early coordination with USDA. Security valid for one year.
- Concern of pedestrian crossing on Anacostia Bridge.
- Why do some of the alignments curve through USNA and not stay on Rt. 50?
- The alignments going through the USNA go over/through one of the last Magnolia bogs in Washington, DC.
- For Alternatives G or G1, can you stay underground so not impacting the USNA on surface?
- Vista near the river is a major concern as it is one of the wildest remaining sections of DC.
- Investigate Coast Guard clearance requirements and/or height restrictions for the Anacostia River.
- There is a Historical Brickyard near the entrance for USNA.
- USNA is on NRHP but not NHL.
- District Police near NY Ave and FBI facility may not be able to be relocated.
- Alignments through the USNA would compromise integrity and visitor experience of the property.
- The transition portal in the USNA would destroy significant portion of the collections. USNA is environmentally sensitive and would not support destroying their property for a transition portal – strongly suggest keeping SCMAGLEV underground in this area.
- What is the clearance for tunnel sections?
- How would the cut and cover portions be restored?
- Slope near the river likely cannot get clearance for transition portals.
- BARC has a superfund site north of J1.
- There is Secret Service facility near Powder Mill Road.
- Old abandoned airport – possible location of future solar farm project.
- RSD sites west of BWP/295 are non-starters in BARC.
- RSD site east of BWP/295 is a more isolated track of land and may be negotiable given mitigation/compensation.
 - Although there are some bogs and wetlands in the area
- Glendale property is getting ready to be surplused.
 - Prince George's County is looking for 4,000 acres for wetland mitigation and the site may be under consideration.
 - Old hospital and the Glendale property cause no concerns from USDA perspective.
- With respect to BARC, the elevated guideway for Alternatives J and J1 is not as much of an issue as compared to Alternative F.
- There is a quarantine and biosecurity facility south of Alternative F and the RSD site.
- In the City of Greenbelt, there is historic dump / brown field at J1 transition portal, it was capped and a ball field is currently on top of it.

SCMAGLEV EIS Team Meeting with NASA**DATE:** June 01, 2017 11:00 am – 12:00 pm**LOCATION:** NASA (8800 Greenbelt Rd, Greenbelt, MD 20771 - Building 26 Room 199)

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- Field work will require coordination with NASA. Assume a day or two notice for escorted approval, but assume at least a month for unescorted approval.
- District Police near NY Ave and FBI facility may not be able to be relocated.
- Alignments through the USNA would compromise integrity and visitor experience of the property.
- Alternative J potentially tunnels under NASA property - What is the depth/clearance for tunnel sections?
 - Approximately 40 to 60 feet, depending on terrain and soil conditions.
 - Also tunnel sections have some surface penetrations for ancillary features (vent shafts, emergency access, substations, etc.)
- NASA noted that north (outside) of the gate/fence would not be a security concern
- Would need to know if tunnel (or any at-grade construction) would affect the road, NASA would not be in favor of road closure. (Approx. 9500 visitors on a given day to the complex).
- There are sensitive receptors on the facility - NASA would also be interested in the memos/reports regarding the noise/vibration/electromagnetic fields/etc.
 - Also vibration during construction is a concern.
- Similar to NSA, there would be security concerns regarding any surface penetration within their fence line. If outside the fence it is not as big of an issue, but they would still like to know in advance and be kept in the loop if an alignment will in their vicinity.
- How would the real estate aspect work? NASA would retain the decision to lease their property (similarly to how they utilize enhanced use leases for cell towers, this is the precedent they would probably fall back on with respect to leasing to private entity).
- If the recommended alignment goes under their property, NASA will want to switch from participating to concurring agency and do their own ROD.
- NASA noted a TCE plume on the property that has monitoring wells and land use restrictions, but it does not appear to be in the vicinity of the proposed SCMAGLEV project.
- NASA noted that the independent utility should be stressed and remove segmentation language in the Purpose and Need and/or other documents.
- Also, NASA would like to see the memo/report on the market, is there sufficient demand?

**SCMAGLEV EIS Team Meeting with M-NCPPC and
Prince George's County Department of Public Works and Transportation**

DATE: June 13, 2017 9:30 am – 10:30 am

LOCATION: Transit Conference Room, Suite 320, 9400 Peppercorn Place, Largo

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- M-NCPPC asked what slows the train down
 - The Project Team explained that reducing the electric power slows the train. It can go from the proposed top cruising speed of 311 MPH to 0 in 2 minutes and 20 seconds and accelerate from 0 to 311 MPH in 2 minutes
 - MTA explained how the train is capable of 374 MPH but is constrained in this corridor but the spacing of the stations and the need to maintain passenger comfort.
- DPW&T asked what is different about this train vs. the prior Maglev EIS?
 - The Project Team explained that the prior study was proposing German Transrapid that is slower with vehicle wrapping around guideway, and this EIS proposes the faster Japanese superconducting maglev that operates in a U- shaped guideway.
- M-NCPPC asked how noisy the train is.
 - The Project Team explained that with no friction, or moving parts, there is a swift aerodynamic whoosh. And with the nature of the design there is no vibration.
- M-NCPPC asked about what would be the first year of passenger service?
 - BWRR explained that the target is 2025 for revenue operation which includes 18 months of testing and certification by FRA.
- DPW&T asked if this is proposing a DC Union Station to Baltimore Penn Station route?
 - The Project Team explained that those locations have been evaluated and there are numerous conflicts with geometry, depth of tunnel, construction sequencing issues other considerations. Mt Vernon in DC is under consideration as well as NoMa.
- As follow up, DPW&T asked if they propose to use the Amtrak route like the prior study?
 - The Project Team explained that closely paralleling Amtrak is one of the alignments being considered.
- M-NCPPC asked for clarification on the maglev station connectivity to DC Metro stations, inquiring if they would be an underground tunnel connecting the two, and how the 'last mile' of any transit trip is so important.
 - The Project Team explained that direct connectivity to Amtrak is part of the concept.
- M-NCPPC asked what are the issues with using/following Amtrak or CSX routes?
 - The Project Team explained there is a minimum 150-foot offset for safety and likely crashwall considerations. There is also the issue of simply not having any right to use those corridors/rights of ways.
- DPW&T asked if the team knew how many property takes would be required?
 - The Project Team responded that those assessments have not been performed but it could be in the hundreds depending on selected routes. The Project Team explained how the guideway will be on piers spaced 120 feet apart.
- M-NCPPC asked why do the routes go above ground in populated areas?
 - The Project Team explained that tunnel is absolute requirement in the extremely dense downtown DC and Baltimore areas and required near the airport station. Economic viability of the project likely dictates that the middle portions run on viaduct.
- M-NCPPC commented that the National Park Service manages the BW Parkway as a National Park with a road
 - The Project Team responded the they have been coordinating with the NPS

- MTA asked for group's initial reaction after having seen the preliminary routings
 - M-NCPPC offered that the alignments that paralleled the BW Parkway appeared to have the least overall impact.
- DPW&T asked about cost to construct the project
 - BWRR responded that the project is estimated at \$10 to \$12 B.
- M-NCPPC asked about where the alignments go with respect to Bowie
 - The Project Team explained the routings in the Bowie area.
- DPW&T asked which alignment would affect the most people.
 - The Project Team responded that those assessments have yet to be completed.
- M-NCPPC commented that if provided maps they can draw alignments that avoid resources, etc.
 - The Project Team responded that we will gladly project hard copy 600 scale mapping and we would welcome ideas, recognizing that small shifts could result in big impacts miles away
- M-NCPPC commented that the Glen Dale hospital site is a historic resource
 - The Project Team explained that the hospital would be considered as such in the EIS, should that alternative remain.
- DPW&T what the impact would be if the maglev is in a tunnel under a community?
 - The Project Team explained the requirement for vent shafts and "plants" Vic described his experience with WMATA boring a tunnel near his former office building near Silver Spring and being able to hear and feel the tunnel boring machine.
- M-NCPPC asked if there were any health issues with the superconducting maglev system.
 - BWRR responded that the short answer is no, and that the system has been in operation for decades in Japan and the World Health Organization has evaluated it.
- M-NCPPC asked if the "need" will be in the EIS
 - The team responded that the "need" will be included in the EIS
- DPW&T asked where the station would be located in NYC?
 - BWRR responded that the NYC station location has not been identified.
- DPW&T asks if we had taken into account new and future development
 - The Project Team responded that we are just starting that process and would appreciate the County's and Park and Planning's assistance in this regard. The Project Team pointed out that the rolling stock depot (maintenance yard) would be near the Landover Metro stop.
- M-NCPPC asked, what is your 'immediate' timeframe?
 - The Project Team responded that we would like initial comments by the end of June.
- M-NCPPC mentioned that a Board Chairperson is interested in the public meetings
 - The Project Team stated that will provide the locations and dates of any upcoming public meeting to be held in the county.

SCMAGLEV Meeting with US Secret Service

DATE: June 13, 2017 10:00 – 11:00 am

LOCATION: JJRTC Eytchison Building (9200 Powder Mill Road, Laurel, MD 20708)

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- The US Secret Service (USSS) noted that there are several projects on the property that are in various stages of approval or waiting for funding. They are working through the NEPA process and completing several CatEx and a Supplemental EA.
- There is a Master Plan identifying 20 to 25 new facilities on the campus. The Plan is anticipated to be finalized the end of the fiscal year.
- It was noted that the 500-acre facility has a hard fence line and is the only facility they own, but there are other leased facilities as well.
- The USSS property boundary is adjacent to Baltimore-Washington Parkway and the current Alternative J (BWP-East) would impact the indoor firing range facility.
- The East side of BWP is a non-starter as USSS is not going to give up any property inside the hard fence.
- A tunnel under the facility would need to be discussed with the Special Agent in Charge and Headquarters.
- The USSS recommend staying at least 100 ft. outside their fence so the east side of the BWP is not possible, but the west side of BWP may be negotiable. However, line of sight may be an issue.
- If USSS property is needed for the project, there is no project. USSS need all 500 acres for the planned facilities (20 to 25 new facilities). He also noted that the USSS leases all sites except for this campus.
- Similar to the NSA, line of sight is a concern for above ground portions
- Security and access to the guideway also concern (either above or below grade).
- Information (traffic studies) the USSS collected during EA development for their projects may be useful to SCMAGLEV project once it is published.
- Noted plume on Beltsville property.

SCMAGLEV NEPA Meeting with Fort Meade

DATE: June 14, 2017 2:00 – 3:00 pm

LOCATION: **Bldg. 4216, Roberts Ave. Fort George G. Meade, MD 20755**

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- Potential impact of Alternative E1 is approximately 12.09 acres while Alternative J is approximately 18.17 acres. The WB&A alignments do not impact Fort Meade.
 - Potential impacts are based on the primary zone (50' each side of centerline) to be consistent with the screening report methodology.
 - The alignments will be refined, and the impact anticipated to possibly reduce as the LOD is defined in more detail.
 - The J1 alignment is listed as zero as we assume no significant impacts when in tunnel/underground
- Fort Meade was wondering if Wi-Fi would work on the SCMAGLEV train as the speeds may be too great between towers, and also concerned about the potential power drain or reduction to existing facilities along the route.
- Fort Meade asked about Central Maryland Transportation Development Program involvement.
- What is the Pro/Con of underground versus above ground?
 - Underground is approximately 2 times more expensive to construct, passenger views and visual impacts are a few.
- Similar to the NSA, line of sight is a concern for above ground portions
- Security and access to the guideway also concern (either above or below grade).
- Construction access and transport of the massive amounts of dirt is a concern.
- No longer doing golf course on CSL, but it is a capped landfill and the area has soil stockpile. Now slated as a future solar farm.
- The gun ranges are heavily utilized, and other agencies have been asking for improvements.
- There is a benzene plume and methane extraction system with passive vents in the CSL as well. What is the noise impact/decibel level of the SCMAGLEV system?
- Fort Meade would be looking for fair market value even for permanent easement if an alignment were on the property
- At this point Fort Meade is a participating agency, but if an alternative does include Fort Meade property they would want to be switched to concurring agency.
- How far along is the real estate portion?
- What is the priority of Federal Lands versus Private lands?
- The Patuxent Research Refuge used to be all Fort property and used for artillery range, so there are potential unexploded ordnances (UXO). There is a standing 'UXO Construction Support on site" under the DOI agreement requires a UXO escort or support services for the PRR.
- There are high tension lines that the team needs to be aware of during design. These high-tension lines probably provide power to half of the State.
- There is a large water main that would need to be avoided as well.
- FTX training land on the east side of the CSL that would be hard to replace, and it is one of only two remaining on Fort Meade.
- Thus, the east side alignments through the CSL have many obstacles that would need to be addressed and probably increase construction cost as well.
- Since there are people also living on the 5000-acre property, as well as schools and other facilities – it may be beneficial to have a community meeting for the Fort.

- Fort Meade is currently a Participating Agency but if the alternative impacting Fort Meade moves into the DEIS, Fort Meade will become a Cooperating Agency.

Joint Evaluation Meeting - *Baltimore-Washington SCMAGLEV Project*

DATE: June 28, 2017 12:30 – 2:00 pm

LOCATION: U.S. Fish & Wildlife Service (177 Admiral Cochrane Drive, Annapolis, MD)

This is a highlight summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- After introductions, BWRR began walking through a SCMAGLEV PowerPoint presentation regarding the background, players, and the technology. BWRR is a commercial enterprise and would need to make a profit from revenue service, so travel time is important to attract riders. Anticipated 15-minute trip time between Baltimore and Washington, DC with proposed SCMAGLEV as compared to approximately:
 - § 30 minutes currently on Acela
 - § 45 minutes currently on Northeast Regional
 - § 60 minutes currently on MARC Camden
 - § 90 minutes (+/- depending on congestion) currently by car
- Project schedule includes EIS completion/ROD in February 2019, followed immediately by permits, then design/construction commencing October 2019. BWRR will be the permit applicant.
- Utilizing the FAST Act process to achieve the aggressive schedule is also a relatively new process and it varies slightly from the traditional steps in an integrated NEPA/404 process where a preferred alternative is not identified in the DEIS, so the public can provide comments during the joint Public Hearing and then a preferred alternative is identified. In the FAST Act a preferred alternative is identified in the DEIS.
- The viaduct pier spans are anticipated at 120' typical spacing. However, this distance may be extended to 150' – 160' in order to minimize or avoid a particular resource, or the design could possibly be modified to an arch structure to bridge longer distances if required.
- BWRR provided further explanation on the ancillary facilities for the JE group.
 - Vent plants/shafts will be needed for the tunnel sections. Number will be dictated by the length of each tunnel. Possibly hidden in a building or combine with portal locations where appropriate.
 - Transition portals will be needed between the tunnel sections and the elevated viaduct sections. These portal areas will also serve as Tunnel Boring Machine (TBM) launch locations during construction.
 - Rolling Stock Depot (RSD) site will also be required within the study area to store/maintain/clean/repair the SCMAGLEV trains. This is the largest ancillary facility. BWRR is working on reducing the footprint (initially estimated at approximately 300 acres).
 - Power substations (approximately 4) will be needed along the project. May be able to hide these somewhat or possibly incorporate at the RSD and at station locations where appropriate.
- USACE requested explanation of the intended ROW acquisition process for the 40 miles considering BWRR is a private company. BWRR responded that it would be a mix of private and public land, depending on the alignment selected, but BWRR views the project as a public use. Therefore, they hope to lease public land for SCMAGLEV use and then purchase the required

private land as a first choice. However, BWRR noted it can exercise eminent domain as a last resort to acquire land if necessary.

- MDE noted that normally the Purpose and Need statement is broad enough not to constrain the process, and originally thought the SCMAGLEV Purpose and Need might have been too specific. However, after the last meeting with MDE and the Corps earlier in the month, he understands why it needs to be the way it is. He noted MDE would still defer to the USACE on the matter.
- Project Team responded to a question regarding comments received on the P&N (given the initial comment/concurrence deadline of June 14th). The draft Purpose and Need Document was distributed to 36 Cooperating and Participating Agencies including four Concurring Agencies. About half of the agencies have responded with no or minor comments. To date 14 agencies have responded, two of which are concurring agencies (USFWS and FAA). USFWS noted that he responded regarding comments, but that does not mean USFWS concurrence. That may need to be another person within the agency. USACE noted they would be responding in the next 2 to 4 weeks regarding the Purpose and Need.
- As this project has been presented to IRM and now JE meetings, BWRR asked which group should come first going forward. USACE commented that combining the NEPA and 404 using one process is the way the project is headed, but the right people need to be in the meetings regardless of which one. The Project Team could continue to do both meetings, or possibly try to combine the IRM and JE meeting into one large project briefing. Otherwise if the right people cannot make the larger meetings the Project Team could even have separate specific meeting for an individual agency to accommodate the need. Since agencies have multiple departments and different people in charge of various aspects you may have to deal with many people at the same agency. For instance the person responsible for the resource that will make the decision on the permit is not necessarily the same person responsible for the real estate/lease agreements. NPS representative not here today and typically do not attend the JE meetings.
- USACE asked what happens when a resource agency says no, what would BWRR do? BWRR responded that the agency has the right to say no and that makes the process much harder as it could eventually lead to an act of Congress to get the position changed.
- It was noted that the wording of the preserve/refuge is very specific in some respects. For instance, there is a ball field that must be maintained as nothing other than a ball field in the regulations. Similar situation to the NSA ranges, it is written into the law. Even though DOI/USFWS maintains some areas you would not expect, it is due to the way the policy/law was approved.
- The Base Relocation and Closure (BRAC) laws were very specific and the policy is very strict, even for easements for the use of the land. For instance, NEC expansion, Tipton Airport improvements and others have previously been rejected or declined due to the various policies/laws.
- BWRR noted that it may take a change in the policy or law eventually, but the first step is to get a response regarding concurrence or not from the agencies.
 - √ *An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies.* (<https://energy.gov/sites/prod/files/G-CEQ-40Questions.pdf>)
- It was noted that going under Patuxent Research Refuge may be a possibility that would not interfere with their resources/mission.
- Public Land, Forest Conservation, Scenic and Wild Rivers, RTE species are all issues that will need to be included in the review in more detail and agencies will need time to make sure the right person is reviewing or pulling the correct data before responding. The Project Team would follow up offline after this meeting to discuss these issues further and possibly advance some of them, especially the RTE species staff.

**SCMAGLEV EIS Team Meeting with USACE and MDE
Draft Preliminary Alternatives Screening Report**

DATE: August 29, 2017 1:30 pm – 2:30 pm

LOCATION: USACE Baltimore District HQ 10 S. Howard St, Baltimore 21201

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- After introductions, MTA described the purpose of the meeting and discussed the project status.
 - § An Environmental Impact Statement (EIS) is underway and is currently in the preliminary alternatives development stage. The NEPA team has completed a draft *Preliminary Alternatives Screening Report* (PASR) that documents the first steps of a screening evaluation on alignments to be carried forward into a more detailed Alternatives Report and then the alternatives retained recommendation concurrence point. It has been reviewed by the Federal Railroad Administration (FRA) and asked that it be shared with the USACE.
 - § USACE acknowledged that they received the draft PASR but have yet to examine. The NEPA team explained that it is fine and one of the goals of this meeting was to provide an overview of the report content to facilitate their review. The NEPA team will also be providing the draft report to MDE as discussed in a related meeting earlier that day held by the project sponsor, BWRR.
 - § The NEPA team also explained that another goal for the meeting was to establish whether the USACE, as a concurring Cooperating Agency, was comfortable with the progress to date and the results of the draft PASR. If so, then FRA could begin to schedule another round of public meetings to present the draft PASR alternatives.
- Project Team walked through handouts that contained highlights of the draft PASR.
 - § Screening level 1 evaluated the initial alignment alternatives (including four alignments from the prior maglev DEIS of 2003) based on meeting acceptable geometric/curve radius requirements to obtain highest practical/optimum speed for SCMAGLEV technology.
 - § Screening level 2 results included an evaluation of constructability and an analysis of environmental and community features. The NEPA team explained they used customized primary and secondary zones around the centerlines to characterize possible impacts to resources. The impact analysis was quantitative and not qualitative, meaning the total number or properties and potentially impacted areas were “calculated” within the buffer zones and assigned a high, medium or low value based on threshold values. The analysis also considered whether the proposed alignment was in a deep bored tunnel section (little to no surface impact assumed), a transition portal (possible greatest impact) or on an elevated structure/viaduct. The PASR also includes an initial analysis of the station zones.
 - § The overview concluded with the NEPA team identifying the recommended alternatives for further study in the Alternatives Report.
- Areas of Concern/Open Discussion
 - The group discussed the process at a high level and how the SHA standard “highway process” would be applied. Everyone concurred that this PASR was not an official concurrence point (as is the Purpose and Need and upcoming alternatives retained recommendation) but that it was good to review the logic and comprehensive process that got the NEPA team to this point.

- § The group also discussed the value in continuing to study these alignments at the same level of detail throughout the next Alternatives Report phase and DEIS phase with consistent methodologies. MDE commented that it was good to see that the draft PASR has alternatives that include three diverse corridors (i.e. along the WB&A, Amtrak and the BW Parkway).
 - § The group discussed the possibility of deferring detailed field wetland delineations and archeology (on private property in particular) until preferred alternative is established. As long as a consistent methodology is used for the build alternatives in play up until that point.
 - § The group discussed setting up a field walk once many of deciduous leaves are off the trees to facilitate viewing the possible alignments, particularly along the WB&A trail. (Mid-November target).
- USACE will take a few days to review the draft PASR and have a discussion with the FRA on Friday September 1, regarding the status of the report and possible upcoming public meetings.

Joint Evaluation Meeting - Baltimore-Washington SCMAGLEV Project**DATE:** August 30, 2017 3:00 – 4:00 pm**LOCATION:** U.S. Fish & Wildlife Service (177 Admiral Cochrane Drive, Annapolis, MD)

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- **Presentation by SCMAGLEV NEPA Team**
 - Project Team walked through handouts that contained highlights of the draft PASR. Specifically, Screening Level 1 was based on meeting geometric curve radius requirements.
 - § Screening level 1 evaluated the initial alignment alternatives (including four alignments from the prior maglev DEIS of 2003) based on meeting acceptable geometric/curve radius requirements to obtain highest practical/optimum speed for SCMAGLEV technology.
 - § Screening level 2 results included an evaluation of constructability and an analysis of environmental and community features. The NEPA team explained they used customized primary and secondary zones around the centerlines to characterize possible impacts to resources. The impact analysis was quantitative and not qualitative, meaning the total number or properties and potentially impacted areas were “calculated” within the buffer zones and assigned a high, medium or low value based on threshold values. The analysis also considered whether the proposed alignment was in a deep bored tunnel section (little to no surface impact assumed), a transition portal (possible greatest impact) or on an elevated structure/viaduct. The PASR also includes an initial analysis of the station zones.
 - § The overview concluded with the NEPA team identifying the preliminary alternatives recommended for further study in the Alternatives Report.
 - Next Steps were presented.
- **Comments from attendees**
 - How were station locations evaluated for Constructability/Feasibility?
 - Did you include SHA’s existing wetland mitigation sites in your analysis?
 - Did you consider potential impacts to residential properties in your analysis?
 - What is the guideway width?
- **Action Items**
 - Project Team to contact MDE to discuss obtaining GIS inventory of wetland mitigation sites

SCMAGLEV Interagency Meeting

DATE: October 3, 2017

10:30 AM – 12:00 pm

LOCATION: Maryland State Highway Administration (SHA), Greenbelt Office
(9300 Kenilworth Avenue, Greenbelt, MD) and via Webinar

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

- o **Introductions**
- o **Project Team Presentation**

§ SCMAGLEV Update

FRA made opening remarks and thanked everyone for their participation. FRA informed the agencies that the agency coordination plan is being prepared which will lay out the involvement of all agencies and the timeline for the review of documents for the rest of the life cycle of the project over the next two years. FRA requested agencies to review the document upon receipt and provide comments within two weeks. FRA also mentioned that the Purpose and Need is being finalized based on the comments we received from the agencies. The Purpose and Need will be posted on the website by the end of this week.

FRA walked the group through the first dozen slides of the presentation. FRA indicated that an alignment has not been chosen yet. FRA noted the citizens' concern about the suburban benefits (perception of greater downtown), property takings, and confusion about the NEPA process. FRA also noted that FRA is not going to reopen the scoping period but indicated that all comments received via different channels throughout the project will be included on the record. FRA mentioned that FRA has been working with BWRR, the private project sponsor, regarding the 311 mph language in the technical requirements. FRA hasn't fully signed off on the issue yet, so they have not approved the technical memo, but they are comfortable enough to put it in the Preliminary Alternative Screening Report (PASR) and to see if anyone will provide comments. For instance, FRA is questioning if 300 mph or 290 mph through some curves to avoid/minimize impacts is reasonable, instead of insisting 311 mph for every curve. Later in the discussion FRA noted that it is pushing back on 311 mph and 15 minutes travel time language in the Purpose and Need Document as well to avoid any litigation. However, since it is a propriety technology, FRA is working with the project sponsor to resolve the issue. A technical memo will be coming. FRA also added that the team is aware of agencies concerns and encouraged them to reach out to FRA with any questions or concerns.

FRA and MTA noted that the Draft PASR report should be uploaded to the website on October 10th, which also corresponds to the briefing for the elected officials (a similar webinar like this for the elected officials) prior to the October public open house meetings that begin on October 14, 2017. Public meeting locations and dates are posted on the project website. It is FRA's intention to keep the PASR in draft form into November to allow agencies and the public to comment on the alignments.

FRA indicated that it is independently working on a ridership study that encompasses all the way up to New York. FRA wants BWRR to show independent utility and ridership projections for the segment between Baltimore, MD to Washington, DC. FRA then turned the presentation over to AECOM to discuss the PASR results.

§ Preliminary Alternatives Screening Report (PASR) Results

AECOM walked through the remaining slides discussing the PASR results. The team studied many alignments along established transportation corridors (I-95, BW Parkway, Amtrak, and WB&A) that

included a review of previously studied routes and new ones recommended by the team. There will be one station each in Downtown Washington, DC, BWI Marshall Airport, and Downtown Baltimore, MD. The actual station location will depend on the alignment selected – the PASR retained ten station zones at this level of the project; two in Washington, DC, one at BWI Marshall Airport, and three in Downtown Baltimore, MD.

AECOM highlighted that Screening Level 1 was a fatal flaw analysis utilizing the curve radius requirements (to operate the SCMAGLEV at the highest practical speed) as a key criteria. The station zone evaluation was a qualitative analysis utilizing four criteria categories (geometric feasibility, construction feasibility, intermodal connectivity, and parking availability). Finally, the Screening Level 2 evaluation was a combination of a qualitative and quantitative analyses based on constructability and a high level environmental review (based on desktop GIS analysis utilizing generous buffer areas that were larger than the anticipated LOD for this initial order of magnitude potential impact assessment). AECOM emphasized that even though some of the criteria was qualitative, the criteria was based on quantitative information. Crossing the NEC track, as well as parks and federal lands were critical considerations. Station zones are not evaluated at this level of screening.

The primary zone along the elevated guideway extends 50 feet out from the centerline on each side (100 feet total width) while the primary zone around the transition/portal areas extended 75 feet out from the centerline on each side (150 feet total width). The secondary zone along the elevated guideway extends out 200 feet beyond the primary zone on each side, while the secondary zone around the transition/portal areas extends out 175 feet beyond the primary zone for Parks and Federal Lands. The secondary zone for the Residential Properties and Cultural Resources (historic landmarks/sites/districts) was based on an expanded LOD. The secondary zone along aerial structure extends out 450' beyond the primary zone while the secondary zone around the transition portals extends out 425' beyond the primary zone.

Once the PASR is finalized the next step is to take the surviving alignments into the more detailed Alternatives Report, which will produce the Alternative(s) Retained for Detailed Study (ARDS) that would be the next concurrence point before feeding the results into the draft EIS.

FRA noted that the amount of tunnel in Japan is very high. There are differing amounts of tunnel on this project (as indicated on screening table for Level 2) depending on the alignment, which could still change moving forward. However, FRA anticipates that the amount of tunnel would only change in favor of more tunneling in order to avoid/minimize surface disturbances.

o **Agency Questions/Comments**

- EPA – Is FRA/Project team responding to public letters/comments?
 - o The team is planning to send an email blast response directing people to the FAQs page on the project website. The FAQs were recently uploaded to the project website (as of yesterday) and answers many commonly asked questions by the public. FRA added that the FRA admin contractor will be pulled in to help with managing public letters/comments. He added that the team is aware of the agencies' concerns and encouraged them to reach out to him with any additional questions or concerns.
- NASA/ Goddard – If a participating agency has a parcel that is directly affected by an SCMAGLEV alignment, are they upgraded to a cooperating agency?
 - o Agencies will be moved from participating to cooperating as needed. The coordination plan has more detail regarding the subject.
- NASA/ Goddard – What approval is needed to go through (under or over) Federal properties?
 - o This is not cut and dry, as each agency has different protocols, so it will depend on the specific agency. Agencies need to be involved in the process and to provide efficient documentation. We defer to the agencies if we need permission based on their regulations.
- USACE - Baltimore District – Is a formal NEPA schedule being published? The public should be

- made aware of the NEPA schedule.
 - o A timeline/schedule is on the project website, but the Coordination Plan will have more details on the NEPA schedule.
- NPS pointed out that impacts to Historic Landmarks and Eligible National Register Sites and District is rated low for alignments along the Baltimore-Washington Parkway (BWP), even though BWP is a significant resource and a registered historic site. She suggested BWP should be considered under multiple categories (probably 4, 5, and 6) because it is a Cultural resource, as well as Park and Federal Land. So, a table with a low value may be misleading?
 - o Team responded that this analysis was based on a number of such properties that are present along the proposed alignments. It did not necessarily look into the significance of each property. Though significant, it may be that the alternative along the parkway only impacted the BWP, while other alternatives impacted several smaller properties. FRA said that the project team will revisit the evaluation for this particular alignment and assessment.
- BWRR – Was the evaluation criteria weighted?
 - o Criteria for this high-level screening/evaluation was not weighted or ranked since the screening utilized broad buffer zones in the desktop GIS study.
- STB – How did the project team determine which alignments to retain if the criteria was not weighted/ ranked for Screening Level 2?
 - o Engineering judgement was utilized as a team to qualitatively analyze the high-level quantitative environmental data. For example, one of the major factors was constructability. If an alignment crossed Amtrak's NEC, it was considered a no go. As the alternatives are further developed and the LOD for the ancillary facilities becomes available, we will refine the weighted criteria. A more detailed methodology of analysis is provided in the PASR.
- MD DNR commented that documentation needs to be clear for the agencies' reference in the future, in the event they need to be reminded of the process and to justify their decisions.
 - o The Alternatives Report and the EIS will provide further detailed information, as well as referencing the PASR. The ARDS will be where agencies can provide concurrence. The PASR will include all past decisions. The PASR is an extra step many projects do not publish, but the team wants to share the information with the agencies and the public.
- NASA/ Goddard – What does concurrence mean?
 - o In addition to the NEPA process, there is a Section 404 permitting process we follow in Maryland. Some Section 404 steps are parallel to and can be shared with the NEPA process steps, but concurrence is needed at certain points. The team came to the agency at Concurrence Point No. 1, which was the Purpose and Need. ARDS will be the next concurrence point in the joint process. Four agencies have been identified as concurring agencies. If you are an agency who's environmentally sensitive property is being impacted, that property is open to our continued to study.
- FWS asked what happens if an agency doesn't provide concurrence?
 - o FRA acknowledges FWS concerns and requested that the agencies review the coordination plan when it comes out, but if an agency has an issue and feels it cannot concur, to please reach out to FRA and the issue will be revisited with the involved parties on a case by case basis in order to come to a resolution.
- FWS mentioned that just because no comments were provided on the Purpose and Need, it does not mean that the agency does not have any concerns with the project.
- FWS questioned why Patuxent Research Reserve (PRR) was not a hard no?
 - o The Council on Environmental Quality (CEQ) published *Forty Most Asked Questions Concerning CEQ's National Environmental Policy Act Regulations*, which states that "An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Section 1506.2(d). Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because

- the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies. Section 1500.1(a).”
- The definition of reasonable alternative states - “Reasonable alternatives include those that are practical or feasible from the technical and economic standpoint and using common sense, rather than simply desirable from the standpoint of the applicant.”
 - Source = <https://energy.gov/sites/prod/files/G-CEQ-40Questions.pdf>
 - It was noted that cooperation is not concurrence.
 - NCPC – Can we see clearer alignment renderings near the station zones to more clearly see the impacts to federal lands, especially in Washington, DC? It was noted that better mapping than the overview maps provided in the PASR will be required to make informed decisions.
 - The team can provide more detailed mapping moving forward.
 - USDA – BARC noted that the Rolling Stock Depot (RSD) sites are in the middle of BARC property. Two points: (1) Where would mitigation opportunities be available for land replacement, stream relocation, and roadway/infrastructure relocation, if it is negotiated that the RSD is to be located on BARC property, and (2) can more detailed electronic versions of the alignments (for instance KMZ files) be provided?
 - (1) The project team met with BARC representatives in the summer and will plan follow up meetings as the project continues. It was noted that an RSD is a maintenance and storage facility for the SCMAGLEV trains – which is one of the ancillary facilities that the project team acknowledges but did not quantify in the PASR. The initial size of the RSD was approximately 300 acres, but the engineers are working to reduce that footprint. As mentioned earlier, the PASR focused primarily on the alignments/routes. The information was included to make everyone aware that such a facility will be needed for this project but the location and the size are not finalized. We are looking into several options and the detailed information on additional ancillary facilities are intended to be included in the Alternatives Report as additional engineering and refinements to the concepts are evaluated as part of the next stage of the process. The project team will coordinate with USDA when further details are available.
 - (2) The project team is preparing a set of 1” = 600’ scale maps that will be available as part of the October public open house meetings. This set of 9 sheets shows the four recommended alternatives that survived Screening Level 2, and PDFs will be posted on the project website under meeting materials soon. KMZ files can be shared with all agencies in the future as well.
 - BCDOP: Further information on station footprints and transition portals will need to be provided to Baltimore City in advance of further discussions.
 - The project team will meet with agencies and project stakeholders throughout the process as information becomes available. The project team is in the process now and will coordinate with Baltimore City (as well as Washington, DC and BWI) regarding stations.
 - USDA – It was noted that the NPS at Anacostia Park was listed in the table comments but Alternative G and G1 go through the USDA National Arboretum as well. USDA doesn’t want the National Arboretum forgotten. The legends aren’t correct on the map for the National Arboretum.
 - We have met with USDA during the agency meetings this summer and have not forgotten about the concerns. The PASR is a snapshot in time and potential tweaks to the alignments to further minimize or avoid resources will be ongoing throughout the EIS process. We are looking at the alignments with the engineering team and trying to minimize impacts by potentially tunneling under or skirting the property. As with the comment regarding BARC earlier, the project team will coordinate further with agencies, including USDA, as additional engineering development occurs and as refinements to the concepts are evaluated as part of the next stage of the process.
 - AECOM will update the comment block in the table to also include USDA (National Arboretum).
 - When will the PASR be released?
 - The PASR will be released in Jan 2017, via the project website.

- EPA asked who is invited to the elected officials briefing?
 - Federal, state, and local officials are invited. The main focus will on local officials.
- BMC asked if it was possible to send the details of the briefing intended for the elected officials to him so that he can inform the officials of BMC member jurisdictions.
 - Team will share the information when finalized.

- **Action Items**
 - § Share draft PASR with agencies and the public.
 - § Share final Coordination Plan.
 - § Send KMZ file when ready.

- **Next Steps**
 - § **Next Interagency Meeting** – December 2017

- **Adjourn**

SCMAGLEV EIS Team Meeting with National Park Service (NPS)**DATE:** November 20, 2017 10:00 am – 11:30 pm**LOCATION:** National Park Service, National Capital Region, 1100 Ohio Drive SW, Washington, DC

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

FRA provided the purpose of the meeting which is to update the National Park Service (NPS) on the project status since the October 3rd Interagency Meeting. A summary of discussion for agenda items is provided below.

- **Coordination Plan**
 - NPS indicated they did not receive a Coordination Plan and requested it be sent to NPS for review.
- **Preliminary Alternatives Screening Report (PASR) Results**
 - § AECOM described changes in the preliminary alternatives screening results since the October 3rd Interagency Meeting. Alternative G1 (WB&A Modified) was dropped from further study prior to October Public Meetings. The study team is also eliminating Alternative E1 (Amtrak Modified) from further study, following analysis of comments received at the five October public meetings.
 - § NPS asked if the alignments are still 150 feet off the Parkway. Project team explained that is generally the case now but that the team will be looking at alignment modification options to reduce impacts so that may change in the future.
 - § NPS asked if the Team's rationale for dropping Amtrak was strictly based on public outcry and controversy. MDOT explained that was not the case and there is NEPA rationale regarding the potential alignment not being feasible. NEPA rationale includes:
 - Impacts to historic areas of "old town" Bowie, Odenton, a historic black college (Bowie State University) and surrounding areas;
 - Impacts to natural and environmentally sensitive areas including most sensitive areas of Anacostia Park and the National Arboretum;
 - Impacts to community facilities

These concerns were in addition to the high number of impacts to residential properties contributing to the Public's top concern.

- § NPS asked if this can be 100% tunnel, since one alternative is already up to 75%. MDOT explained why 100% tunnel may not be financially feasible, but the Team will take the question back to Baltimore Washington Rapid Rail (BWRR).
- § NPS stated that there needs to be a reasonable range of alternatives for Section 4(f), so can't claim least harm by choice. The NPS stated that they think Amtrak alignment should be retained until a 4(f) analysis has been completed, but project team explained that a full-blown 4(f) analysis is not required at this stage in the process. The analysis will occur when we move forward into the DEIS with feasible and reasonable alignments and the team will give a full examination of avoidance and minimization of the various Section 4 (f) qualifying resources. The 4(f) analysis is also to minimize and mitigate for specific locations on the alignment (4(f) resources) NOT the entire Alternative. A complete avoidance alternative appears not possible in the study area.
- § FRA and the NEPA Team noted that there is no full avoidance alternative in the project study area.

- **MD 295 Discussion**
 - § NPS is not actively working on the transfer of ownership because there is no existing authority to turn the road and ROW over to the State of Maryland (there was authority years ago). NPS is not sure what the State's plans are. If Congress handed over the Parkway to the State, NPS said it isn't as bad as handing over the Grand Canyon. However, the NPS looks at every parcel the same (parkway is just as much a NPS resource as the Grand Canyon).
 - § NPS stated that, if by some stretch MD 285 becomes a state road, we need to have a land swamp discussion. NPS noted that if there is a land swamp, NPS would make that decision.

- **Section 4(f) Implications**
 - § NPS stated this project is privately owned and operated, so how does NPS apply Section 4(f)? If the private owner is taking land, does 4(f) apply? He mentioned that they have limited leasing authority, can't sell the land, and really only sees a land swap occurring. However, a land swap is complex and difficult, therefore NPS recommends avoiding NPS property if possible. FRA will look into these matters with FRA legal.
 - § When asked if an aerial structure spanning NPS property with piers touching down outside NPS property was an impact, NPS responded that air rights equal a taking and therefore trigger Section 4(f).
 - § NPS said "a bridge or two over the parkway property is not a big deal," in comparison to 30 or so. He also stated that if the alignment is 150 feet off the parkway and not visible, the alignment is more palatable.
 - § NPS asked why we are moving forward with the East Alternative if Patuxent said "no!" NPS also asked if this project is profitable. MDOT explained a ridership study update is underway and will be an important component of the profitability analysis. NPS indicated it preferred the West Alternative.
 - § NPS said that his preference for an alternative is (1) Not near the Parkway (2) If near the Parkway, not visible from the Parkway. If NPS land is being used, Peter explained that NPS can say "no;" however, Section 4(f) is determined by FRA. Therefore, NPS can say no to the Section 4(f) use but FRA can overrule (given a solid 4(f) analysis)).
 - § NPS explained that the SCMAGLEV proposal would likely be seen as a positive by current administration leadership because of the private investment and would have likely been attractive to the prior administration's leadership as it about taking cars off of the road.
 - § NPS stressed the need to include financial analysis on tunnel only option justifying the need to eliminate it from further study and impacts to environmentally sensitive areas for all alternatives under consideration in support of least harm alternatives. NPS also stated that tunneling under NPS property would not be considered an avoidance measure, only minimization.

- **Next Steps**
 - Send Coordination Plan/Signature Form to NPS
 - § The Coordination Plan and Signature Form were emailed to NPS on 11/20/17 following the meeting. NPS indicated they received the plan and will review and provide a response by Wednesday, November 29, 2017.
 - Schedule Workshop with NPS to get input on refinements (avoidance and minimization) on proposed alternatives
 - Follow up discussions on transfer of land to private entity.

**SCMAGLEV Inter-Agency Meeting (Webinar):
Update of Preliminary Alternatives Screening Results**

DATE: Thursday, December 7, 2017 10:00 AM – 11:00 AM

LOCATION: Webinar (17th Floor- 7 St. Paul Street, Baltimore, MD 21202)

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

The SCMAGLEV Project Team held an inter-agency meeting webinar to discuss the updates to the preliminary alternatives screening results since the October 2017 Interagency Meeting. After introductions, the Project Team briefly presented the screening results and next steps/schedule of key milestones. Then, agency attendees were given the opportunity to ask questions or make comments.

- Project Name: Baltimore-Washington Superconducting Maglev (SCMAGLEV)
 - Presentation Focus: Update of Preliminary Alternatives Screening Results
 - FRA Project Manager: Brandon Bratcher
 - MTA Project Manager: Suhair Al Khatib
 - MTA Environmental Manager: Kelly Lyles
 - Consultant Project Manager: Angela Jones, AECOM
-
- Welcome and Introductions – (AECOM)
 - Presentation – (AECOM)
 - **Alternatives Screening Process Review**
A review of the screening process presented in October meeting and the timeline were provided. The 14 initial preliminary alternatives went through two screenings. Screening Level 1 was a fatal flaw analysis based geometric factors and Screening Level 2 involved qualitative and quantitative analyses of engineering factors as well as preliminary environmental features. The team concluded the process with the recommended preliminary alternative alignments for detail study for analysis in the next phase of the project.
 - **Project Updates Since October 2017 Interagency Meeting:**
Alternative G1 (WB&A Modified) was dropped prior to the October Public Open Houses. The WB&A Alternative presented the highest number of potential impacts on residential properties and communities including sight and sound impacts, it potentially impacted numerous state, county and local parks in Prince Georges and Anne Arundel Counties and the WB&A Trail, and it potentially impacted the most sensitive areas of Anacostia Park and the National Arboretum.

Alternative E1 (Amtrak Modified) is being dropped, after the October Public Open Houses. This decision has been made due to a high number of impacts to residential properties; potential impacts to multiple natural and environmentally sensitive areas including Patuxent Research Refuge; direct impacts to historic “old town” Bowie, Bowie

State University, Odenton Town Center and community facilities, and the Fort Meade gun range; and constructability issues with the Maglev support facility and the Seabrook MARC Station.

Based on these changes, the Revised Screening Level 2 Results are the following alignment alternatives:

- . No-Build
- . Alternative J (BWP Modified – East)
- . Alternative J1 (BWP Modified – West)
- **Public Comments on the Preliminary Alternatives:**

As of November 1, 2017, over 1,200 comments had been submitted on the SCMAGLEV project and the project team anticipates receiving much more as the project progresses. A summary was prepared and presented that displayed the top comment types that have been received to date, including property impacts, opposition to the project, outreach, cost and funding, WB&A Alternatives, Amtrak Alternatives, reopening scoping, tunneling, and the BWP Alternatives. These comments were also summarized by zip code in a map format. High concentrations of public comments have been sent from the Odenton and Bowie areas.
- **Next Steps:**

The project team discussed the next steps that will be taken in the project and the schedule of the key milestones. Next steps include:

 - . Finalize Public/Agency Coordination Plan
 - . Finalize Preliminary Alternatives Screening Report
 - . Analyze Alternatives Retained for Detailed Study
 - . Schedule meetings with agencies impacted by Alternatives J & J1
- Comments and Responses

Comment 1: On the project map, the alignments do not appear to align with the potential Port Covington station zone. Will the remaining alignments be able to align with the Port Covington station zone alternative? (*Baltimore City Planning*)

Response: Yes. All alignments will have the ability to align with the potential Port Covington Station Zone.

Comment 2: If Alternative E1 is dropped, we won't have issues with the readiness with the tenants that would have been impacted. With Alternatives J & J1, there will be safety concerns with impacted tenants and a meeting will be required. (*Fort Meade*)

Response: We are now looking at the alternative alignments in more depth. We will attempt to minimize impacts as we move forward and get more detailed in our design.

Comment 3: What further consideration is being given to Section 4(f)? (*NPS*)

Response: We are beginning the Section 4(f) evaluation process. We don't currently have anything conclusive, but we will get more detailed as we move forward.

Comment 4: The project team needs to look at one alternative for the purposes of Section 4(f). (*NPS*)

Response: We agree. We have analyzed several alternatives, as documented in the Preliminary Alternatives Screening Report. This information will be available to agencies in the final report. We'll be in touch with agencies regarding these results and to go over questions and comments on alternatives being carried forward into the Alternatives Analysis.

Comment 5: USACE is concerned that we are only looking at one corridor (Baltimore Washington Parkway) and a no-build option. The alternative selection is not a popularity contest. One of the remaining alignments will likely need to be eliminated due to the conflict with Patuxent River State Park. (USACE)

Response: Although they run along the same corridor, we are still evaluating two alternatives along the BW Parkway corridor, including Alternatives J & J1. An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies.

Comment 6: What is Patuxent Research Refuge's stance on this project as one alternative? (NPS)

Response: It is essentially two alternatives in one corridor. (USFWS)

Comment 7: Adam is concerned that we did not show the tunnel portion of the alternative to the public at the potential Port Covington station zone. (STB)

Response: We acknowledged his concern and we will address through the alternative development stage moving forward.

Comment 8: With the Hyperloop project being in the media, NCPC has concerns about how it will impact the SCMAGLEV project. (NCPC)

Response: Hyperloop is not at a level of development where we are considering it in the SCMAGLEV project. The Maryland State government made an announcement regarding the utility permits. FRA indicated that we see them as independent projects. The Hyperloop team is eyeballing an approximately five year turnaround, but we don't know how feasible that is. The State did not confirm that there has been a Hyperloop ground breaking.

Comment 9: Has there been conversations between the State and NPS about turning over the rights of the BW Parkway to the State? (USACE)

Response: There has not been an official transfer as of now.

Comment 10: Is this Interagency Meeting Presentation available online? (USEPA)

Response: The Interagency Meeting presentation was sent out to the meeting invitees via email yesterday (December 6, 2017).

Comment 11: Question for Corps: Is this project being followed in terms of campus development in the northern part of Fort Meade as both courses of action along the BW Parkway cut into that project. (Fort Meade)

Response: I am not aware if anyone from the campus development team is involved, but I will start copying them on correspondences related to this project. (USACE)

Comment 12: Will there be a follow up meeting with regulatory agencies? (DNR)

Response: This presentation will be presented at the next Joint Evaluation meeting with Maryland permitting agencies on December 20, 2017. We will have more meetings and information exchanges with the agencies in the coming months, including the possibility of another agency field meeting.

Comment 13: What would coordination look like should MDOT acquire BW Parkway from NPS? (SHA)

Response: Moving forward, we are assuming that there will be no transfer.

Comment 14: BWRR is looking forward to working with FRA, MTA, and AECOM to make the schedule more efficient so that we have FEIS and ROD prior to 2019. (BWRR)

Response: We acknowledged this comment.

- Closing Comments/Adjournment – We will keep the agencies up to date as the project team evaluates the alternatives in further detail. The meeting was adjourned.

Joint Evaluation Meeting - *Baltimore-Washington SCMAGLEV Project*

DATE: December 20, 2017 2:00 – 3:00 PM

LOCATION: U.S. Fish & Wildlife Service (177 Admiral Cochrane Drive, Annapolis, MD)

This is a summary of the discussion, not a direct transcript. Meeting attendees or expanded summary details are available upon request.

○ **Presentation by SCMAGLEV NEPA Team**

- Team walked through the PowerPoint and handouts that highlighted updates of the preliminary alternatives screening results. A review of the screening process and the timeline was provided. The 14 initial preliminary alternatives went through two screenings. Screening Level 1 was a fatal flaw analysis based on geometric factors and Screening Level 2 involved qualitative and quantitative analyses of engineering factors as well as preliminary environmental features. The team concluded the process with the recommended preliminary alternative alignments for detailed study at the next phase of the project.
- Alternative G1 (WB&A Modified) was dropped prior to the October Public Open Houses. This decision was made because the WB&A Alternative presented the highest number of potential impacts on residential properties, there was growing public opposition to the alignment, and it potentially impacted the most sensitive areas of Anacostia Park and the National Arboretum.
- Alternative E1 (Amtrak Modified) is being dropped, after the October Public Open Houses. This decision has been made due to a high number of impacts to residential properties; potential impacts to multiple natural and environmentally sensitive areas; direct impacts to historic “old town” Bowie, Bowie State University, Odenton Town Center and community facilities, and the Fort Meade gun range; and constructability issues with the Maglev support facility.
- Based on these changes, the Revised Screening Level 2 Results are the following alignment alternatives:
 - § No-Build
 - § Alternative J (BWP Modified – East)
 - § Alternative J1 (BWP Modified – West)
- As of November 1, 2017, over 1,200 comments had been submitted on the SCMAGLEV project and the project team anticipates receiving much more as the project progresses. A summary was prepared and presented that displayed the top comment types that have been received to dates, including property impacts, opposition to the project, outreach, cost and funding, WB&A Alternatives, Amtrak Alternatives, reopening scoping, tunneling, and the BWP Alternatives. These comments were also summarized by zip code in a map format. High concentrations of public comments have been sent from the Odenton and Bowie areas.
- Next Steps were presented.

○ **Comments from attendees**

- DNR: In the dropping of alternatives, how has the project team included Rare, Threatened, and Endangered (RTE) Species?
 - § As we go into the alternatives analysis, there will be further coordination. We have added your recommended agency comments to the Public/Agency

Coordination Plan. We are initially examining at a higher level, but will get into further detail as the project moves forward.

- DNR: Even though there is the ability to tunnel and use piers, DNR is still concerned with RTE Species. There will need to be a discussion and resolution in terms of permitting.
 - § We will now develop a Limit of Disturbance (LOD) at the corridor level and will sit down with the agencies to discuss and will have follow up emails.
- USACE pressed the issue regarding the two remaining alternatives (i.e. we only have one alternative with two options along the BW Parkway) and cautioned that the process should not prematurely eliminate multiple alternatives due to public pressure, quadrupling down on his comments made at the Interagency Meeting. He thinks that we should have at least one other alternative. He is concerned about the viability, especially with the other technology (Hyperloop) that plans to tunnel the whole line. He wonders why Maglev is not being completely tunneled. He is very concerned that we won't have a project as a result.
 - § We have two alternatives along the single BW Parkway corridor that are separate alignments. Within the full Screening Report, we have full documentation to justify our decisions that we have made so far in the project. We want the cooperating and participating agencies to be very clear on our process, which we plan to achieve through continued meetings and workshops. We are working on coming up with ways to minimize, mitigate, etc.
 - § The need to account for potential Amtrak derailments, which would require either a 150 ft. buffer or a very large/expensive crash wall is another reason why Amtrak was dropped. It would not be financially feasible to tunnel the entire line. The transition zone would also be very disruptive, due to the required space, along the Amtrak lines, including at a MARC station. The two BW Parkway alignments are different. The eastern alignment goes into Patuxent somewhat and affects other governmental properties. The western alignment affects private properties, but not federal properties. The Rolling Stock Depot would be on the east side of the BW Parkway and would include a bridge over the parkway. There are limitations for this by the BARC property requirements.
- DNR: The sooner the project team gets a date stamped letter on the RTE Species, the better off we will be. He thinks that this would be the perfect time to do so. He thinks that we haven't scoped all of the resources to the degree that is necessary yet.
 - § The project team has not taken this step yet, but it will be part of the next steps moving forward.
- USACE: Do we have natural resources associated with the alternatives?
 - § We released the results of the preliminary alternatives screening to the public using High/Medium/Low thresholds. We do however have specific numbers that we can share with the agencies.
- MHT indicated that it is important that the agencies be able to demonstrate that they have properly evaluated the alternatives at the state and federal levels.
- USACE shared a message from USFWS in his absence and NPS concerns. USFWS thinks that the BW Parkway alternatives do not impact the Patuxent Refuge. If the Patuxent Refuge is impacted, USFWS will need to be included as a cooperating agency.
 - § The BW Parkway West does not impact the Patuxent Refuge, however, the BW Parkway East impacts the refuge. We are attempting to avoid the refuge; therefore, we are currently keeping USFWS as a participating agency, but will make them a cooperating agency if the Patuxent Refuge is still impacted.

- § The *Preliminary Alternatives Screening Report* is currently with FRA for review and/or comment. The project team will meet with FRA and review comments first, then we plan to meet with USACE again.
- DNR was more positive in saying we proved enough for where we are in the PASR and gave direction to gather more environmental information (i.e., RTE). DNR really appreciated the “quantities matrix” that AECOM shared for the natural resources. We should gather RTE information, as well as other layers for the Alternatives analysis.
 - **Action Items**
 - The project team will arrange another round of agency specific meetings, as needed.
 - The project team will coordinate with USACE and DNR regarding the quantities and values utilized in the high/medium/low table from the *Preliminary Alternatives Screening Report*.

Appendix C - Attachment B: Public Comments



THE MARYLAND GENERAL ASSEMBLY

December 19, 2017

Secretary Pete K. Rahn
Office of Secretary
Department of Transportation
7201 Corporate Center Drive
Hanover, MD 21076

Dear Secretary Rahn,

As you know, we and many of our constituents strongly oppose the proposed MAGLEV routes which would disrupt our neighborhoods with little or no value to local residents.

We appreciate that you have dropped the WB&A route. We urge you to drop the Amtrak route promptly and identify and mitigate any community impacts of underground routes.

Sincerely,

Senator Jim Rosapepe
Senator Joanne Benson
Delegate Barbara Frush
Delegate Joseline Peña-Melnyk
Delegate Ben Barnes
Delegate Ereik Barron
Delegate Jazz Lewis
Delegate Pam Beidle
Delegate Theodore J. Sophocleus
Delegate Mark Chang

cc: Federal Railroad Administration

No.	Date	Forum	Comment
<i>Comments Regarding Alignments from Previous Round of Public Outreach on Project Scoping</i>			
1	10-Dec-17	Open House	<p>Linthicum already deals with the airport, the MARC train, and the light rail. Adding another transportation system to our community is OVERLOAD!!</p> <p>We have a sparse amount of woods and open space for wildlife. Please do not take that away!</p> <p>Suggestion: Run the MAGLEV train down the median of 295. Leave our community intact.</p> <p>Question: How do you plan to study the impact on wildlife (deer, foxes, beavers, birds) that live in the surrounding woodlands?</p>
2	10-Dec-17	Open House	<p>What is the potential of running this line along existing infrastructure such as 295?</p> <p>This seems to be an interesting yet incredibly expensive project with the possibility of becoming a boondoggle.</p> <p>It may be time to invest in our current rail infrastructure (Amtrak/MARC).</p>
3	10-Dec-17	Open House	<p>There was some talk about 2 years ago. That one of the routes would tunnel under part of Linthicum.</p> <p>Would rather see path parallel 295.</p>
4	12-Dec-17	Open House	<p>WBA R/W seems perfect option, subject to a few new homes</p>
5	12-Dec-17	Open House	<p>This project should be built, but I think operating subsidies will be required. Nobody will pay \$50 to travel from DC to Baltimore.</p> <p>The old WB&A right of way through Prince George's and Anne Arundel County would be an excellent route for this line.</p> <p>There would be little to no disruption to Amtrak if this route were selected. Also the ROW goes straight to BWI Marshall Airport, unlike the NEC line.</p>
6	14-Dec-17	Open House	<p>Why is DC Department of Transportation not a cooperating agency? It should be (or some other arm of the DC government).</p> <p>The scope area should expand northwest to Rt. 29 in Maryland, unless this has already been studied.</p> <p>While not the most direct, there is an open median that could be used to route the line.</p>

No.	Date	Forum	Comment
7	15-Dec-17	Open House	<p>Hold further meetings when alternative routes have been selected.</p> <p>Consider communities (engage citizen) through which routes go but which will receive no benefit.</p> <p>Citizens will be particularly interested in noise mitigation, health issues, and aesthetic matters.</p> <p>As to the latter, questions like how the structures will add to the division of a community are important.</p> <p>Protect woodlands, streams, and wetlands, no matter how seemingly insignificant. Consult with local environmental groups.</p> <p>It is deficient to provide significant impact with no comparative material.</p> <p>Routes: The Amtrak parallel is already congested, having passenger train, freight, Metro, route 50, Lower Beaverdam Creek, residential and industrial all in a narrow corridor. All those uses already bisect Cheverly. Moreover, the tracks are on CSX right of way.</p> <p>In addition, that corridor includes floodplains.</p> <p>Please remember that we need to preserve what little woodland, streams, and wetland we have left. Do we really need this?</p> <p>The project seems to duplicate existing rail services. The existing services, moreover, truly serve the communities they run through. A typical trip between Cheverly and Baltimore, for example, takes 30 minutes by car.</p> <p>Maglev promises 15 minutes, hardly much of a time saving.</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
<i>Comments from April 2017 Open House Meetings</i>			
1	3-Apr-17	Open House	Well this was informative. I was a little skeptical primarily because I assume the maglev would be very expensive, and, I did some research and tried to find estimate of fare prices - no luck. If the Maglev can make a profit, I am more able to support it. Station should be in dense area of city - inner harbor. Too early to consider connections to stations via transit - but connection w/ Metro/LR would be awesome. Very exciting - Thank you!
2	4-Apr-17	Open House	Anticipated length of construction? What happens if the money dries up? What happens if you build it and nobody comes? Northern Anne Arundel County has taken the brunt of development and transportation infrastructure for 50 years. We're tired of it. Could you buy out Amtrak's NE Corridor service?
3	4-Apr-17	Open House	From looking at the proposed sites, it seems to me that the 295 corridor would have the least impact on the residential areas surrounding the airport as we continually have impact from commercialization due to the airport. From a life long resident of Linthicum and was here before the airport was built. Thank you.
4	4-Apr-17	Open House	1) Has there been a study done on the effect of the load of maglev on whatever station it drops at in Washington DC? (For example, check out the effect of MARC Penn Line on Union Station -- it is chaos.)
5	5-Apr-17	Open House	I do not support spending upwards of ten billion dollars at this point!! Should this proceed however I strongly oppose any alternatives beyond J and F. The route through the existing stable residential neighborhoods particularly along the WB&A trail G&I would be incredibly disruptive. These trains are huge and loud and I cannot believe these options re being in any way considered viable.
6	5-Apr-17	Open House	I cannot believe the citizens of Bowie/ Glen Dale would ever want this monstrosity! It will destroy wildlife, property values, quality of life, the quiet of the suburbs, and the benefits that Bowie provides. Bowie doesn't even benefit from a hub so what do we gain. My family uses the trail on a weekly basis -- the trail was one of the biggest selling point for our home purchase. Why build in the middle of a neighborhood of established communities? Are you all going to make up the monetary difference when our property values drop thru the floor? People w/ families have the right to peace in their own houses, not the sound of construction and then constant train noise day and night. Put people first not money!
7	5-Apr-17	Open House	My concern is what is the direct benefit for residents of Bowie seeing that the nearest stop would be Baltimore it appears the company have a greater need to utilize this area to build the project opposed to being a benefit for the Bowie residents. My other concern is the safety of the train traveling at that speed. After looking at the plans I'm against this project. Bowie residents will suffer from the construction.
8	5-Apr-17	Open House	I strongly object to any alternative affecting the WB&A trail. The reason I live here in Bowie is the WB&A Trail. It is utilized by many residents and affects our quality of life. I also have concerns environmentally related to the WB&A Trail. The wildlife and birds will be adversely impacted. What is the benefit to Bowie? Why Prince George's County? Is it because of racial profiling (i.e. primarily black and low income)? Why not Montgomery County or Anne Arundel County? Who benefits from this? Certainly, not Prince George's County. I bought my house here for its central location to Baltimore & DC and my commute. I also bought my house because of the WB&A Trail. If this change is made to the WB&A Trail, I feel there is no reason to continue living here in this community. Again who benefits from this? We are letting Japan build our infrastructure and benefit from this. Why? What is in it for them -- money!! What is in it for us? Adverse impact to our home values, quality of life, and environment. Why is this not aligned to existing railroad tracks & Amtrak? That makes more sense.
9	5-Apr-17	Open House	I live in the greater Bowie area. I have a very strong concern about the building and running of such a project. Why is this necessary. We do not have transportation issues in this area. The idea of such a high powered/speed train being run over our communities is dangerous and absurd. It does not seem to encourage the quality of life one would expect in Bowie. I am vehemently against the concept, building and running of this train through our neighborhoods. Can it be built and run on the existing tracks that run by Bowie State University. Please consider scrapping the project. This seems to be another ploy to build something in a county where you feel its constituents cannot stop the building of such a large project. It would not be proposed in Montgomery, Anne Arundel, Howard, or any other county. This is horrible.
10	5-Apr-17	Open House	WB&A is a historic trail with watershed impact. It also runs through residential properties and up against historic properties and farms. The local emergency responders would be inadequately prepared to respond to emergencies and there would be an extreme and irreversible negative impact to community and natural resources. Following the Amtrak lines would be a better option with fewer environmental or community impacts. Please do not destroy this historic multi use trail!
11	5-Apr-17	Open House	As a resident of Bowie (near Fairwood in Old Stage) I definitely do not approve of the proposed routes G or H. The impact of this on residences, farms, and wildlife is unacceptable to me. I am more in favor of adding on to or building adjacent to existing rail systems, since residential & environmental impact would be considerably less.
12	5-Apr-17	Open House	Concerned about the G&H yellow & green routes in the WB&A areas. E is best route.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
13	5-Apr-17	Open House	There is nothing about this project that I find has any merit using the lines that utilize the WB&A trail. If the federal government is able to refuse right of way to BW Pkwy, the citizens of the Bowie Community have the same right. In the age of development it is more important than ever to maintain our farms & green spaces & the history of this area. Re: funding -- This will require millions if not billions of taxpayer dollars for little to no benefit for our community. Re: environment -- The WB&A Trail & its area is partially wetland & home to myriad flora & fauna that will need to move -- many will move into suburban neighborhoods & wreak havoc because there is nowhere else for them. My home is over 100 years old -- the pecan trees that grace my property come from Thomas Jefferson's plantation. The Normal School Rd was named for the first black school in Maryland. There was once a stop on The Underground Railroad on our property. J. Edgar Hoover used to stay at my house on the weekends to attend races at The Bowie Race Track. There are other alternatives, better alternatives -- Bowie does not need MAGLEV & it does not serve the needs of the community.
14	5-Apr-17	Open House	I purchased my home which backs up to the WB&A for exactly that reason. I trail accessible and nothing could be built behind us. I have since spent \$60,000 on decking upper and lower so I can overlook the scenery. I have a 100 year flood plain to adhere to so there cannot be anything to contaminate my view. This train would be disastrous to us. I would be directly impacted. In addition to me this is the historic value of the WB&A/ wetlands and various farmlands attached to the WB&A. Please contact me so I can show you how this would impact our neighborhoods and newly built neighborhoods. I could also show the wetlands it would impact and how it would destroy the historical value of our area.
15	5-Apr-17	Open House	The alternative that impacts the WB&A is infeasible because it impacts completely residential neighbor. The neighborhood is all black and poor whites. It does not make sense to impact a residential neighborhood when there are alternatives next to existing rail. This project has not been sufficiently publicized and the people of Bowie have not had sufficient time to obtain information about the impact on our property rights. The fact that this project is even proposed to be near my home impacts the property value of my home which the government will never be able to compensate me for. This project will also destroy the serenity of Bowie.
16	5-Apr-17	Open House	There are so many issues I don't know where to begin! 1. A commuter train of this magnitude does nothing for me yet impacts my farm and livelihood. 2. The routes proposed along the WB&A trail are absurd. There are horses/farms/houses just 20 feet off the trail. Some are historic, some access their property via the trail itself. Many properties are on the historic register. 3. I operate a non profit horse & animal sanctuary which houses over 20 animals and provides community programs, many for special needs children and adults. To move our facility and animals & programs to a comparable property & location is impossible. We chose our property based on its accessibility. 4. The trail serves as a "highway" for local wildlife whose habitat is dwindling. 5. The sound & vision impact to our area is unacceptable.
17	5-Apr-17	Open House	This will destroy our neighborhood and community with no benefit to the local community. This will destroy valuable green space -- the reason we moved to the area - where is the plan that mitigates the permanent loss of this precious resource? These meetings area thinly veiled PR exercise -- where is the actual data about magnetic field impact & the ROW? The actual demonstration of how loud one of these things is? How are property owners affected going to be compensated for the impact on their home value? How dare you put this on a walking/biking trail? Why is this being planned to push through minority lower income neighborhoods? Run this along 295 -- not through my backyard. Who is getting paid by this project? Because it sells out neighborhood, community & county. The project had better be prepared to compensate all affected ahead of time -- I know these organizations have no care for the communities they destroy; we cannot expect that they will 'make good' on any promises. We live in an historic home in old Bowie -- this project will destroy our home, our investments, and the vision we had for raising our family. I see no consideration of the human impact here which is highly distressing. I will contact my local rep and ask them to oppose this Hogan Trump pet project.
18	5-Apr-17	Open House	The WB&A corridor includes several farms, 2 golf courses, wetlands and protected streams.* Animals that live in this area are owls, deer, raccoons, eagles, hawks, and others. This project would destroy this habitat and the quality of life of the humans living in this tranquil space. The WB&A trail is a long thin park enjoyed by hundreds of thousands each year. This project would cause irreparable harm to this area and these citizens. *Protected stream Horsepen Branch, a tributary to the Patuxent River
19	5-Apr-17	Open House	I'm against the WB&A trail alignment. My home is 416 feet from the trail. I don't want the noise or the drop in property values. Elevate the rail above the exiting Camden or Penn lines. You already have the right of way.
20	5-Apr-17	Open House	Hi I am a resident of Bowie. If you build a high speed rail where the WB&A trail is, you will destroy lands and animal life. Please don't do this.
21	5-Apr-17	Open House	I am against the route going thru the WB&A trail in Bowie. I am also concerned about the environmental impact. Although the maglev might be good idea for public transportation but I am hoping that the project proceed without taking down too many trees and preserving the environment!
22	5-Apr-17	Open House	I live "on" Mockingbird Lane (WB&A options). I am concerned you cannot build along that stretch without affecting 70+ houses that need Mockingbird to get in/out of their communities. The plan calls for a tunnel and I am at a loss where it might start and end. The terrain is not so accommodating. Thank you. See you in a month.
23	5-Apr-17	Open House	The WB&A trail is the only access to my and other's property. Use of this right of way that has not been used by train for 82 years would deny us access to our property. In addition numerous homes like my home are within 200' of the WB&A trail. A 375 mph train would cause structural issues for these homes. This train will cause harm to our homes and structures.
24	5-Apr-17	Open House	The super conducting approach to magnetic levitation of a massive item like a train will require large magnetic field, large electric fields and massive amounts of cryogenes. Many homes like my home are within 200' of the WB&A right of way. My family, pets, and livestock will be subject to these large fields and the real effect of long term exposure is not understood. Does exposure cause cancer, illness, dementia who knows. The WB&A trail right of way is not an acceptable place for this train project.

No.	Date	Forum	Comment
25	5-Apr-17	Open House	Concern if chosen path is WB&A trail that rail will be in front of our house. We access our driveway from the WB&A trail. If path is chosen, how will affected home owners be compensated or worked with to minimize impact to our property and value?
26	5-Apr-17	Open House	I am convinced that mag-lev represents the future of travel and the U.S. is far behind the curve. My focus is on the proper location. In that regard, I find a proposed WB&A trail route very troubling.
27	5-Apr-17	Open House	I strongly disagree with this proposal. To begin with there is no specific details about how those routes are determine, what criteria is used to decide if it above ground or tunnel. I live in a community of horse farms along the Horsepen Branch on the north side of Rt 197. It is one of the last open spaces and green spaces in the Bowie area and there are operating horse farms in this section. Why would this area be considered a good place. I was told because of the old WB&A trail. This was basically a "light rail" system that was operated in the early part of the last century. It has not been an operating rail line since 1930's. The property that I live on is a working 16 acre horse farm. With a lot of historic significance. There is a historic railroad structure. The house that I live in is on the list of historic properties in PG County. These open/farm space need to be preserved. There needs to be more details provided about what criteria are used to determine what route is going to be used, what criteria is used to decide if tunneling is an option. There must already be more detailed engineering and environmental and economic analysis reports or documents. This is bad idea to route this high speed trains on the Green or Yellow routes. Dear Sirs, The superconducting MAGLEV project should not use the WB&A right of way for the following reasons. The option will destroy the property values of all the adjacent properties, cause harm to the environment, eliminate an irreplaceable hiking and biking trail, impact the quality of life of humans and animals and eliminate the access to my property and the property of others. The alternatives currently H and G that use the WB&A trail right of way will cause billions of dollars of real-estate value loss for citizens with adjacent property. The last time the WB&A right of way was used for a train was 1935. Although an existing right of way looks desirable on paper this right of way has not had a train on it for 82 years. The current landowners bought property adjacent to a recreational trail. The property was valued with the WB&A trail as an asset. The WB&A trail is used by thousands of people for walking, running, riding bikes, horseback riding and several competitions per year. A 375 mph train shaking your house and speeding by at all hours of the day and night will shake the tens of thousands of dwellings causing structural harm. For these reasons the WB&A trail should not be used for the Superconducting MAGLEV project. The WB&A trail passes by several open spaces including farms, golf courses and protected streams. The trail provides a much needed outlet to relax and enjoy nature. Once this outlet is gone it will never be replaced. The a animals along the trail include owls, eagles, deer, raccoons, and ground hogs, hawks, snakes and opossums. Just 2 days ago I observed 20 deer in one group adjacent to the WB&A trail. The trail is enjoyed by hundreds of thousands of people per year. On a fall or spring day the users pass my property nonstop. The WB&A trail is a major asset to Maryland Capitol area Parks and Planning a factor ignored by your decision table. The WB&A trail runs alongside the Horsepen Branch, a protected tributary to the Patuxent River and Chesapeake Bay. This unusual lane of tranquility will be destroyed if a 375 mph train is allowed to use the right of way. For these reasons the WB&A trail should not be used for the Superconducting MAGLEV project. The WB&A trail is thru front property line of my farm and the only access to the property. My home and one of my barns is within 200 feet of the WB&A right of way. Using the WB&A trail for the superconducting MAGLEV project will eliminate access to my property for me and my customers, cause structural harm to my house and barn, cause a loss of natural space, disrupt the tranquility of the area and significantly reduce my property value. I am not alone there are thousands of homes along this right of way with values totaling billions of dollars. I the WB&A trail is used for the Superconducting MAGLEV project it will cause me great harm personally and bring harm to my community.
28	5-Apr-17	Open House	I agree the superconducting MAGLEV is a good project for Maryland but the WB&A trail is the wrong place for the project. The right of way has not been used in 82 years the current state of the surrounding property is one of nature, tranquility and residences. The WB&A trail is the wrong place for this project.
29	5-Apr-17	Open House	The WB&A options are poor choices because the right of way has not been used by a train since 1935, 82 years ago. There are thousands of houses, with value in the billions bounding the trail. The WB&A trail is now an asset since it is used for walking, biking, jogging, and horseback riding. The values of these homes are increased by the trail. Using the right of way for a train would cause losses for thousands of property owners. In addition, these structures will be harmed by the vibration from a 375 mph train. The routing shown adjacent to the WB&A trail options H and G go through or under historic properties, the Normal School, slave homes, historic rail road stations and a rail road bridge on the National Historic Register. The WB&A trail routings will cause harm to historic properties.
30	5-Apr-17	Open House	Thank you for the opportunity to weigh in on this project. 1. Need to see a more detailed picture of possible alternatives, magnified to show neighborhoods impacted. 2. Need to know how wide the Right of Way is on either side of the track structure. 3. Concerns about the vibration levels on above and below ground sections. 4. Concerns about noise/decible level of the train when passing by and its impact on homeowners & wildlife. 5. Concerns about the speed (311 mph) and the wind it generates and its impact on plant life (trees, leaves, flowers) & wildlife. 6. Concerns that our taxes will be increased as a result of the MagLev project. 7. Concerns that it will be affordable for use by the average consumer. 8. It is assume dthat homeowners displaced or highly impacted by the above ground Maglev project will be fairly compensated. Will those adjacent to the underground portion also be compensated? 9. We are opposed to the green alternative as our home borders the WB&A trail. 10. Concerns about possible electromagnetic field being generated that could potentially be harmful.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
31	5-Apr-17	Open House	<p>I am particularly concerned about the potential impacts of Alternative F on the walkability and dense, transit-oriented development of the densest and most urban portion of Prince George's County in the vicinity of US-1 and the CSX/Camden Line rail corridor.</p> <p>The Route 1/ Camden Line communities (Mt Rainier, Bladensburg, Brentwood, Hyattsville, Riverdale Park, University Park, Greenbelt, etc) are dense and are the oldest urban built environment in Prince George's County. Current transit improvements (including the Purple Line) and re-zoning to allow denser, transit-oriented development in this area are developing it into one of the most walkable and urban areas of the county.</p> <p>My concern is that putting a new surface or elevated rail line through this area could have similar effects as building a new freeway, cutting up the walkable street grid. Even if road connections are not severed, adding additional bridges or underpasses that pedestrians need to use will impede pedestrian walkability. Furthermore, building alongside the CSX ROW would likely prevent easy widening of the ROW to allow addition of more conventional tracks for all-day MARC service to provide local transit in this dense area.</p>
32	5-Apr-17	Open House	<p>I live in a historic home in Old Bowie, one block from the Penn Line tracks. I don't want the maglev train cutting up our historic town. Nor do I want to lose my home nor suffer from the noise, nor magnetic field, nor vibration.</p> <p>Besides that I don't think going from Washington to BWI or Baltimore would be worth the expense as it will ruin many people's lives along the way. I and my neighbors would get not benefit from it, and it could destroy what our town and heritage society has been trying to preserve. Thank you.</p>
33	5-Apr-17	Open House	<p>I find the route options for this bullet train highly disturbing. All of these routes go directly through low income communities, predominantly minority communities, green space and residential neighborhoods. The loss or diminishing of any one of these resources or benefits would be a detriment to Maryland as a whole. The bullet train has worked to connect urban centers in China and Japan largely through the seizure of uninhabited land through eminent domain. This absolutely cannot work as an approach in the busy and populous northeast corridor of the United States. I intend to join in the legal fight to prevent this project from moving forward, dismantling my community, killing local wildlife & spoiling my home.</p> <p>To suggest that the bullet train will create jobs for Prince George's County residents is disingenuous as well. I have firsthand experience of federal contracting and I know that these projects are bid to large national and international firms. It's certain that a Whitney Turner or similar scale firm will win this award and subcontract the work to a series of out-of-state firms. If Maryland is expecting a boost to the local economy, I hope they get cash in advance. The promised boost and benefits will never materialize.</p> <p>This project is the Governor's distraction from the fact that Maryland infrastructure is sorely in need of repair and upgrades. That will ease travel through the region not a levitating bullet train.</p>
34	5-Apr-17	Open House	<p>We are totally opposed to this project. There is definitely enough ways already between Baltimore & Washington & New York. It is a waste of money, time & energy. There are many houses in close proximity & people, like us don't want this in their front yard. It will kill our housing market in these areas.</p>
35	5-Apr-17	Open House	<p>The best of luck getting support for this project. If it cost \$40B and last 40 years, that would be \$1B/yr. If each day, one took in \$3M and each rider paid \$15 each way so \$30/day that would require 100,000 commuters a day to support the system. Are there that many commuters?</p>
36	5-Apr-17	Open House	<p>Unnecessary/ should replace Amtrak rails and build a rail for maglev and also reduce cost on new lines.</p>
37	5-Apr-17	Open House	<p>This form of transportation is not needed. We have commuter lines, Amtrak, buses, commuter buses, Metro, and several methods by which cars can get from Balto to DC. There is NO need to destroy productive farm lands, or property owned and maintained by high-income residents to create this unnecessary intrusion.</p> <p>There is no to destroy wetlands, wildlife sanctuaries, and other prime environmentally healthy areas to do this. NO!</p>
38	5-Apr-17	Open House	<ol style="list-style-type: none"> 1. Will the maglev compete with MARC ridership? Concern is that too many people riding from DC to Baltimore may switch, decreasing ridership on Penn Line enough that frequencies of trains will drop. 2. Will the tracks divide the community in terms of walking access? Will you be able to freely cross under or will it divide community? We can't divide or place additional barriers. 3. Can the project create or provide enhancements to community assets or amenities in exchange for community impacts? <p>Suggestions for MTA for improved outreach at next meeting based on what I've heard from others: 1. Show demonstration of how loud the train will be when 24 ft or XX ft from homes. Perhaps explain it being elevated and encapsulated in glass will significantly reduce noise. Plus noise will be only for a split second due to speed. 2. Show a rendering of the cross section with the beloved WB&A trail below it, so people recognize this will not destroy the trail. In fact, could it help the trail by extending it? Can they fund the Little Patuxent River Bridge crossing for the WB&A trail? That would excite the trail advocates.</p>
39	5-Apr-17	Open House	<p>Not sure what comments I may have later. Please continue to communicate w/ me via e-mail and especially when there will be a public "hearing" on this matter.</p>
40	5-Apr-17	Open House	<ol style="list-style-type: none"> 1. Pier construction must be resilient enough to withstand terrorism attacks and be capable of being replaced quickly. 2. You need to demonstrate the chemicals used in cryogenic cooling system will not harm the environment or humans. 3. A hiker-biker trail bridge over the Patuxent River at the WB&A location should be constructed with the project. The bridge is currently in design.
41	5-Apr-17	Open House	<p>The thought to go from DC to NY in an hour is interesting. The impact on our neighborhood and church property is a concern.</p> <p>Location: Home - Northridge, Bowie, MD 20720 Church - Seabrook, MD 20706 Current MARC and Amtrak trains seen near the church property</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
42	5-Apr-17	Open House	<ol style="list-style-type: none"> 1. Parking for vehicles; cost to park? 2. How close to Wood North Community? 3. Impact on Woodmore North 4. Noise factor to Woodmore North 5. Disruption time re traffic delays and construction 6. Estimated time to be in Fairwood & Woodmore North communities 7. Environmental impact on total Rt 450 corridor 8. Estimated/ approximate date to begin construction on Rt 450; estimated end time on Rt 450 construction 9. Compensation for disruption to our neighborhood 10. Cost to ride 11. Tunnel -- how much will be needed to complete the project?
43	5-Apr-17	Open House	Show or explain how tunnel pressure changes are managed. Are vents to surface needed? If so, how are they designed and located?
44	5-Apr-17	Open House	<ol style="list-style-type: none"> 1. What reports are available and where can we find them from scoping process and preliminary alternatives process? 2. What is the preferred route the FRA is proposing? We've heard use of the WB&A Trail is under consideration but can't seem to find any mapping to verify this. 3. Is the Environmental Impact Statement being done for all routes that are being identified, not just the "alternative" routes? 4. What information is available or where can we find it about the "No Action Alternative" that is being used as a baseline to compare the impact of the project? 5. In the process when are the other public input opportunities and deadline dates? 6. Given research has been done in countries like Japan <ol style="list-style-type: none"> a. what happens to the high voltage power lines on/very near the route? b. what is the impact or conflict between high voltage power lines and an elector magnetic field? c. What about noise and vibration impacts? d. What about water impacts, flood prone and flood plains along the routes? e. How wide or large is the electromagnetic field ? f. What is the impact of electromagnetic energy on human health? 7. When looking at the high speed train transportation currently in use in our country and other counties now, they don't travel at the high speed rates provided in marketing materials (e.g. 311 MPH or more). So it seems like in all practically the average speed of high speed trains like Maglev in use are similar to Acela type trains : <ol style="list-style-type: none"> a. Why are we not investing in what it takes to bring our current trains like Acela up to it's higher average speed? b. what is the cost difference between improving the tracks/systems/local ordinances for Acela verses a whole new system of Maglev? c. What's the level of anticipated public funding (state and federal)? d. Who is the project Private sponsor? What is the level of funding from that sponsor?
45	5-Apr-17	Open House	Thanks for the information! Very impressive. This train is a great idea and I would support it being built. I am glad you are seeking input and keeping in mind NEPA as you design and build. I would be fine with the track coming through Bowie, provided there is little disruption to the environment and existing development.
46	5-Apr-17	Open House	The right of way along most of the proposed routes is not wide enough to incorporate this project. Construction will require purchase the same amount of property regardless of the route chosen. <ol style="list-style-type: none"> 1. Why has the choice of routes been limited to the routes chosen 2. How are you going to deal with collateral damage due to the construction -- for example cracked foundations
47	6-Apr-17	Open House	<ol style="list-style-type: none"> 1. Bike trail through tunnel to DC 2. Need a demonstration of noise level
48	8-Apr-17	Open House	I will be very surprised if NPS is willing to agree to either of the BW Pkwy alternatives, but those make enormous sense in terms of leveraging existing ROW. The SCMAGLEV's low operational impacts (from sound & vibration) make it an attractive technology, one that should reassure residents and businesses wary of nearby rail service. Alternative G shows significant promise, but impacts to the existing bike trail in the ROW (both during construction and after service begins) need to be studied carefully.
49	8-Apr-17	Open House	<p>What is the estimated total cost of construction?</p> <p>What will be the funding source of this project?</p> <p>How will this proposed rail in compete against existing rail and air travel options? Economic viability.</p> <p>Have you considered the impact on families living along the WB&A corridor? Glendale, Bowie, Odenton, etc.?</p> <p>Long term, what is the long term plan for operating? Private or public operated utility?</p> <p>How do you intend to acquire land along WB&A?</p>

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50	8-Apr-17	Open House	Please reconsider your end/start point in D.C. Union Station currently serves (& will increasingly serve) as the transportation hub for D.C. All intercity buses use Union Station as their end/start point not to mention VRE, MARC, Amtrak, Metro & the Circulator buses (intra-city). In addition, the streetcar (H St) will be extending along H St into Georgetown. It too connects very closely to Union Station. You also need a 3rd track to create work-arounds both for potential bottle necks caused by accidents etc. and to run both local and express trains more frequently. Plan for 60 years out - not 20.
51	8-Apr-17	Open House	So very excited about the project. Despite potential negatives, the outlook is amazing for long term growth of the region. Since the negative comments tend to be heard the most, it would be helpful to see how general public can get more involved and help advocate for the project as it is one of the most exciting public transportation initiatives in the US. Please do start promoting the project on social media to get more millennials involved as it will affect them directly in a few years. Keep up the good work and thank you for working on making SCMAGLEV a reality for people of DC & Baltimore. A+.
52	8-Apr-17	Open House	Very excited about potential airport connectivity to Philly & NJ. Also the tourism traffic between Washington DC & Baltimore.
<i>Comments from Website</i>			
1	5-Apr-17	Website	Our community is concerned about MAGLEV tunneling under us. Our soil is basically sand. How would that work? Also, our community is on well water. Our water company, Utilities, Inc., has several wells in the area that the community uses for our water. How would that be affected? Our residents are also concerned about how we would be affected in case of an emergency, like a fire or explosion in the tunnel.
2	6-Apr-17	Website	Thank you for the meeting in Bowie. We are unanimously opposed to any route through Bowie. Thanks for adding me to your list.
3	6-Apr-17	Website	WE DO NOT WANT THIS to come through our BOWIE neighborhoods. It would not do anything positive for our streets or community. Please take a look at our current traffic on a daily basis not on a holiday or a weekend. We are congested all the time. We as Bowie citizens have had enough!
4	6-Apr-17	Website	Two questions. 1. While way too early to be specific, do you have any insight into what a typical fare fm D.C. To Baltimore might be? 2. How would deep snow be removed from these elevated structures? Would the system have to shut down during severe winter weather?
5	7-Apr-17	Website	I have just heard of this project and am very concerned about the impact on residents of the areas under consideration for the different routes. I live in Hillmeade Station very near the WB&A trail. The thought of a "super train" going in nearby certainly doesn't appeal to me. Another aspect of the whole plan that bothers me is the fact that while it goes through many areas one would have to travel to Washington, DC or to Baltimore to use it. What possible benefit does that afford to others living along the proposed routes. I would also like to see a readable map of the proposed routes, the one on this site is too small to actually see details. Thank you
6	7-Apr-17	Website	I hate this idea. One of the proposed routes passes less than a half mile from my house, but if I hadn't just joined Nextdoor, I would not have known about this project, the meeting, or anything. What's up with that? My concerns are: NOISE, Construction, the proximity of the routes to current neighborhoods, and the fact that my property values will drop when there's a giant train going through the backyard.
7	7-Apr-17	Website	I'm very concerned about the routes that go through very populated residential areas of Bowie. We are middle class homeowners who likely will not use the train, given there is no stop in Bowie, but you are proposing tunneling under our houses. You will
8	7-Apr-17	Website	I am writing to register my opposition to this project. As wonderful as high-speed transit between DC, Baltimore, and New York sounds on the surface, the simple fact of the matter is that the only beneficiaries will be the few who are wealthy enough to afford the cost of the ticket, all at the expense of the property a number of us will be forced to give up through eminent domain, decreased property values for those in close proximity to the line, and the loss of our quality of life and community resources such as (potentially) the B&A trail. Not only that, but the line will not even be stopping in my community so, for all of the damage and disruption, it will bring zero economic benefit through increased commerce in and around a station. I will be contacting the municipal, county, state, and federal representatives to register my dissent as well.
9	8-Apr-17	Website	Please do not do this to my community. This is going to turn the urban areas in to a city. I pay more in taxes so i can be away from the noise of city living. The Big Purple Barn have been around for years.
10	8-Apr-17	Website	I extensively utilize the WB A path for biking and enjoy its environmental benefits. I will vigorously oppose the project through my local, state, and congressional representatives. There are enough options to get from Baltimore to D.C. already. Can't we
11	8-Apr-17	Website	As a resident of Bowie I am concerned that the path of the train is through Bowie and the noise level it will produce to our area and this is a pass thru. Why can't the existing tracks be used in New Carrollton. That would limit it to the same locations.
12	11-Apr-17	Website	The only way I could even begin to support this concept is if it used current avenues of access to the maximum extent. It would have to be above current rail/roadways for >75% and have minimum impact to private land, wetlands, etc. Until I see some impact studies and how the routes affect current landowners, I would have to hold back my support and encourage others to do the same. Let's see some well-advertised town hall meetings first.
13	11-Apr-17	Website	I am not against new modes of transportation, and I am not necessarily against this one. My chief concern is that none of the proposed routes are along current traffic corridors (e.g., I-95, I-95, route 50 or the Amtrak-MARC rail). Safety research shows that developing this mode along current transportation corridors would be best. There is no need to disturb current wetlands, greenspace or large housing developments to build this line.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
14	12-Apr-17	Website	I just learned of the open houses that recently took place regarding the SCMAGLEV project. I was shocked to hear that my neighborhood may be one of the ones affected and the meetings had already taken place. Please send me any information that is available
15	12-Apr-17	Website	Bowie is home to many professionals in the Information Technology industry. We love technological progress, and wouldn't oppose something as fascinating as SCMAGLEV technology out of petty, selfish concerns for property values or a desire for peace and quiet. That said, I support alternatives F and J and revile alternatives G and I. I do not want the SCMAGLEV to come anywhere near valuable Bowie assets such as the WB&A Trail and the Big Purple Barn, a non-profit horse sanctuary whose steeds are regularly used in both physical and emotional therapy applications.
16	13-Apr-17	Website	I have been looking at the map that shows the various possible routes. Was it done on purpose to have the details be obscure? I cannot tell if the one of the proposed lines is going to go through my backyard. Is there a map that is more legible? Not
17	14-Apr-17	Website	I just sent another comment about proposed route Alternative F, and wish to add another one. On your website, you say that you will consider factors such as residences, low-income populations, and minority populations. Cottage City is approximately 50%
18	14-Apr-17	Website	I am writing to object to using "Route F" for the proposed high speed railway. This is the route that, just as it leaves DC, goes along the northwest border of Cottage City, between Cottage City on the southeast side, and Mt. Rainier and Brentwood on the
19	17-Apr-17	Website	I would like to be informed on any information on the study to help in preparing for the possible sale of my property. I am a home owner on the BW Trail. I would like to plan for the future and the possibility of having to sale my home. To help wi
20	17-Apr-17	Website	I recently saw on social media that you are considering building a Maglev going directly through my neighborhood in Bowie, MD. I can't believe that this would even be a consideration. I am a homeowner in the Rockledge subdivision, and the G (WB&A) alternative would destroy my neighborhood and ruin our home values. We have an elementary school that it would pass right over or under. The path of this proposed track would go directly over or under my house, where I have lived for over 15 years. How can you do this to people? Keep your Maglev out of Bowie. It will only hurt our city. We do not want or need it.
21	17-Apr-17	Website	I am writing to object to using "Route F" for the new proposed high speed railway. This route goes along the NW border of Cottage City, between Cottage City, Mt. Rainier and Brentwood. The area is residential with homes in very close proximity to the track all along the rail lines. The residents of Cottage Terrace, including me, live directly across the street from the railroad, with nothing between us and trains but the street. Many residents have back yards that closely abut the railroad line. There are no fences between residents and the railroad track as the railroad has refused to install one to help mitigate both the danger and on-going noise created by trains running through our community at high speed, both day and all night. We very strongly request that the Route Alternative F NOT be used to further disrupt our community. In recent years, train traffic has increased exponentially in our community and quality of life has become an issue because of it. Our community STRONGLY requests that another route be selected for this new high speed railway. NOT Route Alternative F. We appreciate your consideration and concern for our quality of life in Cottage City. Joan Daniels Cottage City Resident
22	18-Apr-17	Website	Please, PLEASE do NOT build a Washington-to-Baltimore SCMAGLEV train along our beautiful WB&A trail!! Even considering it is an insult to our scenic hiker/biker trail that is enjoyed by countless Maryland residents. It crosses designated historic areas that need to be preserved. I am not opposed to the SCMAGLEV train project; but build it along one of the existing high-speed, high-throughput transportation corridors such as I-295 (or preferably but seemingly not considered) I-95.
23	19-Apr-17	Website	(1) None of the proposed routes for this new mode of transportation are along current transportation corridors (e.g., route I-95, routes I-97 and route 50, the shared Amtrak-MARC rail). Instead, most of the routes are planned for within two miles of our house (!), traversing current wet lands, farms, greenspace and homes; (2) The National Park Service has already said "no" to the idea of having the SCMAGLEV over or along with the BW Parkway; however, the proposed system is an elevated rail, so this may still be an option for a route; and (3) The only reason we were informed about the open house is because our MD state delegate happened to hear about the proposal, and when she did, the lobbyists were ducking her. We find it curious and disturbing that the supporters would not try to engage her given that all of the routes traverse her district in one or another way. There is no need to condemn large tracks of greenspace and homes to develop SCMAGLEV.
24	20-Apr-17	Website	Horrified at prospect of losing WABA bike trail and the wonderful natural environment it has. Best route would be over BW parkway which is already too congested to remain viable option for future cars. We can't just keep building more highways! Love the idea of Maglev, just not at the expense of ruining the quality of life of the Bowie area whose people will get little benefit at great cost.
25	20-Apr-17	Website	I don't think this project would benefit my community of Bowie at all. It would decrease property value, ruin the environment and is it really necessary? I take the MARC train every day from Bowie to DC and it is a reliable mode of transportation. Is a study being done on how many passengers would really ride it? And how much would it cost to ride? Is this project included in the proposed changes they are planning to make to Union Station in Washington, DC? I think you will be hard pressed to find any citizens that support this train.
26	21-Apr-17	Website	My husband and I are adamantly against this line running through Bowie. We have lived in our home since 1980 and plan to stay here into very old age. The disruption to the community would be unthinkable.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
27	23-Apr-17	Website	Hi, as a Bowie resident, I would like to comment that I am excited about the project, but would strongly prefer the track be built along existing Amtrak or BW parkway lines instead of along local bike trails to preserve recreational areas and bird habitats. Thank you.
28	25-Apr-17	Website	I am severely opposed to this train running through residential Bowie!
29	28-Apr-17	Website	Hello, What are the proposed routes? Is the preferred route right through the middle of Bowie MD? Why is there so little media information about this project in our community? Why aren't the current rail lines a viable option rather than destroying long established communities in Bowie, Piney Orchard and others. How will residents be compensated for their homes? When is the next meeting about this project? Why isn't it being publicized in the Washington Post so citizens have notice and can attend the meetings? Will this run right through the middle of the National Arboretum? In addition, will this run through the middle of the Kenilworth Aquatic Gardens? Will it also take out the Bowie Golf Course? Please contact me ASAP as I would like to speak directly with the project manager. 301-335-4491
30	30-Apr-17	Website	I feel strongly that a route that follows the Baltimore Washington Pkwy is the best choice for this planned advance in mass transit. It will be less intrusive in residential areas, require less damage to residential areas, less noise, and will be considerate of some of the more rural type parts of communities that have cattle and crops that can be impacted by construction and high speed transit. It will also be safer refraining from coming into contact of areas where children frequent.
31	1-May-17	Website	The high speed train must NOT be built along the WB&A trail. It is dedicated to recreation and the habitat of rare birds and other animals. There are horse farms and homes along the path that would not be able to continue as they do now. The WB&A was formerly an electric line. It was never what the sc train would be. Wifi and internet might not work. Pacemakers might be affected. Power lines run along the trail. I heard through the grapevine that the BW Parkway routes are out of consideration. If so, then the only options should be along the Amtrak corridor. Ideally no sc train would be built. We need Maryland state to take care of the roads we already have and to promote Amtrak as a reasonable way to get around. Sincerely, Bonnie Roberts
32	3-May-17	Website	My family purchased a home 12 years ago in a new development next to the WB&A Trail. In part, this area near Old Town Bowie was attractive because the trail provides a unique opportunity to exercise and take the family out. The trail is very busy all year round. The trail serves a very important role in providing the little bit of green space left in the county. In particular, the entrance to my neighborhood goes across the WB&A trail and it is unclear to me how the neighborhood will be accessed if the train is built on the WB&A trail. Even if the train is underground, most certainly the neighborhood will be adversely impacted. We already are faced with increased risk by dangerous chemicals being transported by freight. It seems inconceivable that we would now have to endure both freight trains and the MAGLEV in the same community
33	4-May-17	Website	I would not let kento see the maglev go through or enter the city of bowie. Your route disrupts our lives by destroying nature, history, our walking and bike paths. I am not objecting to jobs or the train but I do not want it to effect what the good people of Bowie enjoy,you must also be aware of and when you build you MUST enter into a Community Benefit Agteement that gives back to the City of Bowie if you are in or near our city.
34	4-May-17	Website	Do not build this project through established neighborhoods in Bowie. I cannot understand how this makes any sense. Stop Maglev!
35	4-May-17	Website	I do not want this going through Bowie. This doesn't serve our needs.
36	4-May-17	Website	I would like to be put on the mailing list for town meetings, etc., associated with the MegLev Project
37	7-May-17	Website	I am writing in opposition of the proposed train routes that would uproot residents and permanently change the physical landscape of the city of Bowie. Residents love Bowie because of that very sense of community and small town feel. Bringing a train through the city would affect everyone who lives here by destroying homes, uprooting families, increasing traffic, pollution and noise levels and changing the community in a negative way. We, the residents and tax payers deserve better. We deserve the same consideration that would be given to residents of Montgomery, Anne Arundel, Fairfax, and Loudon Counties would be given.
38	9-May-17	Website	I've lived in Bowie in the R section for almost 20 years. Please don't destroy my home.
39	9-May-17	Website	Subscribe.
40	12-May-17	Website	No train go some where else lets keep neighborhoods
41	15-May-17	Website	I am a resident of the Saddlebrook West community in Bowie, MD. This community as well as my home is relatively new. Most homes are less than 15 years old and are selling in the range of \$500K to 600K. Can't you build a tunnel underground so that people's homes and schools are not condemned? This is not fair to the residents of Bowie or Prince George's County since no stops are planned here. Who is benefiting from this? Wealth businessmen who can afford the ticket prices?
42	18-May-17	Website	I oppose the maglev project running from D.C. To Baltimore in the current described route.
43	19-May-17	Website	This is a terrible idea. Why would you plan on destroying so many communities, schools, and homes so that a few elite business people can travel faster. My understanding is that China's MagLev has become a white elephant as the trains run at less than 30% capacity due to ticket prices. Why can't you tunnel this under the 95 corridor. The government already owns the right-of-way. Your maps show that this will bisect Rockledge Elementary, Whitehall, Saddlebrook West Community, the Bowie Gold Course, medical facilities, and Piney Orchard just to name a few. I would appreciate a response. Best regards,
44	21-May-17	Website	After reviewing the materials for the SCMAGLEV project and looking over the Preliminary Initial Alternatives, I am very concerned about the impact to houses, communities, and businesses. I strongly urge you to reject alternatives E (orange), G(yellow), H(green), and I(red). These routes negatively impact too many citizens, and would take away too many individuals homes and businesses. I also feel that this project does not provide a good solution to the challenges faced by the region. Both Maryland and the federal government have other options that are a more effective use of time and resources to benefit the public good. "No Build" should be the preferred alternative for this project.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
45	23-May-17	Website	I would like to say I am against this project as a whole and especially against the idea of running this train through areas that are currently either parkland or residential. This project will add no value to the state of MD and become a white albatross. When we are in desperate need for real transportation solutions for the commuters and residents of MD to put in something that will take money from AMtrak or upkeep of major highways. No one will be paying for a 15 minute train ride at the same price of a commercial flight when the drive and current commute options can do it in 30 to 45 minutes. I think you are over estimating those people interested. Since neither NY or PA has signed on for the next leg no estimates including those legs should be considered. This is a huge waste of money. In addition the routes considered go through quiet stable communities in PG county which would would think you would not want to negatively impact. The fact that the purple line of metro is a current failure costing millions you would think you would not even consider moving forward. This just seems like a money grabbing scheme from the Japanese business. I object to all aspects of this project but even more when you are disturbing neighborhoods.
46	3-Jun-17	Website	Please keep me informed of upcoming meetings. I'm very concerned about some of the proposed routes and the impact construction will have on our community in Bowie, MD.
47	11-Jun-17	Website	This is an interesting concept, but I have many reservations. Three of the proposed routes could barrel through our home. To whose benefit? No one in Bowie.
48	11-Jun-17	Website	I am very opposed to the train route that comes through Rockledge & the B&A trail. We have a virtually crime free neighborhood, very quiet as it dead-ends: no through streets to other areas, neighbors know & care for each other, black, white, Asian, & ot
49	12-Jun-17	Website	I do not want this train in or near Bowie. Please stop this
50	12-Jun-17	Website	This train will do nothing to solve local transportation problems that need to be solved FIRST before building a train simply to show case Japanese technology - which is not used anywhere else in the world right now besides the Japanese test track. This
51	14-Jun-17	Website	I very concerned about the plans for the SuperMaglev Train and I am adamantly against the train going through established, quiet communities when improvements can be made to existing train lines. I am resident and homeowner in the City of Bowie.
52	14-Jun-17	Website	I'm all in favor of this project. I think that this is really an important improvement to our public transportation system. I have 2 concerns, nevertheless. One of them concerns impacts on wildlife. I would hate to think that this would result in a si
53	26-Jun-17	Website	Is there any public stock offerings in the near future.
54	29-Jun-17	Website	This project has no obvious benefits for the City of Bowie or Prince George's County and will only disrupt the lives of the people in the county.
55	30-Jun-17	Website	I would like to know the Radio Frequency Interference (RFI) generated by the super-conducting magnets passing over the power coils at high speed. My HF antennas are 165 feet from the proposed route thru Bowie, MD. What studies have been done to measure
56	10-Jul-17	Website	There is already a rail connection between DC and Baltimore with trains running on schedule - travel time as fast as 30 minutes for the Acela - how much demand can there be that would justify the cost, for some people to save 15 minutes on a trip ? seems very difficult to imagine how this can be justified and a poor investment of a huge magnitude
57	11-Jul-17	Website	Please do not put this train in Bowie. Your plan will impact the historic district, university and developments with expensive homes. If this train comes those with higher incomes will leave, property value will fall and schools will get worse. This plan will ruin this city. Travel from DC to Baltimore is not a daily commute. These two city's are not similar and there is no need to be able to get from DC to Baltimore in 15 minutes.
58	13-Jul-17	Website	I live at 8606 Race Track Road, Bowie, MD. My neighbor told me that someone was out with the plans at the WBA with the plans last week from doing the environmental study. He stated that this was the preferred route & that my home & about 100 others would be demolished if it went through. I would just like someone to be honest & fill me in if this is the case. I am neither pro or con to this project I just do not want to be left in the dark. thank you,
59	14-Jul-17	Website	When will representatives from MTA/DOT meet with the Bowie City Council and HONESTLY answer questions? When will I receive answers to the questions I submitted to John Trueschler months ago? Why has "Angela" from AECOM not bothered to answer my questions? How is this project being "open" and "transparent"? When you will address our questions?
60	14-Jul-17	Website	Please send me any literature that is available. Thank you. Could you also please send a copy of the May 2017 MTA Scoping Report.
61	14-Jul-17	Website	Why are you proposing to force hundreds of residents from their homes for this train that NONE of us will be able to use? Why do you HATE the residents that live there?
62	15-Jul-17	Website	Request potential Maglev routes in Maryland.
63	15-Jul-17	Website	I just want to say that I love this idea. I live in Baltimore City and can see that this train would give folks in Baltimore better access to the higher paying jobs in DC. Additionally, it will make Baltimore City a very economical alternative to those relocating to the very expensive DC area. It will boost Baltimore property values and thus, City revenue, giving this city a much-needed lift to improve city services and aesthetics. It will revitalize Baltimore's Charm! In my view, you can't build it fast enough!
64	19-Jul-17	Website	Your print function for the boards is down. I can print the title, nothing else.

No.	Date	Forum	Comment
65	21-Jul-17	Website	I am disappointed that the dc to balt maglev project is going ahead against the overwhelming disapproval of informed residents of not just counties directly affected but by neighboring counties. It is inconceivable that people in power and elected officials would only consider a notch in their collective belts and not consider the damage to the towns affected directly. It is my observation and many others (the people that know what's being proposed and green lighted) to what end do the decision makers think the project is a good idea. Point blank: so rich people can get from dc to balt without sitting in traffic. Conveniency. So in the end every neighborhood that will have part demolished and what's left next to a train corridor . I wonder how families that bought their first home,raised their families, retired as kids bought their first homes in Bowie to be close to family can conceive their homes taken away. Sidenote: my father developed alzheimers in his 70s and against us kids wanting to put him in a senior living complex,leisureworld,my mother refused saying this is our house and in speaking with councilors to get them to move we were schooled on their right and need to live where memories and comfort where only in their house. Get a pro to senior-proof their house. All this to say she's 95, father passed, and she lives in the only place on earth she can call truly home. We have the same in Bowie and other affected areas. I can't imagine my mother having her home taken away and told it's progress. The train goes really fast and the upper class can go to balt in 15 minutes. I hope you see how ludicrous the project looks like to people. It's not cracking a few shells to make an omlt. It's destroying people's lives and memories for a very fast train.
66	21-Jul-17	Website	Please do not destroy the community we love and everything we have worked so hard for, rehabbing this house. It is our dream. Please. This is a terrible idea and I truly hope it does not work.
67	23-Jul-17	Website	You need to re-open the scoping process as nobody was notified. You can't just ram this through in secret.
68	23-Jul-17	Website	This is a terrible idea. Distroying peoples lives, homes and businesses for a few elite travelers. This train is a white elephant -- it will not solve traffic issues and will be a huge money loser. So the taxpayers will be left on the hook. We already have trains from Union Station to BWI ... switch to Oscella Trains that go 230 mph and you can use existing infrastructure.
69	23-Jul-17	Website	I am very confused by the maps that have been presented and think that is is absolutely disgraceful that this project will remove so many people from their homes. How can you think that a project like this will be beneficial for the area? I am disgusted.
70	24-Jul-17	Website	STOP THIS TRAIN! My home is on the yellow line, my children are on the yellow line! STOP THIS TRAIN!
71	24-Jul-17	Website	Hello -- I am a resident of one of the communities that would be affected by the proposed alternative routes G and I presently being considered (physical address 1309 Chapelview Drive, Odenton). We have a wonderful neighborhood, an excellent local elementary school where my children are well situated, and I very much oppose the idea of constructing a new railway route through this place we call home. There are existing railway passages to the west of our community, and many of the other routes would seem to have a lesser impact on all the communities between DC and Baltimore. Thank you for your consideration.
72	25-Jul-17	Website	My home and my community are in the path of the MAGLEV project, and I have not received any information from MTA regarding the project. I nor my neighbors have been given the opportunity to comment on the project or participate in the National Environmental Policy Act compliance process. I am formally requesting that MTA reopen the scoping period for this project.
73	25-Jul-17	Website	I am a candidate for Delegate District 21. I also live in Odenton where the proposed SCMAGLEV Project will take place. A lot of people are calling and writing me concerning this project. I remember attending a meeting at the Odenton Middle School. There was a promise that this project will continuously update all the stakeholders. I believe things are moving so fast but you are not updating us. Understand that the people that are going to be affected environmentally speaking or lose their homes will not be sitting idle while you do whatever you want. Start now to communicate otherwise you will have a lot of difficulties in the end.
74	27-Jul-17	Website	Please consider the full impact of what this project will have on the residents. The homes that will be destroyed, the memories that sill be lost and new memories that will never be made. That is something that canpt be replaced. There is more to lose than to gain from this project.

No.	Date	Forum	Comment
75	28-Jul-17	Website	<p>We are in complete agreement with the sentiments of the letter you have received from our citizen coalition in Bowie and we're using their letter as it could not be said any better. You claim to have made adequate attempts at informing the public and engaging them for comment but the fact is you have failed.</p> <p>As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.</p> <p>Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner. In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process. According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.</p> <p>The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process. Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.</p> <p>Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities</p> <p>No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel</p> <p>Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City. Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.</p> <p>Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically: 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment." 40 CFR §1501.7, Scoping states that "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action." 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make diligent efforts to involve the public in preparing and implementing their NEPA procedures. (b) Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents so as to inform those persons and agencies who may be interested or affected.</p> <p>The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. But that is only the minimum requirement..." and scoping must have "appropriate public notice and enough information available on the proposal so that the public and relevant agencies can participate effectively."</p> <p>In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>We await your response to these requests</p>
76	29-Jul-17	Website	<p>Based on information presented at the Bowie City Council meeting, recently, it is apparent that this project is a partnership with a losing/unsuccesful company in Japan that is looking for a bailout to prolong its operations. It is losing money in Japan. The Japanese co. is facing several litigation's from Japanese citizens. Two similar trains in Europe have either been shut down or are being phased out. Only affluent people/corporations will be able to use it due to exorbitant fares. It will do little for Maryland. Property values along the routes are already being negatively effected along and in proximity to proposed routes due to the specter of eminent domain. Pending residential sales have already been canceled because of the specter of eminent domain. By the time eminent domain occurs, property values will have been substantially reduced resulting in substantial loss to families and businesses. State and local politicians should be aware that they will lose thousands of votes in upcoming and future elections if they allow any part of this development process to proceed, including the environmental study. Funds for such study should be put to better use and not squandered on a flawed (see above stated failures) project that on the face is doomed for cancellation.</p>
77	29-Jul-17	Website	<p>I hope this project goes through. Odenton, Piney Orchard and Chapel gate are behind the times. There is a need for transportation in this area, and Chapel gate needs to be cleaned up.</p>
78	29-Jul-17	Website	<p>Homeowner in the Rockledge section of bowie, and just became aware of the proposed routes of this project. I cannot see how losing our beloved home and neighborhood would be anything but a life destroying event.</p>

No.	Date	Forum	Comment
			<p>As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.</p> <p>In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>Despite the fact that the MAGLEV project will require the demolition of homes and destroy green space, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. We have spoken to small business owners, some of whom are of minority ethnicity, along the proposed lines and they had no idea about the impact of the Maglev project on their businesses. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project. Other agencies including the Maryland State Highway Administration routinely send thousands of postcards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner. In addition, the copy of the postcard shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process. According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we, or our neighbors, been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.</p> <p>The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process. Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.</p> <p>Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities</p> <p>No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel</p> <p>Postcards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City. Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.</p> <p>Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically: 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment." 40 CFR §1501.7, Scoping states that "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action." 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make diligent efforts to involve the public in preparing and implementing their NEPA procedures. (b) Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents so as to inform those persons and agencies who may be interested or affected.</p> <p>The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. But that is only the minimum requirement..." and scoping must have "appropriate public notice and enough information available on the proposal so that the public and relevant agencies can participate effectively."</p> <p>In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>We await your response to these requests. Regards, John and Kathleen McGinnis, 12105 Raritan Lane, Bowie, MD 20715</p>
79	31-Jul-17	Website	
80	2-Aug-17	Website	Just want to be kept up to date on project developments.
81	3-Aug-17	Website	<p>Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, no one in our community has been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place.</p> <p>In light of your failure to properly notify the public that will be the most affected by this action — resulting in the gross destruction of our quality of life — we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p>
82	4-Aug-17	Website	Comment: I am against this proposed transit line. Please keep me informed on how I can stop this.
83	7-Aug-17	Website	please put me on US mail mailing list for information and meetings
84	7-Aug-17	Website	I'm sure you know that there was a poor job on your part in notifying the residents of the communities that will be impacted. Most of us knew nothing of this until the the past few weeks. This will destroy alot of communities in the path, including mine .
85	8-Aug-17	Website	please open the scoping process
86	10-Aug-17	Website	How can you possibly justify running route directly through the middle of a family community such as Piney Orchard? One of your proposed routes passes directly over my house which will likely kill its value and make it impossible to sell. How will I be compensated for this? I'm outraged.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
87	14-Aug-17	Website	You tell me when the meeting are going to be held but not where! I want this train to back out of my Town and I will do everything in my power base, which keeps growing, to see that this happens. Anne
88	15-Aug-17	Website	GOOD Afternoon, I am a resident and Councilwoman of the Historic City of Glenarden, Maryland. I moved to Glenarden 58 years ago. I love Glenarden dearly! I am very upset about the MAGLEV Project. It is my opinion that this project is not necessary!!! It will be the project of devastation and of loss to many. The lack of concern of thousands of people loosing their homes, decrease in home value, demolishing towns, increased taxes , exposure to magnetic fields and health problems (cancer, heart/pacemakers ferromagnetic implants, implants of electronic devices), the level of radiation, noise pollution and the overall concern for people. There is no regards to tearing down historic towns, buildings and landmarks. We, the United States do not need this type of transportation. NOT AT ALL. Who can afford to ride it and who can afford to maintain it. I am understanding a loan will be made with Japan. So, will Japan eventually begin to own states here in America because of stupid investments. The thought of this monster tearing through Prince George's County and other counties in Maryland is devastating. Not to mention these counties and cities are not even benefiting from the project. Forcing, tearing, taking over, violating, hurting, selfishness.....sounds like a form of RAPE to me. Yes, I know you can see that I am angry. In addition to what I have said, what frustrates me most is that I don't know what I have been or better yet, where your group have been in relation to me receiving this information when it first came about and/or your group getting the word out. I really want this project to not only be stopped but to cease to exist, however for now I am asking that the scoping process be reopened because: - I did not receive information on the scoping meetings. - I have not been given the opportunity to comment on the scope of the EIS. - I am formally requesting that the scoping period for the SCMAGLEV EIS be reopened. Angela D. Ferguson Councilwoman of Glenarden, Maryland
89	21-Aug-17	Website	NO MESSAGE PROVIDED
90	21-Aug-17	Website	NO MESSAGE PROVIDED
91	23-Aug-17	Website	I respectfully request that Washington-Baltimore MagLev Train Project be terminated before it needlessly ruins the lives of many Maryland citizens. I am speaking on behalf of many in my community would feel that this MagLev Train Project is an appalling waste of valuable tax dollars, similar to the much-ridiculed streetcar project in NE Washington. To spend this amount of money, as well as uproot entire families and communities, when AMTRAK is in desperate need for capital improvements is unbelievable. People will lose homes and property and, with today's escalating real estate market, will not be able to fairly replace their homes even with compensation from eminent domain. This will displace and possibly cause homelessness for many families. It is easy to approve something that is going to affect "other" people. How would you feel if this came through your and your family's neighborhood? We urge you to prevent this wasteful and destructive project from moving forward. Thank you for taking our heartfelt concerns into consideration. We'd gratefully appreciate all of the help from EACH of you.
92	26-Aug-17	Website	I recently found out that this project was in existence. I live in the areas affected and never once heard a word that my neighborhood would be compromised. I am, as well as others in my area, are highly against such a project.
93	27-Aug-17	Website	NO MESSAGE PROVIDED
94	29-Aug-17	Website	I live in Bowie. My neighborhood is between the yellow and green alignments. Either of these alignments impacts my property values and the environment within. The State has failed to provide my neighborhood any notification yet the scoping plan is now closed. This is unacceptable. I nor my neighborhood will benefit in any way by the SCMAGLEV system. Period. Does the Governor have any responsibility to notify residents directly in the path of alignments about plans that will affect them so adversely? Why was our entire neighborhood of 90 + houses not included in any Scoping feedback? We have been kept in the dark, while the planning continues on. Even as late as August 2017, we have not received any Government notification. Word of mouth is not notification. This has all the appearances of a smoke screen project. I will meet with the board of Directors of our HOA in September. I will hand out names of Representatives in Government, including MTA reps. I will make sure to emphasize the Governor's inaction on the part of our concerns. The time has past for any expectation that our concerns are on the Governor's mind.
95	30-Aug-17	Website	NO MESSAGE PROVIDED
96	30-Aug-17	Website	We need information concerning communities that allegedly will be acquired via eminent domain.
97	30-Aug-17	Website	If this Maglev project is approved, will homeowners that live along the proposed route be bought out of their home and property?
98	30-Aug-17	Website	I was wondering what the probability of my house being impacted by this mag lev train actually is? I live at 654 Donaldson Avenue Severn MD 21144.
99	30-Aug-17	Website	Regarding the maglev project, I am not sing the benefit to the local communities, and, as a Bowie resident, am concerned about the impact on the character of the city. I would prefer to see investment in a Moe frequent MARC service between DC and the surrounding area. Thank you
100	2-Sep-17	Website	I hope your project is successful. When is this supposed to happen?
101	2-Sep-17	Website	The website states that there is an "Alternatives Meeting" that is upcoming in Fall 2017. It is now approaching Fall 2017. Can you please update this information with specific dates and locations for these "Alternatives Meetings"?

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
102	4-Sep-17	Website	Hello, I just received information about the SC Maglev train that may be built in my neighborhood (I live in HighBridge Estates).The reason my family and I moved here three years ago is because we love the community feel of Bowie. It reminds me of my hometown (Lynchburg, VA) and Bowie has such a rich history. If you build this train, not only are you taking away homes from people who work hard and raise their families, but you are ending a community that keeps Bowie alive. Please if you have any compassion, please do not let SC Maglev come to our neighborhood or Bowie. Thank you, Rahnesha Mitchell Bowie, MD
103	5-Sep-17	Website	I live in the Rockledge neighborhood of Bowie, MD. My house would be one taken if this project actually happens. I have just one question, why do we need this train when we already have amtrak which gets me to NY in 3 hours or the high speed direct train that will get me to NY in 1 1/2 hrs. I used to commute from DC to NY every week for 4 years and can't figure out why I would need to be in NY in 20 mins. So please give me a reason for this train because I really don't see the advantage. The only thing I see is this is a new toy for some politician. I see no benefits for anyone in the effected areas between Balt and DC. Not to mention from what I know PA, NJ, or NY aren't on board so now I ask why does someone need to get to Balt in 15 mins. from DC. This train DOES NOT benefit anyone in Bowie since we would have to drive to DC to get the train, NEWS FLASH I can drive to Balt just as fast. So again why do I need this train?
104	6-Sep-17	Website	I am a resident of Bowie, Md. I am totally against this "train" to nowhere. It doesn't stop anywhere in Prince George's County. The vast amount of environmental and socio economic destruction alone should stop this project in its tracks - no pun intended. NO ONE at any of the meetings - 3 of which I have personally attended, has ever presented anything to address the financial cost of the entire 40 mile project or the ability of this high speed train to support itself. It will not. Your NEPA process should come to the immediate result of NO BUILD, and stop this ridiculous misuse of public funds. Thank you.
105	8-Sep-17	Website	I was at Thursdays meeting at Charles Flowers High School. We live at 6913 Emerson St Hyattsville Md and like Bowie,Glenarden, Hyattsville does not want a MagLev train taking any property in our neighborhood. So I vote NO to a train. Please understand we worked 40 plus years to make a home and have something to leave our children. Thanks
106	10-Sep-17	Website	What have you done in concerns to outreach about citizens/home owners that will be impacted by the development of this high speed rail that targets the rich and the wealthy living outside of these major city hubs, but not the residents that it directly affects. How about those, like myself, that maybe displaced because of it. We moved into our first home in Odenton, MD last year in April 2016 and I have not heard anything about this. What benefits does this have for any of the people between DC and Baltimore other than lost homes, a monstrosity in someones backyard, noise? and by the sound of it Maglev isn't so successful so all this could be for nothing, and be left to waste. Would love to hear from you.
107	12-Sep-17	Website	My home and my community are in the path of the MAGLEV project, and I have not received any information from MTA regarding the project. Neither I nor my neighbors have been given the opportunity to comment on the project or participate in the NEPA compliance process. I am formally requesting that MTA reopen the scoping period for this project.
108	13-Sep-17	Website	Our Washington Metro system is in need of extensive repair and upgrading. This will require millions of dollars and there is no dedicated funding for the system. Before any money is spend on this fantasy rail project I recommend any funding be spent on the Metro which serves many more people every day . Lets fix what we have before we embark on new projects. This is consistent with current government policy which projects billions in spending for infrastructure upgrading. My second comment relates to possible route alignments. If the project does move forward the most practical route is the center strip on the Baltimore-Washington Parkway. No land need be purchased and no homes are disrupted. A similar alignment route is now used in the Metro silver line out to Dulles airport. Thank you for your consideration.
109	14-Sep-17	Website	The proposed train system operating between Washington DC and Baltimore MD will utilize a high-speed superconducting magnetic levitation (SCMAGLEV) vehicle. The trip between Washington, DC and Baltimore, MD (with an intermediate stop at BWI Airport) has been proposed as taking a time of only 10 minutes. In this 40 mile trip, how does the train mitigate against breaking passenger's necks during starting and stopping time periods? Please provide applicable performance and design specifications.
110	14-Sep-17	Website	I HAVE BEEN TALKING TO PEOPLE ABOUT THIS SNEAK PROJECT. I WAS SURPRISED AS TO HOW MANY PEOPLE WERE NOT AWARE THAT THEIR HOMES MIGHT BE TAKEN AWAY. I AM BUYING AND HAVE BEEN HERE FOR 24 YEARS. I AM 75 AND THAT MEANS I MIGHT HAVE TO UPROOT AND FIND A PLACE TO LIVE. I HAVE PUT MY ENTIRE 24 YEARS INTO KEEPING MY PROPERTY UP. I AM SINGLE AND RETIRED BUT BACK AT WORK TO MAKE SURE I CAN LIVE A DECENT LIFE AND NOW I AM BEING PUT THRU THESE CHANGES. I AM ALSO A TEN YR. BREAST CANCER SURVIVOR. I DONT NEED THIS STRESS. I HAVE NO FAMILY IN THE AREA. I HAVE BEEN A NERVOUS WRECK SINCE HEARING ABOUT THIS POSSIBLE MOVE I MIGHT HAVE TO MAKE WHILE MINDS ARE BEING MADE UP ABOUT WHO THEY PUT OUT ON THE STREET. I UNDERSTAND WE HAVE TO WAIT UNTIL MAY 2018 BEFORE WE ARE TOLD. HOW MUCH TIME IS THAT GOING TO GIVE US TO FIND A PLACE TO LIVE. I AM JUST SICK.
111	15-Sep-17	Website	This week is the first I've heard of this actually being considered and in the works. How are you even contemplating taking as many as 4000 homes or more for this. Why, since I am apparently one of those homes, have I not personally been reached out to? This is my home since 1986. it's paid for and I intend to be able to retire here. You don't get to just dream up something as crazy as this and decide to "make it happen"...
112	16-Sep-17	Website	I have just learned of this proposed project and want to know why residents that could possible be effected by this have never been notified. I live on Rita Drive in Odenton and it appears my home is in the path of a couple of the options. This could possibly also effect my sons home on King Malcolm in Odenton and my daughters house on Monterey, also in Odenton.
113	18-Sep-17	Website	Stop the train!!!
114	19-Sep-17	Website	I want to have this train follow existing railroad lines so that it will not impact my home and the homes of my neighbors in Prince George's County. If that is not possible, I will do all in my power to encourage my neighbors to oppose this train.
115	20-Sep-17	Website	What is the ultimate goal of the project since the Olympics will not be coming here, a faster commute to D.C.? I guess that is more important than the displaced families who will lose their homes, the children that will have to relocate schools and the neighborhoods that will vanish. That is what is expected from the MTA, the same agency that has jeopardized neighborhoods with the light rail.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
116	20-Sep-17	Website	What is the time for the MAGLEV Open House in Gambrills? Thanks!
117	21-Sep-17	Website	I think it is absolutely appalling that the neighborhoods that lie directly in the paths of the alternative routes have not received widespread notice of the possibility that their homes might be at risk for the construction of this train. I have lived in Odenton Heights for nearly my entire life and bought a house there less than a year ago and now I'm finding out that I might lose it to a train that is only a small piece of a project that has not even been initiated beyond MD & DC. I am absolutely OPPOSED to this project.
118	21-Sep-17	Website	Please tell me what the format of the October 2017 open houses will be. I would like to know what to expect from the meetings. Will there be formal presentations and a question and answer session? Or, is this another set of poster presentations like the April open houses?
119	21-Sep-17	Website	glenarden donot need a high speed coming threw our town.the town is a black town we worked hard for our houses i am 66 years old where do i go!
120	22-Sep-17	Website	My name is Regina Carroll and my family and I live at 7933 Dellwood Avenue, Glenarden Maryland. I am writing in regard to the MAGLEV project that will affect 1,465 homes in Glenarden according to a flyer that was distributed at a recent meeting. I have been a resident since 1966 and WE DO NOT WANT TO LOSE OUR HOMES. I have been to a meeting at Charles Herbert Flowers on Sept. 7 which was orchestrated by Senator Benson and Glenarden Municipal Center on Sept. 20 which was orchestrated by the Glenarden Council members. We DO NOT WANT THIS TRAIN TO GO THROUGH OUR TOWN. This study has been going on for a long time and the citizens of Glenarden are just hearing about it. We are highly offended with the way the information has not been made public to the citizens. Glenarden is a black historic community in Prince George's County and we do not want it demolished for a MAGLEV train. This will not be an asset to our community. We are highly against it.
121	22-Sep-17	Website	hummmm Why are there no scheduled meetings in the BWI region????
122	22-Sep-17	Website	1.What funding is the Japanese Government or JR Central providing? There have been conflicting reports over time. 2. The Governor announced a plan to acquire rights to build on the Baltimore/Washington Parkway. Yet a route alignment is not indicated in the BWRR proposal for the Parkway. If there is room for a four-lane addition on the Parkway why is this not a suitable route for a project such as the SCMagLev? Geographically the train requires straight runs and subtle curves. This highway has been historically a green Parkway. If that is going to change, it may serve the FRA better for this train than to add a toll road. Relieving traffic congestion is a stated purpose for building the Washington/Baltimore SCMagLev.
123	23-Sep-17	Website	What is the target date for the exact route? What eminent domain acquisitions are planned and what is that target date? Regards, Terry
124	25-Sep-17	Website	It is our understanding that POCA (Piney Orchard Community Association) has submitted their opposition to this project and that they have represented themselves on behalf of the 4,000 + residents of the Piney Orchard Community. PLEASE be advised that POCA has never contacted any of the HOAs within their umbrella and that no feedback has been sought. As the V.P. of the HOA (The COURTS) representing 198 homes let me state for the record that we do not join them in there position and that we have not taken a position on this matter as of this date. They have misrepresented their authority to speak for us. Ron Grossman, HOA, The COURTS - VP
125	25-Sep-17	Website	Please help me understand how both parties (homeowners and Maglev) gain. As a homeowner in Glenarden Maryland, I do not gain from a speed train going through my neighborhood. This is where I live and I will not be relocated due to a few people thinking that it benefits them wanting to travel to New York in an hour. I believe I speak for most if not all the residence in my community when I say I will not allow this structure to be built in or near the town of Glenarden. There is no gain in it for me only stress, heartache, and confusion. All of which I want nothing to do with at this point in time. I believe in God and I have prayed to the Most High God That this project be re-routed from my community. For the eyes of the Lord are over the righteous and his ears are open unto their prayers. 1 Peter 3:12 God says, My people will live in peaceful dwelling places, in secure homes, in undisturbed places of rest. Isaiah 32:18
126	26-Sep-17	Website	As a citizen of City of Glenarden and a home owner I'm against the Maglev Train from being considered to running thru Prince George's County especially thru or near the City of Glenarden. This train will not stop anywhere in Prince George's County. If the two routes that impact the City of Glenarden are chosen then we would lose homes, businesses, churches, library, parks, recreation center and the newly built Veteran Memorial Park. The City of Glenarden is a Historical Africa American city with a rich history. This decision who destroy the city.
127	28-Sep-17	Website	I am wondering this project make any sense, in term of price. Currently, it only take 30 mins to commute from Baltimore to DC using Amtrak. Does any commuter take Amtrak at all due to price. Commuter paid around \$280 from Baltimore to DC a monthly. How much are you going to charge \$500/month? Who will pay this price to ride? All the construction and impact on housing prices near the proposed route, especially, E,H and G.I. Politician really has to think twice; does it worth the price and cost of commuting? Tax payers eventually bail out the project?
128	28-Sep-17	Website	We have never been formally notified about this project, although our house lies directly in the path of one of the proposed routes! How dare you make decisions about proposed routes that include my home without ever having notified me! I only found out my home was in the path of a route by accident while speaking to someone informally. You should have covered our neighborhoods with leaflets and posters, and sent notifications by mail! Building this track would devastate whole communities and neighborhoods, destroying homes and lives of thousands of people. Bowie itself is a special place, with interconnected neighborhoods, and spanning generations of friendships. Health concerns and environmental concerns are also of paramount concern to those who are left. Citizens bearing the negative impacts of this proposed project have the right to be properly informed about the proposal, and included in preliminary planning, which you completely ignored.
129	29-Sep-17	Website	I am 100% AGAINST the MAGLEV train proposal. The scoping process should be reopened since no one in my community was informed before the scoping process ended. The train is slated to destroy much of Odenton. It doesn't make financial sense to our citizens. A dedicated rail for the next generation of Acela makes a lot more sense for everyone.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
130	29-Sep-17	Website	I would like to understand better what routes are being discussed and how I can see what neighborhoods will be affected
131	29-Sep-17	Website	Hello, once again I'd like to express my opposition of this train coming through Prince Georges County and particularity Glenarden, MD and my concern around the lack of communication and transparency. This project is adversely impacted and takes advantage of African Americans, seniors and many low income residents of Prince Georges County. How can you in good conscience rip through our county, tearing it apart and offer no benefit to the residents. This project and the handling of it has been unethical and morally wrong considering the inaccurate, misleading and false information that is being communicated. At the the upcoming October 14th meeting, I am requesting that accommodations be provided to support our seniors. Specifically, shuttles or golf carts to transport them to the Student Center. As you know, many of the Glenarden residents that came to the meeting Charles Flowers High School were seniors that were using walkers and canes. They will need help getting to the building. I look forward to your support to our seniors who want to come out and express their thoughts on how this project will impact them. Thank you
132	30-Sep-17	Website	Dear Sir, I am writing in regards the construction of the MagLev Train. I am NOT in favor of this project coming to fruition due to the fact that it will not only destroy the City of Glenarden, but its surroundings as well. This train project is in no way beneficial ecologically OR economically to the City of Glenarden and its residents who have been here for more than 30, 40, 50, or 60+ years. The fact that residents within the City of Glenarden were not notified of this project is an insult at its best. Instead of trying to demolish a historically black community, why not use your project to help fix up the Metro system and/or Amtrak, which by the way runs their railroad system via the northeast corridor. It will behoove MTA to carefully think of what they are determined to do to a black community just to make a dollar. Thank You.
133	30-Sep-17	Website	SCMAGLEV project team - As a African-American citizen of Glenarden, MD, I do not support the SCMAGLEV train and the proposed routes. The SCMAGLEV train would service Washington, DC, BWI airport, and Baltimore, MD without any stops in between. Virginia, Washington, DC, and Maryland have been recently identified as the worse areas for traffic congestion; however, the purpose of the SCMAGLEV project would not help alleviate any existing or future traffic congestion in these areas. The SCMAGLEV travelers would pay "the same cost as an airline ticket" for daily travel, which is not the daily travel audience for Virginia, Washington, DC, and Maryland roads to get to/from work. Additionally, two of the proposed SCMAGLEV project routes would directly impact me because I live next to the "existing rights-of-way" Governor Hogan stated he would use for this project. I suggest the project team take a scenic drive along the existing rights-of-way to see how this project would really destroy the natural and scenic environment as well as communities and homes. MDOT and MTA should use the EIS funds to help improve existing mass transportation for the larger population of travelers who walk, ride buses, ride Metro, ride Amtrak, and ride the MARC train. I support the NEC Future project that supports the real daily travelers and not high income millionaires and billionaires. I do not believe the maglev technology is proven technology, just like the Japanese Takata airbags with all of the airbag accidents, car airbag replacement recalls, and the Japanese Takata company filing bankruptcy; leaving someone else holding the "bag". Maryland is being offered a technological unicorn that tax paying Marylanders could potentially have to pay the bill. Stop the SCMAGLEV train from coming to Maryland.
134	3-Oct-17	Website	Because I cant get to the meetings, I am submitting my comments and concerns. I have been living in this house for 24 years, I am 75 and retired on a fixed income. I retired from IRS IN 1974. I live alone and I have no family in the area. I have been fixing my home up for years to make it comfortable for me. Now tell me MDOT where am I suppose to go, if you come and take my home. I am a 10 yr. breast cancer survivor and all of my doctors are in this area. Why are you doing this to us. It is such a ruthless thing to do. From what I have read you picked the most low-income and mixed areas to run your train. I don't care how much money you give me for my home (which will probably be lower then what it is worth), I don't want to go into another 30 yr. mortgage. You have not thought about the lives that will be affected. All you can see is money that you might or might not make on your project. What is being done is thoughtless and mean. I have worked so hard to keep a roof over my head with the little retirement and S.S, I get and now you want to take it away from the homeowners in this area. That train will only be available to the rich. I want to see if I get any materials from your meetings. So far I have been getting nothing BUT lies and unreturned calls and emails from people in our District.
135	4-Oct-17	Website	Is the technology planning for nationwide implementation to make train travel more efficient (similar to Europe, for ex.) and between city to city (DC to Atlanta)? Current train travel in the US is very slow and antiquated. If the rail industry wants to compete with the airline industry, this must improve with new technology as the US lags far behind Europe . Thanks.
136	5-Oct-17	Website	I am writing in opposition to the Maglev high-speed train that is proposed to run from Washington to Baltimore and run directly through my neighborhood. This project would significantly disrupt our neighborhood and/or cause my family to lose our home. Thank you for your consideration of my concern.
137	5-Oct-17	Website	1
138	7-Oct-17	Website	How come I and everyone I know, never heard about the 2016 Scoping meeting?
139	7-Oct-17	Website	I am opposed to having the Maglev train routed through Glenarden and many of the other surrounding communities being considered. The proposed routes will impact about 17 municipalities that have been in existence for more than 50 years.
140	8-Oct-17	Website	Why is a route directly along 295 not being evaluated? You already have the right away and infrastructure and it does destroy communities.
141	9-Oct-17	Website	Our home in Coldwater Reserve Crossing is directly in the path of the yellow route alternative for the MagLev. If that route is chosen for this project, what happens to our home?
142	9-Oct-17	Website	what is and why should there be a SCMAGLEV Project. Does it affect parts of PG COUNTY or the citizens who live in the city of Glenarden.
143	10-Oct-17	Website	This is the first I'm hearing of this and I am afraid it will negatively affect our community

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
144	10-Oct-17	Website	This project is ridiculous, and it must be stopped. Why would anyone think it's a good idea to destroy our already developed communities?? The only benefit is to the builders. Those of us who live here don't reap any reward. We're punished with disruption to our lives.
145	11-Oct-17	Website	need more information
146	11-Oct-17	Website	Since the financial viability of this train is directly linked and dependent upon the development of the rest of the corridor how can you proceed in direct violation of the EIS/NEPA guidelines for linked projects?
147	11-Oct-17	Website	I do NOT support SCMAGLEV. This project will devastate homes, the environment, our tranquility. The cost to build and run the program will hurt our state and property tax revenues as well as driving down the values of our homes (if they survive being destroyed/replaced by the train route).
148	11-Oct-17	Website	I oppose the bwmaglev project.
149	11-Oct-17	Website	What is the exact schedule for Mondays meeting? I only ask because I can not be there at 5pm. Maybe 6:40 - 7pm.
150	12-Oct-17	Website	I would like more info on the path of the maglev as it unfolds.
151	13-Oct-17	Website	I would like to understand what it means by the "NEPA Scoping Process is closed" - and if this is in fact the case? Where in the NEPA process is this project in terms of the opportunity for citizens to submit written comments and objects AND for those comments to be considered in the project review / approval process? Please respond back to me asap as I intend to draft and submit comments. I would like a full NEPA process timeline to be sent as well. Thank you, Laura Riggs
152	13-Oct-17	Website	I think the Maglev is a great idea for the Bowie area, but I do want to make sure that the existing WB&A bike trail that runs in the spot where the train will go can be moved to run parallel to the train. Lots of people in our area use the WB&A. I know Virginia is good about keeping their bike trails when they add new freeways/metro. So I think Maryland can do the same for this project.
153	13-Oct-17	Website	I'd like to request a list of each and every address, both residential and commercial in the 21113 zip code that would be slated for destruction by the construction of the SC Maglev project. Also, separately, I would like to request a list of all other addresses, roadways etc that are known to be affected by the construction in the 21113 zip code. Please provide this list and related information in an excel format and send via email. This request is submitted on 10/13/17.
154	13-Oct-17	Website	Please don't build this. It will split my community in half, and is made irrelevant by Musk's planned transportation system for the eastern coast.
155	13-Oct-17	Website	None of the links on your web site work, except that to "Contact Us" and "FAQs"
156	13-Oct-17	Website	My wife attended our HOA meeting last night and was told that our community of West Brooke in Severn, MD would be impacted by the new train system. This caused my first of many sleepless night if this is true. I hope this project stops right where it is because I really would hate to move at this point. When a project like this comes along and home owners are forced to sale, it never works out for us financially. Amtrak ticket prices are already to expensive and now you want to bring another high cost means of transportation to the area that people can't afford to use. I plan on attending the next meeting in my area so I can get a better understanding of this project.
157	13-Oct-17	Website	It is very concerning, that these alternative routes would (if built) have such an adverse impact on such a vast number of single family detached residential homes. Is this mode of transportation worth, the displacement of so many family homes?
158	13-Oct-17	Website	This whole project is disgraceful. There has been little concern shown in advance to the people impacted. I am on the blue line and it seems your intent is to plow right through my house and others and cut up Bladensburg. Have you given any thought to where people are going to move after you give us "fair market value" for our homes and businesses? Many of us will not be able to afford to sustain ourselves in other locations because we have been here for a long time. Prices to rent or own have gone up markedly. We do not need this train and it will not help our transportation needs. I thought our governor was already taking care of that with the multiple road widenings and the Purple Line. This is only a pretty, shiny Jetsons' plaything for our government to tout and to serve the more affluent in the community. What kind of deals are you cutting with Mr. Trump and other big companies so you can profit off the backs of the regular people?
159	13-Oct-17	Website	Stop this train on any of the routes. It is unnecessary with all of the other road widenings and the Purple Line. We do not want to and cannot afford to move/live elsewhere. The blue route where I live cuts through a populous area and historic areas as well. The whole affair smells of avarice and greed.
160	13-Oct-17	Website	The MAGLEV train will not service the area that it is destroying. We already have several alternative rail systems in place.
161	13-Oct-17	Website	Is there some reason the train station in Laurel is not going to be utilized in the proposed rail line? DC to BWI then Baltimore? Need stop midway.
162	14-Oct-17	Website	This project is of no benefit to anyone and is fiscally irresponsible. Despite decades of research and development, maglev transport systems are in operation in just few countries. None of these have made a profit, and the construction and maintenance cost have been prohibitive. In Japan, JR Central states the costs to build their passenger system has ballooned to around ¥9tn including the cost of the trains. Yes, that is 9 trillion yen, or more than 80 billion USD. This will also tear apart communities, and cause thousands of residents to lose their homes. The three routes proposed in October 2017 go directly through 2,000 or more homes and businesses in Prince George's and Anne Arundel County. Please stop this project!
163	14-Oct-17	Website	I and most people in my community and surrounding communities would have no need for this high cost transportation. It would only be for the more financially able needing a daily/weekly trip. I especially have many concerns to the eminent domain issues for this to become a reality.
164	14-Oct-17	Website	Do not put a train through Bladensburg. This will impact many people. Have you ever thought what would happen if the train malfunctioned? Have you taken into consideration how this would affect the people living here. Im positive your fully aware of what happened to the bullet train in California. Many things went wrong and now the price for it has doubled and the legislators that were for the bullet train are now against it. I am 13 years old and once I was informed this was happening I was infuriated because the people organizing this seem not to care. I live at 5507 Doris Court Bladensburg Maryland which is a hop and a skip away from Elizabeth Seton. How do you think the parents of the kids who attend Seton will feel about a train going over 200 mph that runs right above the school. If you build this train human lives may be endangered.

No.	Date	Forum	Comment
165	14-Oct-17	Website	Hello, You guys are awful when it comes to outreach. I have placed my name on the mailing list several times and I still get no emails. Absolutely nothing. So, what is the point of putting my name on the mailing list. So send me something soon so I know there is someone alive at the other end actually taking note of my existence as a member of the mailing list. Or if you aren't going to use it, then take it off and forget it and don't make a show of being responsive by having this capability to mail out when you don't do anything with it. It is indeed dishonest to say the least. Am I being too harsh? No, not at all. Send me something and all will be well in Never Never Land. Use the email system, and do some outreach.
166	17-Oct-17	Website	My wife and I have lived in Delmont Station for over 27 years at 7901 Tresselt Ct. Since one of the proposed lines for the SCMaglev train would run through our property, we are strongly opposed to its construction for the following reasons. 1) Cost - \$10 billion - waste of tax payer's money. 2) SCMaglev technology has not been proven commercially viable anywhere in the world. 3) There would be a loss of property, property value, tax revenue, and habitat. 4) Its non-existent benefit to the average Maryland. 5) The fact that people in my community who work in Washington already are able to catch a train in Odenton and be in Washington in 35 minutes or less. 6) There was also a lack of proper notification about by the Maryland Transit Administration during the scoping phase of the project. Because of this, I request the scoping process of the feasibility study (completed by MTA between November 2016 and January 2017) be re-opened due to insufficient public engagement. For these reasons and more, this train should be stopped in its tracks before it starts.
167	17-Oct-17	Website	I was told at last night's open house meeting that the project video was on this website, but I do not seem to be able to find it here.
168	17-Oct-17	Website	This project will only benefit the few individuals who work in DC and live in the Baltimore area because it is so much cheaper. Those of us who stand to lose our homes will see absolutely no benefit; only financial devastation and heartbreak over being forced from the homes which we worked so hard for. What about the elderly or infirm who will be pushed out? What about those who bought their homes "at the top of the market" a few years ago and will now face financial ruin when they are paid less than what they still owe on their mortgage when they are kicked out? Many of those who may not actually have their home taken from them will see their property values plummet due to their proximity to this unwanted/unneeded "train". The existing modes of transport MARC,AMTRAK, and Acella are quite adequate and more affordable than this white elephant known as MAGLEV will ever be. It is outrageous and IMMORAL to force this on the citizens of MD who will have no use or need for it. Those who do not want it will be hurt the most and those in favor of it will suffer no consequences whatever. No one has the moral authority to expel people from their homes which they worked hard for to benefit a few others who want to save money by taking up residence so far away from their place of employment. If the MAGLEV can be built with ruining anyone's life or community, and without taxpayer funds go for it. Unfortunately it does not look like that is going to happen.
169	18-Oct-17	Website	While I see the congestion of the traffic between the growing, I don't see the need of a train of this magnitude needed. Especially at the cost of all the homes and families it will be displacing to build it. The budget alone to buy out these homes to where the families can possibly afford to go into another house to suit them is high enough by itself. the cost to ride on such a train is far out of so many peoples reach that I don't think it will be worth all the damage it will cost to build it. we definitely will not make the money back on it. The same trains sit over in Europe abandoned and not used in years for this reason. No one could afford to use them. This area is a working class area and we all live pay check to pay check. Most of the time we don't know where we are getting the money for vacation next summer. Most families have a parent or parents working multiple jobs just to get by. I don't think this is a reasonable option for us as travelers.
170	18-Oct-17	Website	I do not like the E1 option , which would put a tunnel right under my house! How do you propose to build this tunnel without destroying the houses in the neighborhood first?
171	18-Oct-17	Website	I am not at all in support of this project. There are many more ways we can improve our current transportation without spending the money this project will cost. This is not going to make Amazon choose MD for it's headquarters. Spend the money fixing our current metro system.
172	19-Oct-17	Website	How will the trains going 300+ mph through tunnels affect animals (wild and domestic) since they are so much more sensitive to vibrations than humans are?
173	19-Oct-17	Website	How far apart will ventilation shafts be? What will they look like?
174	19-Oct-17	Website	How deep will the tunnels be and what is the point of reference be (top of tunnel diameter, center or bottom)? In other words, how much ground will be between the top of the tunnels to the surface?
175	19-Oct-17	Website	In case of emergencies, how can you evacuate people anywhere from 1.5 to 2 miles from emergency access and then up 100 feet from the tunnels? What kind of training will local emergency personnel receive for disasters?
176	19-Oct-17	Website	When will the final route be decided?
177	19-Oct-17	Website	How will this alleviate traffic? Inter city to intercity traffic doesn't alleviate the suburban traffic congestion.
178	19-Oct-17	Website	What is your plan for the train as it cuts through residential neighborhoods? I've heard no discussion about alleviating vibration, noise, ventilation, and other impacts to our quality of life. Have these been thought through?
179	19-Oct-17	Website	I would like additional details regarding tunneling, emergency access to tunnels, eminent domain for land you will bore through (or are you digging a trench and then back-filling). What water tables are being affected and how will you address vibrations in the soil around tunnels (changes to soil densities)? How will you deal with utilities and well water? Thank you.
180	19-Oct-17	Website	I am a Prince George's County resident and would like to have more details about the tunneling process. How do you decide where to place tunnels and what are the risks or possible side effects for homes on top of the tunnels.
181	19-Oct-17	Website	The train is not needed in Maryland anywhere . The train will not be good for our environment , looking for to your response ,

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
182	19-Oct-17	Website	Good Evening, I live in the community of Linthicum. I would like to know how this is going to disrupt my neighborhood. I read a document that this will affect 500 homes in our community. This is not acceptable. How will this affect the Colonial Pipeline that is in our neighborhood? Why hasn't there been a public forum for our community? For the record, I am opposed to this project. Any information is greatly appreciated. Michelle Mills
183	19-Oct-17	Website	This project wastes MD dollars and will not actually address traffic congestion. The No Build Option is the best way to go. Use MD tax dollars for more efficient projects that are already in our long range transportation plan.
184	20-Oct-17	Website	What is the potential impact to the soil and to water tables from underground train line vibrations potentially destabilizing below ground propane tanks such as the ones in all of the backyards in my neighborhood?
185	20-Oct-17	Website	A document I have from TNEM states that "...magnetic fields and potential impacts will be evaluated in the EIS and by the FRA's Office of Safety." When will this evaluation be done? Will it include a thorough scientific study of the effects of the generated electromagnetic fields on people living in close proximity to the tracks? EM radiation is especially dangerous because it is undetectable to human senses, yet very real. What measures will be taken to prove that the radiation from these trains does not cause harm? I challenge the basic premise that high speed travel is necessary. The same TNEM document- which seems little more than propaganda- states "We believe there is a pent-up demand for safe, reliable high-speed travel for all manner of trip purposes, including commuters." Well whoever "we" is, a statement like that is merely an unsubstantiated opinion. I believe our state would be improved not by high speed travel. The SCMAGLEV project is flawed at its core. If the TNEM claims are accurate, then there will be "spin-off industries." That means more people, more travel, and more congestion on the highways. Building extra lanes on a highway does not alleviate traffic issues. You just get more people in the congestion until it gets to the point where, as my father would say, "No one goes there anymore because it's always too crowded." Let's slow this growth and NOT let all of Maryland be swallowed by Baltimore and DC. DO NOT BUILD.
186	20-Oct-17	Website	I was shocked to see the report on morning news (10/20/2017) that Governor Hogan had approved for Elon Musk to bore a ten (10) mile tunnel in preparation for the MAGLEV. I attended a MAGLEV meeting at Arundel High School on Monday, October 16, 2017, and was told by officials no decision has been made and the environmental study, which is required, will not be completed until next year. WHAT IS GOING ON??? I can't believe anyone and my home is in the path of one of the remaining routes being considered. This train will not benefit my community only destroy it to the benefit of a few. The affluent communities with political clout have been excluded from the path of the train and the remaining communities left in the path are small communities without established home owners' associations. Who is looking out for our best interest? Who is telling the truth? My husband and I are nearing retirement and planned to live peacefully in our home until our government gave permission for a multimillion dollar corporation to destroy our peace of mind, reduce our property values and undermine our security. STOP THE MAGLEV and save our peace of mind!!
187	20-Oct-17	Website	How do I get copies of the proposed routes for the SCMaglev as changes occur and what is the best way to stay informed of how it will impact my home?
188	20-Oct-17	Website	Given that the Governor has issued a permit for a Hyperloop project in Maryland, it would seem that your project is redundant. Are you abandoning your plan for Maryland?
189	20-Oct-17	Website	I am OPPOSED to the SCMAGLEV project. Too many details are missing from your proposals and citizens questions are not being answered. It is not worth tearing up established communities for this.
190	21-Oct-17	Website	I am writing to express strong support for the proposed high-speed, magnetically suspended train project from Baltimore to Washington, D.C. with a stop at BWI Airport. Furthermore I would like to call your attention to a technological approach to the project developed by a Maryland resident. At this link you will find a description of U.S. Patent #7617779: "Linear Brushless D.C. Motor with Stationary Armature and Field and with Integratable Magnetic Suspension" (the patent summary is also attached here). You will note that the system proposed is totally non-contacting, providing emission-free and nearly silent propulsion to minimize impact on surrounding right of way. Note also that the inventor resides in Maryland and has made a patent assignment in the U.S. The assignee has made a working small-scale version of this system as proof of concept. You will find that the system described therein employs the latest technology to achieve the fastest and most comfortable passenger ride at speeds comparable to aircraft, but avoids the need for extensive and costly terminal and landing facilities. The routing should be accomplished without intersecting roadways. Crucially, the concept, design and preliminary development of this technology have all been done in the United States. Respectfully submitted, [PATENT SENT IN SEPARATE PDF]
191	21-Oct-17	Website	I live in the Woodlawn area in PG County. I m not for the bwmaglev.
192	21-Oct-17	Website	There is already the Marc and Amtrak Train that goes to DC and NY. There is not a need for a third train to do what the other 2 trains are doing now.
193	22-Oct-17	Website	This type of development will add more noise to the community. And it will lower property value in PG and AA Counties. We are committed to stopping this train!
194	22-Oct-17	Website	As a private citizen who owns a home within one of the proposed paths of MAGLEV, I very much oppose the train. My understanding is that this train would only serve higher income riders going between Baltimore and DC and not relieve traffic in any significant way. Furthermore, I understand that all profits would benefit a private company whereas costs would fall onto the Maryland citizens for annual maintenance.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
195	24-Oct-17	Website	<p>Hello-</p> <p>I would like to add my voice to the many that are opposed to bringing this train into the area. I appreciate the efforts that have been undertaken to educate us about the potential benefits and impacts of this plan coming to fruition. As a result I continue to resist the notion that the proposed train and the 3 alternate routes for its location is a good idea.</p> <p>The true environmental impact is not clearly stated, including the impact of vibration from tunneling and the effects on ground water tables. The ability for our area to handle the construction traffic and safety needs during construction and operation is not clearly defined. Nor is the impact of an electro magnet field ever discussed.</p> <p>Further, aside from the privately funded cost of the train the associated costs of construction and eventual operation of the train are not clear or the impact of those costs on our community.</p> <p>We have a public transportation system in place from Baltimore to DC and although the proposed system will be faster, the actual benefits in NO WAY justify either the monetary or environmental impacts on our community or the disruption in quality of life of our residents.</p> <p>I STRONGLY oppose the Maglev coming to our area and will continue to fight it in any manner possible.</p> <p>Thank you for taking my comments in consideration and know that one person that takes the time to comment represents a far greater number that actually feel the exact same way.</p> <p>Cordially, Kathy Hayes</p>
196	24-Oct-17	Website	NO MESSAGE PROVIDED
197	24-Oct-17	Website	Not a fan of my property value going down or worse being forced out of it.
198	24-Oct-17	Website	I have intentionally avoided working and even traveling to Washinton, DC because of traffic congestion and metro transit issues. I would seriously consider working in and traveling to DC unrelated to work if there were a reliable alternative to driving or metro
199	24-Oct-17	Website	Please update your Public Meeting Materials with the posters that have been presented at the last several open houses that are not currently on the website. There are at least 7 missing posters.
200	24-Oct-17	Website	I would like to know why all of your communications have only been in English. We need to have ALL INFORMATION in English and in Spanish. A large part of the community being affected is the Spanish community. I have been trying to explain to some of my neighbors exactly what is going on and they were not aware of the scam being put upon us. Stop trying to hide it from certain people. We need everything in both languages. You put it in Spanish and I will make copies and send them out to Riverdale neighbors. If you don't do this then I will make sure it goes public. Enough is enough.
201	24-Oct-17	Website	Please provide Spanish translations for all of the posters you have on the october-2017-open-house-boards page along with any other material that you produce including the 7 missing board from this page.
202	25-Oct-17	Website	<p>Good afternoon</p> <p>My name is Kimberly Sneed and I've been a home owner in South Laurel for over 20 years. Montpelier Hills is the development where my lovely town home is located. It's a diverse community of tax paying voting residents who are deeply troubled by your proposal to possibly build a high speed light rail displacing over 200 residents for a project that won't benefit Laurel or any of the other proposed routes at all.</p> <p>What are you going to tell the first time homeowner who just bought their first home on Blue Moon Court last year after scrimping and saving to live the American Dream?</p> <p>What are you going to tell the the homeowner who is rebounding from the mortgage crisis and currently owes more on their home than the home is worth?</p> <p>What are you going to tell the empty nester who raised their kids, sent them to college and now hoped to retire and remain in Maryland?</p> <p>Hopefully this train won't be built anywhere BUT if you MUST build it, it should be built where there is NO residential homes. Have you thought of what happens in a terrorist-incident or accident? An entire community could be impacted.</p> <p>In closing I hope you will reconsider your plan and leave the residents of South Laurel alone. We have already endured the mortgage crisis in disproportionate numbers. Even if our homes are not destroyed the value of the property will decline due to the nature of a high speed rail so close to the homes -- not to mention potential health issues for people wearing pacemakers and/or hearing aids.</p> <p>Thank you. It's not too late to reconsider.</p>
203	25-Oct-17	Website	Where can I make a formal comment or send a letter for my feedback/comments? I did not have the chance to do it at the presentation yesterday 10/24 in Laurel High School. Your help is greatly appreciated.

No.	Date	Forum	Comment
204	26-Oct-17	Website	<p>superconducting maglev was invented by Drs. James Powell and Gordon Danby in 1966. Since then their system has been demonstrated by Japan at the Yamanashi Test Facility and Powell and Danby have developed an improved design which is more capable than the design demonstrated by Japan. The more advanced SCMaglev is capable of operating in a planar mode as well as a monorail mode. As a result it is capable of electronically switching at high speeds making it much easier to switch off line to passenger stations and parallel guideways in freight terminals. The new magnet design also is capable of lifting much heavier loads and is ideal for carrying freight trucks and delivery vans in roll-on, roll-off Maglev carriers, as well as passengers, at average speeds of 300 mph. We believe that this is the ideal system for the US to build out its heavily traveled interstate/intercity corridors and will eventually be the basis for a national network. See www.magneticglide.com for the concept. We have envisioned a system a 25,000 mile long network that could be built without public funds that could finance itself from lower cost fares and lower cost freight truck shipping from producer locations. Dr. Powell and Danby's story and vision has been published in The Fight for Maglev and Maglev America, available on Amazon. SCMaglev has great potential as a new industry in the US and would be a job creator. In Maglev America we describe a commuter subway Maglev system that could convert our steel wheel rail commuter systems to Maglev. Without rails and very efficient electric power requirements this system could carry commuters at much lower fares than the rail systems currently in use because the operations and maintenance costs are much cheaper. We recommend that the Maryland Transit Administration consider competing the Powell and Danby Maglev 2000 system with any other contenders. You have our permission to release our request to the press and media.</p> <p>James Jordan Executive VP Maglev 2000</p>
205	27-Oct-17	Website	<p>Why our neighborhood? And it will cost us more to move than to stay, most of the families been here over 30 years, and or, on a fixed income, why this project can't be placed in one of the more upper class neighborhood's. I'm not moving and we all feel the same, this Riverdale area where you all want to up root us is all we know. Please think about what and why you all are doing this, the bottom line is money. There is no affordable housing out here period, all the low income housing isn't fit for a dog to live in, you all want to spend a substantial amount of money, where here's a proposal, spend the money on building more affordable housing for low to fixed income families, in decent neighborhoods.</p>
206	29-Oct-17	Website	<p>Do you really know how many people this is going to affect? Can't you guys get together and improve what is all ready here? Why must you do what other countries are doing? We have a lot going for us. Let's take care of what is here. Please give this an honest thought. Think about the people.. Who can really afford to ride something like this. Thanks for letting my voice my opinion</p>
207	30-Oct-17	Website	<p>As a resident of Halethorpe, MD (Baltimore Highlands neighborhood zip code 21227), where this route is scheduled to be built near, I would just like to formally issue my support for this project and have a public comment on record in favor of the superconducting MAGLEV Project. From my understanding, a lot of the construction will be completed underground and the routes proposed look to avoid most major residential/commercial areas. I support advancing the public transit options of this state and country and hope that this project moves forward. I take trips to both Washington D.C. and New York and my quality of life would benefit greatly from reduced travel times. I support the development of public transit as it reduces the need for more roads and increases habitat and species protection, while allowing me to personally reduce my own carbon footprint by taking a shared transit option that is convenient. This project will help put the U.S. on track with other advanced transit systems in other countries and help us remain a competitive region of the world by attracting businesses and residents.</p>
208	31-Oct-17	Website	<p>Have lived on Longcross Rd. in Linthicum since 1953 and Made future plans for my family Members. I have recently been informed of plans to construct a high speed rail system in the vicinity of my property and I am Very much concerned!</p>
209	2-Nov-17	Website	<p>Comment: I am completely opposed to the SCMaglev project proposal that a potential route would run directly through, above, under, or over, Severn, MD. How many homes would be impacted in some way, how many schools, how many recreational areas? It is ridiculous to think that the impact on residents would be minimal regardless of where the routes are located, but in an area like Severn where there are mostly middle class families, and many senior citizens who have resided in Severn for 30+ years, it would be difficult for those impacted to relocate to affordable housing elsewhere in AA County. The area of Severn where the proposed route will travel is quiet, picturesque, and relatively safe. Bringing in the SCMaglev project will bring disruption via construction problems, traffic congestion, workers who don't give a hoot about the people who live in the area, and more than likely a plethora of safety issues. We already live with the noise from the planes from BWI flying over our homes, and the rail trains that go up and down the tracks along Rt. 170 - now you want to add to that with another train? I've lived in Severn a very long time and lived in homes that have stress fractures from those rail trains along Rt. 170. The SCMaglev will also cause disturbances to structures along the routes, as well as wildlife, and the entire natural environment. How will the process impact our ground water? How will the process impact the flooding we sometimes experience because over the past 30 years little has been done to improve rainwater run-off as more and more non-pervious surfaces are constructed with the building of new homes, shopping areas, etc?</p> <p>Many communities in Maryland with much greater (financial) resources and political clout than poor old Severn have already fought against SCMaglev and routes in those areas have been removed from the proposal. Don't take advantage of an area that has fewer political and financial resources to further your own agenda, especially if you don't live in or near the area you're considering. Ask yourselves: Would I want my child to attend a school where the SCMaglev will travel underneath? Would I want my elderly parents to live in a home where the SCMaglev will travel above or under? Do I want my kids who play sports on fields at Severn-Danza exposed to any potential, long-term effects from the MAG (magnetic) in MAGLEV? Do I want my property values to plummet, and when the state/county or whomever comes through buying up our properties for SCMaglev we lose the equity in homes that for decades have been lovingly cared for?</p> <p>When I initially heard about this project, before I saw the proposed routes, I was sure that some common sense would prevail in designing a system of transportation that would not only serve the community, the state and visitors to the area, but that would also respect the surrounding communities. As I study the information that is available, and listen to the (very little) news I get the idea that this could be a something that's trying to be forced through before anyone realizes what's happening. Is it because it's being funded through sources other than government money? I understand that the funding comes from a private organization...maybe I'm misinformed.</p> <p>All I know is that this sounds a bit shady, under-handed, and covert (sort of like when the Colts left Baltimore).</p> <p>Bottom Line: Don't ruin Severn, and the lives of the residents of that area. Respect, respect, respect those that have made their homes there. Put yourselves in our shoes.</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
210	2-Nov-17	Website	Hi my neighbor went to a meeting held at Beacon Heights Elementary in Riverdale Maryland on Oct 26th that Senator Anthony Muse held. Hesaid that if the alternative route chosen comes thru here that our homes would be destroyed, and that the would be looking at giving us double the market value. Can you confirm or deny that statement. I plan on going to the meeting November 9th at Parkdale High school. I live at 6711 Furman Pkwy Riverdale MD 20737. I had not heard of this project until 2 weeks ago. So am concerned as to how this will be handled and the time line. We have lived here 38 years.
<i>Comments from E-mail</i>			
1	28-Mar-17	E-Mail	Hi! Your site mentions that the open houses next week will have proposed routes for the maglev project. Can you tell me where on your site I can find maps with these routes?
2	29-Mar-17	E-Mail	Hello, There is excellent reason to think that this proposed project will be more expensive than can be recovered by passenger fares (plus a reasonable 'public benefit' standard of taking cars off the highways.) & there is excellent reason to think that it would be more expensive, by a large factor (multiplier) than an ordinary passenger train operating at normal or fast speeds. (& excellent reason to think that building it would take much longer to build than any ordinary technology alternative.) The Cato Institute, and other think tanks, can provide the argumentation against that I am leaving out. Regards, David Olson 5445 85th Ave. #1 Lanham MD 20706 davidwmolson@aim.com Opposed as a local taxpayer, and as a national taxpayer, since no doubt the builders are going to want Federal \$\$\$ (\$\$\$,\$\$\$,\$\$\$,\$\$\$)
3	31-Mar-17	E-Mail	Folks, Have a comment on this MAGLEV train proposal. Going to cost a lot of money and land just for 3 train stations. Would be a lot cheaper to just add more trains to the existing MARC train line. If you wanted a few high speed trains then don't have them make all the stops between Baltimore & Washington and use the existing middle track.
4	5-Apr-17	E-Mail	I think there needs to be a station located in the Washington suburbs somewhere between I-95 and US 50 for it to be of any benefit to the residents of Montgomery and PG County. I would never go into DC to use it to get to BWI Airport, Baltimore city or perhaps future extensions further north.
5	6-Apr-17	E-Mail	To whom it may concern: Could you please pass along a detailed map of the current routing alternatives? The maps available on your website are not interactive. I need a way to view in close detail where these routes would cut through. Thanks!
6	6-Apr-17	E-Mail	Dear Mr. Trueschler and Del. Valentino-Smith, Todd Turner, Eric Simms, and Lisa Tavares Bodiford, Mr. Trueschler, it was nice meeting you at the Bowie open house yesterday. Thank you for taking the time to explain the project to me, and my neighbor, Eric Simms, who I am CC'ing. I am also CC'ing our state delegate, Geraldine Valentino-Smith, and Todd Turner, our county councilman, to make they are aware of my primary concern, and another citizen, Lisa Tavares Bodiford, whose neighborhood is directly on the WB&A trail. In taking a close look at the website, starting with the first slide titled NEPA and Targeted Timeline, I am struck that the December meetings we were told happened last winter are not listed. The first public meetings listed on the timeline slide are the April open houses, which we were told were the second round of meetings. There were never December meetings listed on the timeline, yet we were told they occurred. If they did, I find it difficult to believe that state senators and delegates and communities were adequately informed. Those meetings were supposedly held in areas far from Bowie, so it is possible the meetings were planned and held on short notice, without advertising them to those of us near the southern routes. Many of us who attended yesterday only just heard about this issue. While I am composing comments to submit for your consideration, my first objection is that this process and timeline have been rushed to the point that it would be impossible to think that all potentially affected towns and communities could have been informed and educated about the project. This is, in and of itself, a huge flaw, and should be corrected asap. It is unreasonable to think that affected communities could, within less than one month, have time to discuss pros and cons of proposed routes, and give adequate feedback. Years ago I lived in Laurel, MD when Jack Kent Cooke wanted to build a football stadium at the racetrack. We had at least a year and a half of input before hearings were held. Our entire community had time to learn about the pros and cons and weigh in. A matter of weeks is not enough time. I strongly propose that the timeline be adjusted so that the review and comment period allowed communities be extended by several months at a minimum, before the project managers choose preferred routes. I appreciate the hard work that has gone into the planning process, but believe that to derive true community support for it, affected communities deserve adequate time to do their part. I look forward to hearing from you and hope that the project managers will announce an extension of the comment time. Thank you very much. Sincerely, Bonnie Roberts 13111 Martha's Choice Circle, Bowie, MD 20720

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
7	6-Apr-17	E-Mail	<p>Attention: Mr. John Trueschler Good morning, Mr. Trueschler: I am Secretary of the Greenbelt, MD, Rotary Club, and we are very interested in the Magnetic Levitation project. Would you or someone from your organization be able to speak to our members about the project? We meet each Tuesday at the Greenbelt Marriott Hotel, 6400 Ivy Lane, Greenbelt, MD, over breakfast at 7:30 a.m. Tentatively, I'd like to suggest possible dates of June 13 or June 20. Please let us know about your availability. Thank you, and I look forward to your reply. Sincerely, Scott Scott Sibley Secretary</p>
8	7-Apr-17	E-Mail	<p>Operating of a high-speed superconducting magnetic levitation (SCMAGLEV) system between Washington, DC and Baltimore, MD (with an intermediate stop at BWI Airport) has been proposed as taking a time of only 10 minutes. In this 40 mile trip, how does the train mitigate against breaking passenger's necks during starting and stopping time periods? Please provide applicable performance and design specifications. Thank you. Respectfully, ek</p>
9	7-Apr-17	E-Mail	<p>I am writing to oppose the build of the Baltimore-Washington SCMAGLEV. My home is along one of the proposed routes (Alternative G - WBA Route). In fact, if this alternative is selected there is a high probability I would need to relinquish my home and property in order for the line to be built. Please have this serve as a public record - I do not support the build of the Baltimore-Washington SCMAGLEV, nor do I support the selection of the Alternative G route.</p>
10	9-Apr-17	E-Mail	<p>Dear Mrs. Seckman: We have received a letter from another "M" section resident. The following is the response from the Mayor: Thanks for the note and your thoughts and concerns about the proposed "MagLev" project. Rest assured you are not alone in your concerns. Council has received some early documents regarding this proposal and we have initiated requests for a formal presentation to the city and the community in the coming months This is, as you suggest, a plan that can significantly and adversely effect us - so we will follow it closely. What we know at the moment is that the State of Maryland is supportive, that it proposes a public/private action with dollar cost projected at over 10 Billion with an as yet unidentified environmental impact that will obviously generate serious concerns I encourage you to stay engaged. I do not as of today have a potential date for further public meetings and public hearings but they will come. Please follow the city's web site as this progresses Thanks again for the note and for your concerns. Fred Robinson, Mayor Diane M. Polangin Councilmember, District 2</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
11	9-Apr-17	E-Mail	<p>Dear representatives and project officials,</p> <p>I am writing to you as a Bowie resident who is deeply concerned about the proposed Maglev route through Bowie. Let me say at the outset that I generally support public transit, but "public" is the key word in that phrase. MARC and Metro are both underfunded and in need of maintenance. They serve daily commuters in Bowie and beyond. As a taxpaying citizen, I would far prefer to see the MARC and Metro infrastructure improved and maintained than see a new system, one that will not even ease commuter traffic and congestion in the region, be built.</p> <p>The Maglev train will not really serve Maryland residents who are daily commuters. The Acela already provides a faster option between Baltimore and D.C., but even Acela is cost-prohibitive for most middle class commuters. The Maglev will not have a stop in Bowie and will offer no benefit to Bowie residents.</p> <p>The proposed route runs right through Bowie neighborhoods. The map posted on the project website indicates a solid line through Bowie, meaning an aboveground train. However, people affiliated with the project have been misleading some residents by saying that the portion through Bowie will be underground. The project website, however, does not reflect that. Bowie residents deserve details about how they will be affected. A good start would be some clarity on whether the Bowie portion of the train route will be above or below ground.</p> <p>Regardless of whether the train runs below or above ground, it will most certainly impact the neighborhoods in Bowie through which it runs. Bowie homeowners are being asked to sacrifice and see harm to their largest asset (their homes) for a project that has absolutely no benefit either to them personally or to the City of Bowie (or even the county, for that matter) in general. In fact, the project has limited benefit to most middle class commuters in the region.</p> <p>Furthermore, while MARC and Metro are underfunded with no resolution in sight, we are looking at building a third system with less practical value that will still likely not pay for itself and there is little talk of how the maintenance of that infrastructure will be maintained once it is built.</p> <p>I am writing to voice my opposition to this project and my deep concern about its adverse impacts to my community. I am also writing to express that little outreach has been made to the very residents who will be most impacted if the yellow route goes forward: Bowie residents. To date, there has been one information session in Bowie. That session was held at 5:30 p.m. on a weekday. Most working (commuting) adults in Bowie can't make a 5:30 meeting. The meeting was not heavily publicized. And even the information provided at the meeting lacked details. Neighbors of mine who were able to attend left with more questions than answers. Some were even given contradictory information.</p> <p>I have a deep suspicion that we are being intentionally given vague and ambiguous information so that we are kept in the dark just long enough for the project to be approved, at which point we will have little say over what happens to our neighborhoods.</p> <p>Thank you in advance for your attention to this matter.</p> <p>Sincerely, Cristina L. Seckman resident of the neighborhood of Meadowbrook (also known as the M Section) in Bowie, Maryland</p>
12	11-Apr-17	E-Mail	<p>Hello,</p> <p>I believe I might live in the path of at least one of the proposed lines for the maglev project. It's hard to tell from this file I obtained from the website (attached). Would it be possible to send me a kml or shapefile of this map?</p> <p>Thanks! -marc</p> <p>[Included pasted proposed alternatives map in email]</p>
13	12-Apr-17	E-Mail	<p>Hello, unfortunately, I could not make it to the recent April meeting. I noticed a couple of the routes use WB&A trail. Will the trail still be available for use?</p> <p>Regards, Kim Lyon</p>

No.	Date	Forum	Comment
14	12-Apr-17	E-Mail	<p>Hi, I am writing in regards to the proposed MAGLEV Project that has been proposed for the Baltimore Washington Corridor. After viewing the presentation last week (April 4, 2017 at Lindale Middle School), I can appreciate that the proposed routes have been changed and it will now be underground, most likely in response to local protests about plunking this thing in the middle of a prosperous and well maintained historical community – Linthicum. However, the necessity of the ventilation buildings for an underground system like this is still an issue. There is very little undeveloped property in the area – someone is going to have to either surrender property under Eminent Domain, or learn to live with ungainly, unsightly buildings.</p> <p>I cannot stress enough that we in Linthicum have already had to live with public projects that were unwanted, namely the walk-up light rail station in Linthicum (not the North Linthicum Station that has parking). The addition of this station to our neighborhood brought crime to our area that we didn't have previously. Thanks to our active citizenry and the Anne Arundel County Police, that has largely been controlled. I do not think it is in any way fair to penalize our community again with this unwanted (and from my point of view, unnecessary) infrastructure that has been proposed. As a community, we have worked hard through difficult economic times, as well as with the crime wave accompanying the Light Rail Station opening, to maintain our safe community and to thrive. At last, we have rising real estate values that are only good for those of us invested here, and who would lose everything if property values were to plummet.</p> <p>Additionally, even if the funding for this wildly expensive project could be found, cost overruns seem to part of the equation, so to speak. Along with that are the inevitable delays, all of which will have an enormous impact on our community, with little or no upside for us. One gentleman, David Henley of BWRR, explained to me that after the completion of the “Big Dig” in Boston, the area thrived. I must remind him and everyone else that there were nearly twenty years of construction mess as well as the eight- fold cost overruns to attain that. That will kill a neighborhood such as ours, and will financially destroy so many of us who have everything we own invested here in Linthicum.</p> <p>In sum, I cannot support in any way an infrastructure project that will benefit so few – only the very wealthy or powerful government workers – at a cost to so many of us who are having to foot the bill, in spite of reassurances otherwise. I have heard too many of those unkept promises to not be completely skeptical. Further, it is totally and utterly unfair to thrust this kind of project on a neighborhood that is doing everything right in spite of difficult circumstances.</p> <p>-- Kathy Strauss kathystrauss.com</p>
15	15-Apr-17	E-Mail	<p>We are opposed to having MAGLEV installed, particularly along the WB&A trail, for the following reasons:</p> <ol style="list-style-type: none"> 1) The Trail is literally behind our house. It would be very disruptive to the quality of our lives if the MAGLEV was built along there. The value of houses dropped dramatically several years ago and if the MAGLEV is installed, housing values will drop even more. Homeowners will be unable to sell their homes without losing a significant amount of money. This is unacceptable and unfair to homeowners who reside along the trail. 2) Having the MAGLEV near the trail will disrupt the peace of individuals and families seeking recreation and relaxation along the trail. Many people (including us) use the trail regularly and daily. The MAGLEV barreling down at 311 mph near the trail will be extremely disruptive to the peace of the natural environment and the goal of connecting with nature as well as enjoying family or personal time on the trail. 3) We already have the Metro, MARC and Amtrak-why do we need yet another train? It is not worth the money and the 15 minutes of time saved by speeding quickly from D.C. to Baltimore. 4) The MAGLEV uses magnetic forces, which will disrupt internet usage in the vicinity. We rely heavily on the internet to use our videophones, which is the only way we can make phone calls, including to 911 services. I use the videophone to communicate with my deaf clients and hearing individuals regarding business or personal concerns. Disruption to internet services will be very crippling to us, as deaf individuals residing near the Trail. 5) The MAGLEV will impact the environment, disrupting wildlife and reduce the flora and fauna in the area along the Trail.
16	21-Apr-17	E-Mail	<p>Dear Mr. Smith,</p> <p>We attended the recent informational meeting at the Bowie Community Center about the proposed Maglev train and were astonished by the lack of detailed information available on the potential impact this line would have on Bowie neighborhoods and by the misrepresentation by Maglev of the potential benefits to the Bowie community.</p> <p>We were fortunate to be able to meet and speak with many of our neighbors and fellow Bowie residents at the meeting, many of whom were hearing details of the proposed bullet train for the first time. All were worried about the damage the train would cause to their neighborhoods. We met residents worried about declining property values as a result of a decade-long construction project outside their front doors and of the resulting concrete elevated train line blighting their rural home. We met farm owners concerned about the impacts to their animals caused by a 300mph train whizzing by at all hours of the day and night. We met outdoor enthusiasts concerned about damage to the Patuxent Research Refuge and about the threat of losing the solitude of the WB&A trail. In fact, we did not meet a single person there with a vested interest in the location of the line who was happy about the possibility of its construction.</p> <p>Maglev would like us to believe that Bowie will reap the benefits of a vast creator of local jobs, and that all we have to do in return is provide them with the land to build it on. The reality is that Bowie's community and landscape will be forever changed by a train line that does not stop here and is built and run by people from outside of our city. Please don't allow Maglev to leave its enormous footprint on Bowie's future.</p> <p>Sincerely, Peter and Naomi Crellin Bowie</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
17	21-Apr-17	E-Mail	<p>My first comment is that until and unless the Washington Metro system funding problem is resolved (with a permanent budget and including the completion of the Purple line), the building of a maglev system is NOT warranted.</p> <p>Assuming that is resolved, here are my comments on the proposed routes. First it is worth noting that the maps that have been presented (both at the public presentations and on the web site) do NOT have sufficient detail to seriously consider possible impacts.</p> <p>Nevertheless, I am prepared to say that the alternatives which utilize the old WB&A route (alternatives G, H and I) have far too much impact on the city of Bowie and surrounding communities, not to mention the impact on the bike/hiking paths which now follow that route. I am therefore against alternatives G, H and I.</p> <p>Following the existing Amtrak right of way (Alternative E) appears to be the most prudent approach.</p> <p>The routes which utilize the Baltimore Washington parkway corridor (Alternatives F and J) would be a second choice with Alternative J which utilizes a tunnel to minimize impact as perhaps the better of those two.</p> <p>Thanks for your consideration of my thoughts.</p> <p>Regards, Douglas Dearie 7309 Quantum Leap Lane, Bowie, MD</p>
18	23-Apr-17	E-Mail	<p>I absolutely believe that until the designated funding for the Washington Metro (and this means a realistic amount of money for them to operated and maintain the system without having to struggle to get more money), and the money for THE Purple Line are in place, the building of the Maglev, should NOT proceed.</p> <p>I also want to comment on the maps used on your website and at the public meetings. They lacked detail and made it very difficult to be able to see, even in general, where the most impact would be. So my initial recommendation would be the NO BUILD.</p> <p>In general why should we support something like this going in our area, the city of Bowie and my neighborhood particularly, when our residents will get only minimal benefits.</p> <p>But in particular I will say that I am absolutely against the alternatives which utilize the old WB&A route (alternatives G, H and I) and therefore impact the impact on the city of Bowie and surrounding communities, not to mention the impact on the bike/hiking paths which now follow that route. I am therefore against alternatives G, H and I.</p> <p>Following the existing Amtrak right of way (Alternative E) makes the most sense to me.</p> <p>I think the Baltimore Parkway route (Alternative J) would be a second choice because it utilizes a tunnel to minimize impact.</p> <p>Thank you for the opportunity to comment.</p> <p>Christine T Dearie 7309 Quantum Leap Lane Bowie, MD 20720 301 464-4432</p>
19	24-Apr-17	E-Mail	<p>Please do not destroy the beauty that is left of the Bowie area. I would prefer that you use the Baltimore Washington routs for your high-speed transit project. Without any local stops in Bowie, I don't really see the point of going into Bowie.</p> <p>Sometimes what we call "progress" isn't.</p> <p>Respectfully, Anne Simonetti 12211 Round Tree Lane Bowie, MD 20715 410-721-0936</p>
20	26-Apr-17	E-Mail	<p>Dear Sir or Madam,</p> <p>I'm writing with a concern regarding the potential Maglev train plans. I'm sure you have heard the same from others but I am substantially concerned regarding the consideration of building the train line along the Baltimore Washington Annapolis trail.</p> <p>My home and many others are 500 feet from the trail and current tracks. These tracks run 3 trains per day. It is unacceptable to increase the train traffic in a long established residential neighborhood as it will certainly decrease the property values substantially. It is far more logical to build along the current Amtrak and commuter tracks.</p> <p>My questions are:</p> <ol style="list-style-type: none"> 1. What government agency will be involved in decision-making and have to sign off on this plan? How can I get in contact with whomever is in charge? 2. What are the plans to compensate home owners for the loss of property value or moving costs if they choose? <p>I would appreciate detailed responses. I just purchased this home 4 months ago and would be angry if my local government allowed such a disruption and financial sacrifice of its constituents. I do not see similar types of plans in other nearby counties.</p> <p>Sincerely, Aisha Barber</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
21	26-Apr-17	E-Mail	<p>to whom it may concern: I have been researching the proposed Maglev project, including impacts on homeowners and its actual economic benefit. Today I write to urge the corporate interests in this project (I'm well aware these are the entities with the real power), the State of Maryland, and Prince George's County to attempt to have some humanity about this project and to please think about the impact a through train would have on the Bowie community. This train will not stop in Bowie. It will not actually bring jobs to Bowie; however, two of the proposed routes would destroy the peace, tranquility, and beauty of Old Town Bowie and its local parks and trails. It would cause home values to decrease as no one wants to own a house near a train capable of creating a sonic boom. It is clear that the train should move through the existing BW Parkway, but for some reason the option to hurt Bowie residents is still on the table. I know it is attractive in today's society to do the most harm and to convince oneself that hurting people is the only way to be successful; but, I assure any who receives this message that this is not the only way to do things. In fact, you can make immense amounts of money without harming actual human beings and their peaceful community. I will be staying abreast of this project and doing everything in my power to ensure that Bowie is protected. And I will use my rights as a citizen to ensure that any elected officials, whether state or local, who work with any corporate interests to destroy Bowie will be held responsible by losing their elected positions. Be responsible and, for once in your life, actually take into consideration the majority Black residents who are homeowners and doing what they need to do to good citizens - Keep the Maglev train out of the Bowie! - Cherise Neville, Esq.</p>
22	27-Apr-17	E-Mail	<p>I do not support the high speed train---lets keep out neighborhoods. Danny Lange, 2907 Blueberry Lane, Bowie, MD 20715</p>
23	27-Apr-17	E-Mail	<p>Hi, I'm writing to express my opposition to the Maglev project. Thank you, Scott Vining</p>
24	2-May-17	E-Mail	<p>Respectfully, sir, I, and about 50 other persons at this meeting, were vehemently opposed to the above-mentioned Project. I was surprised to learn that after this project is completed, it would have NO VALUE to the community of Bowie. It appears that this high-speed train would be "zooming" through Bowie. So, it is not another option for cheap travel to avoid the many automobiles ALREADY plaguing the Bowie highways. Sincerely, Micky Noble 12600 Craft Lane, Bowie, MD 20715</p>
25	2-May-17	E-Mail	<p>to whom it may concern: I am writing to express my concerns AGAINST the Maglev Train route through Bowie, MD. Last week I learned that the preferred route for this new project is the yellow route which will go right through my neighborhood and possibly my house. Like many others in our neighborhood, we have worked so hard to pay for our home - our only home of 26 years. It will be paid off in one year and never in my wildest dreams did I think something like this would happen to our 50 year old, established, quite, safe neighborhood complete with schools, churches, a golf course, and shopping. Rockledge is an eclectic mix of young families to retirees all with the same desire of affordable housing that is convenient to work. I implore you to select another route and I ask that you acknowledge receipt of the email. Thank you. Gail Sinkovic Raritan Lane, Bowie, MD 20715</p>
26	6-May-17	E-Mail	<p>Please leave Bowie out of your project plans! Residents are overwhelmingly against it. Cynthia Cummings 3910 York Lane Bowie, MD 20715</p>
27	6-May-17	E-Mail	<p>Possibly through my neighborhood of Kimberwickbetween Old Race Track and Bowie State.... I am very dismayed about the possibilities as we are upgrading our home at this time... Where do we come for meetings on this subject? Sharon Youngdahl 14817 Kimberwick Drive Bowie</p>
28	8-May-17	E-Mail	<p>Hello, I am checking on any update to the May meetings regarding alternative routes? Can you please let us know when the next round of meetings is expected to be scheduled? Thanks so much. Sincerely, Peggy Callahan Legislative Director Delegate Geraldine Valentino-Smith, District 23A</p>

No.	Date	Forum	Comment
29	8-May-17	E-Mail	<p>Hello! I am reaching out because I know your mission is to meet the transportation needs of the growing markets you're trying to connect and bringing the Northeast Maglev to reality. As you likely know, WTOP is a trusted and respected source for traffic, news, weather, & information in the DC Metro Market and a large portion of WTOP's audience, both on-air and online, would be prime consumers of the TNEM. (Must be why every time we carry a story about TNEM, the viewership numbers online are super high!)</p> <p>I was hoping to set up a meeting with you. I specialize in working with county governments & transportation organizations to educate and create awareness of their services and programs. I noticed you were recently holding 'town hall' public meetings- WTOP & WTOP.com were recently the only medium used for the Montgomery County Dept. of Transportation in promoting their town halls (as they try to bring Bus Rapid Transit to reality), and the turnout for their multiple events were well attended beyond expectations as a result.</p> <p>Typically, what we find works best, is to have an introductory meeting to find out the current goals of your organization and then share our capabilities that are relevant to those initiatives, both short and long term. I'm also happy to share with you all the great things we are doing with other similar organizations to help them to achieve their success.</p> <p>Would you be open to meeting with me at some point over the next two weeks for about 30 minutes either in person or by phone? Please let me know when you might be available to speak with me.</p> <p>Thanks so much, Diane</p>
30	9-May-17	E-Mail	<p>To whom it may concern,</p> <p>My name is Garin Sinkovic, I am a sophomore in high school, 16 years old, and a graduate of Youth Leadership Bowie. The reason I am writing this email is to voice my opinions AGAINST the Maglev Train Project which has a preferred route through my hometown, Bowie. I have lived in Bowie, Md my entire life and it has been the best place to live. It's safe, quiet, modern, and a wonderful place to grow up. I don't want to see this train run right through my neighborhood, Rockledge, which would completely change Bowie in a negative way.</p> <p>Bowie will not be the same if there is a big train running through it. It will no longer be safe and quiet with the train. There are already many cost effective ways of transportation such as cars, buses, and other trains. In the world we live in today, where there are very few rich people, the hard working class will not be able to afford what this train will cost. If you must waste your money on this train, please find another route that won't take away people's houses and property that they have worked so hard to pay for. Please reconsider your thoughts and I would appreciate to hear back from the email as soon as possible.</p> <p>Sincerely, Garin Sinkovic</p>
31	11-May-17	E-Mail	<p>I am very concerned about the possibility of the maglev high speed train coming through Prince George's County, especially the yellow route through Bowie. Has anyone walked, driven by or inspected these routes? The yellow route runs right through my development which is relatively new, numerous neighborhoods, at least on elementary school, a golf course, soccer fields, and a medical facility in Glenn Dale, just to name a few. This project team needs to do a much better job at reaching out to residents and informing them of this threat to their homes, property values, and amenities like the beautiful Washington, Baltimore & Annapolis Trail. The vast majority of my neighbors have not heard about this threat. I will be knocking on doors and passing out flyers to help make people aware of what is in jeopardy.</p> <p>MagLev offers no benefit to our city or County. It also appears that the intent is to run this through areas that are primarily minority and/or low income. This is unacceptable. I urge you to consider running this train underground to minimize the impact on residents or utilize the existing Amtrak route or the BW Parkway. Doesn't the government already own the rights-of-ways for these options? That should be less expensive than acquiring the rights to tear down new homes, while displacing thousands upon thousands so the wealthy people who can afford tickets can travel faster. Who is the ultimate decision maker? Please provide me with that person's name as soon as possible so I can reach out to them.</p> <p>I would appreciate a reply with answers to my questions.</p> <p>Letitia Carolina-Powell lcarolinapowell@msn.com 8107 River Gate Ln Bowie, MD 20715</p>
32	12-May-17	E-Mail	<p>Hello John -</p> <p>I understand from my elected representatives that I should be able to get a higher resolution of the map posted on your public web site from the project staff and was given your name as a contact.</p> <p>Could you please forward a high res image, or give me the contact information of who can provide this to me? Thank you.</p> <p>Sincerely, Laurie Thompson lauriejo@verizon.net</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
33	14-May-17	E-Mail	<p>I have attended a couple of meetings regarding this project, read the materials available on your website, and spoken to representatives from WMATA, BWRR, FKA, and MDOT. I have to say, I am still barred by the purpose of this project. Everyone agrees that MAGLEV between DC and Baltimore will be for the "elite" business traveler and will do nothing to help the average commuter. The expense of the line will be cost prohibitive for commuters. The impact on traffic will be negligible as very few individuals will actually use this train. Given that and the incredible opposition from the community, again I ask: "What is the point?"</p> <p>I am told that this is a first leg of a line connecting DC and NY and Boston. There is no evidence to support that this leg of the line will do anything for anyone in the DC/Baltimore corridor. If you are interested in promoting this type of travel for the "elite" traveler, number one- run it through their homes and communities and leave the "average" commuter out of it and, number two- try starting the line in Baltimore and go north to NY first.</p> <p>In looking at the proposed routes between DC and Baltimore, there is no route that meets the criteria that does not impact communities negatively....and with no benefit. This is supposed to be a democracy...shouldn't the needs of the many supersede the needs of the few (and wealthy)?</p> <p>To date, I have heard no single individual other than those involved in building this ridiculous project support it. Again, I ask: "what is the point?"</p> <p>I look forward to hearing a reasoned response to this email. Linticum managed to defeat this once, please be prepared to be defeated again. You can not destroy homes and communities so that a few wealthy individuals can get places faster. That is not the American way.</p> <p>Shame on all of you.</p> <p>Aviva Nebesky 8306 Laurel Bowie Road Bowie, MD 20715 www.horsepenhill.com</p>
34	20-May-17	E-Mail	<p>My home backs the WB&A trail and I am in STRONG opposition to the idea of your taking away a bike/walking trail that I use weekly and replacing it with a high speed train! Given that you have two other options to route the train next to the Amtrak train or down the middle of the BW parkway....I'm outraged that you would even consider taking the WB&A trail from my community and routing a train through a quiet residential neighborhood? NO! I have already contacted all of my local, state and federal politicians and will continue to fight your WB&A route!</p> <p>Chad shumate</p>
35	22-May-17	E-Mail	<p>Dear Mr. Truesdler,</p> <p>I live in Bowie and met you at the meeting held there about the proposed demonstration sc Maglev. I am writing to find out from you if what I have heard is true that the chosen routes are along the WB&A trail, rather than the BW Parkway or the Amtrak line.</p> <p>Sincerely, Bonnie Roberts</p> <p>26-May-17 Dear Angela Jones,</p> <p>Earlier this week I contacted John to find out the most recent info regarding proposed routes for the sc maglev. He directed my question to you. I had hoped to hear back already. I checked the website, but cannot find anything new (not sure that I would be able to locate it, though, on the site).</p> <p>I understand that everyone is busy these days, but if there is new info on the routes that are remaining as options, I really need to know since I am an impacted citizen. I attended the meeting in April, and we were told that by May, routes would be eliminated. Surely it is time to share that information. Relying on hearsay is not helpful to or anyone.</p> <p>Sincerely, Bonnie Roberts Bowie, MD</p>
36	24-May-17	E-Mail	<p>Hi - I am looking fir detailed information on the project. I'd like to know how the costs of the different alternatives were estimated, if you have any information in the # and/or locations of the properties that will be impacted by the different route options, and the current schedule of the project.</p> <p>Thank you, Khristi</p>
37	26-May-17	E-Mail	<p>Does this process include public comment? Where can this be done? There seems to have been no follow up to the Open House comments. Will someone actually be walking/driving the proposed routes? Is this something that the public can also do? The current maps are insufficient to give the detail necessary to see the line of travel and then to go 500 feet on either side to see the full impact.</p> <p>Aviva Nebesky Aviva Nebesky www.horsepenhill.com</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
38	9-Jun-17	E-Mail	<p>Hello, I am writing to express my deep concern for the proposed routes for this train. As it stands, one of the proposed routes cuts through my neighborhood in Bowie, MD and would likely uproot the homes of myself and my loved ones. I was raised in that neighborhood. There are still many people who both raise their families and then retire there. I have no doubt that this train is wonderfully efficient at getting people from point A to point B and so on. However, this neighborhood and the surrounding area are so precious to those of us who have worked hard to preserve it. Please reconsider the route for this train. There is plenty of govt owned land in nearby jurisdictions that could easily support this project without sacrificing the livelihood of the working class. The residents of this particular section of Bowie DO NOT WANT THIS. There will be no benefit to anyone living or working in Prince George's as there is no stop in our county. What is the point of uprooting the homes of innocent residents who want to live the quiet life they have worked hard for so people from a completely different city (Baltimore) can get to another city (Wash DC) in under 15 minutes. That is unfair and not to mention outrageously expensive for those who can otherwise afford the gas fair to make their way between said cities. I know this a lengthy email but I just want our concerns to be heard. Please reconsider this route to someplace less devastating. We have history and culture and pride in our little corner of PG. Please don't snuff us out.. Sincerely, A concerned citizen.</p>
39	11-Jun-17	E-Mail	<p>No pressing need. Overly expensive. Promised, promises, promises but precious few facts. My guess is that opposition will grow and citizen's objections will be systematically brushed aside. John W. Waddell CAPT, USN (ret)</p>
40	12-Jun-17	E-Mail	<p>I rode the high speed train in Europe recently. It was all above ground, and traveled through open spaces. This train should be routed over the B.W.I. Parkway, and NOT! through Bowie, a heavily densely populated area where so many lives would be disrupted!!!</p>
41	16-Jun-17	E-Mail	<p>I would like to know why no comments from the meetings in April were considered or reported?</p>
42	3-Jul-17	E-Mail	<p>Hello, My name is Adam Zielonka and I'm a reporter for the Bowie Blade-News. I tried to reach John Trueschler by phone last Friday but I believe he was out. I'm aware that SCMaglev officials will be making a presentation to the Bowie Mayor and City Council next week on the 10th. I'd like to know which specific officials will be present and what the presentation will generally entail. Thank you, Adam Bowie Blade-News</p>
43	5-Jul-17	E-Mail	<p>From Jacqueline Thorne (MDOT): Brad, John Cabala a resident along Alt G, has several inquires in regards to the Noise Level, imminent domain, standoff distance and home value etc. Mr. Cabala works for FAA and is familiar with the NEPA process. Can someone get back to him or these inquires? He can be reached by phone or email. John Cabala jrockjrock@comast.net</p>
44	11-Jul-17	E-Mail	<p>This is a horrible idea...Vote NO</p>
45	12-Jul-17	E-Mail	<p>Thank you very much for the info. Please keep me updated as you move forward.</p>
46	14-Jul-17	E-Mail	<p>Your contact at AECOM has NEVER responded to me. So much for transparency and open communication..... Aviva Nebesky</p>
47	15-Jul-17	E-Mail	<p>Mir. Trueschler, Mir. Smith, and Mir. Bratcher, We want to voice our strong opposition against the SCMAGLEV project that will negatively impact the Saddlebrook West area and the WB&A trails. We moved here in 2003 and our family has grown to 5 since then. My son recently completed his Eagle Scout project along the WB&A trail and we use the trail on a regular basis. I personally ride over 1000 miles a year on this trail. My regular rides and our family walks help us alleviate the stresses in our lives. We ask that you do everything in your power to prevent this SCMAGLEV project from occurring. It will devastate our neighborhood and our quality of life. Thank you, Mike Shieh Cassandra Shieh Zachary Shieh Brandon Shieh Lauren Shieh</p>

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48	19-Jul-17	E-Mail	<p>Dear Mr. Trueschler, Can you explain to me why the map has routes H, I, and J on it http://baltimorewashingtontsmaglevproject.com/images/boards/april_2017/SCMAGLEV_Map_Board_April_2017_Page_15.jpg But they are not listed on the screening level one summary? http://baltimorewashingtontsmaglevproject.com/images/boards/april_2017/SCMAGLEV_Map_Board_April_2017_Page_16.jpg Does this mean they have been ruled out as options? Thanks</p>
49	20-Jul-17	E-Mail	<p>Dear Sir or Madam, My name is Melissa Dolin and I currently reside in a house situated due north of the WB & A trail/railroad bed, just east of Rt. 197. I have a variety of concerns regarding the possible construction of the Maglev train along Alternative G, the WB & A trail. My address is 8299 Laurel-Bowie Rd in Bowie. Unfortunately, the house will not appear on google maps, nor on most physical maps. The property is owned by Tom and Joan Kay, and it wasn't until a few years ago that the postal service assigned physical addresses to the residences at this location. All of which, are located in very, close proximity to the WB & A. Given that the houses in this rather small community are in such proximity to the trail, and the relatively narrow width of the trail at this location, I assume that we will no longer be able to reside here. This brings me to my first concern. On the alternative routes table presented at your last public meeting, you listed the WB & A grade as "abandoned." I am quite certain you have greatly oversimplified the current surrounding environs along the WB & A. Although, some of these issues may come forth during the NEPA process, I am concerned that there was, or is, an entirely erroneous assumption, during the current, or previous, scoping processes that the trail is abandoned. Not only have you presented oversimplified data to the public, you run the risk of wasting tax payer money in the process by completing yet another EIS that will conclude major negative impacts to the surrounding communities. I can't imagine what the cost to the taxpayer of even the attempt to mitigate those negative impacts could eventually be. That's not to mention the time it would take to document the previously unrecorded historic and prehistoric sites along this trail, which are numerous. I think the MDOT, would be erroneous in assuming that all of the areas along the trail have been disturbed to such an extent that would preclude a full investigation under Section 106. There are prehistoric and historic sites located along private property in proximity to the Horsepin Branch tributary. Furthermore, I am somewhat certain that the context of these prehistoric sites have remained relatively untouched by modern activities. I suppose the details that I include here are to highlight some of the potential negative impacts of the Maglev along alternative G, if constructed. Secondly, I believe a simple viewshed analysis would demonstrate the negative visual impacts to the surrounding communities. I am aware that the viewshed analysis will most likely come at a later date; however, if it were to be completed now, I believe the communities involved would have a greater understanding of what the impacts to their communities may be. I understand some of the concerns listed in this e-mail may come to light during the NEPA process. However, I am greatly concerned that the information provided during the scoping process was not accurate in terms of conveying the negative impacts to the surrounding communities. By proclaiming that the trail is simply an abandoned railroad grade Sent from my iPhone</p>
50	20-Jul-17	E-Mail	<p>Thank you for your robot message to a somewhat well thought out, human message. Next time I'll bring the robot, auto messenger. July 20, 2017</p>
51	20-Jul-17	E-Mail	<p>Messrs. Henley, Smith, and MTA I am a resident of the Saddlebrook West subdivision of Bowie since 2001. My family and I live on Red Ridge Place in likely one of the first houses completed in this community. I attended the July 10 MAGLEV presentation at Bowie City Hall because I was concerned about the proposal and what it would mean to my residence and community, especially since one of the proposed routes for the MAGLEV train would essentially bisect our subdivision and another would abut the subdivision along the W, B, and A trail. While I believe both routes may not directly cause the razing of my home to make way for the overpass, a MAGLEV train going through my community would severely reduce the subdivision's quality of life by negatively impacting property values, creating a daily eyesore, causing construction traffic nightmares for years to come, and raising the potential for environmental hazards to humans and animals that may not manifest themselves until years after completion. I was heartened by the turnout at City Hall and the well-reasoned arguments against the proposal. Though I understand the need for new transportation alternatives for the Eastern Seaboard, the impact of the MAGLEV project on citizens of Bowie in terms of lost residences, businesses, schools, and greenspace is just too great and the benefits way too small. I am writing this letter to as many officials with decision authority to make my voice heard. This family is against the MAGLEV as it is currently proposed. I also think that the people of this community deserve to have this matter resolved as soon as possible, as it is unfair to have the MAGLEV issue hanging over our heads for x number of years. Any effort you can make to bring an end to the speculation on the MAGLEV routing would be most appreciated. Thanks for your time. Arnett Sanders, 15121 Red Ridge Place, Bowie MD, 20715, fourpt1@msn.com</p>
52	20-Jul-17	E-Mail	<p>Hello, Although the proposed project may be appealing on paper, have the proposed above ground routes been studied from the perspective of how many homes and communities would real eminent domain? The reason I bring up eminent domain is because this process hasn't been very well published and recently the word has spread and I think you are going to have a huge fight on your hands. Has a totally underground route been proposed? Maybe in the Amtrak easement for cost savings? Thanks. -Shane</p>

No.	Date	Forum	Comment
53	21-Jul-17	E-Mail	<p>I recently learned my house is located near 2 proposed routes for the MagLec train under consideration. To my surprise and disbelief! Is there no standard of notification to affected residents on projects under consideration when they directly impact their lives, their property? I have not been contacted by local, state or Federal officials while these studies have been conducted. As a citizen of Maryland, I am shocked and angered. My opinions, my voice has been smothered by the secrecy of the planning officials. This is totally unacceptable. I understand there were hearings. Why was this not published to the audience most affected by these propositions? Let's be plain spoken, this is an outrage. Shame on the responsible parties. I will start attending any and all meetings and public hearings. I will make a point of volunteering my time and resources to expose this clandestine activity.</p>
54	21-Jul-17	E-Mail	<p>No notification to residents. How does that work?</p>
55	23-Jul-17	E-Mail	<p>Hi, I am very interested in knowing what the proposed 6 routes of this high speed train might be, as well as the expected travel distance of the noise. Many thanks, Julie Mair</p>
56	24-Jul-17	E-Mail	<p>Sir/Madame: Can you please provide the following information: • verify the SCMaglev Project is still in the planning stage, and no location has been selected or confirmed as of today • provide a listing of neighborhoods in Bowie, Maryland that will be within the route (affected by) this train • provide the date, time, and location of your next public hearing • provide a map with a level of detail showing the trains' proximity to each affected neighborhood (not the one from your web site that does not provide residents enough information) Please let me know, via reply, how soon I can anticipate this information. Thank you, Donna Stevens, Bowie Resident</p>
57	25-Jul-17	E-Mail	<p>Dear Misses. Smith and Trueschler, As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project. Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project. Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner. In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process. According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public. The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process. Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project. • Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities • No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel • Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City. Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared. Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically: • 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment." • 40 CFR §1501.7, Scoping states that "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action." • 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make diligent efforts to involve the public in preparing and implementing their NEPA procedures. (b) Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents so as to inform those persons and agencies who may be interested or affected. • The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. But that is only the minimum requirement..." and scoping must have "appropriate public notice and enough information available on the proposal so that the public and relevant agencies can participate effectively." In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p>

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We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.</p> <p>Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.</p> <p>In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.</p> <p>According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.</p> <p>The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.</p> <p>Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.</p> <ul style="list-style-type: none"> • Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities • No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel • Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City. <p>Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). 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Specifically:</p> <ul style="list-style-type: none"> • 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment. • 40 CFR §1501.7 Scoping states that: "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action."
59	26-Jul-17	E-Mail	<p>I am very much opposed to this as I live in the Chapelgate Comm. I bought my house 10 years ago and am under water. Doing this project would put me in quite a bind financially and would be very difficult for me to find another house with my low income. There are many many people who would be affected by this in a bad way.</p> <p>Thanks, Bonnie Sallet</p>

No.	Date	Forum	Comment
60	29-Jul-17	E-Mail	<p>Dear Mr. Smith and Hueschler, I am a resident of Bowie, MD. My home is directly under the yellow corridor for the proposed MAGLEV. If these plans go through, my home will be taken and destroyed. I have worked hard for over 16 years to pay for my house, and this could destroy everything I have worked for. Bowie is a wonderful place to live. This MAGLEV would destroy our city, and provide zero benefits to residents. I strongly oppose the MAGLEV project in any area of Maryland. Instead of a MAGLEV for the wealthy few traveling long distances, it would make much more sense to invest in updating our already existing MARC and Amtrak lines for the everyday commuters in our community. As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. 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The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared. <p>Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:</p> <ul style="list-style-type: none"> • 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment." • 40 CFR §1501.7, Scoping states that "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action." • 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make diligent efforts to involve the public in preparing and implementing their NEPA procedures. (b) Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents so as to inform those persons and agencies who may be interested or affected. • The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. But that is only the minimum requirement..." and scoping must have "appropriate public notice and enough information available on the proposal so that the public and relevant agencies can participate effectively." <p>In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>We await your response to these requests. Regards, Jessica Phelan, 12012 Rockledge Drive, Bowie, MD 20715</p>
61	31-Jul-17	E-Mail	<p>Hello,</p> <p>Two of the proposed paths appear to go directly under the Piney Orchard residential area. Can you please point me to some information on how proceeding with either of those plans would impact my community through construction and operation of the train?</p> <p>Thanks, Courtney Dahlke Piney Orchard Resident</p>

No.	Date	Forum	Comment
62	31-Jul-17	E-Mail	<p>To whom it may concern,</p> <p>I am writing to ask your support in stopping the SCMAGLEV project which appears to be on a "fast track" to begin construction in late 2019 even though most citizens in the "R" (Rockledge) section of Bowie and Prince George's county knew nothing of this project until this spring!</p> <p>I am sure others have written citing the lack of benefit to Bowie or Prince George's county, or the lost tax revenue if some 250+ homeowners in the R section are forced out of their homes to make way for this pilot of a proposed New York to DC high speed train. I won't elaborate on those issues because they don't matter to me in the least if I am forced to move to allow this project to proceed. If they put a stop in Bowie, so that Bowie benefits from the MAGLEV, I'm still forced out of my home!</p> <p>I retired from the Air Force and federal government a few years ago, recently paid off my home, and looked forward to spending my retirement years in Bowie – without having another move hanging over my head. Sadly, just a few months ago, I first learned of this SCMAGLEV project and the possibility that I might be forced to move again! Both of my daughters live in Bowie, and the eldest recently had twin boys for which my wife and I babysit. Having to move will cause major disruptions to our lives.</p> <p>What galls me is that this project is moving along quickly – like the trains – and yet the City of Bowie, Prince George's county, and I expect other jurisdictions along the proposed route were not aware of this project until recently and therefore were unable to weigh in on the impact to both their communities and their citizens until now. THIS IS UNACCEPTABLE!</p> <p>For the federal government and the state of Maryland to be this far ahead on the project, while the city and county most directly impacted by the SCMAGLEV were not involved in the discussions is beyond belief! Furthermore, the fact that most citizens – especially those in the R section of Bowie most likely to be directly impacted if the Yellow line is selected – only learned of this fiasco via word of mouth is inconceivable.</p> <p>If this pilot project that will cost billions of dollars to get into operation turns out to be impractical, the area will end up with eyesore trestles and bridges scattered from Baltimore to DC. Will the Northeast MAGLEV program tear them down and rebuild the 250+ homes that they destroyed just to play with their trains?</p> <p>I know others are writing about the lack of benefit to Bowie, but that is really a red herring. Whether the SCMAGLEV directly benefits Bowie or not is irrelevant to me and, I suspect most R section homeowners. As many of us are elderly, likely with paid-off mortgages, being forced out of our homes via eminent domain may force us to look at downsizing or moving into apartments or senior housing much sooner than we had intended, when we had planned to remain in our mortgage-free homes as long as possible.</p> <p>I urge you to take some time out of your work schedule to come to Bowie and take a ride around the R section that would be destroyed if this project went forward. You will see what a beautiful, peaceful community it is. Then I ask that you support the citizens of the Rockledge section of Bowie and put a stop to this disaster!</p> <p>Alan R. Kolski, Ph.D. 12306 Rolling Hill Lane Bowie, MD 20715</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
			<p>along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.</p> <p>In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p> <p>Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.</p> <p>Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner. In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.</p> <p>According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.</p> <p>The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process. Prince George's County as a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.</p> <ul style="list-style-type: none"> • Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities • No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel • Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City. <p>Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.</p> <p>Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:</p> <ul style="list-style-type: none"> • 40 CFR §1500.2, Policy (d) states that agencies must: "Encourage and facilitate public involvement in decisions which affect the quality of the human environment." • 40 CFR §1501.7, Scoping states that "There shall be an early and open process for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action." • 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make diligent efforts to involve the public in preparing and implementing their NEPA procedures. (b) Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents so as to inform those persons and agencies who may be interested or affected. • The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. But that is only the minimum requirement..." and scoping must have "appropriate public notice and enough information available on the proposal so that the public and relevant agencies can participate effectively." <p>In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.</p>
63	1-Aug-17	E-Mail	<p>We await your response to these requests. Regards, Paul A Hubanks</p> <p>Requesting questions and answers on this maglev project ?</p>
64	1-Aug-17	E-Mail	<p>Questions on the maglev in comparison also with hyperloop project?</p> <p>As a resident how do you decide on who goes directly to New York from DC?</p> <p>How soon will they be raising the prices on the ones that voted on this project and also that will be using this as their means of transportation on a daily basis?</p> <p>How would they be configuring this transportation by using maglev opposed to the ones that drive not too far with the roads be more congested than what they are once this project is completed for example BW Parkway and Route 50?</p> <p>How would the owners of the property be reimbursed for their property, will they be appraising on the home value of the , buyers will stop buying property in the neighborhood, the value will be dropping in the neighborhood, or would we be getting back what we deserve as far as upgrades on our property and the value prior to the maglev project hitting the news in 2017?</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
65	2-Aug-17	E-Mail	<p>Good Afternoon/MAGLEV Project Planning Representative, As a resident of the Glenn Dale community that will be impacted by the plans to route the MAGLEV project within close proximity to my home, I do have a few concerns and also some ideas to suggest. CONCERNS - The sound of the MARC train horn, screeching brakes, and movement on the track can be heard from inside my home and the MAGLEV location is planned in a location that is even closer. I am concerned about the possibility of additional noise as well as vibration or movement felt at my home that would be disturbing/disruptive to sleep and daily solace. The blockage of Glenn Dale Rd during the construction and project completion as currently planned, would negatively impact the traffic flow in my community, especially during rush hour and in a possible emergency situation. IDEAS/SUGGESTIONS - If tunnels or overpasses can be built with soundproofing construct for the MAGLEV train, perhaps Glenn Dale Road traffic patterns could remain intact. Sound proofing barriers would eliminate the concern for noise pollution. Otherwise, plan tunnels or overpasses that maintain the current transportation connections in Glenn Dale for automobiles, bicycles and pedestrians. Also explore alternative vibration/sound proofing prototypes that could be selected for a peaceful residential solution. If the WB&A route options are used, planning should include a hiker / biker bridge for the WB&A trail across the Patuxent River into Anne Arundel county. If the AM Track rail options are used, planning should include a separate pedestrian overpass / underpass allowing access to Rt. 564 so that pedestrians and bicyclists don't have to use the shoulder of Rt. 193. Any route for the train should include adequate wildlife overpass / underpass areas. Thank you very much for considering the previously mentioned concerns, ideas, and suggestions to be incorporated into the current project plans. Mrs. Karen Barnes Strawberry Glenn Community</p>
66	3-Aug-17	E-Mail	<p>Good afternoon. I wanted to find out who I can contact for more information regarding the Maglev Project. I am the Association Manager for the Patuxent Glen Homeowners Association in Severn, MD and I am concerned that I am only just now hearing anything about this project. It appears that "alternative G" is going right through our community and I am shocked that this information has not made it to my office or to anyone in the community prior to this point. Please contact me as soon as possible with some information on how this will impact our community and when the next public forum is to discuss this project. Thanks so much,</p>
67	3-Aug-17	E-Mail	<p>Dear John Trueschler, On behalf of the residents of Saddlebrook West, I'm writing to voice my strong opposition to the proposed construction and operation of a high-speed superconducting magnetic levitation (SCMAGLEV) train system between Baltimore, MD and Washington D.C. The proposed routes adjacent to Saddlebrook West will be devastating to our COMMUNITY. Please see my attached e-mail and I hope you will remove our community as an option. Best, Millissa S. Hare</p>
68	5-Aug-17	E-Mail	<p>Dear Mr. Trueschler: As homeowners in Bowie, MD for 39 years, we are appalled at the proposal of the SCMAGLEV Project through our community. We truly were blindsided by this project. We did not receive information on it until much after the fact. We were not contacted on the scoping meetings. We were not given the opportunity to comment on the scope of the Environmental Impact Statement (EIS). We are formally requesting that the scoping period of the SCMAGLEV EIS be reopened. Certainly a major concern of ours is the environmental impact of magnetic levitation. The exposure to electric and magnetic fields (EMF) is unacceptable. Then there is the economic impact of this project which drastically lowers the value of our home. We have planned on the equity in our home to be a huge portion of our retirement. This seriously undermines these plans. We are in limbo one way or the other: If the proposal proceeds with destroying our neighborhood, our home value greatly diminishes. If the proposal is held up for years of seemingly endless debate and studies, our home value is diminished due to a lack of resolution. Either way, our home's value and our quality of life are greatly compromised. We respectfully request your input into cancellation of this project and in a timely manner. Please consider the financial and health future of not just us, but of all of our neighbors on which this will have a devastating impact. Sincerely, Chris Connolly Mary Pat Connolly</p>

No.	Date	Forum	Comment
69	7-Aug-17	E-Mail	<p>To Whom It May Concern:</p> <p>I am writing to you as a concerned homeowner that has just been made aware of the possible plans to put in a Superconducting Maglev (SCMAGLEV) Train between Washington D.C. and Baltimore with a possible stop at BWI Airport.</p> <p>I was surprised to learn that the area of study included my neighborhood, and shocked to see that 2 of the proposed routes actually would require the demolishing of my neighborhood and home as I did not receive any information on the scoping meetings and I have lived at my current address since 2004.</p> <p>The fact that I had to learn of this once the study was complete is unfortunate, as there was seemingly little to no public input sought during the scoping process. I certainly did not have the opportunity to comment on the scope of the Environmental Impact Statement (EIS), and would formally request that the scoping period for the SCMAGLEV EIS be reopened.</p> <p>The fact that there were only 75 comments regarding the project when there would be more than 75 homes in my neighborhood alone that would need to be leveled to make room for this project is very telling. I am appalled that there was only 1 flyer location in all of Anne Arundel County where much of the proposed route would follow and no less than 17 locations in Baltimore City, 19 locations in Washington D.C. and 18 locations in Prince George's County. Shame on you for not providing mailers to every possible homeowner in the affected study area. You also should be ashamed for not making homeowners aware in the areas that actually might care to see this project not move forward, and could actually organize against it.</p> <p>I seriously question the need for a multi-billion infrastructure development project with minimal to no gain for the commuters between Baltimore and Washington D.C that the project is intended to benefit. I would welcome the opportunity to see the studies that show that this train would actually benefit the lives of the commuters between Baltimore and Washington D.C.</p> <p>There is already an existing MARC commuter train that serves this purpose with a regular schedule and is reasonably priced for ~\$7 each way. The competing AMTRAK train has significantly less passengers than MARC and has a value fare of ~\$16+ each way, and the higher speed ACELA has even less passengers and charges ~\$40+ Each way. I fail to understand how a super high speed train will attract more passengers than the current Acela train or Amtrak that are already both operating at much less than full capacity. It will certainly not do it if the price for a one way ticket is more than the Amtrak.</p> <p>Even if a project would improve the lives of Maryland Commuters, there seems to be sufficient land already owned by the State and Federal government that would more than suffice to run the maglev train between Washington DC and Baltimore with a stop at BWI airport. These routes should be chosen first before displacing thousands of families from their homes and communities.</p> <p>It would also make better use of the taxpayer's money to improve the existing commuter service between Baltimore and Washington DC by adding more tracks and train service along the existing Penn and Camden lines or widening the BW Parkway to accommodate 3 lanes of traffic each way.</p> <p>Best regards, Brian Bohs 8203 West Hatton Court Severn, MD 21144</p>
70	7-Aug-17	E-Mail	<p>Hello,</p> <p>I am the Council Vice President in the City of Glenarden. I am writing because I just recently learned about the proposed MagLev project. I happened to attend a community meeting on July 15, 2017 hosted by my 24th district legislative representatives and it was mentioned in their meeting. It was also brought to my attention by a Bowie Maryland resident.</p> <p>I am very disturbed about this project because two of the 6 proposed routes go directly through the middle of my community along route 704. The routes would literally cut the City of Glenarden in half while also displacing some citizens homes, maybe two churches and City Hall. It will leave an elevated track that will definitely destroy whatever equity that the homeowners may have in their homes.</p> <p>I see that there were community meetings held in Cheverly and West Lanham Hills, two areas that would have minimal impact, but not in Glenarden where there could be a major impact. The notification cards must have only been mailed out to Cheverly and West Lanham residents because no one in my community have heard anything about it.</p> <p>Information is now getting out and citizens are very worried about their homes and how this project will affect them. What I would like to know is if you can come out and give the citizens in my community a presentation of this project? I think this would clear up any misconceptions the citizens may have.</p> <p>Thank you, James Herring, Council Vice President 8600 Glenarden Parkway Glenarden MD 20706</p>
71	9-Aug-17	E-Mail	<p>Hello Baltimore-Washington Maglev,</p> <p>I'm writing from the Volpe Center in Cambridge, MA. I am working with a client on an FRA report that looks at shared track systems and the potential impacts of high-speed trains on those systems. We would like to use an image that I believe your firm created – see attached. The image was used in a story written by Martin Di Caro that appeared on wamu.org - http://wamu.org/story/15/07/24/maryland_proposes_maglev_marriage_between_dc_baltimore/</p> <p>Could we have your permission to use this image in our report in a non-commercial capacity? The report is essentially a top level literature review of current research being done in this area. We would cite your organization and give proper credit.</p> <p>Kind regards, Nathan Grace</p>
72	10-Aug-17	E-Mail	<p>STOP THE TRAIN!!!</p> <p>Anne Simonetti</p>

No.	Date	Forum	Comment
73	5-Sep-17	E-Mail	<p>John Trueschler, What is the benefit to Prince Georges County with a superconducting magnetic levitation high speed rail system? This out of touch with our community needs. The MARC train and Amtrak move fast enough. Who will own this high tech mode of transportation? A I vote no to the superconducting magnetic levitation high speed rail system. Stay away with this crap.... Everyone works remotely and families on vacation are not in a hurry. Take this to another county in Maryland and stay out of Prince Georges County with this non-sense. Springdale Resident, Denise K. Bush</p>
74	5-Sep-17	E-Mail	<p>John Trueschler, What is the benefit to Prince Georges County with a superconducting magnetic levitation high speed rail system? This out of touch with our community needs. The MARC train and Amtrak move fast enough. Who will own this high tech mode of transportation? A I vote no to the superconducting magnetic levitation high speed rail system. Stay away with this crap.... Everyone works remotely and families on vacation are not in a hurry. Take this to another county in Maryland and stay out of Prince Georges County with this non-sense. Springdale Resident, Denise K. Bush</p>
75	5-Sep-17	E-Mail	<p>To whom it may concern, I am writing to express my concerns and opposition to the Baltimore-Washington SCMAGLEV project. Both the Alternative G (yellow on map) and Alternative I (red on map) routes cut through exiting established neighborhoods and will cause the destruction of churches, businesses, schools, veterans institutions, and many other buildings. I do not understand how those routes could have been chosen as there is currently no right of way for railroads along Quaterfield Road in Severn Maryland and will only cause the destruction of homes. Additionally I am concerned about the environmental impact of this project. This project will destroy the limited wooded area within Anne Arundel County and severely impact the wildlife in the area. They are already stressed by the continued development and this project will further impact them. The Alternative G (yellow on map) and Alternative I (red on map) routes will severely impact the severn river watershed. Also I worried about the electromagnetic radiation that will be emitted from this train that will impact not only the natural wildlife but also the people that live around the area. Thank you, Tom Edwards</p>
76	6-Sep-17	E-Mail	<p>To Whom It May Concern, During last night's town hall meeting at the City of Bowie, Maryland officials indicated the project has developed a Purpose and Need statement for the BW Maglev Project. Please provide me the Purpose and Need Statement for the project as referenced in last nights town hall meeting.</p>
77	6-Sep-17	E-Mail	<p>I was not providing a comment. I was requesting information.</p>
78	6-Sep-17	E-Mail	<p>Dear Mr. Trueschler: My neighborhood, the W section in Bowie, will be directly affected by the proposed MagLev train route, and I am asking you to stop threatening my community. I just want to remind you that behind every "number" and "figure" of the houses and properties being destroyed due to the construction of this unnecessary train, you are talking about upsetting the lives of real people, real families, real neighborhoods. It's not just "one school." It's a school that my neighbors send their children to learn and grow, to make friends and make life-long memories. It's not just "one playground." It's a playground I walk to with a neighbor, so our kids can play together, learn real life lessons and be kids. I'm tired of my community being destroyed by unneeded and unwanted construction. Please stop considering this project.</p>

No.	Date	Forum	Comment
79	6-Sep-17	E-Mail	<p>I can understand why Japan would like to showcase their maglev technology in the highly visible dc corridor since they sunk 40 billion dollars into something they have not been able to sell. I CANNOT however understand why anyone in this region would think jamming something this huge through a highly congested corridor is a good idea – maglev works as a long haul transport between metroplexes, not THROUGH them. Maglev sounds all gee whiz and tommorrowland but in reality the technology is 40 years old and maglev projects are being scuttled all around the world in favor of more flexible alternatives like the french TGV and other high speed rail options. Even our own Acela could travel at comparable speeds in this corridor if a dedicated fourth line was built at an estimated 15% of the cost. The CURRENT rail service can get from BWI to DC in 25 minutes – the maglev would take 9. The Japanese and their lobbyists would like us to believe that we should spend 10 billion dollars to save 15 minutes in travel time.</p> <p>Oh but it is going to go to New York you say! That's the big advantage! But where is the buy in from the rest of the region? Where is the plan for developing the rest of the route? What is the timeline for the other phases? Is it contingent on this leg being financially successful?</p> <p>Extending the train to NYC would take massive amounts of federal dollars – 100 billion in todays dollars – where is the buy in from the rest of the country to fund this? Without a firm commitment to extend the train beyond baltimore or dc - Maryland could be stuck maintaining a train to nowhere that decimates the eastern baltimore dc corridor for the benefit of the elite business traveler and project investors. To start this project without solid buyin from New Jersey and New York, not to mention the country, is absolute folly.</p> <p>The comparable maglev train in china can only fill about 20% of its seats and is operating at a significant deficit because the train doesn't go where people want to go, and the end to end travel time getting to the airport is comparable to taking a taxi. The maglev trip itself takes 10 minutes, but the combined time of getting to the station, parking, waiting for the next train, often takes an hour or more. Things will be similar here – if I as a Bowie resident wanted to say take the maglev to BWI, I would need to drive half an hour to get to Union Station, park, pay for parking, wait for the next train, and then pay airline prices for a 10 minute ride to BWI. I would do this exactly once.</p> <p>Should the project go forward it would sap funds from Amtrak's high speed Northeast Corridor - the agency's major source of revenue for maintaining the rest of the country's less populated but indispensable routes.</p> <p>It is also not clear that this is the correct technology for the problem.</p> <p>Recent developments of high speed rail have reduced the advantage of Maglev in higher speeds, so that the differences in travel times are small especially in smaller corridors like this one. High speed rail has a huge advantage of Maglev due to its compabilility with existing rail networks. High speed rail involves lower investment cost. Operating costs on Maglev are still uncertain. Energy consumption is estimated to be lower for high speed rail.</p> <p>If there is money for improving rail transit, it should first go towards upgrading existing infrastructure and enhancing commuter options in one of the most congested regions of the country – and not be used to support a Japanese enterprise desperate to demonstrate their technology by bisecting entire neighborhoods that will gain nothing from the project.</p> <p>Eminent domain is supposed to be for the common good. This train will serve the elite traveler, the investors, and the Japanese. Interesting choice of technology given the Trump era buy america first campaign. It a clear abuse of eminent domain by the politically connected taking advantage of the politically weak.</p>
80	7-Sep-17	E-Mail	<p>Wow. I am sending this from the meeting. You got played. Folks think you are going to take their churches and homes. That is all they care about. Need to plus up your presentation. I will send tips tomorrow. Goodness, you need to a FAQ as a handout.</p>

No.	Date	Forum	Comment
			<p>First, congratulations on your new position at MDOT. I am contacting you for your help with a project that will destroy the lives of many families, including mine. Please stop the unnecessary destruction of our communities, thousands of residences, seniors/retirees, historic places (and the obvious paths, all proposed to go through the black communities) for a train we CAN'T EVEN AFFORD TO RIDE, according to proposed fares! Why would you want to sacrifice our citizens to copy a transit system that is a failure in Japan?! "Taiwan's high-speed rail system, which uses Japanese shinkansen bullet-train technology, is caught in a financial crunch after eight years of service. High depreciation expenses and interest payments have left the company deep in the red." (Nikkei Asian Review, Feb. 13, 2015)</p> <p>I respectfully request that Washington-Baltimore MagLev Train Project be terminated before it needlessly ruins the lives of many Maryland citizens. I am speaking on behalf of many in my community would feel that this MagLev Train Project is a slap in the face to residents for even considering taking this private funding to uproot over 4000 homes, and an appalling waste of whatever valuable tax dollars are spent towards it, similar to the much-ridiculed streetcar project in NE Washington. If this train were such a good idea, then why is it such a failure in Japan? It has been and is continuing to lose revenue there! And to spend this amount of money, as well as uproot entire families and communities, when AMTRAK is in desperate need for capital improvements, is unbelievable. People will lose homes and property and, with today's escalating real estate market, will not be able to fairly replace their homes even with compensation from eminent domain. This will displace and possibly cause homelessness for many families.</p> <p>Background</p> <p>The Federal Railway Administration and the Maryland Transit Authority have quietly implemented a \$28 million study to eventually build a \$10 to \$20 billion Super-Conducting (SC) Magnetic-Levitation (MAGLEV) train from Baltimore to Washington DC (with a single stop in-between at the Baltimore-Washington (BWI) airport). This train is being built and promoted for the "elite business traveler" and not for the masses due to limited access stations (just 3 access points, with 2 terminus stations and 1 intermediate station) and very high ticket prices. As currently proposed, the train will destroy historic Maryland communities, businesses, churches, schools, farms, and green spaces in Prince George's and Anne Arundel counties and force thousands of residents from their homes through eminent domain.</p> <p>The damage from the SCMaglev Train will be severe. It will cut densely-populated, quiet, well-established neighborhoods in two, demolish historic, religious, and civic buildings, isolate local businesses, wreak havoc on fragile ecosystems, and destroy precious open space. Thousands of homes will be forcibly taken from longtime residents through eminent domain. And in the end, it will put State and Federal taxpayers at risk for exorbitant maintenance and repair costs -- and if the project fails, it will fall on taxpayers to clean up the mess through State and Federal bailouts.</p> <p>MAGLEV projects around the world have failed miserably, either going bankrupt or being abandoned in development, leaving the governments that contributed funds holding the bag. For example, the State of Virginia is owed \$7 million for a loan to the American Maglev company on a 2.2 mile train at Old Dominion University that failed as soon as it was complete. There is little chance Virginia will ever see its money.</p> <p>The Japanese company that makes MAGLEV systems can't sell them at home or abroad, so the Bank of Japan is bailing it out by pledging a \$5 billion loan to Maryland to finance construction.</p> <p>The Maryland MAGLEV development process has been pushed along with very little notice to residents and very little stakeholder input. Only 16 residents were present at the first and only stakeholder meeting in Prince George's county because the administration mailed out about 600 postcards statewide and failed to advertise the meetings in major newspapers and social media sites. I found out about this project 2 weeks ago when a child put a flyer in my door requesting my attendance at a community meeting! And, as you may know, the project is in its final phase of the Environmental Study.</p>
81	10-Sep-17	E-Mail	<p>It is easy to approve something that is going to affect "other" people. How would you feel if this came through your and your family's neighborhood? It IS slated to come through mine in Springdale (see</p>
82	12-Sep-17	E-Mail	<p>I strongly believe this type of project is wrong for Bowie. Many homeowner move to Bowie to get away from this type of noise and hassle of the city life. If a project like that ever gain traction Bowie will loose many of great residents who made this area great to be in!</p>
83	12-Sep-17	E-Mail	<p>Gentlemen: Requesting time and date of all up and coming meets for train projects. we live in Beacon Heights area 6900 Block of Emerson st. And we are not looking forward to any lose of homes in our neighborhood. So please keep us imformed of all meeting and times. Sincerely Concerned Home Owners</p>
84	15-Sep-17	E-Mail	<p>is there a meeting on MAGLEV at Bowie State University September 23rd? Thank you Marianne Baird, Administrative Assistant City of Bowie, Planning and Economic Development Department 15901 Excalibur Road, Suite 230 Bowie, MD 20716</p>
85	15-Sep-17	E-Mail	<p>I am very angry that my parents and the other residents of Prince George's county were not warned that this route could take their homes! How dare you! We are see the world! I find it interesting that black neighborhoods were not told of this maglev train! You should be ashamed of yourselves! I will spread the word to whoever needs to be told to put a stop to this! I will not let you take the home I grew up in! I will not stand for this! How dare you!</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
86	15-Sep-17	E-Mail	Your website links do not work. No information available. However, seeing notes from recent community meetings to which your reps were invited, the communities are being fed patronizing platitudes, like it will bring jobs and "benefits" to Maryland. So far it looks like lots of public finance for a private project, no help to residents other than DC and Baltimore who might be able to afford it. For anyone with property anywhere near the proposed routes, their property values have already probably dropped. All of the proposed routes go through well established middleclass neighborhoods, which once again shafts hardworking families to benefit rich people. Anyone with common sense can see this will be a huge cost to all taxpayers of Maryland. Is there even one successful, financially viable maglev existing anywhere in the world? I see big business trying to make money while trying to hornswaggle ordinary citizens. I hope our politicians don't get dazzled by the promise of a forward looking, but so far unproven, failed fiasco. In order to change the minds of the voting and taxpaying public you're going to have to give some very specific benefits that outweigh the devastation to many communities.
87	19-Sep-17	E-Mail	Hello, Is it possible to get the details on this fall meeting so that I may be present. My neighborhood and the home of myself, along with two of my children's homes are in two of the proposed routes and we would like to know how our lives may be effected. Thank you, Paul Bednarik Rita Drive, Odenton, Maryland
88	19-Sep-17	E-Mail	Good Morning, My name is Kenneth Howell and I am a concerned homeowner in Laurel, Maryland. The majority of the routes go through Bowie, however there are 2 that come through South Laurel. My home is 11509 Laurelwalk Drive Laurel, MD 20708. It appears on the map that the section of Alternative Route J and F are above ground and at the point it will reach the BW Pkwy the line particularly for route J goes straight through my condo association. Can you please provide more details on this if this is truly the case?
89	19-Sep-17	E-Mail	When are the next Open House dates and locations? We were unable to find them on your website. Thank You Justin Szech Board of Directors Linthicum-Shipleigh Improvement Association
90	19-Sep-17	E-Mail	Good afternoon, Please provide details regarding the exact times and locations (building, room #, etc.) for the MAGLEV project planning meeting on 10/14 at Bowie State University. Citizens have been advised to attend as a part of community outreach, but no details regarding the meeting have been provided, and the school advises they have no knowledge of such a meeting. Your website also offers no details. Thank you for your prompt attention to this matter.
91	1-Oct-17	E-Mail	Greetings: As a result of visits to the Web site, the following are requested: 1. A list of purpose(s) for which the SCMAGLEV is designed. 2. A complete (paper) printed document and the source for others should the need arise. 3. A complete list of MD/DC current government (local, city, state, US) proponents of the project, and those who are NOT in favor. Thanks for the information. Regards, Terry Hoebeke District 2 – J. Grasso Severn, MD
92	2-Oct-17	E-Mail	If you want to improve MARC train service have at it. At a fraction of the cost. Keep this thing out of Bowie. Keep it out of Maryland governor. And I voted for you. I am very sorry I did. Leave my home alone. Leave my neighbors and community alone. Many who are active duty or retired military. I did not serve 10 years in the Navy and Army for this extremely dumb idea that costs way too much. So let's see. I lose my home at a fraction of its value. So this veteran is now living on the street. I certainly won't be taking the Maglev. Have New Jersey and New York even signed up for this thing? Eminent Domain is fascism. Fascism is defined that the power of the State trumps the rights of the individual. Well governor do you and your friends meet this definition? Think about it. NO MAGLEV GET OVER YOUR TOY TRAIN SET!!!!!!!!!!!!!! I send you on for Christmas if that is what you really want.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
93	3-Oct-17	E-Mail	I am against the Maglev entering Bowie through and around the Patuxent/Horsepen Branch/WB&A all of which are spurs that go into our great Chesapeake Bay. The area is also full of wetlands and full of all kinds of wildlife. There is not much open space left in Bowie for wildlife to roam free. I live on almost six acres next to the WB&A trail and do not want to see nor hear a train come by every few minutes. I love the peaceful trail that Park & Planning built it's a one of a kind. It would be horrible to have a train over top of you, that is not getting away from the hustle and bustle of the city while you take a quiet walk. This train does nothing for our city, no jobs, brings us no business and by no means makes us look beautiful, so we really don't want it here. Go find a nice highway that is already noisy and looks bad to run next to. Stay away from our homes, parks, and open space. Hit the highway or train tracks!
94	3-Oct-17	E-Mail	<p>To whom it may concern:</p> <p>I looked at the maps of the proposed though not yet finalized lines for the maglev trains. I am against any plan that would take my property and it looks like these lines will be coming through my neighborhood in Landover Hills.</p> <p>Are there any plans to upgrade the existing train tracks that exist between Baltimore and Washington? Also, instead of spending billions on this maglev option are there any plans to just run trains more frequently between Baltimore and Washington? There needs to be more trains heading north towards Baltimore. I commute between Baltimore and Landover Hills and I would much rather keep my house and catch the MARC train. But the MARC's current lines run so infrequently that it is faster for me to drive.</p> <p>I think the maglev is better left alone since it would be standalone technology. If the Washington, DC area will be the only place in the country with this technology then it will be expensive to maintain and keep up-to-date. I have lived in the area for 30 years and one thing that I have learned is that none of levels of government wants to pay for maintenance for anything -- "costs too much" is the perennial refrain.</p> <p>I think if it could be guaranteed that I would not lose my home and that maglev technology was being adopted by more than just this area I would be interested. Right now I am not hearing anything that encourages me that this will be good for the region or my neighborhood.</p>
95	4-Oct-17	E-Mail	<p>Dear Sirs,</p> <p>As a homeowner along the proposed MAGLEV train route I am shocked and appalled that I am only now hearing about this project.</p> <p>I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. This would have entailed a significant effort as my husband is in the US Army and we are currently stationed overseas.</p> <p>The home we own in Bowie, MD was purchased because we love the close access to the rail trail, for running and biking, and the variety of wildlife, as well as our beautiful historic home and quiet neighborhood. We have been looking forward to our soon approaching return, only to receive the news from friends that 3 of your proposed routes significantly impact our home.</p> <p>I realize that you have not yet completed the environmental impact study, but I fear that the limited methods used to notify citizens and property owners in the affected study area of the project reflects a hasty and slipshod approach to the entire project. I question the need to create a new system of rail, when the available already interconnected means of public transit is under utilized and poorly maintained.</p> <p>Respectfully, Kirstin Camp</p>
96	4-Oct-17	E-Mail	<p>We do not need this high speed train coming through our Community. This train will force folks to leave their home and their communities. Why can't this train go through Virginia or Montgomery County. It is apparent that this is nothing but GREED on your part. SHAME ON YOUR for being so GREEDY. This is not going to go well in OUR NEIGHBORHOOD. LEAVE US ALONE and route that train somewhere else!!!!</p>
97	6-Oct-17	E-Mail	<p>As a longtime resident of Prince Georges County Maryland, residing in the town of Bowie, MD, I DO NOT SUPPORT THE MAGLEV TRAIN GOING THROUGH BOWIE.</p> <p>Not part of the deal moving to PG County, find somewhere where people do not live. Or use the Amtrack tracks already established and does not involve imminent domain need.</p> <p>Concerned Resident</p>
98	6-Oct-17	E-Mail	<p>Dear Concerned,</p> <p>MTA said the documents for the upcoming meeting would be available prior to the meeting. When will they be available?</p> <p>Thanks</p>
99	7-Oct-17	E-Mail	<p>How come I and everyone else I know in Bowie never heard anything about the 2016 Scoping meeting?</p>
100	7-Oct-17	E-Mail	<p>How come the Maryland MTA did not attend the July meeting at the Bowie City Hall? If you need our input you would have been there. So far this MagLev project has been a huge attempt to keep the citizens of Prince George's and Anne Arundel Counties in the dark.</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
101	9-Oct-17	E-Mail	<p>Mr. Smith, et al.,</p> <p>I am formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened. My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas. I request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p> <p>Regards, Daniel E. Woomeer & Patricia E. Filaseta 6242 Woodland Road Linthicum Heights, MD 21090</p>
102	9-Oct-17	E-Mail	<p>Mr. Al Khatib and Colleagues:</p> <p>I see the obvious benefits of having such a rapid transit system in place between Washington and Baltimore and support studying the feasibility of placing along the BWI Parkway, if environmental, cultural, and other concerns can be mitigated appropriately. However, my neighbors and I VEHEMENTLY OPPOSE ANY effort to place SCMAGLEV right away through Odenton, be it the WBA option or along the existing Amtrak corridor. The noise and vibration of a 300+ mile per hour bullet train will be highly unacceptable, not to mention unknown effect of high powered magnets in proximity to organic matter (humans especially), and other dangers of such a high speed projectile like object transiting residential areas.</p> <p>Sincerely, Richard E Barrett Jr, MSIT Captain, United States Air Force</p>
103	10-Oct-17	E-Mail	<p>request to reopen the scoping phase. Mr. Smith, et al., I am formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened.</p> <p>My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. My neighbors and I have been lied to and taken advantage of before with the MTA's Light Rail Project,, and with the Casino Projects in the State of Maryland. We will not be taken advantage of yet again. My Community of Ferndale has suffered and is now paying the price of the aforementioned projects, destroying our neighborhoods, our shopping, our personal safety,, and we will not submit to further degradation by the SCMAGLEV project. Our Community,,, the Linthicum / Ferndale populations must have our say to the possible destruction of our homes, our neighborhoods and the degradation of our communities!</p> <p>Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas.</p> <p>I request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p> <p>Regards, Don and Bonnie Bender</p>
104	10-Oct-17	E-Mail	<p>I am asking that the scoping process be reopened due to lack of notification.</p>
105	10-Oct-17	E-Mail	<p>Mr. Nissenbaum:</p> <p>We are formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened. My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas.</p> <p>We request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
106	10-Oct-17	E-Mail	<p>To Sunair Al Khattab,</p> <p>I am writing to today to express my concern for our historical structures, for our environment and finally the administrative process that followed that can loosely be defined as "scoping". On the planned routes lies vast stretches of mature vegetation containing thousands of animals. On the proposed route, lies many structures that could be defined as historical structures. It is on most concern for me that a decision of this magnitude.--A decision affecting my family and countless families like mine took place without me being informed of the plan. It came as a surprise to myself and my neighbors that the scoping process had opened and closed without our knowledge during a holiday week in 2016. We were not given notice of the Scoping Meeting or an opportunity to comment. I believe that the timing and limited notification to the affected parties (people like myself) was an attempt to circumvent due process. I ask that the scoping period for SCMAGLEV EIS be re-opened, allowing for true due process to occur.</p> <p>On a personal note, I cannot fully comprehend how a project whose price makes it destined for failure followed by decades of state and local subsidies all at the tax payer's expense would be considered in the first place. I question whether the devastation that this project would cause to the hard working families of Maryland directly in the line of MAGLEV and the families whose property values would be greatly diminished due to the proximity of their property(effectively eliminating the vast majority of their life savings) to the MAGLEV was considered. I question whether the property taxes caused by the reduction in property values was considered.</p> <p>I question why a project with no redeemable financial incentive whose building would do immeasurable harm would even be considered.</p> <p>Thank you for your consideration.</p>
107	10-Oct-17	E-Mail	I do not want the Bowie routed chosen for the high speed train.
108	12-Oct-17	E-Mail	We do not need this train coming through BOWIE or any of the surrounding areas. Take this train to Montgomery County and/or Virginia. WE DON'T WANT THE TRAIN
109	12-Oct-17	E-Mail	<p>I grew up in Lanham, I attend church in Landover and was educated at Bowie University receiving my BS and MBA. In fact while attending Bowie I had to write a paper on the radar. I found out that based on census data PG county has had the highest concentration of Black Wealth for many years. Montgomery County was added the last census. I have traveled by car and train many times to NY and fact every northern with the exception of Rhode Island. So I am not understanding how engineers have designed the route to leave DC going to NY and you go through Springdale then veer left and go through Mitchellville and then veer right and go through Bowie. When it makes more sense to go from DC through Montgomery and Howard county, which are north of PG county. Also, why would you think that it would be alright and that it would make any sense to destroy land that has housed a Historical Black College or University (HBCU) that has been around and educating people for over 150 years. The meetings you have been having not one single person has been able to answer to anyone's satisfaction how this was pushed through prior to any meeting with the community.</p>
110	12-Oct-17	E-Mail	We do not need this train coming through BOWIE or any of the surrounding areas. Take this train to Montgomery County and/or Virginia. WE DON'T WANT THE TRAIN!!
111	12-Oct-17	E-Mail	<p>Are you seriously considering taking out entire neighborhoods in Severn and reducing the housing values of entire communities for this MagLev train?</p> <p>We already have to deal with airplane noise as BWI grows and grows and are now subject to this behemoth. I suggest you seriously consider the costs, both in human terms with people being uprooted and in financial terms with so many people being directly impacted financially by a MagLev running near their neighborhood. The new highway improvements and widening should be enough. Please don't "railroad" your citizens.</p>
112	13-Oct-17	E-Mail	<p>Can you tell an "innocent" citizen, but caring for the neighborhood - what this tongue twister headline means -</p> <p>In simple sentences and maybe just pros and cons?</p> <p>I have no clue and coming to a session probably is above my head.....</p> <p>Thank you,</p> <p>Christine</p>
113	13-Oct-17	E-Mail	how can you thank you for my input when I had none and why do you not read the question so we can get an answer from a "human" and not a push-button response?
114	13-Oct-17	E-Mail	Send me a comment form on the proposed high speed rail line. Just email it to me, please. Thank you. Spencer McNeil.
115	13-Oct-17	E-Mail	<p>Hi,</p> <p>I live in College Park MD, and while I am an extremely avid supporter of passenger rail and especially high speed rail, the MAGLEV proposal between D.C. and Baltimore is completely unrealistic and a poor idea. The distance of just D.C. to Baltimore is extremely short and would simply not justify any sort of MAGLEV line due to the extremely high cost per mile to build. Having a MAGLEV line to a city farther away, most likely up the northeast corridor, would be great, although still most likely cost prohibitive without heavy federal subsidy. Overall I would highly discourage the MTA going ahead with this proposal due to the lack of financial viability for the extremely short distance.</p> <p>Thank you, Jasper Shapiro</p> <p>College Park, MD</p>
116	13-Oct-17	E-Mail	<p>Hi,</p> <p>I would also like to add that we already have MARC providing nearly identical service to the proposed MAGLEV service. The infrastructure is there for the Marc Penn and Camden line and the state should simply increase funding and improve service.</p> <p>Thanks, Jasper</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
117	13-Oct-17	E-Mail	Good afternoon, Is there a reason why there won't be a public meeting in the Linthicum region for the upcoming alternatives and screening meetings? There have been past meetings in this location. I'm sure the local community would appreciate it since they would be affected by any of the 3 proposals. Thank you, Charles
118	14-Oct-17	E-Mail	The Baltimore - Washington Superconducting MAGLEV PROJECT would help to reduce the use of fossil fuels.
119	14-Oct-17	E-Mail	I'm out of town but can u send some info
120	15-Oct-17	E-Mail	We area new to the whole issue... by virtue of a neighbor cruising the area to announce the high probability of our residences being absorbed under the assumption that the project will benefit our community(s). Understanding exactly what benefit(s) will accrue is important. Who will benefit? Are the routes going to apply rules to compel the sale of our life-long family property(s)? How far ahead are those decisions? Will litigation against eminent domain cause extensions of deadlines and jeopardize completion? It is difficult to see cost-effective travel on MAGLEV... particularly on a single two-station route from BWI Thurgood Marshall Airport to Washington, DC. Noticing the current DC Metro finance concerns and shortfalls, a hint about who/what will absorb the shortages for sure is of interest... short and long term, is vital information effecting the case for transportation "overkill" People always bring in the notion that our Balt/Wash infrastructure as a whole needs primary consideration as it is still/will contain the primary media for a long time to come. The SCMAGLEV plan seems like unwarranted competition. Finally, it appears from cursory study that foreign enterprises are not only financing, but encouraging lobbying of federal, state, and local governments. What effects will that have on US citizens who may have to sacrifice family locations and settle elsewhere... perhaps even at very late stages of life? I hope this helps answer your question, Terry On behalf of our Severn, MD neighborhood
121	15-Oct-17	E-Mail	This better not happen over or under my home. We live in communities having paid \$300-600k. You are not building this monstrosity in my community! Whoever came up with this ridiculous idea is an idiot! You will be devaluing thousands of properties...but I guess that's the gov't and big business' plan! I will be at the meeting in Laurel on 24th.
122	15-Oct-17	E-Mail	Thank you for sending information about October meetings. It is truly a shame that none of the meetings are near Cheverly, one of the areas that may be impacted.
123	17-Oct-17	E-Mail	Hello! I am a student living in DC, and I personally think the estimated cost of \$10-15 billion would be much better spent upgrading and improving existing rail infrastructure. New Carrollton is a major chokepoint for both MARC and Amtrak trains leaving and departing DC. The station and surrounding areas should be upgraded to a four-track mainline instead of two. This allows faster express trains to bypass others making their station stops at New Carrollton and other MARC stations between DC and Baltimore. This money will also be more than enough to build the Baltimore & Potomac Tunnel. Thanks for your consideration. Best, David
124	17-Oct-17	E-Mail	to whom it may concern: I am writing about my concern about the proposed SCMaglev Project. I recently heard of the project through an email from a concerned parent at Eleanor Roosevelt High School. I currently teach and coach within Prince George's County Public Schools. I am very concerned about the potential harm this project could have environmentally and physically for residents of the county and students of our school systems. I also feel that more public awareness should come forth before any decisions are made on this project. I hope that you and those you work with will carefully look into this project and its' potential cons before any decisions are made on it. I hope that our future and the potential risk for our students and children are taken into account as well. Sincerely yours, Ian Gleason Concerned Resident, teacher, and coach
125	17-Oct-17	E-Mail	Dear SCMaglevProject team, I am writing out of concern for the NEPA process. One of the current proposed routes would go within 25 yards of my property or closer, the J1-BWP Modified West alternative. Yet, the residents of my neighborhood are under-informed about the project and unaware of the public meetings which are now being conducted. If more people knew of the potential impact, the meetings would have far more attendees. In particular, it appears that one of the routes goes right through Hebron-Harmans elementary school, the most highly populated elementary school in Anne Arundel County. This is the assigned school for my children and I wonder the impact that would have on their learning environment. I would urge greater effort to notify the public, particularly those who are directly in the path of the 3 alternate routes. I would like see to see an additional meeting scheduled as far north as Harmans as it has the potential to be greatly affected by the project. -Greg Werner

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
126	18-Oct-17	E-Mail	Good Morning- I would like to schedule a meeting with your marketing team to discuss ways to get information out to the community about the Maglev Project. I grew up in the Linthicum Ferndale area my entire life. My family is a railroad family, my dad worked for Amtrak for over 40 years. I knew about the possibility of this train when I was younger and was completely amazed about the project... but it never got off the ground. I think there was a lot of confusion in the community about the project back then and I believe the community is still confused. While information is easily and quickly received through social media... so are untruths and fear. I am very excited to work with your team in promoting and educating the communities about the positives that the Maglev Project will bring.
127	18-Oct-17	E-Mail	Why would you destroy peoples homes and lives in Linthicum when you can go along or near the path of the Amtrak line where there are no houses. I've been in Linthicum for 58 years since I was 5, stayed there, bought and paid for a home and now you are going to destroy our lifestyles and property values? Unacceptable!
128	19-Oct-17	E-Mail	Please reconsider this project as to the impact on lost taxable revenues due to lost homes. How will this be replaced
129	19-Oct-17	E-Mail	John: Our crew numbered about 150. The rooms were packed. Consultants ate first then opened the doors to the crowd, right on time, 1700 hrs. Toward to end of the 3-hour session... people were still asking questions. The project director, Furgan Siddiqi, was interesting conversation... he is candid but without trading his obligations. He is well-travelled, having visited the overseas MAGLEV projects, and seems well-versed in Governor Hogan's approach to the local Baltimore-DC plans. Last, but encouraging, is the diminished number of routes being considered. The MTA was represented as well as perhaps one or two members of the Maryland Legislature. Hopefully, the 1900-2330 conflicting council meeting was as productive and challenging as our constituent get-together! Three more are scheduled in the very near future. Regards, Terry Hoebeke
130	19-Oct-17	E-Mail	To whom it may concern, I have engendered a steep interest in the SCMAGLEV project. As I graduate in May 2018, I would love to learn how joining in such an effort would be possible. If any information can be provided as to how this may be achieved, please contact me, or redirect me. Please see attached for my credentials and recommendations.
131	20-Oct-17	E-Mail	How can you justify this considering it will not be for the average commuter. Who is your target and what will the ride cost.
132	21-Oct-17	E-Mail	Good morning, I'm just seeing this information this morning about a train coming through my neighborhood. When were you going to inform everyone? When the dump trucks were parked on the streets!!!! No information was mailed or reports. WOW
133	21-Oct-17	E-Mail	My name is Charmane Croll and I live in the affected (Woodlawn) area where the Bwmaglev project is being proposed. I am not for this project. There is already a purple line coming to our area, we do not need a high powered train to come to PG county. Why do you want to put a high powered train in a low to mid income area. If this train is approved, where are you going to place these folks? Some already cannot afford housing. NO BWMAGLEV IN PG County.
134	21-Oct-17	E-Mail	I was wondering if you could tell me why the Hyperloop project did not have to go under the NEPA process like the MAGLEV train?
135	21-Oct-17	E-Mail	Dear Ma'am or Sir, I am writing to express strong support for the proposed high-speed, magnetically suspended train project from Baltimore to Washington, D.C. with a stop at BWI Airport. Furthermore I would like to call your attention to a technological approach to the project developed by a Maryland resident. At this link you will find a description of U.S. Patent #7617779: "Linear Brushless D.C. Motor with Stationary Armature and Field and with Integratable Magnetic Suspension" (the patent summary is also attached here). You will note that the system proposed is totally non-contacting, providing emission-free and nearly silent propulsion to minimize impact on surrounding right of way. Note also that the inventor resides in Maryland and has made a patent assignment in the U.S. The assignee has made a working small-scale version of this system as proof of concept. You will find that the system described therein employs the latest technology to achieve the fastest and most comfortable passenger ride at speeds comparable to aircraft, but avoids the need for extensive and costly terminal and landing facilities. The routing should be accomplished without intersecting roadways. Crucially, the concept, design and preliminary development of this technology have all been done in the United States. Respectfully submitted, Philip A. Studer, 3126 Gracefield Rd. #209, Silver Spring, MD 20904

No.	Date	Forum	Comment
136	22-Oct-17	E-Mail	<p>Mr. Trueschler, I am formally providing my opposition to the SCVMAGLEV project. In addition to all of the points provided in the form letters you have already received, the project does nothing to alleviate daily commuter issues. (Nor do I believe will Mr. Hogan's proposal for more toll roads, but that's another letter.) I believe the information provided to the public only shows how it negatively impacts us. I have seen no good justification for this project. The benefits will be to the builders and the politicians. All this does for citizens is destroy the communities and friendships we've built. I have never had to rely on public transportation, and honestly, don't think I every would. I hear too many stories of unreliability and personal danger.</p> <p>As a supporter of StopThisTrain.org, we haven't even hit on all of the health and environmental dangers inherent to drilling underground around our homes, schools and roads. All I can envision is breaking news stories about sinkholes that 'appear out of nowhere'. Is the need for this project really worth this risk?</p> <p>Maryland has been my home all my life, specifically Anne Arundel County for the last 40 years. I have seen unprecedented growth, and no plans in place to control it. All of these new efforts for increased roads haven't even considered cause and effect. It is extremely distressing to see new homes pop up in every available spec of land, and in some instances, 2 homes pop up in a lot that used to contain only 1 house.</p> <p>Where are our standards?? I would like to believe politicians are working for my best interest, but in this instance, this isn't it. Curtail new home growth for the time being, watch traffic normalize, and then see if there really is a need for this.</p> <p>I thank you for your time, and consideration of utilizing your influence to stop this train.</p>
137	23-Oct-17	E-Mail	<p>You are not going to put a high speed train through our neighborhoods from Washington, D.C. to BWI. You are not going to tear down our homes, schools and historical sites for this over priced costly train. I will vote any political official out of office that supports this train construction.</p>
138	25-Oct-17	E-Mail	<p>Hi I live in Northwood Estates off of WVBA Road. I have lived here for 30 years and don't plan on moving ever. We took almost 10 years and finally found a place that had at least 1 acre of ground to build our dream home. There is plenty of land next to the Marc train tracks on the west side of RT 170, Camp Meade Road. You do not need to up root hard working people who are retired or getting ready to retire. All the people here in Northwood Estates are either retired or getting ready to retire! It would make no sense to make people who are late in life to make them move. I highly oppose any thoughts of running this train under or near Northwood Estates. Sincerely, Danny Riggin SR. 410-761-8505 7606 Northwood Estates Court Severn MD. 21144</p>
139	25-Oct-17	E-Mail	<p>Good afternoon</p> <p>My name is Kimberly Sneed and I've been a home owner in South Laurel for over 20 years. Montpelier Hills is the development where my lovely town home is located. It's a diverse community of tax paying voting residents who are deeply troubled by your proposal to possibly build a high speed light rail displacing over 200 residents for a project that won't benefit Laurel or any of the other proposed routes at all.</p> <p>What are you going to tell the first time homeowner who just bought their first home on Blue Moon Court last year after scrimping and saving to live the American Dream?</p> <p>What are you going to tell the the homeowner who is rebounding from the mortgage crisis and currently owes more on their home than the home is worth?</p> <p>What are you going to tell the empty nester who raised their kids, sent them to college and now hoped to retire and remain in Maryland?</p> <p>Hopefully this train won't be built anywhere BUT if you MUST build it, it should be built where there is NO residential homes. Have you thought of what happens in a terrorist-incident or accident? An entire community could be impacted.</p> <p>In closing I hope you will reconsider your plan and leave the residents of South Laurel alone. We have already endured the mortgage crisis in disproportionate numbers. Even if our homes are not destroyed the value of the property will decline due to the nature of a high speed rail so close to the homes -- not to mention potential health issues for people wearing pacemakers and/or hearing aids.</p> <p>Thank you</p>
140	25-Oct-17	E-Mail	<p>The MagLev train, which is proposed to run in parallel with the BW Parkway, is likely to produce noise equally annoying as road traffic, and significantly worse than a standard intercity train. At high speeds, the MagLev apparently can produce noise as great as some aircraft. This issue has been ignored, probably because most people assume that a levitated train would produce less noise.</p> <p>Any plan for the train would require noise abatement planning, especially as the noise will be added to the existing traffic noise. I do not support construction of the train.</p> <p>Ref: Vos J. Annoyance caused by sounds of a magnetic levitation train. J Acoust Soc Am. 2004 Apr; 115(4): 1597.</p> <p>https://www.theguardian.com/science/2004/apr/15/research.science</p> <p>Christina M. Yuan, Laurel, MD</p>

No.	Date	Forum	Comment
141	25-Oct-17	E-Mail	<p>I have reviewed the maps showing the 2 proposed MagLev tracks paralleling the BW Parkway, and the alternative track that is shifted eastward. All of the proposed track and maintenance yards abut, and in some cases appear to be inside the borders of the Patuxent Environmental Science Center and the Research Refuge. They impinge the grounds of the Beltsville Agricultural Research Center, the Montpelier Historic Site, Maryland City Park, Patuxent River Park, and the wetlands surrounding the Patuxent River—most significantly in the areas north and south of the 197/BW interchange. These consist of areas of special concern and Chesapeake Bay critical areas.</p> <p>It would be impossible for this project not to adversely impact precious green space and wetlands—and in areas specially set aside to protect them. Not only that, the electrical power needed to operate the trains (the system is cryogenically cooled) and the infrastructure to deliver it are also likely to adversely affect the environment. This is the antithesis of a “green” project.</p> <p>Not only that, these proposed routes are also likely to impinge on/impact schools (notably Maryland City School, Eleanor Roosevelt High School, and Bowie State University), as well as impacting Ft Meade, the NSA, the FBI, NASA Goddard, and the WSSC water treatment plant. Many neighborhoods will be fragmented (particularly Maryland City and Russet), and some will lose their homes/or their homes will be less valuable and less livable. Roads that cross the track, and some interchanges of the BW parkway will have to be rebuilt (their ramps would cross the MagLev)—and access to areas “across” the track will likely to be complex. Who is going to pay for all of the roadwork? How will that impact the environment?.</p> <p>Quality of life for residents and business along the track will deteriorate. There will be increased noise, pollution, and years of road/track and electrical infrastructure building that will interfere with/and likely worsen traffic flow.</p> <p>The MagLev Project has not demonstrated any general public benefit that would compensate for the social disruption, environmental loss, and energy/financial expenditure that will be caused. This is not public transportation—there are no stops in Prince Georges’s county. The only travelers to benefit will be those traveling between Washington DC and BWI Airport/Baltimore. The citizens who bear the burden of the construction will not benefit in any way. At worst, the citizens who would lose their property to eminent domain, or suffer a loss in the value of their property, will experience no commensurate general benefit to the community or improvement in local transportation. The reverse is likely.</p> <p>This is a vanity project, which has not even been shown to be economically viable as public transportation, and will have severe adverse environmental and transportation impacts. It should be abandoned.</p> <p>Christina M. Yuan, Laurel, MD</p>
142	26-Oct-17	E-Mail	<p>I’m a resident of Laurel, Md Sumner Grove neighborhood along I-295 south bound side. The MagLev project speaks in great detail about the environmental impact of this undesirable project. However, I’m struggling to find any data on the site that speaks to the human impact of this project. Similar to any other details, the potential impact on displaced humans. Neighborhoods, businesses, grocery stores, gas stations, apartment building, parks, habitats, etc, that will be impacted this project. Please direct me to this data.</p>
143	26-Oct-17	E-Mail	<p>My family and I live at 9010 Sumner Grove Dr, Laurel, Maryland in the Sumner Grove housing development. This development runs parallel to BW Parkway on the southbound side, just north of Power Mill Road and south of Route 197. The project has failed to properly notify Prince George county citizens of the ongoing study.</p>
144	26-Oct-17	E-Mail	<p>I am writing on behalf of the City of Greenbelt. In reviewing the interactive map of the route alternatives being considered there appears to be some errors. Greenbelt Parks are not shown on the parks map layer. There is land shown on the west side of the BW Parkway in Greenbelt as Federally owned land which I believed is owned by the City of Greenbelt. Also, the map should have a layer that shows historic designations (i.e., National Historic Landmark, National Historic District, Designated historic sites, etc.)</p> <p>Thank You, Terri Hruby Terri Hruby, AICP Acting Director of Planning and Community Development City of Greenbelt, 15 Crescent Road, Ste. 200, Greenbelt, MD 20770</p>
145	26-Oct-17	E-Mail	<p>I sent the following to the Baltimorewashingtonscmaglevproject: Superconducting Maglev was invented by Drs. James Powell and Gordon Danby in 1966. Since then the their system has been demonstrated by Japan at the Yamanashi Test Facility and Powell and Danby have developed an improved design which is more capable than the design demonstrated by Japan. The more advanced SCMaglev is capable of operating in a planar mode as well as a monorail mode. As a result it is capable of electronically switching at high speeds making it much easier to switch off line to passenger stations and parallel guideways in freight terminals. The new magnet design also is capable of lifting much heavier loads and is ideal for carrying freight trucks and delivery vans in roll-on, roll-off Maglev carriers, as well as passengers, at average speeds of 300 mph. We believe that this is the ideal system for the US to build out its heavily traveled interstate/intercity corridors and will eventually be the basis for a national network. See www.magneticglide.com for the concept. We have envisioned a system a 25,000 mile long network that could be built without public funds that could finance itself from lower cost fares and lower cost freight truck shipping from producer locations. Dr. Powell and Danby’s story and vision has been published in The Fight for Maglev and Maglev America, available on Amazon. SCMaglev has great potential as a new industry in the US and would be a job creator. In Maglev America we describe a commuter subway Maglev system that could convert our steel wheel rail commuter systems to Maglev. Without rails and very efficient electric power requirements this system could carry commuters at much lower fares than the rail systems currently in use because the operations and maintenance costs are much cheaper.</p> <p>We recommend that the Maryland Transit Administration consider competing the Powell and Danby Maglev 2000 system with any other contenders.</p> <p>You have our permission to release our request to the press and media.</p> <p>James Jordan Executive VP Maglev 2000</p>

No.	Date	Forum	Comment
146	26-Oct-17	E-Mail	<p>Hi,</p> <p>I was informed by my Maryland representative regarding a community meeting about the MagLev train this coming November 9th. This was the first that I had even heard about this train. I intend to attend that meeting, but I have several questions regarding the proposed routes for the MagLev train in the meantime. I looked on your three possible routes on your website, and my street is included in one of them for a proposed tunnel route underneath. On the tunnel routes, am I correct in assuming that the houses above the tunnel will not be taken by eminent domain or be forced to sell at fair market value? I am assuming this because these areas are heavily populated by residential homes, businesses, schools, and churches. Please let me know if my assumption is not correct. If my assumption is correct, what safety measures will be put in place to assure that our foundations and sewer, water, and gas lines will not be cracked when boring begins. We had sewer lines replaced a couple of years ago, and it was as if there was an earthquake every day they were working on them. What about the safety of residents living above such a widespread magnetic field. Will this affect our health and our appliances and devices? Are there going to be vibrations felt in the house every time a train passes underneath? There have been reports that when Metro switched over to the newer train cars, residents above the Metro tunnels now feel their houses shake when a train goes underneath. This is unacceptable. I need some answers regarding these specific questions, and please don't just send a scripted generalized response. Surely you must have someone dealing with responding to individual questions such as mine. I imagine that as more people learn of this train in the community, you will be getting many more questions such as these. No one in my neighborhood had even heard about this train proposal yet.</p> <p>Thanks, Anita Melichar</p>
147	26-Oct-17	E-Mail	<p>Please make sure the ESIS evaluates the impacts of construction works, including traffic, laydown yards, camps, and associated facilities. For example, the road 197 near Bowie State University is the main corridor from Bowie to the Baltimore-Washington Parkway—and is a two lane road that is very congested at several times of day (making a left turn nearly impossible, at times). If you have a maintenance depot on the Jericho Park side of Bowie State University, not only does it show that it cuts off 4 baseball fields for the community, but the traffic to and from the maintenance yards would be devastating to this little two lane road—you would need to widen the road and have an overpass or exit to the maintenance yards to allow traffic flow.</p> <p>I've been working in the area of ESIA and stakeholder engagement on infrastructure projects for 25 years, and the biggest deficiency in ESIA's that I see are the failure to assess community impacts, to evaluate ancillary facilities and construction impacts—all too often, they focus on the impacts of the operational stage of the project (e.g., train noise, vibrations, route), and completely ignore laydown yards, temporary or permanent traffic flow changes, etc. I hope that there has been a detailed scoping exercise of the ESIA, so that none of these associated (direct or indirect) impacts are omitted—whether the maintenance yard is near Bowie State University or elsewhere, all of these impacts should also be assessed.</p> <p>Best regards, Elizabeth Smith Elizabeth Temple Smith Etemplesmith@aol.com</p>
148	26-Oct-17	E-Mail	<p>Please use the home e-mail address I included, and not a work one. etemplesmith@aol.com</p>
149	27-Oct-17	E-Mail	<p>What is the date by which comments should be submitted for input to the next phase of the process?</p> <p>thanks</p> <p>Dear Mr. Al Khattab,</p>
150	28-Oct-17	E-Mail	<p>I write to oppose SCMagLev even though my property and my neighborhood aren't threatened. There are many issues, to the best of my knowledge, that haven't been addressed, for example, emergency management issue regarding fire and/or other emergencies requiring passenger evacuation. It seems to me that access points should have 100 ft of a buffer within which all buildings must be made of fire-resistant materials. The shafts have to be ventilated and should be wide enough to extricate passengers in case of emergency. Seems to me that even the tunnels will have a severe impact on the neighborhoods above.</p> <p>I have another concern regarding the financial viability of the project during its operational phase. It's not clear there will be enough demand for the service to pay for the operational costs. Even Amtrak needs constant public funding to stay afloat. Who is going to pay for the deficit in the operational costs, not to mention the depreciation of the capital assets?</p> <p>Given the expected cost of travel \$1-\$2/mile between DC and Baltimore, I wonder how many commuters will be willing to pay for the ride. In my honest opinion, there aren't too many commuters between DC and Baltimore for whom SCMagLev will be a viable option. Most commuters drive from somewhere between DC and Baltimore to DC or to Baltimore. Since having a stop in Anne Arundel County (other than BWI) or PG County defeats the purpose of high-speed rail, this proposal offers no benefit to either of these two counties while severely impacting these counties in terms of neighborhood dislocation, quality of life given electromagnetic radiation and noise level associated with high-speed trains.</p> <p>I am also concerned about the impact of this train on falling value of the property near and above the track resulting in the loss of property tax revenue for Anne Arundel & PG counties. Citizens of these counties will be asked to bear the cost of this rail without any benefit.</p> <p>Amtrak is planning to upgrade its Acela trains which will significantly lower the travel time. There are already MARC trains as well as Metro rails that take commuters to DC. I, for one, travel on Metro rail every day from Bowie to DC. In the end, I don't see any benefit to the citizens in Anne Arundel and PG counties but a potential higher taxes to fund the deficit in the operating costs of SCMagLev and loss of tax base.</p> <p>To summarize, I am opposed to SCMagLev. Please count me out as far as SCMagLev is concerned.</p> <p>Sincerely, Sujit Ray, 13315 Yarland Lane, Bowie MD 20715</p>
151	28-Oct-17	E-Mail	<p>While public transportation options are an important part of reducing congestion and pollution, I oppose all current MAGLEV proposals. They do not serve Prince George's county as there will be no stop located here. But the MAGLEV train will be above ground increasing noise pollution and congestion. It is below ground both inside the district and in Anne Arundel county. Secure the financing to put it underground in Prince George's county too and please don't base your cost savings plan on race and class.</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
152	29-Oct-17	E-Mail	I am greatly opposed to the MAGLEV train running through the Greenbelt Forest Preserve. My family, friends and my Girl Scouts enjoy taking walks through the forest. We are also concerned about the added noise level in our community. This project will not benefit me or my neighbors but we will be greatly effected by its development.
153	30-Oct-17	E-Mail	I have been a resident of Greenbelt since 2006, and I am writing to oppose the proposed Maglev Line that would cut through Greenbelt Woods. The woods are an important part of our community and have been fought for by members of our community again and again over time. I do not see this line as a potential benefit to our area, and it is not a resource that serve a significant portion of our community. As someone who lives bordering the woods and chose to buy here for that reason, I am opposed to the destruction of the Greenbelt Woods for this project.
154	31-Oct-17	E-Mail	The bullet points should be changed to what is actually needed - several of the bullet points are existing issues. If the poster is read as is, as an example, it is saying that the region needs decreasing mobility. This is not true and is a concern based on how the poster is presented. The bullet points should either be changed to discuss the needs and not deficiencies, or it should separate needs from existing issues. Thank you, Adam Greenstein, Laurel, MD
155	31-Oct-17	E-Mail	Hi there, This is Andrew Zaleski, a freelance reporter with CityLab, the city-focused news site published by The Atlantic in D.C. Is there someone at BWRR I could get on the phone tomorrow? I have a few questions about the maps released last week showing potential routes of a maglev train between Baltimore and D.C., and I also have some questions about how the environmental impact study is progressing. Just looking for about 15 minutes by phone. Tomorrow I'm free between 10 a.m. and 2 p.m. EST, and again from 3 p.m. EST on. Best, Andrew
156	1-Nov-17	E-Mail	R U kidding me? Insane project uprooting so many of us who have lived our entire lives in this area. U gotta do some more thinking on this one and come up with an alternative route WITHOUT disrupting our lives. For the record, I am OPPOSED to this project.
157	1-Nov-17	E-Mail	Good Morning, My name is Nyla Howell, I am a student Journalist for my schools newspaper at Eleanor Roosevelt High School. I am writing an article concerning the plans for MAGLEV Train for the city of Greenbelt. I have a few questions on how it will affect our community and what benefits would come from the construction of this train. I look forward to hearing from you, thank you for your time. Sincerely, Nyla Howell
158	1-Nov-17	E-Mail	Hi, Just following up about speaking with someone at the BWRR about the maglev project. I caught the Sun piece from today, but wanted to ask some questions of a BWRR rep directly. Tomorrow my day is open from 11 a.m. to 2:30 p.m. EST. Best, Andrew
159	31-Oct-17	E-Mail	Dear Mir Khatib: I would like to suggest we elevate urban ecology by constructing vertical gardens on the MAGLEV pillars near our most populated areas. It has successfully been done in Mexico City, Mexico. Their greenway project has transformed hundreds of pillars supporting flyovers and elevated roads into vertical gardens in order to reduce pollution while improving the cityscape. The process includes placing metal frames and cloth on top of the pillars to avoid damage to the concrete structures. This cloth has pockets and a special density that allows the root of the plants to interweave into this cloth. Each column has an automatic watering system that also works together with rain water. The urban vertical garden will lift the air quality and the mood of the people passing by! To view a video: www.mobile.reuters.com Thank you for all your efforts in this matter! Best regards, Lori Obispo, 2930 Levee Dr, Odenton, MD 21113
160	11/1/2017	E-Mail	Is the project really a super conductor? What is the science behind this?
161	2-Nov-17	E-Mail	I would to know if there will be any more town hall meetings on the high speed rail?
<i>Comments to Governor's Office</i>			
1	16-Apr-17	Gov. Office	Good evening, I'm trying to understand your thinking to place the Meglev train replacing the WBA trail and destroying green space. Why not use the current Amtrak Acela tracks? Acela loses money so this route offers current infrastructure with a viable way to make money. Out of curiosity, what trade deals are you making with Japan for this system that benefits you? Also, how much is Japan contributing to your next campaign? I look forward to speaking with you. I am a three war vet, clergy person and federal constituent. I deserve a response and honest conversation with you. Dr. Karis Graham 2029051713
2	21-Apr-17	Gov. Office	I am writing AGAIN. Still no response other than the auto response that comes seconds after I hit send. I would APPRECIATE a real response. The proposed MAGLEV train has 6 routes under discussion. Two of them will run through my farm and destroy my business. A third will significantly impact my community (Bowie). I would like to know the Governor's stand on this ridiculous project and what he will do to protect our community.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
3	28-Apr-17	Gov. Office	Please stop this train from coming anywhere near Bowie or destroying anyone's neighborhoods. This will be detrimental to the neighborhoods wherever is built. The only people that would benefit from this is Washington DC and the rich who would use it to go back and forth. It would not serve your average marylander so why ruin our homes for this. I have supported most of your initiatives and I voted for you and I'm not happy that this is even being considered in our state.
4	2-May-17	Gov. Office	I am writing to express my concerns AGAINST the Maglev Train route through Bowie, MD. Last week I learned that the preferred route for this new project is the yellow route which will go right through my neighborhood and possibly my house. Like many others in our neighborhood, we have worked so hard to pay for our home -- our only home of 26 years. It will be paid off in one year and never in my wildest dreams did I think something like this would happen to our 50 year old, established, quite, safe neighborhood complete with schools, churches, a golf course, and shopping. Rockledge is an eclectic mix of young families to retirees all with the same need for affordable housing that is convenient to work. Governor Hogan, I implore you to recommend that this development be routed AWAY FROM BOWIE and my home that I'm in danger of losing if this happens. Thank you and I would appreciate that you acknowledge receipt of this email. Gail Sinkovic
5	5-May-17	Gov. Office	As a constituent and as a person likely to be closely affected by the proposed maglev train I urge you to shelve this project. This train would benefit a small number of elites while imposing heavy costs on the communities along the route, both in the construction and operation. This is not how a governor of ALL the people should govern. Thank you for your attention, Andrew McIntire
6	9-May-17	Gov. Office	Dear Governor Hogan, My name is Garin Sinkovic, I am a sophomore in high school, 16 years old, and a graduate of Youth Leadership Bowie. The reason I am writing this email is to voice my opinions AGAINST the Maglev Train Project which has a preferred route through my hometown, Bowie. I have lived in Bowie, Md my entire life and it has been the best place to live. It's safe, quiet, modern, and a wonderful place to grow up. I don't want to see this train run right through my neighborhood, Rockledge, which would completely change Bowie in a negative way. Bowie will not be the same if there is a big train running through it. It will no longer be safe and quiet with the train. There are already many cost effective ways of transportation such as cars, buses, and other trains. In the world we live in today, where there are very few rich people, the hard working class will not be able to afford what this train will cost. If you must waste your money on this train, please find another route that won't take away people's houses and property that they have worked so hard to pay for. Please reconsider your thoughts and I would appreciate to hear back from the email as soon as possible. Sincerely, Garin Sinkovic
7	11-May-17	Gov. Office	I am very concerned about the possibility of the MagLev high speed train coming through Prince George's County, especially the yellow route through Bowie. Has anyone walked, driven by or inspected these routes? The yellow route runs right through my development which is relatively new, numerous neighborhoods, at least one elementary school, a golf course, soccer fields, and a medical facility in Glenn Dale, just to name a few. This project team needs to do a much better job at reaching out to residents and informing them of this threat to their homes, property values, and amenities like the beautiful Washington, Baltimore & Annapolis Trail. The vast majority of my neighbors have not heard about this threat. MagLev offers no benefit to our city or County. It also appears that the intent is to run this through areas that are primarily minority and/or low income. This is unacceptable. I respectfully ask for your assistance in having the project team consider running this train underground to minimize the impact on residents or utilize the existing Amtrak route or the BW Parkway. Doesn't the government already own the rights-of-ways for these options? That should be less expensive than acquiring the rights to tear down new homes, while displacing thousands upon thousands so the wealthy people who can afford tickets can travel faster. Please provide me with the name of the ultimate decision maker as soon as possible so I can reach out to them as well as answers to my questions. I would appreciate a reply. Best regards, Letitia Carolina-Powell
8	17-May-17	Gov. Office	To Governor Larry Hogan I am opposed to the Superconducting MagLev Project being built through Bowie, MD. The MagLev line will not benefit the community, and will greatly devalue homes in the area. Please do everything you can to make sure the MagLev project does not come through Bowie. Sincerely, Patricia Grossmann
9	23-May-17	Gov. Office	I am opposed to the NEMeg lev project and want you to stop supporting it for the following reasons: 1. De-Rails needed funding for current highway,bridges,tunnels, subway and Amtrak's Avelia project. 2. Takes private property 3. Destroys the environment, historical areas, recreational areas, property values. 4. Life cycle cost is not beneficial to citizens 5. Too costly in terms of tax dollars. Baltimore to DC line \$250 million/ mile and Baltimore to NYC line at \$48 trillion. 6. Why did Montgomery and Howard county turn project down ? 7. Will not achieve claimed speeds in the Baltimore-BWI-DC section. 8. You were sold a "bill of goods" by Japan
10	26-May-17	Gov. Office	Did you know the SCMAGLEV project will do nothing for relieving commuter pressure? You can pay for a limo with a driver for the same trip for less than they're proposing and still have change left over for coffee! Don't destroy tax generating homes and businesses for this project. ALSO YOUR FORM EMAIL BOUNCES THIS IS WHY YOU AREN'T GETTING COMMENTS 5.3.0 - Other mail system problem 550-"5.7.1 Unauthenticated email from yahoo.com is not accepted due to domain's\n5.7.1 DMARC policy.
11	29-May-17	Gov. Office	Dear Governor Hogan, Consider speaking out about one essential fact: before any high speed train is possible, a straight line right of way between Washington and Baltimore is necessary. IF fixed, Acela, now constrained to "slow," could then offer high speed travel between subway serviced city centers using existing resources, including the new B&P tunnel. Without it, there will be no city center to city center high speed train. Edison, also greatly respected, championed DC. He did not prevail. Special interest groups tried to push Boeing into building "America's" supersonic transatlantic aircraft. Boeing, evaluating the economics, said no, leaving only the Concorde as a continuing economic burden on the taxpayers for the benefit of the few. By remaining quiet, people may associate you with Edison. For your benefit, you should stop the needless MAGLEV hysteria starting because of you MAGLEV train ride and statements. Public concerns are also about a private consortium promoting that Maryland tax payers pay for building and subsidizing MAGLEV train, which they - the taxpayers - don't want and don't need. Because of today's economy, and denied federal funds, the time for Maryland to lead in MAGLEV is long past. Respectfully, Bob Schunemann, Odenton, Maryland

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
12	31-May-17	Gov. Office	Governor Hogan: I sent this email below to info@BaltimoreWashingtonSCMaglevProject.com and received no response. I believe my question was a reasonable engineering query! Please try to speed up a response! Thank you. Respectfully, From: EHK Sent: Friday, April 7, 2017 7:55 PM To: info@BaltimoreWashingtonSCMaglevProject.com Subject: SCMAGLEV System between Washington DC and Baltimore MD Operating of a high-speed superconducting magnetic levitation (SCMAGLEV) system between Washington, DC and Baltimore, MD (with an intermediate stop at BWI Airport) has been proposed as taking a time of only 10 minutes. In this 40 mile trip, how does the train mitigate against breaking passenger's necks during starting and stopping time periods? Please provide applicable performance and design specifications. Thank you. Respectfully, ek
13	21-Jun-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Tracy Thompson Content_2017-06-21.pdf
14	11-Jul-17	Gov. Office	This is being submitted on behalf of Home Owner. My relationship to the organization is Member. Original Message: This BWRR MagLev project is a very bad idea & destructive to the Bowie community. If this goes through you will not be on the top of my list during the next election. And I hope all the Bowie residence feel the same when its time to elect a governor for Maryland.
15	11-Jul-17	Gov. Office	I attended an overflow meeting in Bowie City Hall last night where Bowie learned about a more then potential plan to build a MagLev line between DC and Baltimore. We learned that a potential route through Bowie would require the destruction of over 250 homes and a grade school in the Rockledge section of the city. Obviously more home would be destroyed in Anne Arundel County in Odenton and elsewhere to build this line. There is no intrinsic benefit to Prince Georges and Anne Arundel County if the line is built. What was most disturbing about this is that there have been multiple approvals granted for this line and an obvious attempt to keep the information on the line quiet so that opposition would be too late and futile. This plan would make it impossible for the home owners to do anything with their property as the planned transport line would essentially freeze their properties. Of the more than 25 people who spoke only one was in favor. When the audience was polled everyone stood up in opposition to the Mag Lev Line.
16	11-Jul-17	Gov. Office	I am contacting you to ask that you NOT support the proposed smaglev train from Baltimore to Washington DC. At a time when the state of Maryland is struggling to pay for schools, roads and healthcare I think it is fiscally reckless and irresponsible to finance a \$5-10 billion magnetic train. This proposed train will harm the environment and displace hundreds of homes, local business' and schools. Please respond and let me know what your position is. I voted for you and I am hopeful that you will Change your mind and NOT support this project! Thank you
17	12-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Bob Schunemann_2017-07-12.pdf Dear Governor Hogan,
18	19-Jul-17	Gov. Office	I have attached an image overlay which will allow you to zoom in and see the potential impact of the MAGLEV preferred yellow line on your constituents. It is not precisely aligned but very close. This will enable you to launch google earth in a chrome browser, load the overlay and zoom in to your area of interest to view the enormous housing impact to communities in Prince Georges and Anne Arundel Counties. Instructions: Download attached kmz launch google earth in a chrome browser Enable kml import in settings (It should prompt you if not go to settings) Go to my places and add MaglevOverlay.kmz Zoom to area of interest If I have time I will try make a more precisely aligned map. The Maglev train will destroy the homes of thousands of your constituents in Bowie and Piney Orchard communities that helped vote you into office along with many other communities. You have enjoyed great popularity thus far in your term. The word is starting to get out about the impact of this project however, and your popularity will most certainly be tarnished. Please reconsider your support for this ill conceived show case for Japanese technology. A high speed rail on a dedicated track would offer comparable speeds for a fraction of the cost and much less disruption.
19	22-Jul-17	Gov. Office	Dear Governor Lawrence Hogan, I am writing to you regarding the Yellow (Alternative G) SCMAGLEV train route proposed through Bowie, Maryland. One of the proposed routes which is Yellow (Alternative G) comes through our wonderful Rockledge neighborhood here in Bowie, Maryland. I just purchased my 'forever' home here in April of 2016 and plan on retiring here. I enjoy my home very much and love my neighborhood. If this proposed route (Yellow Alternative G) comes through our Rockledge neighborhood me and all my neighbors will be displaced by eminent domain. Please Governor Hogan, do not allow this Yellow Alternative G route to run through Bowie, Maryland.
20	22-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Andrew McIntire_2017-07-22.pdf
21	23-Jul-17	Gov. Office	Please consider your concerned citizens and loyal Republican voters when determining where or if this train will be installed in Anne Arundel County. From the research that I have done, this train will devalue many homes in the Piney Orchard, Odenton and Gambrills area. It will not alleviate traffic for those citizens since the only way to use this train will be to drive to D.C. Or Baltimore. This issue could really lose the next election for you.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
22	23-Jul-17	Gov. Office	Dear Governor Hogan a? "As one of the many members of the Odenton/Gambrills/Piney Orchard Communities we are incensed with your support of the Mag Lev proposal. We in these communities surely believe we will suffer the many physical, economic, and emotional externalities inflicted by the Meg Lev Train. In fact we already have a?" the masses are upset, houses are beginning to go up for sale and the result will be declining property values. Many residents are concerned about eminent domain. The others fear that will be in shadow of the Mag Lev and will get nothing except a reduced home value and a future of financial turmoil if they are just in the wake of this albatross. In the last election you received 67% of the vote in Anne Arundel County. Generally, people here have liked and supported you a?" however, when you go the various community meetings or talks with our neighbors the sentiment has vastly changed a?the pitchforks have come outa?f. You and the administration are simply seen as the enemy. We can guarantee you the entire corridor between Linthicum, Severn, Odenton, Gambrills, Piney Orchard through Bowie and into Prince Georges County will be well organized going into the 2018 election and will oppose any and all candidates or administration supporting the Mag Lev. Our homes and families are too important not to. I very much believe that you or other high officials in your organization need to get involved, feel our pain, and hear our concerns because we are very angry. As a starter I suggest that you or your representatives begin to attend the Bowie Town Hall Mag Lev Meetings. They meet every other Thursday at 7:30 PM with the next meeting scheduled for 8/3/17. Additionally I am adding a link to the initial grass root site http://stopthistrain.org/ which opposes the Mag Lev proposal.
23	24-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Marsha Salzberg_2017-07-24.pdf
24	24-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Arnett Sanders_2017-07-24.pdf
25	25-Jul-17	Gov. Office	As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project. Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project. Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.
26	25-Jul-17	Gov. Office	This is a letter of Protest. I am a 22 year resident of Bowie. In the last couple of weeks, I was told by a neighbor about the Governors support for a MagLev passing through my neighborhood. Surprise. That is a gross understatement. There was supposedly a public hearing on this proposal? I want to know why My neighbors and I were not notified about any of this. The impact of the proposed routes is significantly negative to my property value, my neighborhood as a whole. There is no benefit of this plan to my city. Why has the Governor failed to notify people affected? Is this a back door deal? As a public servant, this is a betrayal of trust, with deep personal implications. Why was I not notified of public hearings!
27	28-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Rose Smith_2017-07-28.pdf
28	29-Jul-17	Gov. Office	I am a resident of Bowie, MD. My home is directly under the yellow corridor for the proposed MAGLEV. If these plans go through, my home will be taken and destroyed. I have worked hard for over 16 years to pay for my house, and this could destroy everything I have worked for. Bowie is a wonderful place to live. This MAGLEV would destroy our city, and provide zero benefits to residents. I strongly oppose the MAGLEV project in any area of Maryland. Instead of a MAGLEV for the wealthy few traveling long distances, it would make much more sense to invest in updating our already existing MARC and Amtrak lines for the everyday commuters in our community. As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.
29	29-Jul-17	Gov. Office	I am protesting the way in which the entire Mag Lev transportation project is being foisted on the Maryland public. Bowie is in the path of this nongovernmental private project and the citizens of Bowie seem to have little say in the future of the project. Meanwhile home values are being distorted and the ability to make decisions about future life events are being seriously affected. We should not be the objects of private experimentation with transportation.

No.	Date	Forum	Comment
30	31-Jul-17	Gov. Office	Please be advised you will receive a hard copy of this email via US Mail July 31, 2017 Maryland Department of Transportation Bradley Smith, Director of Office of Freight and Multimodalism /201 Corporate Center Drive, Hanover, MD 21076 SCMAGLEV Project, c/o John G. Trueschler Maryland Transit Administration 6 Saint Paul Street, Baltimore, MD 21202 RE: SCMAGLEV a? Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act Dear Mssrs. Smith and Trueschler, As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses. In addition, the FRA and MTA have failed to publicize citizensa?T rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.We await your response to these requests.
31	31-Jul-17	Gov. Office	Dear Governor, I thank you for your service to our great state of Maryland. As a Democrat, I have been pretty pleased with how you have worked for all of its citizens, and I was thinking you d have my vote in the next elections. However, recently I've heard details of the MAGLEV high speed train that would cut through Odenton. I've recently learned that two of the proposed routes would have severe negative impacts on me and my neighbors. My family often walks along the WB&A walking trail. It is beautiful with thriving plants and animals and it's well used by Odenton citizens, both for exercise and to get to work and shops. By building the MAGLEV train along that path, people and animals would lose their homes and our property values would plummet. The other proposed path is even worse for my community because it goes right down my street, meaning my neighbors and I would all lose our homes. I purchased my home for around \$660,000 in 2005. After various expensive projects, we've put in around \$750,000 into this house. The housing market tanked, but we didn't worry because we would be in this home until we retired, and by then the property values would certainly recover. But if the MAGLEV train is built on either of these paths, there is no hope for us to recover from the purchase of our home. Are you prepared to pay \$750,000 or more for all of the homes in Chapel Grove that would be destroyed? My final point is that this train would almost exclusively benefit corporations and the wealthy. Amtrak is already out of reach for the middle class, and this train will be much more expensive. We already have the Acela high speed train in place. Is this really worth the billions of dollars of taxpayer money to benefit the wealthy and corporations? Please look at the real impact on your citizens, and reject the MAGLEV train. If you do the right thing and reject the train, I will be a true Hogan supporter.
32	31-Jul-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Alan Kolski_2017-07-31.pdf
33	1-Aug-17	Gov. Office	Dear Mssrs. Smith and Trueschler: As there has been no public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project. In addition, the FRA and MTA have failed to publicize citizensa?T rights to participate under Section 106 of the National Historic Preservation Act. My home and my community are in the path of the MAGLEV project, and I have not received any information from MTA regarding the project. Neither I nor my neighbors have been given the opportunity to comment on the project or participate in the NEPA compliance process. I am formally requesting that MTA reopen the scoping period for this project. Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. I heard of the MAGLEV project from a neighbor and friend. Citizen word of mouth has been the only outreach on this project. This entire thought is all about the high classed dollar bill and not about your citizens of Maryland. To even think to uproot hundreds of families for a high speed train that the lower class and blue collar worker could never afford is absolutely ridiculous and unconscionable.
34	1-Aug-17	Gov. Office	Dear Governor Hogan, You have been a refreshing change in leadership and I appreciate your efforts. I must urge you to not support Maglev development which would cause the destruction of many homes and neighborhoods in Bowie. Technology is great but not when it comes at the expense of one's home or hometown, the cost is outrageously out of sync. Very little benefit accrues to Bowie itself but to relatively affluent outside travelers. I can not support a decision that has middle class citizens of Bowie pay a price to mainly benefit more well to do travelers. Nor will I be able to support an elected official who does not realize that cost is unfair. It is a completely different story if the Maglev is only considered being built along the existing Amtrak right of way.
35	1-Aug-17	Gov. Office	Why are residents just finding out that their houses, their neighborhoods, their property values are at risk for a local experimental train? More than a year has passed and people in this area are left groping in the dark. Does the Governor understand that proper notification was not given to thousands of residents within the proposed routing? Does the Governor have time to talk to the residents of Bowie? Will he be able to promise that property values will be subsidized by the state if they are devalued as a result of this proposal?
36	2-Aug-17	Gov. Office	Good Afternoon. I have recently become aware of a project that could possible affect my family, my home, and my community. After doing a bit of research, it has become clear to me that the steps that have been made thus far, have been done in a way, so that our community has not been involved, nor had a say in this enormous proposition. I live lived in Anne Arundel for 30+ years. I have raised a family in our current home for 16 years, and the idea that a train could possible be at my back door is very scary. The idea that I may lose my home is even scarier. I have a special needs child, that his wheelchair bound, and our house was built to accommodate him. I can't imagine having to leave here. If our house would not be taken, the value would surely go down, because no one would want to live here. I live in Four Seasons, and we will directly be affected, along with a new community Piney Orchard. I ask you from the bottom of my heart.....please do not let them build a train near our home or our community. There is more to life then how fast we can get from one city to another. This project will severely affect lives. Please, do not give the budge approval for this project. I ask of you...please do not.
37	3-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Kim Vejarano_2017-08-03.pdf
38	3-Aug-17	Gov. Office	As a concerned Bowie resident, I am writing to you about the proposed maglev train. One major thing that concerns me is the estimated cost of \$10-\$12 billion dollars. Recently, there was a feature on Public Television about Japana? Ts effort to expand its maglev train system. The cost estimate was \$1 Trillion (thata?Ts a T) dollars for every 10 miles. This is a far cry from the estimates for our train. Given all of the concerns for the disruption this project will entail and whoa?Ts going to ride this train, we are against this project. William & Gail Bagaria

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
39	3-Aug-17	Gov. Office	Understand you met with Bowie City Council re Maglev train "plans". Their current "preferred" route runs thru my house and 300 others in Bowie. There's no reason to build this, no possible profit, and great risk for the state. But my concern is our homes. Most think this is a pipe dream that will never happen. Meanwhile, our property values will take a hit and sales will be nearly impossible until this is resolved. I am a retiree who has lived in my home for 45 years. Please end this nonsense quickly. Japan still has this train on a test track. Send them back for more testing! Thanks.
40	5-Aug-17	Gov. Office	As a concerned Bowie resident, I am writing to you about the proposed maglev train. My house is in the path of one of the proposed sites. I do not want to lose my home. One major thing that concerns me is the estimated cost of \$10-\$12 billion dollars. Recently, there was a feature on Public Television about Japan's effort to expand its maglev train system. The cost estimate was \$1 Trillion (that's a T) dollars for every 10 miles. This is a far cry from the estimates for our train. Given all of the concerns for the disruption this project will entail and who's going to ride this train, we are against this project.
41	7-Aug-17	Gov. Office	Dear Governor Hogan, I am writing to you as a concerned homeowner that has just been made aware of the possible plans to put in a Superconducting Maglev (SCMAGLEV) Train between Washington D.C. and Baltimore with a possible stop at BWI Airport. I was surprised to learn that the area of study included my neighborhood, and shocked to see that 2 of the proposed routes actually would require the demolishing of my neighborhood and home as I did not receive any information on the scoping meetings and I have lived at my current address since 2004. The fact that I had to learn of this once the study was complete is unfortunate, as there was seemingly little to no public input sought during the scoping process. I certainly did not have the opportunity to comment on the scope of the Environmental Impact Statement (EIS), and would formally request that the scoping period for the SCMAGLEV EIS be reopened. The fact that there were only 75 comments regarding the project when there would be more than 75 homes in my neighborhood alone that would need to be leveled to make room for this project is very telling. I am appalled that there was only 1 flyer location in all of Anne Arundel County where much of the proposed route would follow and no less than 17 locations in Baltimore City, 19 locations in Washington D.C. and 18 locations in Prince George's County. I am shocked that the project did not provide mailers to every possible homeowner in the affected study area. I was also surprised to see that homeowners in the areas that actually might care to see this project not move forward, and could actually organize against it were not made more aware. I seriously question the need for a multibillion infrastructure development project with minimal to no gain for the commuters between Baltimore and Washington D.C that the project is intended to benefit. I would welcome the opportunity to see the studies that show that this train would actually benefit the lives of the commuters between Baltimore and Washington D.C. There is already an existing MARC commuter train that serves this purpose with a regular schedule and is reasonably priced for ~\$7 each way. The competing AMTRAK train has significantly less passengers than MARC and has a value fare of ~\$16+ each way, and the higher speed ACELA has even less passengers and charges ~\$40+ Each way. I fail to understand how a super high speed train will attract more passengers than the current Acela train or Amtrak that are already both operating at much less than full capacity. It will certainly not do it if the price for a one way ticket is more than the Amtrak. Even if a project would improve the lives of Maryland Commuters, there seems to be sufficient land already owned by the State and Federal government that would more than suffice to run the maglev train between Washington DC and Baltimore with a stop at BWI airport. These routes should be chosen first before displacing thousands of families from their homes and communities. It would also make better use of the taxpayer's money to improve the existing commuter service between Baltimore and Washington DC by adding more tracks and train service along the existing Penn and Camden lines or widening the BW Parkway to accommodate 3 lanes of traffic each way.
42	7-Aug-17	Gov. Office	We are against the SCMAGlev. Those of us living in Odenton do not want our homes sold nor do we want our properties to decrease in value. None of us would even benefit from this train. It would only serve those in Baltimore and DC. Why should our homes and neighborhoods be destroyed for a high speed train tearing our town apart. The almighty dollar should not take precedence over families and their homes. Stop this train.
43	7-Aug-17	Gov. Office	Dear Governor Hogan, I am a resident of Glenn Dale, Maryland. I am very upset to read about the plans for the new MAGLEV. This is not something the residents of Maryland need. We need to help to ease the traffic congestion within the Beltway and surrounding areas. It can take 45 minutes just to go 10 miles on the 495 Beltway during rush hour. This high speed train will not make our life any better.. It will destroy thousands of homes. It is a HUGE waste of money. I hope the project will never see realization.
44	8-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Kathryn Paolucci_2017-08-08.pdf
45	9-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Richard Barrett_2017-08-09.pdf
46	12-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Julie Nauroth_2017-08-12.pdf

No.	Date	Forum	Comment
47	13-Aug-17	Gov. Office	Recently I learned of the possible impact of the MAGLEV Train to our Bowie neighborhood of nearly 33 years. I feel there has been insufficient public notice about the MAGLEV project, and I am requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analysis. Personally, I feel that there has been an injustice here. Most people I have spoken to know very little, if anything at all, about the impact this train may have to their home or the community of Bowie. In closing, my husband and I learned that our home was one in the path of possible destruction. We have lived in Bowie nearly 33 years and raised our three children in this town. The Acela Express runs on the already existing tracks. The need for a train that travels from Washington, DC to Baltimore quickly will not serve those living in Bowie or nearby communities. The plan to build a faster train to serve few is not in the interest of the people who live here in Bowie or the surrounding areas. The plan as I've seen it will destroy neighborhoods, family homes, and more. I ask, if this train were coming thru your own backyard would you take more care in the process, would you even consider such a thing? I don't believe so. I would like it to be noted that I am totally against the MAGLEV train coming through our neighborhood of Bowie, Maryland. Thank you. "The sacred is in the ordinary...it is to be found in one's daily life, in one's neighbors, friends, and family, in one's own backyard" - Abraham H. Maslow
48	15-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_John Lawson_2017-08-15.pdf
49	22-Aug-17	Gov. Office	Sir: I am a home owner in Odenton, Maryland. Yesterday, I found out that one of the proposed routes for the SCMAGLEV run through my home. Literally, the western edge of the Red Line Option runs through my backyard. I bought my house brand new in 1994. I raised my four kids in that house. Over the twenty-three years, my wife and I have worked and improved this house so that it is now perfect for us. To find out that a bunch of high rollers, foreign interests and politicians are considering taking that away from us is more than a little distressing. Particularly upsetting it that I had to find out from a neighbor months after the NEPA Scoping Process was already closed. Tens of thousands of people are going to be impacted no matter which route is chosen and the vast majority of us did not even know it was going to happen. At the very least, the NEPA Scoping process needs to be reopened. Further, the people in the proposed path of the SCMEGLEV need to be informed that their homes, business, church and schools are in jeopardy. I am joining the "Stop This Train" organization and I am going to do all I can to make sure that my neighbors are aware of this property grab. Respectfully, Richard Jacobs
50	23-Aug-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Anne Simonetti_2017-08-23.pdf
51	26-Aug-17	Gov. Office	I have recently found out that there is a proposition to build a SMAGLEV system from Baltimore to Washington right through my area. I never once received any word that such a project was underway if it weren't for people in my community trying to put a stop to it. I find it a horrible idea to build such a system through so many people's home, business, parks, etc. I would like to add my voice to those that oppose such an undertaking. Sincerely, Larry Burris
52	27-Aug-17	Gov. Office	Governor Larry Hogan, I am writing you today to express my concern about the proposed routes chosen for the Superconducting MAGLEV project. Those limited routes proposed all appear to run through established neighborhoods, requiring the destruction of thousands of state resident's homes, as well as numerous schools, churches, and businesses. In addition to the thousands of homes, schools, churches, and other buildings that would need to be destroyed, many more thousands will experience their property value significantly drop. Most of those affected would not even benefit from such a project. None of the routes appear to minimize this destruction by utilizing existing highways (I-95, MD- 205), not exiting land dedicated to rail lines. Additionally, I am concerned that with several commuter trains that already service transportation between Baltimore, BWI, and Washington, that such a project would be able to cover the costs and be profitable long term. For these reasons I urge you to request a reexamination of the proposed routes, as well as the economic impact and burden such a project would place on the citizens of this state. Sincerely, Tom Edwards
53	28-Aug-17	Gov. Office	There needs to be a county wide or at least the jurisdictions impacted meeting held on a Saturday to review the project status. Each individual jurisdiction is having different meetings and it would be helpful and informative for ALL stakeholders to have one joint meeting.
54	1-Sep-17	Gov. Office	Thank you for taking the time to read my email. I am formally requesting that the scoping period for the Super Conducting Magnetic Levitation (SCMAGLEV) Environmental Impact Study be reopened. My community would be severely impacted by this project, yet I only learned of it yesterday by means of Facebook. Do you think that a project of this magnitude should be discovered while scrolling through Facebook? Is that the appropriate way to inform the general public? I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient! Most of my neighbors have been living in their homes since this community was established in 1969 and have no idea about this proposal. I am requesting that I be given the opportunity to comment on the purpose for the project, need, alternatives to be considered, the scope of the environmental studies and the overall impact analysis. Currently, the Maryland Transit Administration (MTA) is failing to hear the issues and concerns of the affected public.
55	1-Sep-17	Gov. Office	Thank you for taking the time to read my email. I am formally requesting that the scoping period for the Super Conducting Magnetic Levitation (SCMAGLEV) Environmental Impact Study be reopened. My community would be severely impacted by this project, yet I only learned of it yesterday by means of Facebook. Do you think that a project of this magnitude should be discovered while scrolling through Facebook? Is that the appropriate way to inform the general public? I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient! Most of my neighbors have been living in their homes since this community was established in 1969 and have no idea about this proposal. I am requesting that I be given the opportunity to comment on the purpose for the project, need, alternatives to be considered, the scope of the environmental studies and the overall impact analysis. Currently, the Maryland Transit Administration (MTA) is failing to hear the issues and concerns of the affected public.
56	5-Sep-17	Gov. Office	I am a resident of Bowie, Maryland and am concerned about the MAGLEV project. I support mass transit and show this commitment by commuting every day on the MARC Penn line. I am concerned on the impact the MAGLEV on the communities in Maryland. I am also concerned about the potential diversion of funds from MTA to support operating costs of the MAGLEV train. I would ask that State consider these impacts and engage the Federal Railroad Administration to put a stop to the MAGLEV project. Thank you for listening to this feedback and providing to the appropriate member of the staff.

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
57	6-Sep-17	Gov. Office	Dear Governor, I had the pleasure of meeting you during the Brothers Osborne concert at the Calvert Marine Museum a couple weeks ago. I told you that I had never voted Republican in my life until I voted for you, a decision I stand by. I appreciate your common sense approach to things. That's why I am sending you this note to state that I am firmly in opposition to the proposed SCMagLEV project. I could go into much detail as to why I am against this project, but I am sure you have heard from many others in my community as to the lack of need, lack of benefit, and the level of disturbance and disruption associated with it. If this unnecessary joint Public/Private venture between BWRR and the Federal Government is to come to fruition, it should be located on federal land and aligned with the existing Baltimore Washington Parkway, which is of course on national parkland. Please take these comments into consideration when deliberating on this matter. Thank you.
58	6-Sep-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Susan Boyd_2017-09-06.pdf
59	12-Sep-17	Gov. Office	I am writing in opposition to the Maglev high-speed train that is proposed to run from Washington to Baltimore and run directly through my neighborhood. This project would significantly disrupt our neighborhood and/or cause my family to lose our home.
60	13-Sep-17	Gov. Office	Mr Hogan, I voted for you and I have been a vocal supporter of yours and all that you and your team has done. That streak will end if you support the SCMaglev train. I haven't spoken to one person that is in favor of this project and I would venture a guess that nobody that's not on the payroll supports this idea. This train will destroy my neighborhood and many others in Bowie thus destroying the legacy that I have bought and paid for over the past 20+ years for my children. You have proven to be the guy that stands up for the little guy so please hear our calls for help in this matter. Thank you
61	16-Sep-17	Gov. Office	To whom it may concern I am a resident and community leader in Seat Pleasant, MD. Please say NO to the latest proposal for a high speed train designed to come through our community. There are tons of things the MD DC area could benefit from but I assure you a high speed train is not one of them. Please Say NO to the outside investors who are not invested in our community. They do not share the valued interest of the community residents and stakeholders. I appreciate your time and considering my viewpoint.
62	18-Sep-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Greg Klinger_2017-09-18.pdf
63	18-Sep-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Maria Delores Sasso_2017-09-18.pdf
64	20-Sep-17	Gov. Office	\\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office
65	21-Sep-17	Gov. Office	Dear Governor Hogan, I know you can't make everybody happy but I wanted to let you know that there are some of us that approve of this new train system. The yellow line on the plans represent where I live. I live in an old drug infested area that is not easy to sell homes in. I would not be upset at all if that line went right through my home! I've lived in this neighborhood for over 30 years and have watched it decline. Please do consider this route for the train. Thank you!
66	24-Sep-17	Gov. Office	Please do everything in your power to stop the Maglev train project from destroying our community. It is not wanted or needed. It will not only destroy a way of life that is disappearing in our country, but will also do irreparable damage to the environment. If we, the constituents of Anne Arundel County, must have it, use the Amtrak route which is already in place. That SHOULD be the most logical approach. Many of us in the Ches Mar community, have lived here for more than 40 years. We are a tight-knit community, and do not want our homes, our schools and, churches and sports areas destroyed, for the convenience of a relatively small number of people who MIGHT use this system. New churches, and (new) Ridgeway Elementary School have already become assets to the area, and a new Old Mill High School is being planned for our burgeoning area. Do not let this part fo the "American Dream" be destroyed.
67	25-Sep-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Beverly Maliner_2017-09-25.pdf
68	25-Sep-17	Gov. Office	You are proposing to destroy my neighborhood and at least 268 other single family homes in my neighborhood. You are wasting taxpayer funds by spending \$20 plus million on a failed technology that will only serve to enrich consultants, developers and politicians, which has failed in Europe and Japan. Even though you apparently had a nice ride in Japan it is highly subsidized by their government. Do not destroy our homes, lives, businesses. And please do not respond to this with a "it's only in the exploratory stages" type of response. You will lose at least a minimum of 10,000 votes if you do not take routes through Maryland neighborhoods off the MagLev proposal. I and many, many others will work diligently to insure those votes are lost to you. Spend the money on improving the AmTrak line-an intelligent move.
69	26-Sep-17	Gov. Office	Good afternoon Mr Rann, My name is Paul Bednarik and I live at 564 Rita Drive in Odenton Maryland and I am very concerned about this maglev train that I have just now been made aware of. My wife and I have lived here since March of 1986, raised three children here and intend to retire and spend the rest of our life right here. Also our daughter and her husband and our son and his wife have also purchased a houses on Monterey and Avenue and King Malcolm Avenue and are beginning their life and families in the same community that they grew up in. To find out that we have been overlooked in this entire procedure is very disheartening and we have no intention of relinquishing our homes and community without being heard. Some of my concerns: - Being forced from our homes and community. - There has been no notice provided to our community at any point that our homes and neighborhood may be taken from us. - There is no updated information as to current meetings being posted or being made available to the public. Feel free to contact me at any time by email or my cell phone, 301-325-7400

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
70	27-Sep-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Jim Cawleys_2017-09-27.pdf
71	30-Sep-17	Gov. Office	I am writing you to express my opposition of the high speed magnetic train that is expected to come through Prince Georges County and particularly the historic town of Glenarden, MD. I am concerned about the lack of communication and transparency regarding this federally funded project that adversely impacts and takes advantage of African Americans, seniors and economically disadvantaged residents of Prince Georges County. This project rips through our county offering no benefit to the residents. The handling of this project has been unethical and morally wrong considering the inaccurate, misleading, and false information that is being communicated. At the the upcoming October 14th meeting at Bowie State University, I am requesting that accommodations be provided to accommodate our seniors and disabled residents. Specifically, shuttles or golf carts to transport them from the parking lot to the Student Center. Most of the Glenarden residents that came to the meeting at Herbert Charles Flowers High School were seniors that were using walkers and canes. They will need help navigating the campus and getting to right building. I look forward to your support in stopping this train from coming through Glenarden and for providing assistance to our seniors who want to come out and express their thoughts on how this project will impact them. Thank you
72	1-Oct-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Anna Lawson_2017-10-01.pdf
73	2-Oct-17	Gov. Office	Gov. Hogan, Below is the text of an email sent to Rep. Pam Beidle. I encourage you to discuss the issues about MAGLEV with her. "Of all State Government Agencies-MDOT has absolutely NO INTEREST or thought of responsibility to the citizens of this state. They are not elected and think the general public are a nuisance that they must put up with. Public meetings are only to TELL the public what MDOT WILL DO. Unless you and the rest of "OUR " (sic) ELECTED REPRESENTATIVES get off your high horses and start to truly work together to represent the people who elected you there will be no democracy. The entirety of elected officials will be no better than the two unacceptable candidates we were left to vote for the last presidential election. The Governor has demonstrated a reasonable ability to make informed decisions rather than exercise blind stupidity. Ask him to direct his MDOT to not only hold a public meeting in Linthicum, but ask him to show up and hear what the people have to say. There are available underutilized major rail rightaways already existing. Why screw up other communities? One even goes by the Airport and MDOT Headquarters!
74	4-Oct-17	Gov. Office	Good evening Governor, I hope today finds you well! Myself and many other citizens in the area are deeply concerned about the idea of the MAGLEV train running through our neighborhood as well as destroying our home values. Would you be able to tell me what your stance is on the MAGLEV? And what you'll do to support the citizens in the area that vehemently oppose this new train system? Additionally, would you be willing to attend A town hall style meeting with a local citizens can voice our opinion is to you? Thank you in advance
75	5-Oct-17	Gov. Office	Dear Governor Hogan, I respectfully request you please put a stop to the Meglev train that is posing significant displacement of thousands of Marylanders if allowed to be built. Maryland simply does not have the open space to accommodate this project without impacting the citizens of Maryland at their very core, their homes. I invite you to attend a meeting at Arundel High school on October 16 at 5pm where Odenton residents and surrounding community members will discuss these impacts. Thank you.
76	6-Oct-17	Gov. Office	Dear Governor Hogan, How about sending the bowie residents a update on the SCmaglev train? Respectfully
77	7-Oct-17	Gov. Office	Please find the attached letter that is being sent. This matter is of great concern to me because my home of the past 24 years is in one of the proposed direct routes. This makes me sick. I am very unnerved by the fact that meetings have taken place with out public notice and invitation. This matter needs to be reopened and addressed publicly. I am begging you to keep this plan that will affect thousands of homeowners and families from happening. Please fight for us and keep us publicly informed. Sincerely, Susan Stepp Bradley Smith, Director of Office of Freight and Multimodalism 7201 Corporate Center Drive, Hanover, MD 21076 SCMAGLEV Project, c/o John G. Trueschler Maryland Transit Administration 6 Saint Paul Street, Baltimore, MD 21202 Office of Railroad Policy & Development Federal Railroad Administration 1200 New Jersey Avenue, SE Washington, DC 20590 RE: SCMAGLEV a?" Request to Reopen the scoping period Dear Mr. Smith and Mr. Trueschler, I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public. Regards
78	7-Oct-17	Gov. Office	I am writing to voice my opposition to the MAGLEV project. The project will negatively impact the communities in Prince Georges and Anne Arundel counties both environmentally and financially. I do not want the communities where I live and work to be demolished and altered both physically and emotionally. Each day I take two walks through my peaceful beautiful neighborhood. One in the morning to set my intention for the day, and one in the evening to give thanks for all that I am grateful for that day. The serenity that my neighborhood brings to all who live there is priceless. I ask that the MAGLEV project be abandoned.
79	7-Oct-17	Gov. Office	Governor, I know Del. Pam in charge of transportation has requested a meeting here in Linthicum about this and I am hearing you are saying no. We in Linthicum want a meeting and want to know what is going on with this from you.

No.	Date	Forum	Comment
80	7-Oct-17	Gov. Office	<p>Dear Governor Hogan, You are, I believe, being unwittingly extremely careless with my life and the life of the community I live in. I live in Linthicum. And I am a republican and I voted for you because as a Republican you support more of what matters as a Christian. My very big problem is this: you are supporting the destruction of my town and the very neighborhood I have lived in since 2002 and a very historic town in order to save commuters 15 minutes travel time between NYC and Washington, DC via the proposed MAGLEV. Shameful, if you only appreciated how significant the destruction of our town is and our neighborhoods and our property values and homes and years of investments in our homes just to save some people 15 minutes commute time. Unthinkable really. Shame on you if you allow it. And, do you realize this is a heavily republican community in a heavily democratic state? Even for selfish reasons I would think you would not allow the destruction of our town. What do you have to say for yourself? Praying you will have a change of heart and mind and not support and not allow the MAGLEV to be built. It's sinful honestly to destroy whole communities for 15 minute saving in commute time. We don't need to create jobs. Are you willing to destroy my town for no good reason. I'm 54 years old with a wife a two children. I've worked 29 years in D.C. I commute via the MARC Train. Do you know that it only takes 23 minutes for the express MARC train to travel from Union Station to BWI Station? I am angry. This is an inexcusable way to treat people.</p>
81	7-Oct-17	Gov. Office	<p>See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Natalie Murray_2017-10-07.pdf</p>
82	9-Oct-17	Gov. Office	<p>I am formally requesting that you advocate for reopening the scoping period for the SCMagLEV Environmental Impact Study. I request that those in the crosshairs of the MAGLEV be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses.</p> <p>I am a resident of Colonial Park, a subdivision within Anne Arundel County (District 33), that according to the maps located on the Baltimore – Washington Superconducting MAGLEV Project website, is in two of the potential paths of the SCMagLEV route and within close proximity of two other routes. The news of this project came as a surprise to me and my neighbors, as we did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place – meetings that took place in December 2016 while many were focused on holiday activities, winter graduations/school schedules, and weather. The lack of proper notice and timing are without a doubt disturbing, and are viewed as underhanded and meant to prevent community input.</p> <p>Though I support progress and that of better transportation options, my view is that we should invest in improving the current infrastructure. You have recently identified improvements to I270, I495, MD 295, MD 32, and MD 50, all projects that will positively impact the region. THANK YOU!!! I ask that you extend your efforts of improving transportation congestion by supporting the improvement of the existing rail lines and that of the AMTRAK Northeast Corridor highspeed train modernization effort. For the past three years I have used the MARC Penn Line train between Odenton, MD and Union Station, Washington D.C. as a primary means of commuting to/from home and work. I can attest that making the much needed improvements to the MARC/AMTRAK rail lines, stations (local), roads and parking will benefit the local and regional community – something that the MAGLEV project does not do. Done well, these improvements could save billions in costs and will address the need for reliable mass transit, safety, and convenience needed in the BaltimoreWashington corridor.</p> <p>Given the AMTRAK modernization effort that makes use of existing rail routes and improve infrastructure and traveler experience, the MAGLEV project isn't warranted and the expense severely outweighs any perceived benefit. The selected MAGLEV route will grossly and negatively impact citizen's homes, schools, churches and businesses and will disturb historic sites, conservation areas, and green space. Additionally, whereas AMTRAK is accessible to tourists, commuters, government personnel, and locals, the MAGLEV will only be affordable to the elite or specialized traveler. The communities disrupted during construction and then bypassed without stops/stations will not recover economically. Moreover, I do not support foreign investment in this particular project. Thank you for consideration of this matter.</p>
83	9-Oct-17	Gov. Office	<p>I am formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened. My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas. I request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p>
84	9-Oct-17	Gov. Office	<p>See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Elinora Wixon_2017-10-09.pdf</p>
85	9-Oct-17	Gov. Office	<p>Dear Governor, My name is Edward Vesely. I live in an over 55 community in Odenton MD. A few weeks ago our HOA board was informed that there may be a very real possibility of a "Bullet Train" route to be constructed approximately 300 to 500 yards away from our house! When we purchased our house 12 years ago we had absolutely no idea that some day we would be able to feel our house vibrate every time a "Bullet Train" sped past. I am writing to you to let you know my wife and I are VEHEMENTLY opposed to this idea. I certainly hope you will veto this idea before it actually becomes a nightmare for our community.</p>

No.	Date	Forum	Comment
86	10-Oct-17	Gov. Office	<p>request to reopen the scoping phase. Mr. Smith, et al., I am formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened. My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. My neighbors and I have been lied to and taken advantage of before with the MTA's Light Rail Project., and with the Casino Projects in the State of Maryland. We will not be taken advantage of yet again. My Community of Ferndale has suffered and is now paying the price of the aforementioned projects, destroying our neighborhoods, our shopping, our personal safety., and we will not submit to further degradation by the SCMAGLEV project. Our Community,,, the Linthicum / Ferndale populations must have our say to the possible destruction of our homes, our neighborhoods and the degradation of our communities!</p> <p>Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas.</p> <p>I request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p>
87	10-Oct-17	Gov. Office	<p>Governor Hogan:</p> <p>We are formally requesting the scoping period for the SCMagLev Environmental Impact Study be reopened. My community would be impacted by this project and yet I, and my neighbors, only recently learned of the SCMagLev project and the proposed routes through our homes in Linthicum and Linthicum Heights. Neither I, nor my neighbors, received any notification of the proposed project or of the scoping and alternatives meetings that took place. No meetings to present this project and to receive feedback were held in our community. Outreach for these meetings was insufficient and apparently nearly absent if communities in the crosshairs of this project, such as ours, are only now learning of this project. Had the Linthicum community been notified, I (and many of my neighbors) would have been present to share our significant concerns and alternative ideas.</p> <p>We request that we be given the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. MTA has failed to hear and consider the issues and concerns of the affected public.</p>
88	10-Oct-17	Gov. Office	<p>Dear Governor Hogan I am opposed to the possibility of the SCMAGLEV project becoming a reality. The proposals are so very close to family homes, there seems to be no concern for our homes or lives. At this stage of our lives, it would make a great impact on our life. With the noise, sacrificing the green space, and decrease of our home value, you must consider the benefits of going faster between two cities.</p>
89	10-Oct-17	Gov. Office	<p>See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Linda Thompson_2017-10-10.pdf</p>
90	11-Oct-17	Gov. Office	<p>I have been a lifelong Anne Arundel County resident. Bringing this train through my community would be devastating to my family. I am a few years from retirement and am counting On the equity of my home for retirement. If the train comes through and I'm forced to sell my home for less than what it's worth it would jeopardize my retirement. I can't sell my home now because it is well known that my area is the favored route for the train.</p> <p>We have always been a supporter of you as our governor please support us and stop this train.</p>
91	11-Oct-17	Gov. Office	<p>Dear Governor Hogan, I'm a lifelong Republican and proud Hogan voter. I greatly appreciate your leadership and dedication to the people of Maryland. However, I am also a homeowner in Linthicum Heights and am deeply concerned about the proposed Maglev project. Of the proposed routes, all of them go through my town. Several go through the ball fields where my kids learned to play ball and the community pool where all they learned to swim. Two of the proposed routes cut directly through my neighborhood, directly overlaying my street, and would likely result in the demolition of the home where we have raised our five children. While we are told the Maglev won't be built for years and perhaps decades, this is not comforting to us, as we have a 3-month-old baby and planned to grow old together in this house and this neighborhood. The Maglev debate is a passionate one, but I believe it is important to have a respectful and cordial discussion on all sides. In that spirit, I would like to extend a friendly invitation to you, and any members of your transportation staff who'd like to come, to come have dinner with us at our home on any day that works for you. As the Maglev discussions continue, we'd really appreciate the opportunity to show you our beautiful street and our beautiful town, which will be affected no matter which routes are selected. I can be reached at 4109673346 or pricedoran@gmail.com. I look forward to hearing from you and, again, would welcome you and anyone from your office to visit, meet our family, and see our lovely neighborhood which this project has potential to place at risk.</p>
92	12-Oct-17	Gov. Office	<p>I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. There has been grossly insufficient public notice about the MAGLEV project. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.</p>
93	12-Oct-17	Gov. Office	<p>I WILL TRY TO MAKE IT SHORT. WHY ARE YOU GOING ALONG WITH THIS PROJECT? I AM 75 AND LIVING ON A FIX INCOME, BUYING MY HOME, TRYING TO WORK TO MAKE ENDS MEET. RETIRED FROM IRS AFTER 32 YEARS. 10 YR. BREAST CANCER SURVIVOR. YOU TAKE MY HOME WHAT AM I GOING TO DO SIR? HAVE YOU THOUGHT ABOUT US AT ALL? I AM TOO OLD TO START OVER AGAIN. I HAVE LIVED HERE FOR 25 YRS. I CANT AFFORD ANOTHER 30 YR. MORT. RIGHT NOW I AM A NERVOUS WRECK. WONDERING WHAT AM I GOING TO DO. I WORK AS A SUBSTITUTE TEACHER IN PGCPs. I HAVE TO WORK TO MAINTAIN MY HOME AND I LIVE ALONE. I HAVE BUILT MY LIFE AROUND MY HOME TO MAKE SURE I HAVE A DECENT PLACE TO LIVE. WHY ARE YOU GOING TO TAKE IT AWAY FROM ME? MY GOD SIR, PLEASE THINK ABOUT WHAT IS BEING DONE. I STILL HAVE A MORT. TO PAY AND HAVE NEVER BEEN LATE IN 24 YRS. WHAT IS THIS GOING TO DO TO ME AND OTHER PEOPLE. DONT TOSS THIS ASIDE OR HAVE A REPRESENTIVE ANSWER. I WANT TO HEAR FROM YOU.</p>
94	12-Oct-17	Gov. Office	<p>See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_Leah Bonistalli_2017-10-12.pdf</p>

PRELIMINARY ALTERNATIVES SCREENING REPORT



No.	Date	Forum	Comment
95	13-Oct-17	Gov. Office	I fail to see advantages for this train, either locally, statewide, or nationally. Plus the expense is too way too high. Please vote against it.
96	14-Oct-17	Gov. Office	See PDF: \\USBAL1FP001\Data\Data\Departmental Shares\Pm\60485181 - MAGLEV\300-Communications\330 External\Comments Received\Governor's Office\Correspondence_MAGLEV_LaJuanGraham_2017-10-14.pdf
97	17-Oct-17	Gov. Office	I am writing about my concern about the proposed SCMaglev Project. I recently heard of the project through an email from a concerned parent at Eleanor Roosevelt High School. I currently teach and coach within Prince George's County Public Schools. I am very concerned about the potential harm this project could have environmentally and physically for residents of the county and students of our school systems. I also feel that more public awareness should come forth before any decisions are made on this project. I hope that you and those you work with will carefully look into this project and its' potential cons before any decisions are made on it. I hope that our future and the potential risk for our students and children are taken into account as well.
98	21-Oct-17	Gov. Office	I am writing to express strong support for the proposed high-speed, magnetically suspended train project from Baltimore to Washington, D.C. with a stop at BWI Airport. Furthermore I would like to call your attention to a technological approach to the project developed by a Maryland resident. At this link you will find a description of U.S. Patent #7617779: "Linear Brushless D.C. Motor with Stationary Armature and Field and with Integratable Magnetic Suspension" (the patent summary is also attached here). You will note that the system proposed is totally non-contacting, providing emission-free and nearly silent propulsion to minimize impact on surrounding right of way. Note also that the inventor resides in Maryland and has made a patent assignment in the U.S. The assignee has made a working small-scale version of this system as proof of concept. You will find that the system described therein employs the latest technology to achieve the fastest and most comfortable passenger ride at speeds comparable to aircraft, but avoids the need for extensive and costly terminal and landing facilities. The routing should be accomplished without intersecting roadways. Crucially, the concept, design and preliminary development of this technology have all been done in the United States. Respectfully submitted, [PATENT SENT IN SEPARATE PDF]
99	24-Oct-17	Gov. Office	I may have voted for you in the last election, but the lack of consideration for those of us who have resided along the BW Parkway (almost 50 years in my case) for the purpose of encouraging the MAGLEV and/or HyperLoop trains without our input, absolutely means I won't vote for you next election.

Received

SEP 19 2017

Office of Planning

September 13, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to re-open the scoping period

Dear Mssrs. Smith and Trueschler,

We are formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. Our community would be impacted by this project, yet we only learned of it recently. We did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only now learning of it. Had we been notified, we would have been present and very vocal.

We request that we be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,

Doug and Shirley Alexander
1393 Patuxent Ridge Road
Odenton, MD 21113



cc:

Lawrence J. Hogan, Governor
Christopher Van Hollen, Jr., U.S. Senator
Benjamin L. Cardin, U.S. Senator
John P. Sarbanes, U.S. Representative
James C. Rosapepe, State Senator
Barbara A. Frush, State Representative
Benjamin S. Barnes, State Representative
Joseline A. Pena-Melnyk, State Representative
Andrew Pruski, AACo County Council
Anne Arundel County Board of Education
Stacy Korbela, District 21 Representative

July 26, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Mr. Smith and Mr. Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

July 26, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 2 of 3

According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

Prince George's County as a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel
- Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City.

Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."

July 26, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 3 of 3

- 40 CFR §1501.7, Scoping states that “There shall be an early **and open process** for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action.”
- 40 CFR §1506.6, Public involvement states that “Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
- The Council on Environmental Quality 40 Most Asked Questions (Q13) states “The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. **But that is only the minimum requirement...**” and scoping must have “**appropriate public notice** and enough information available on the proposal so that the public and relevant agencies can participate effectively.”

In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

We await your response to these requests.

Regards,



Denise Barone
12107 Raritan Lane
Bowie, MD 20715

CC:

EPA Region 3
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Prince George's County Executive Rushern Baker
Prince George's County Councilman Todd Turner
Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)

Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Trough (District 4)

August 9th, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

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In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. I request that I be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, I nor anyone in my community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. I have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. I have spoken to stakeholders such as the Washington Area Bicycle Association. No one I have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, I learned about the MAGLEV project from a neighbor who learned about it from a friend. *Citizen word of mouth has been the only outreach on this project.*

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In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

According to the May 2017 scoping report (issued 6 months *after* the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is

proof that outreach for these meetings was utterly insufficient. Had I or my neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities.
- No notices were placed in newspapers serving much of the project area including the *Washington Post*, the *Bowie Blade*, the *Capital Gazette*, or the *Sentinel*.
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Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."
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In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, I formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, I request that I be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

I await your response to these requests.

Regards,



Richard E. Barrett, Jr
722 Thornwood Drive
Odenton, MD 21113

CC:

EPA Region 3
Advisory Council on Historic Preservation
Council on Environmental Quality
MD Historical Trust
U.S. Senator Chris Van Hollen
U.S. Senator Ben Cardin
Congressman John Sarbanes
Governor Larry Hogan
State Senator James C. Rosapepe
Delegate Benjamin S. Barnes
Delegate Barbara Frush
Delegate Joseline A. Pena-Melnyk

County Executive Steven R. Schuh
Councilman Peter Smith (District 1)
Council Chairman John Grasso (District 2)
Councilman Derek Fink (District 3)
Councilman Andrew Pruski (District 4)
Councilman Michael Peroutka (District 5)
Councilman Chris Trumbauer (District 6)
Council Vice Chairman Jerry Walker (District 7)

August 15, 2017

Maryland Department of Transportation
Bradley Smith
Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mssrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be severely and negatively impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project, or of the scoping and alternatives meetings that have taken place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

I have conducted my own analysis of the impact, based on the proposed routes from the MagLev project team. This analysis was performed using Geographic Information System (GIS) software to identify the impacted property parcels, community facilities, and natural resources, among other geographic coverages. As part of my analysis, I have identified the individual homeowners and businesses that will be directly impacted by each proposed route, using the County tax and property records.

The impact of these proposed routes is severe and will forever change the community landscape, not to mention the economic burden of lost homes and businesses, and the future tax burden on the citizens of the State of Maryland.

I firmly believe that the scoping period needs to be reopened so that those impacted may be properly notified, and have the opportunity to address the impact of this project.

Thank you for your attention in this matter,



William G. Boone
12304 Rustic Place
Bowie, MD 20715

CC:

Barbara Rudnick
 NEPA Team Leader
 EPA Region 3
 1650 Arch St, Philadelphia, PA 19103
 rudnick.barbara@epa.gov

US Senator Chris Van Hollen
 Washington, DC Office
 110 Hart Senate Office Building
 Washington, DC 20510

US Senator Ben Cardin
 Washington, DC Office
 509 Hart Senate Office Building
 Washington, DC 20510

Congressman Steny Hoyer
 Washington, DC Office
 1705 Longworth House Office Building
 Washington, D.C. 20515

Governor Larry Hogan
 100 State Circle
 Annapolis, Maryland 21401

State Senator Doug Peters
 James Senate Office Building, Room 120
 11 Bladen St.
 Annapolis, MD 21401

Delegate Geraldine Valentino-Smith
 House Office Building, Room 427
 6 Bladen St., Annapolis
 MD 21401

County Executive Rushern Baker
 14741 Governor Oden Bowie Dr.
 Upper Marlboro, MD 20772-3050

County Councilman Todd Turner
 District4@co.pg.md.us

Mayor Fred Robinson
 mayor@cityofbowie.org

Councilman James Marcos (At Large)
 jmarcos@cityofbowie.org

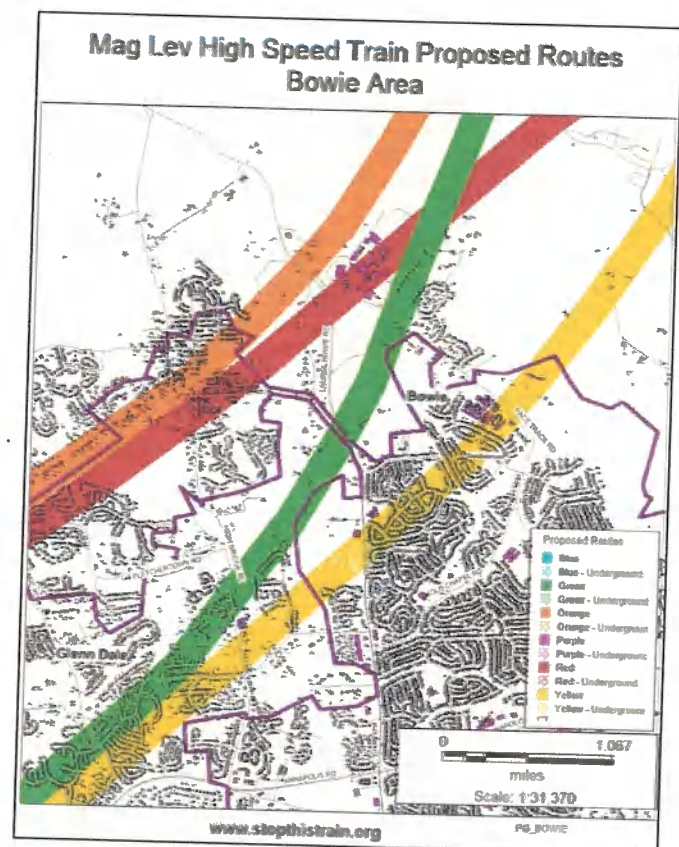
Councilman Henri Gardner (At Large)
 hgardner@cityofbowie.org

Councilman Michael Estève (District 1)
 mesteve@cityofbowie.org

Councilwoman Diane Polangin (District 2)
 dpolangin@cityofbowie.org

Councilwoman Courtney Glass (District 3)
 cglass@cityofbowie.org

Councilman Ike Trouth (District 4)
itrouth@cityofbowie.org



6 September 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

Received
SEP 12 2017
Office of Planning

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202


Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mr. Smith and Mr. Trueschler,

My home and my community are in the path of the MAGLEV project, and I have not received any information from MTA regarding the project. I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,


Susan D. Boyd
4722 Ramsgate Ln
Bowie MD 20715

September 27, 2017

Mr. Smith

We are the Cawleys, Jim and Barbara, who live on Reston Lane in the Rockledge section of Bowie, where the proposed Maglev Train is to be built. We and four other families moved here in 1967---50 years ago and still live here on Reston Lane. We are in our 70's and 80's along with many other seniors in the proposed area who will be affected by this Train.

We are NOT ready for assisted living or nursing homes. Where will we go at this stage of our lives??? We do not handle change well and are not prepared to handle the rigors of adjusting to a new life "somewhere"!!!

NEVER did we guess that we would be FORCED out of our homes ---where our children and grandchildren grew up---where a TRAIN would have such an impact on destroying homes, schools, churches, community and an environment that took 50 years to build!!!

DO NOT ALLOW THIS TRAIN TO DESTROY THE BOWIE AREA AND ALL OUR LIVES!!!

PLEASE STOP THE MAGLEV TRAIN FROM COMING TO BOWIE AND THE SURROUNDING AREA!!!

*Jim & Barbara
Barbara F. Cawley
Bowie, Maryland*

Citizens Against the SCMaglev

October 10, 2017

SCMaglev Project
c/o Suhair Al Khatib
Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21202

Dear Mr. Al Khatib,

The Citizens Against the SCMaglev sent a letter in June 2017 requesting that the NEPA scoping process be reopened due to lack of public notice on the part of the Maryland Transit Administration. To date, we have not received any response from MTA.

As we will not have our concerns ignored, we are providing the following scoping comments. The project scoping report must be amended to include these issues, and they must be addressed in the EIS.

PURPOSE AND NEED

1. Provide the need for the SCMaglev project. While a purpose of the proposed project was provided at the April 2017 public meetings, the need for a SCMaglev train was not provided. The public has not been provided any information that leads to the conclusion that an SCMaglev is needed. We strongly believe that there is a need for regional transportation improvements in the Washington-Baltimore corridor that will serve all residents, visitors, and businesses in the region. However, we have seen no data that proves that a train that serves only Washington, BWI, and Baltimore is needed or will provide relief from the transportation challenges in the region.
2. The Notice of Intent, published on November 25, 2016 (the day after Thanksgiving), states that:

“The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent. Similarly, the demand on the transportation infrastructure between Baltimore and Washington will continue to increase along major roadways and railways including Interstate 95, the Baltimore-Washington Parkway (MD 295), US 29, US 1, and the Northeast Corridor (NEC) thereby decreasing the level of service, reliability, mobility, and potentially decreasing safety.”

This statement does not provide proof that there is a need for a transportation investment of this magnitude with significant environmental impacts that only serves DC, BWI, and Baltimore. Rather it demonstrates that there are transportation issues that extend throughout our region. Analysis of these needs will demonstrate that these issues will only be solved by intra-regional transportation investments like rapid bus transit, light rail, and roadway improvements that serve the entire traveling population.

Provide detailed analysis of transportation needs of the Baltimore-Washington area.

Page 2 of 12
 October 5, 2017
 SCMaglev Project
 c/o Suhair Al Khatib

3. The Notice of Intent makes the following claims:

“Without additional transportation improvements and capacity within the Baltimore-Washington area, economic development and growth opportunities will be restricted. As congestion increases on the NEC and on the region's highways, the demand for continued economic development will be impacted, including, for example, tourism.”

Provide documentation of this hypothesis. Specifically, provide documentation that the “transportation improvements... within the area” would be served by a rail system that only stops in DC, BWI, and Baltimore. Provide documentation that tourism, which is fueled by the great historical and governmental significance of the Washington, DC area would decline without a train that ONLY brings people from Baltimore.

4. Further the Notice of Intent quotes the NEC Future Program:

“To address these issues, in 2012 FRA launched the NEC FUTURE program to consider the role of rail passenger service in the context of current and future transportation demands and to evaluate the appropriate level of capacity improvements to make across the NEC. Through NEC FUTURE, FRA will determine a long-term vision and investment program for the NEC documented in a Tier 1 EIS and Service Development Plan. FRA published a Tier 1 Draft EIS in November 2015; however, the Draft EIS evaluated steel-wheel technologies as a way to serve the passenger rail needs of the region. It left open the possibility and did not preclude the study of and investment in advanced guideway and other new technologies, such as SCMaglev, to meet the transportation needs of the Northeast, including the Baltimore-Washington area.”

We note that this statement says that the NEC future EIS leaves open the possibility of studying the SCMaglev. While we concur, this statement does not support the need for the SCMaglev and calls into serious question why alternatives other than SCMaglev are not being studied in this EIS. See comments on Alternatives below.

5. Independent Utility/Segmentation - Provide detailed analysis proving that the proposed project has independent utility. Provide financial data showing how a train from DC to Baltimore can sustain short-and long-term viability without government funding and without an extension past Baltimore.

As BWRR is a subsidiary of The Northeast Maglev, LLC (TNEM), and TNEM has announced plans to build the SCMaglev from Washington, DC to New York, the entire SCMaglev project is a connected action. Separation of the project into pieces violates the prohibition on segmentation per the Council on Environmental Quality (CEQ) NEPA implementing regulations.

The CEQ NEPA regulations state that:

§1502.4 Major Federal actions requiring the preparation of environmental impact statements: Proposals or parts of proposals which are related to each other closely

enough to be, in effect, a single course of action shall be evaluated in a single impact statement.

§1508.25 Scope: To determine the scope of environmental impact statements, agencies shall consider 3 types of actions, 3 types of alternatives, and 3 types of impacts. They include: 28 (a) Actions (other than unconnected single actions) which may be: (1) Connected actions, which means that they are closely related and therefore should be discussed in the same impact statement. Actions are connected if they: (i) Automatically trigger other actions which may require environmental impact statements. (ii) Cannot or will not proceed unless other actions are taken previously or simultaneously. (iii) Are interdependent parts of a larger action and depend on the larger action for their justification. (2) Cumulative actions, which when viewed with other proposed actions have cumulatively significant impacts and should therefore be discussed in the same impact statement. (3) Similar actions, which when viewed with other reasonably foreseeable or proposed agency actions, have similarities that provide a basis for evaluating their environmental consequences together, such as common timing or geography. An agency may wish to analyze these actions in the same impact statement. It should do so when the best way to assess adequately the combined impacts of similar actions or reasonable alternatives to such actions is to treat them in a single impact statement.

6. Provide data demonstrating the following required for financial viability:

- Number of daily trains in each direction
- Number of cars per train
- Number of riders per train, per day, and per year
- Cost per ticket
- Cost to operate and maintain rail line
- Any financial incentives (funding, grants, tax breaks, loans, etc) needed from governmental sources (Federal, State, or local) for construction and operation in the short-term and long-term
- Provide information on foreign investment including required purchase of foreign-made rail components/trains
- Provide examples of where the SCMaglev is currently operating in the world and provide financial data on that operation demonstrating that it does not require continual or intermittent government subsidies.

October 5, 2017

SCMaglev Project

c/o Suhair Al Khatib

7. Amtrak's annual ridership on the Northeast Corridor (from DC to Boston) for 2016 was 11.9 million. Further Amtrak estimates NexGen ridership in 2030 of 25 million for the ENTIRE Northeast Corridor (<https://www.amtrak.com/ccurl/214/393/A-Vision-for-High-Speed-Rail-in-the-Northeast-Corridor.pdf>).

In 2016, a total of 5,098,562 riders boarded Amtrak trains at the Union Station in Washington, DC, and a mere 1,030,161 riders boarded Amtrak trains in Baltimore <https://www.amtrak.com/national-facts>. Only 689,042 riders boarded/off-boarded at BWI in 2016.

(<https://www.amtrak.com/ccurl/111/103/MARYLAND16,0.pdf>). It is inconceivable that most of these 6.8 million riders were solely traveling between Washington and Baltimore.

2016 ridership on the MARC Camden and Penn lines totaled approximately 1.3 million riders (<https://data.maryland.gov/Transportation/MTA-Average-Weekday-Ridership-by-Month/ub96-xxqw>), but MARC serves numerous stations and communities in between Baltimore and Washington – commuters that would not be served by the SCMaglev. In addition, in June 2016, MARC reported ridership DECLINES: “Currently, the MARC service is facing ridership declines, according to Erich Kolig, director, MARC train and commuter bus service. “They are rather slight, only about a percent down from last year,” he explains. “But we are not experiencing the 3% growth that we have been maintaining for the last 10 years.” (<http://www.metro-magazine.com/rail/article/713751/maryland-s-marc-railroad-upgrades-fleet-service-to-bolster-ridership>)

In its filing with the State of Maryland, BWRR claimed that “Annual ridership on the SCMaglev system connecting Washington to Baltimore would be between 10.2 million and 15.4 million annual passengers in 2030 (42,200 one-way trips per day on average), while still assuming continued Amtrak and MARC service.”

Provide proof and solid data on how the SCMaglev will achieve 10 times the current Washington/Baltimore station onboard/offboard riders and 41 to 62% of the projected 2030 riders on the entire Amtrak Northeast Corridor in the short line from DC to Baltimore while “still assuming continued Amtrak and MARC service”! Include detailed origin and destination data for riders, type of riders (commuters, business travelers, tourists).

See **Environmental Impacts** for further issues regarding Amtrak.

8. If the need for an SCMaglev train from DC to Baltimore cannot be defined, and independent utility for this short segment of rail cannot be established and sustained over the short- and long-term, the EIS must be revised to include the entire corridor (whether to Philadelphia, New York, or Boston) needed to sustain the financial viability of the project in accordance with 40 CFR 1502.4a and 40 CFR 1508.25.

ALTERNATIVES

The structure of this EIS based solely on studies for the SCMaglev, precludes all other possible decisions including the decision to construct Amtrak NextGen trains. As such, an EIS that does not study these other alternatives is not in compliance with the National Environmental Policy Act.

As stated by the Council on Environmental Quality (40 Most Asked Questions):

Question 2a. Alternatives Outside the Capability of Applicant or Jurisdiction of Agency. If an EIS is prepared in connection with an application for a permit or other federal approval, must the EIS rigorously analyze and discuss alternatives that are outside the capability of the applicant or can it be limited to reasonable alternatives that can be carried out by the applicant?

A[nswer]. Section 1502.14 [of the CEQ regulations] requires the EIS to examine all reasonable alternatives to the proposal. In determining the scope of alternatives to be considered, the emphasis is on what is "reasonable" rather than on whether the proponent or applicant likes or is itself capable of carrying out a particular alternative. Reasonable alternatives include those that are practical or feasible from the technical and economic standpoint and using common sense, rather than simply desirable from the standpoint of the applicant.

2b. Must the EIS analyze alternatives outside the jurisdiction or capability of the agency or beyond what Congress has authorized?

A. An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Section 1506.2(d). Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies. Section 1500.1(a).

In addition, Section 4(f) of the Department of Transportation Act prohibits the use of Section 4(f) property unless there is no feasible and prudent avoidance alternative to the use of land from the property. If the SCMaglev project uses public parkland, wildlife or waterfowl refuge land, or historic resources, it may not be constructed if there is a prudent and feasible alternative. It should be noted that the WB&A Trail is public parkland – NOT a railroad right-of-way, and as such is protected under Section 4(f).

Lastly, as noted in Amtrak's scoping comments for this EIS, the NEC Future EIS and Record of Decision, selected improvements to the existing rail alignments and "discarded the new alignment alternative."

October 5, 2017

SCMaglev Project

c/o Suhair Al Khatib

1. NexGen – The Amtrak NexGen technology is a “reasonable” alternative to Maglev and therefore, in accordance with NEPA and the CEQ implementing regulations (40 CFR 1502.14), must be studied in the EIS. ADD INFORMATION ON NEXGEN – SPEED, TIMING, COST, ETC. While outside of the Congressional legislation, the NexGen technology is a “reasonable alternative as it is practical and feasible from the technical and economic standpoint and must be included using common sense, rather than simply desirable from the standpoint of” BWRR. In addition, as construction funding is not available at this time, it is prudent to provide Congress with an equal analysis of all alternatives to facilitate informed decision-making.

Lastly, the Amtrak NexGen is a prudent and feasible alternative to the SCMaglev and would avoid the use of public parkland (including the WB&A Trail), wildlife and waterfowl refuges, and historic properties. Therefore, the Amtrak NexGen must be studied as an alternative to the SCMaglev.

2. Elon Musk and SpaceX have proposed constructing an underground Hyperloop railroad to travel between Washington, DC and New York. As with NexGen, this technology must be analyzed alongside the SCMaglev to provide decision makers with information on the environmental impacts of each technology prior to making a decision.
3. Detail on alternatives must include design details including but not limited to: limits of disturbance; right-of-way; staging areas; station areas and impact areas for these; connections between stations and existing transportation hubs (i.e. BWI); maintenance yards and impact areas for construction of these; utility connections; security features including fencing; haul routes for construction equipment, materials, construction debris, and mining spoils; and disposal areas for construction debris and fill materials.

Environmental Impacts

1. Environmental Justice – Provide a detailed analysis on the disproportionate impacts that low-income and minority populations in Washington, DC; Prince George’s County; and Baltimore City will suffer under the SCMaglev plan including but not limited to loss of property values and housing opportunities, loss of parkland and greenspace, increases in noise levels, and impacts to health and safety. Specifically, the above ground portion of the project will bisect Prince George’s County while not providing ridership opportunities to the citizens of the County. Provide analysis of the EPA EJScreen demonstrating the disproportionate impacts already suffered by the residents of the County and the impact that a SCMaglev train would have. A baseline EPA EJScreen along the WB&A Trail shows an EJ score of over 90% compared to 50 to 60% in communities in Montgomery and Howard counties.

2. Property Acquisition – Provide information on all properties to be acquired and displaced as a result of the proposed action including:
 - a. Number, types, and locations of all of acquisitions/displacement of residents, parks, businesses, schools, churches, etc;
 - b. Method by which properties will be acquired including information on eminent domain and what entity will exercise eminent domain;
 - c. How compliance with the uniform relocation act will occur including analysis of the availability of comparable housing of equal price, value, and setting
 - d. Impact to remaining homes including loss of home values, changes in setting, visual and noise impacts. Provide information on how close a residence can be from the rail line
 - e. Impacts to remaining businesses including change of access and resulting loss of business.
3. Economic Impacts – Provide thorough, quantitative analysis on the negative economic impacts associated with the proposed alternatives including:
 - a. Loss of property values (especially home values) from the proximity of a high-speed train where there is currently more compatible, highly valued land uses such as parks, trails, forested areas, and residential areas
 - b. Loss of tax base from the loss of properties and from the devaluation of remaining properties
 - c. Impacts to Amtrak ridership and revenues in the Washington/Baltimore corridor, the Northeast Corridor, and nationally as many of Amtrak's lines outside urban areas count on revenues generated in areas like the Northeast Corridor
 - d. Impacts to viability of NexGen Acela as it is probable that there is not sufficient ridership/need for both technologies to operate in the same market
 - e. Impact to Marc ridership, revenues, and viability with the proposed loss of riders to the high-speed train; this assessment should include any additional State, local, or Federal funds that will need to be expended to keep the Marc system viable. If the Marc system is no longer viable or must raise fares to maintain viability, the impact to commuters that board the train at locations between Baltimore and Washington must also be analyzed
 - f. Analysis of permanent job creation with SCMaglev vs. job losses at Amtrak and Marc
4. Parks and Recreation/Section 4(f) – Provide impacts to parks and recreation areas in Prince George's and Anne Arundel Counties including the loss of access to these critical community features. Impact analysis should include changes to these facilities including changes to park settings, park features, park noise levels, and the overall changes in the user's experience. Note – the WB&A Trail is public

parkland; owned and operated by the Maryland National Capital Park and Planning Commission and the Anne Arundel Recreation and Parks Department. The trail is a critical component of the East Coast Greenway and the American Discovery Trail.

5. Historic Resources – Provide impacts, including visual, noise, and vibration impacts, to historic resources including archaeological features, historic sites such as the Glen Dale Hospital, the Marietta Mansion, the Bowie Racetrack, private historic residences and farms, and historic districts including the Odenton historic district.
6. Visual Resources - As the only renderings provided by MTA thus far show a train in the middle of a forest with no residences nearby, provide more accurate renderings of the train line. Specifically:
 - a. Provide before and after renderings of the proposed rail line and train in ACTUAL locations showing the view to and from historic properties, parks, and residential properties that will remain after construction of the train.
 - b. Renderings to and from historic properties, parks, and residential properties should include day and night views and views with and without leaf cover on trees.
 - c. Provide information on the visual intrusion of graffiti into neighborhoods that may occur on the elevated rail line.
7. Topography – Provide details on changes to topography including cut and fill; provide detail on whether the proposed train and track system will cut through hills (creating environmental impacts) or rise up over them creating visual impacts to large numbers of communities and citizens.
8. Water Quality – Provide quantitative data on the increases in impervious surface created by the track, stations, maintenance yards, and parking.

Provide quantitative data on the resulting stormwater runoff and impacts to erosion, sedimentation, and TMDLs in the already degraded Patuxent River and its tributaries including Horsepen Branch.

Provide information on the chemicals to be used for snow removal and deicing and the impact runoff of these chemicals will have on the Patuxent River and its tributaries.
9. Bird Strikes – Provide data and analysis on the impact of bird strikes and how they will be prevented from a train going over 300 mph. There are nesting bald eagles on the Patuxent Wildlife Refuge in close proximity to the WB&A Trail. Bald eagles also frequent the Patuxent River between Prince George's and Anne Arundel counties. In addition, there are numerous other bird species including barred owls, pileated woodpeckers, and red-tailed hawks nesting along the WB&A Trail and along the river.
10. Bats - Provide data and analysis on the impact to bats and how strikes will be prevented from a train going over 300 mph. Provide detailed studies of bat habitat, bat populations, and presence of white-nose bat syndrome.

October 5, 2017

SCMaglev Project

c/o Suhair Al Khatib

11. **Wildlife Displacement** – Provide data and analysis on wildlife displacement from construction of the proposed train. Habitats in the vicinity of the proposed alternatives are at capacity and any loss of habitat will force wildlife further into developed areas including residential areas. Detailed wildlife surveys must be conducted to document species, numbers, health of population (including over population), and capacity of habitat.
12. **Vegetation** – Provide data on number, species, size, and locations of all trees to be removed for construction of the rail line. Provide types of vegetation to be removed. Provide information on how the project will prevent invasive species from being introduced into areas where forest are bisected and native vegetation is removed. Provide mitigation including where and what types of vegetation will be replanted, how reforestation areas will be monitored, and how the rail line will be screened after vegetation is removed.
13. **Noise** – Provide 24-hour noise analysis for all types of properties in the vicinity of the proposed alternatives (residences, schools, churches, libraries, historic sites, parkland, etc.). Provide existing noise levels and noise modeling when the train is in operation. Noise models should take into account the loss of existing vegetation and buildings that would be removed and modeling should show day and night noise levels.
14. **Electromagnetic Fields** – Provide information on electromagnetic fields including health effects from long-term exposure to an SCMaglev train. Provide health information from populations currently exposed to SCMaglev trains.
15. **Light** – Provide information on how the rail line, parking areas, rail yards, stations, etc. will be illuminated at night. Provide analysis of light intrusion into residential areas, parkland, and historic sites.
16. **Energy Usage** – Provide information on the energy needed to operate the SCMaglev train including the source of the power. Provide locations of any new substations or other utility upgrades needed to power the SCMaglev. Provide information on how power interruptions would affect train operation.
17. **Utilities** – Provide information on temporary and permanent utility disruptions to residences, businesses, and other property owners. Provide information on the location and type of new utility lines needed to serve the proposed project. Provide information on impacts to the power grid and other utilities.
18. **Community Cohesion** – Provide information on impacts to neighborhoods that will be bisected or will be cut off from neighboring communities by the SCMaglev Train.

Provide information on the effects to communities such as Glenarden, Glen Dale, Bowie, Piney Orchard that will be bisected by the proposed train. Include analysis of the cumulative effects of bisection by transportation projects to communities such as Glenarden.

19. Land Use and Land Use Planning – Provide detailed analysis of changes in land use that will occur due to undesirable proximity to the train and from the economic development that BWRR claims will accompany the train. Provide detail on where this development will occur and how it will affect sprawl in the Baltimore-Washington Area
20. Community Services –
- a. Provide analysis on effects to police, fire, and EMS services including responding stations, personnel levels, training (how much it will cost, who will provide it, and how often it will occur). Provide information on BWRR provided police services and a comparison of those services to those provided by Amtrak police. Provide information on how terrorist threats will be mitigated at stations and on the rails.
 - b. Provide information on how other community services will be impacted including schools, hospitals, churches, etc. Include analysis of cumulative impacts based on the economic growth projected to occur solely because of the SCMaglev construction (as based on BWRR claims).
21. Safety and Security – In addition to the police information requested above, provide information on how the SCMaglev rail line will be secured such that people cannot climb onto the line resulting in injury or death and/or vandalize the line. Provide renderings of any fences or other barriers that will be placed along the rail line, rail and maintenance yards, parking areas, etc. Provide information on the distance fence lines will be placed from the line, height of fences, and type of fencing.
22. Construction Impacts – Provide information on the construction related activities and impacts including but not limited to:
- a. Limits of disturbance for construction of each of the alternatives – temporary and permanent land use
 - b. Areas of cut and fill
 - c. Staging areas
 - d. Tunneling/mining locations
 - e. Tunneling technology
 - f. Haul routes for construction equipment and disposal of tunneling spoils
23. Traffic – Provide impacts from:
- a. Construction vehicles traffic
 - b. Temporary and permanent roadway closures including travel delay times, creation of land-locked properties, changes in access to properties

- c. Additional vehicular traffic, especially at Union Station, BWI, and in Baltimore generated from train riders accessing the rail line; analysis should include intersection level turning movements and delays
- d. Traffic from economic development that BWRR claims will accompany the SCMaglev; the Notice of Intent states that “The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent.” If this growth will occur without the SCMaglev, and the SCMaglev claims it will radically increase development in the region, the cumulative impact analysis must quantitatively analyze the traffic that will accompany this level of development and explain how the rest of the regional transportation network will handle this development or where levels of service will decrease to the point of complete gridlock.

24. Airline/Plane Traffic – As part of BWRR’s economic and ridership “promises” include increased use of BWI and development around BWI, increases in air traffic must be analyzed in the EIS.

This analysis must also include resulting increases in noise from increased air traffic. It should be noted, that Governor Larry Hogan recently instructed the State Attorney General to sue the FAA for noise violations at BWI.

25. Union Station –

- a. Provide information on the impacts to the historic Union Station in Washington, DC
- b. Provide information on how the SCMaglev Train will impact current renovations taking place at Union Station
- c. Provide analysis of how a SCMaglev Train will affect the Washington Union Station's 2nd Century Plan which is being developed to accommodate the Amtrak NexGen train.

26. Cumulative Impacts – A project of this magnitude requires a comprehensive analysis of cumulative impacts including quantitative analysis of the significant harmful impacts transportation projects have had and continue to have on communities in Prince George’s and Anne Arundel counties. Included in this cumulative impact analysis must be the impacts of the supposed development that SCMaglev claims it will generate. In addition, the cumulative impact analysis must analyze any track widening or realignments planned by Amtrak.

The Notice of Intent also provides grossly inaccurate information on the study area of the proposed project. The NOI states that the study area is bounded by the former Washington-Baltimore & Annapolis Electric Railroad alignment while in reality, the study area extends 3 miles to the east of this alignment. Maps of preliminary alternatives have been provided by MTA which MTA now states should not be relied on. BWRR CEO Wayne Rogers, as recently as September 14, 2017, stated in an interview with ABC 7 news that ““We’re very early in the process. They’re called initial preliminary screening alternatives. They’re not even to the preliminary to the alternatives to the preferred alternatives, so

October 5, 2017

SCMaglev Project

c/o Suhair Al Khatib

we're years away," when in fact alternatives have been submitted to FRA for approval. These examples of misinformation by the MTA project team along with abysmal public notice on the scoping process for the project, demonstrate that this project should be restarted to provide meaningful opportunity for public input and decision-making.

As MTA has not responded to repeated requests to reopen the scoping period for this project, we request that you provide a formal reply to this letter and an affirmation that the issues outlined herein will be thoroughly analyzed in the EIS.

You may reach us at:

Citizens Against the SCMaglev
PO Box 669
Bowie, MD 20715

Sincerely,

Citizens Against the SCMaglev



On behalf of:

Dennis Brady, Chair
Dawn Wampler, Vice-Chair
Aviva Nebesky, Vice-Chair
Breanne Reynolds, Secretary
Joan Glynn, Secretary

Cc:

EPA Region 3
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Delegate Marvin E. Holmes, Jr.
Delegate Del. Joseph F. Vallario, Jr.

Prince George's County Executive Rushern Baker
Prince George's County Councilman Todd Turner
Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)
Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Trough (District 4)

Federal Railroad Administration
Department of Transportation
1200 New Jersey Avenue, SE
Washington, DC 20590



Maryland Department of Transportation
7201 Corporate Center Drive
Hanover, Maryland 21076

To Whom It May Concern:

On November 25, 2016, the Federal Railroad Administration published a Notice of Intent (NOI) to prepare an EIS for the Baltimore-Washington Superconducting Magnetic Levitation (SCMAGLEV) Project. This notice provided for thirty day period to provide comments in writing and four public meetings. The NOI's Purpose and Need statement was to "increase capacity, reduce travel time, and improve both reliability and mobility options between Baltimore and Washington."

In May of 2017, a Final Scoping Report was completed to capture the results of the initial scoping effort. The document states that 57 comments were submitted at public meetings, 16 comments were submitted via project email, and two comments were submitted via mail for a total of 75 comments.

The proposed project is multi-billion infrastructure development that initially identified six possible routes that transect numerous private lands in multiple jurisdictions. Despite repeated efforts to engage the project proponent, we have been unable to garner additional information on the exact proposed routes and even documents justifying the purpose and need of the project. In fact, the project manager indicated that they identified the proposed routes through computer simulations and had not even walked area under consideration. Now the website shows two possible routes without any explanation as to the narrowing of options or engagement with impacted communities.

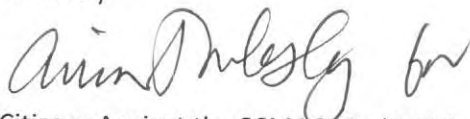
Many of us would agree that MAGLEV technology offers benefits in terms of reduction harmful emissions and the current traffic congestion plaguing our region. However, a project of this magnitude deserved more public engagement than 30 day during the holiday season. Moreover, there needs to be more discussion regarding the purpose and need of this project given existing infrastructure rail needs along the Northeast Corridor and within the DC Metro area.

We respectfully request that you work with the project proponents to re-open the Scoping Process and provide for more meaningful engagement with landowners, local governments, elected officials, and federal partners before moving forward with the selection and analysis of alternative to consider in the full Environmental Impact Statement. Further, we would appreciate more direct engagement and information related to the purpose and need for the proposed project.

While we appreciate that the Scoping process is the first in a series of opportunities to engage, it is critical for landowners and communities who are potentially impacted by the review of routes impacting their lands. Other major infrastructure projects in the DC Metro area have indicated that the failure to productively engagement communities and landowners results in a process mired in litigation. As landowners, community members, and Maryland taxpayers, we don't feel that this scoping process was adequate.

Thank you, in advance, for your consideration of this request and we look forward to your response.

Sincerely



Citizens Against the SCMAGLEV (CATS)

Keep Our Community Beautiful

(8306 Laurel Bowie Road

Bowie, MD 20715)

Cc: Maryland Congressional Delegation
Prince Georges County Elected Officials
Council on Environmental Quality
Advisory Council on Historic Preservation



City of Glenarden

October 6, 2017

SCMAGLEV Project
c/o Bradley M. Smith, Project Lead
Maryland Department of Transportation
7201 Corporate Center Drive
Hanover, Maryland 21076

SCMAGLEV Project
c/o Suhair Al Khatib, Deputy Administrator
Maryland Transit Administration
6 Saint Paul Street
Baltimore, Maryland 21202

SCMAGLEV Project
c/o Brandon Bratcher, Environmental Railroad Specialist
US Department of Transportation, Federal Railroad Administration
1200 New Jersey Avenue, SE, MS-20
Washington, DC 20590

Re: Environmental Impact Statement (EIS) Baltimore-Washington Superconducting Magnetic Levitation (SCMAGLEV) Project

Dear Mr. Smith, Mr. Al Khatib, Mr. Bratcher:

The City of Glenarden respectfully oppose the construction of the Baltimore-Washington SCMAGLEV train between Baltimore, Maryland and Washington, DC using the proposed right-of-way route directly impacting the City of Glenarden, Maryland.

CITY HISTORY

The Town of Glenarden, Maryland was incorporated by an Act of the Maryland General Assembly in 1939 (Chapter 650. House Bill 851.), becoming the third African-American municipality in Prince George's County. In two years, the City of Glenarden will be celebrating its 80th anniversary. The town has grown to become a city with over 6,000+ residents. The City has a Municipal Center that also houses a Senior Citizens Nutrition Center; two elementary schools - one identified as a Magnet School Program for children; four open space parks – Martin Luther King, Jr. Park, Dellwood Park, Hayes Street Memorial Park, and 7th Street Park; several places of worship, and a Veteran's Memorial dedicated to the veterans who proudly served their country and lived in the City of Glenarden. The City's Police Department has helped our city become one of the safest cities in Maryland for the past three years. The city has ongoing and new residential development in progress and is making great strides to better the community for its citizens.



City of Glenarden

COMMENTS: NOTIFICATION PROCESS

Our first comment addresses the SCMAGLEV project team's notification process for obtaining public comments for the scoping process. The project team published notice in the Federal Register / Volume 81, No. 227 on Friday, November 25, 2016 with the Environmental Impact Statement (EIS) comments submittal date ending December 27, 2016. The May 2017 **Final Scoping Report, Section 2 Public Scoping Process and Comments, Section 2.2.4 Flyer Distribution (p.7)** states *"Environmental Justice (EJ) communities, populations with high concentrations of minority and/or low-income individuals, may be less likely to view online communications. In order to reach these communities, hard copy flyers were distributed on December 5, 2016 by the project team in person or via email to the 58 different location types listed in Table 2-1."*

The City of Glenarden is 92% Black or African-American (U.S. Census 2010).

Question: What analysis did the project team perform to determine that *"minority and/or low-income individuals, may be less likely to view online communications?"* The statement appears to be discriminatory. The SCMAGLEV project notification process has already reduced the number of comments submitted from impacted citizens living within the City of Glenarden to zero (0) for the November 2016 – January 2017 project timeframe because we were not properly notified.

We, the City of Glenarden Council, are also concerned the project team did not notify all elected officials of the project even though the May 2017 **Final Scoping Report, Section 2 Public Scoping Process and Comments, Section 2.2.2 Mailings (p.6)** stated, *"Letters were sent to elected officials whose jurisdictions intersect the project study area. Phone calls were also placed to at least one elected representative for each town, municipality, and ANC (in DC)".* Please ensure all City of Glenarden elected officials, Mayor and each Councilmember (7), are notified on future correspondence for this project. You may contact the City of Glenarden Council Clerk at 301-322-1473 to obtain email addresses of current elected officials.

COMMENTS: PROPOSED ROUTES

Our second comment addresses the six proposed SCMAGLEV routes the project team identified. Two of those proposed routes, identified with yellow and green lines in the SCMAGLEV project maps, run through the City of Glenarden.

Question: What criteria was used to initially identify the 6 proposed routes?

Question: During a Public Informational Meeting the City of Glenarden held on September 20, 2017 concerning the SCMAGLEV project, BWRR representative David Henley informed citizens the 6 proposed routes would be reduced to 4 on October 10, 2017. We would like to request the criteria used to eliminate the 2 routes from the project study area.



City of Glenarden

We received a copy of the letter, Pete K. Rahn, Secretary Maryland Department of Transportation sent to Senator Joanne C. Benson, District 24 (dated September 7, 2017) that was shared during Senator Benson's Forum at Charles Flowers High School. The letter stated Governor Hogan's position concerning the proposed routes, *"It has always been the understanding of the Hogan administration this proposed project would only use **existing rights-of-way** intended for these types of purposes and have minimal impacts on those who live and work along the final route should the project come to fruition. The Hogan administration's position in regards to using **existing rights-of-way** for this proposed project **has not and will never change.**"*

The Washington, Baltimore and Annapolis Electric Railway (WB&A) provided passenger and freight service on an electrified route from 1908 to 1935. The WB&A right-of-way was converted to Maryland Route 704 (George N. Palmer Highway, later renamed to Martin Luther King, Jr. Highway). The existing right-of-way, Route 704, runs through the heart of the City of Glenarden. The City of Glenarden suffered during the 1960's Capital Beltway Interstate 495 construction project that eliminated several homes intersecting 6th Street and Glenarden Parkway. The City of Glenarden opposes the two proposed routes (yellow and green) for the SCMAGLEV train.

COMMENTS: PROPOSED ROUTES - IMPACTS

If Route 704 right-of-way is selected for the project, homes, schools, churches, the municipal building, businesses, parks, and memorials would be demolished.

COMMENTS: BULLET TRAIN AND MAGLEV TECHNOLOGY

The bullet train technology has not been successfully implemented in the United States. For example, the California bullet train project has faced costs overruns and unrealistic expectations (*Reference: "Why missed deadline for California bullet train is no surprise."*, *The San Diego Union – Tribune Editorial Board, September 30, 2017*). Japan boasts of bullet trains successfully reaching high speeds; however, these high-speed trains do not use the maglev technology. Japan is still testing the maglev technology.

The State of Maryland should not embrace this technology that has not been proven successful in this country and could potentially cause major problems later, such as the Japanese Takata airbags, in which the manufacturer has filed bankruptcy. The Japanese company offering the technology to the State of Maryland would be the sole proprietary owner of the technology. The State of Maryland and taxpayers would be dependent upon this company for maglev train operating costs and train maintenance.



City of Glenarden

The City of Glenarden requests the SCMAGLEV project team properly study the maglev technology environmental impacts. The study should include, but not be limited, to the following:

- Impact of the maglev technology on citizens' health (e.g., respiratory, different cancer types, etc.),
- Impact from the removal of trees to construct the maglev train route. (Trees produce oxygen, reduce smog, reduce carbon dioxide and greenhouse gases in the air, enhance water filtration and retention, cool down temperatures, produce habitats for wildlife, and reduce stress in humans.),
- Impact from the electromagnetic waves emitted from the magnetic fields needed to move the maglev train,
- Impact from the maglev train's underground vibrations and its impact on above-ground foundations,
- Impact from the maglev train's high energy consumption from the electrical grid that would be needed to power the train,
- Impact from the maglev train on air pollution on surrounding areas,
- Impact from the maglev train's external noise pollution on surrounding areas, which is produced by the train when it accelerates to the 300+ mile an hour speeds (i.e., aerodynamic noise),
- Impact of maglev train accidents,
- Impact of maglev train proposed routes on existing transportation networks used by citizens and commuters on a daily basis, and
- Financial impact on citizen's taxes.

COMMENTS: TRANSPORTATION ALTERNATIVES

Citizens in our community commute to work via cars, car pools, trucks, vans, or public transportation, including Metro, Amtrak, MARC, and ACELA trains. The SCMAGLEV project team should review transportation alternatives that would benefit the majority population, income levels, and demographics residing between Baltimore, MD and Washington, DC. SCMAGLEV trains would not reduce the daily traffic congestion around the 495 Capital Beltway, Route 50, Route 202, Route 450, or Route 704. These transportation networks could potentially be eliminated for SCMAGLEV construction.

The U.S. Department of Transportation and Federal Railroad Administration recently approved Northeast Corridor (NEC) Future Tier 1 Record of Decision that identifies an investment program to improve passenger rail service alternatives and future population and ridership projections for the year 2040 from Washington, DC to Boston, MA. (Reference: www.necfuture.com). This solution is a more viable solution to reducing the congestion and providing rail service for the majority population and income levels, without destroying existing communities and surrounding environments. It appears the SCMAGLEV project is only designed to support the daily travel of millionaires.



City of Glenarden

In summary, the SCMAGLEV train would **not benefit** the City of Glenarden citizens, improve our citizens' quality of life, or improve the transportation networks our citizens currently use in their daily travel and commutes.

We oppose the construction of the SCMAGLEV train in our community and we hope our elected officials in Prince George's County, State of Maryland, U.S. Senate, and U.S. House of Representatives support our position.

Sincerely,

Deborah Eason

Robin F Jones

City of Glenarden Council

Carolyn Smallwood
James A. Herring
Angela D. Ferguson
Celestine E. Wilson
Don L. Williams

Cc:

Hon. Steny Hoyer, U.S. House of Representatives
 Hon. Anthony G. Brown, U.S. House of Representatives
 Hon. Benjamin Cardin, U.S. Senator
 Hon. Chris Van Hollen, U.S. Senator
 Hon. Lawrence J. Hogan, Governor of Maryland
 Hon. Joanne C. Benson, Maryland State Senator, District 24
 Hon. Erek L. Barron, Maryland State Delegate, District 24
 Hon. Jazz M. Lewis, Maryland State Delegate, District 24
 Hon. Carolyn J. B. Howard, Maryland State Delegate, District 24
 Hon. Rushern L. Baker III, County Executive, Prince George's County
 Hon. Andrea C. Harrison, Councilwoman, Prince George's County Council, District 5
 Hon. Edward Estes, Mayor, City of Glenarden
 Hon. Deborah Eason, Councilwoman, City of Glenarden, Council President
 Hon. James Herring, Councilman, City of Glenarden, Council Vice-President
 Hon. Angela Ferguson, Councilwoman, City of Glenarden
 Hon. Robin Jones, Councilwoman, City of Glenarden
 Hon. Caroline Smallwood, Councilwoman, City of Glenarden
 Hon. Donjuan Williams, Councilman, City of Glenarden
 Hon. Celestine Wilson, Councilwoman, City of Glenarden
 Ms. Toni Taylor, Council Clerk, City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County
 The MagLev project would have a negative impact on both the homes and quality of life for the residents
 of Glenarden

MORE SIGNATURES FORTH COMING

Date

1/26/2017

Print name

Signature

Address

Print name	Signature	Address
1 Carolyn Smallwood	Carolyn Smallwood	7922 Ludmont Ave
2 Marion Clomax	Marion Clomax	7929 Echols Ave.
3 Nathaniel Pottman	Nathaniel Pottman	7935 Echols Ave.
4 Frances Stevenson	Frances Stevenson	7937 Echols Ave
5 Jacquelin Thompson	Jacquelin Thompson	7940 Echols Ave Glenard
6 JOHN C. THOMAS	John C. Thomas	7918 Echols Ave.
7 THOMAS OSBORN	Thomas D. Osborn	7912 Echols Ave
8 Emily O. Banks	Emily O. Banks	7912 Echols Ave
9 Douglas H. Banks	Douglas H. Banks	7912 Echols Ave
10 Ellen P. Brooks	Ellen P. Brooks	7904 Echols Ave.
11 M. Duver	M. Duver	7900 Echols Ave.
12 Imogene Key	Imogene Key	7909 Echols Ave
13 Octavia Jones	Octavia Jones	7903 Echols Ave
14 Yarnell Thomas	Yarnell Thomas	7918 Echols Ave
15 Sumie Michael	Sumie Michael	7924 Echols Ave
16 Lawrence Tucker	Lawrence Tucker	7924 Echols Ave
17 WALTER YOUNG	Walter Young	7927 Echols Ave
18 Sharon Culley	Sharon Culley	7930 Echols Ave
19 Robert Smallwood	Robert Smallwood	7928 Echols Ave
20 Margaret J. Dade	Margaret J. Dade	8714 Malain Ave
21 Joseph C. Dade	Joseph C. Dade	8714 Malain Ave
22 MARION LEWIS	Marion Lewis	7909 Cawker Ave
23 Vincent Lewis	Vincent Lewis	7909 Cawker Ave
24 Barbara S. Armstrong	Barbara S. Armstrong	7907 Cawker Ave
25 PASHAN KASSA	Pashan Kassa	7903 Cawker Ave

we the Citizens of the City of Glenarden are strongly opposed to any route being proposed for the GLEV Project
 PRELIMINARY ALTERNATIVES SCREENING REPORT
 Baltimore Washington DC
 North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING to 10-2-17

	Print name	Signature	Address
1	Harry Christian	Harry Christian	7814 Fiske Ave
2	Delma Howard	Delma Howard	7812 Fiske Ave
3	NANCY HOWELL	Nancy Howell	7802 Fiske Ave
4	Rodney Matthew	Rodney Matthew	7809 Fiske Ave
5	Sandra Hall	Sandra Hall	7800 Grant Dr.
6	DEIMA N Seals	Delma N. Seals	7801 Grant Dr.
7	George Grant	George Grant	7907 Fiske Ave
8	Jamie Twilley	Fannie Twilley	7909 Fiske Ave
9	Francenia E. Tucker	Francenia E. Tucker	7828 Fiske Ave.
10	Irene E. Wanger	Irene E. Wanger	7813 Fiske Ave.
11	Rose Noel ROSE NOEL	Rose Noel	3646 Cousins Dr
12	Irene Douglas	Irene Douglas	6221 Nether Ln. Bowie, Md. 20720
13	Rosalind Davis	Rosalind Davis	711 Henry S Truman Dr. #215 Jays, MD 20774
14	ADA A. Whitley	Ada A. Whitley	209 Station Dr Kettering, MD 20774
15	Geraldine Curry	Geraldine Curry	1530-5th St. Glenarden, MD 20706
16	Thelma Smith	Thelma Smith	3615 Cousins Dr. Glenarden 20774
17	Mildred Hill	Mildred Hill	9000 Wallace Rd Lanham MD 20770
18	Patricia D. Kenner	Patricia D. Kenner	3407 Edwards St. Springdale 20774
19	Terry Hardy	Terry Hardy	1501 2nd St Glenarden, MD
20	Mary Sheppard	Mary Sheppard	9700 Buttercup Dr. Springdale, MD 20774
21	Mervine Stevens	Mervine Stevens	10118 Campy Wdy So. #203 Upper Marlboro, MD 20774
22	MARTORIE MOORE	Marjorie Moore	5999 Emerson St #529 Bladensburg, MD 20710
23	DAISY SAUNDERS	Daisy Saunders	12810 Peachley Ct Upper Marlboro MD 20774
24	Louelyn P. Stephens	Louelyn P. Stephens	821, Caston Way #101 Landover, MD 20775
25	Catherine Williams	Catherine Williams	3713 Harmon Ave Hyattsville, MD 20784

North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

late 10/3/2017

	<u>Print name</u>	<u>Signature</u>	<u>Address</u>
1	JUANITA O. SCOTT	Juanita O. Scott	3304 Hayes ST Glenarden, MD 20706-1731
2	Ruby L Strachan	Ruby L Strachan	2806 Berrylane, Springdale MD
3	Romella Pickett	Romella Pickett	9102 Parkway ST ²⁰⁷⁰⁶ Lanham, MD
4	Delores Pender	Delores Pender	12704-Millstream DR. Bowie, MD ²⁰⁷⁰⁶
5	BRENDA TAYLOR	Brenda Taylor	55010 BELVA CT, LANHAM, MD ²⁰⁷⁰⁶
6	Isata Conteh	Isata Conteh	7809 Dellwood Ave
7	Warren Davis	Warren Davis	7801 Beddington Ct ^{Olinton} 20735 ^{MD}
8	Roger Kinchen	Roger Kinchen	7824 Dellwood Ave
9	Barbara Kinchen	Barbara Kinchen	7824 Dellwood Ave
10	Vincent Ward	Vincent Ward	7905 Dellwood Ave
11	L.R. Brown	L.R. Brown	7906 Dellwood Ave
12	Delores C. Brown	Delores C. Brown	7906 Dellwood Ave
13	Frenzella Faison	Frenzella Faison	7908 Dellwood Ave
14	Jack Jones	Jack Jones	7924 Dellwood Ave
15	Meola Jones	Meola Jones	7924 Dellwood Ave
16	Sarah E. Riddasill Davis	Sarah E. Riddasill Davis	7918 Dellwood Ave
17	Ledia Motley	Ledia Motley	7916 Dellwood Ave
18	Michele Thompson	Michele Thompson	7914 Dellwood Ave
19	Regina Carroll	Regina Carroll	7933 Dellwood Ave
20	Donusha Ballard	Donusha Ballard	7930 Dellwood Ave
21	NETTALI ARGUETA	Nettali Argueta	7931 Dellwood Ave
22	Ester Lopez	Ester Lopez	7941 Dellwood Ave
23	Candyn McCallum	Candyn McCallum	7949 Dellwood Ave
24	Renée Buckner	Renée Buckner	7953 Dellwood Ave
25	Calvin Harris	Calvin Harris	8000 Delwa. Ave

we the Citizens of the City of Glenarden are strongly opposed to any route being proposed for the MAGLEV Project
 PRELIMINARY ALTERNATIVES SCREENING REPORT
 North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

Date 10/4/2017

	Print name	Signature	Address
1	Keith Washington	Keith Washington	8210 Dellwood Court
2	Schelia Stewart	Schelia Stewart	8210 Dellwood Court
3	Michael Beaks	Michael Beaks	8204 Dellwood Court
4	Certrude P Edwards	Certrude P Edwards	8217 Dellwood Court
5	Shakey Williams	Shakey Williams	8219 Dellwood Court
6	Heanna Queen	[Signature]	8242 Dellwood Ct
7	Chels/Moore Swamey	Chels Moore	8218 Dellwood Ct.
8	Yvonne Wright	Yvonne Wright	8218 Dellwood Ct.
9	Robert Williams	Robert Williams	8244 Dellwood Ct
10	Maimouna Cisse ^{Williams}	Maimouna Cisse	8244 Dellwood Ct
11	Chynia Lewis	Chynia Lewis	8228 Dellwood Ct
12	Gregory Evans II	Gregory Evans II	8221 Dellwood Ct
13	Claudine H. Evans	Claudine H. Evans	8221 Dellwood Ct
14	Jalyn King	Jalyn King	8221 Dellwood Ct
15	Charles Taylor	Charles Taylor	7937 Glenarden Pkwy.
16	Boranne King	Boranne King	7905 Glenarden Pkwy
17	Falegh Kull	Falegh Kull	7815 Glenarden Pkwy
18	Annie Greene	Annie Greene	Matthews 7813
19	CHARLENE Miles	Charlene Miles	7809 Glenarden Pkwy
20	GILBERT MUSTI	Gilbert Musti	7713 Glenarden Pkwy.
21	Joyce Butler	Joyce Butler	7711 - GLENARDEN Pkwy
22	Camille Baxter	Camille Baxter	7707 Glenarden Pk way
23	Vaquan Harris	Vaquan Harris	7707 Glenarden Pkwy
24	C G Butler	C G Butler	7705 Glenarden Pkwy
25	Jason Wilson	Jason Wilson	7937 Echo SAU

North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

Date 10/5/2017

	Print name	Signature	Address
1	ROBIN F JONES	Robin F Jones	1513 2nd St Glenarden Md
2	Teley Hardy	Teley Hardy	1501 2nd St Glenarden MD 20706
3	M.C JONES	M.C Jones	1508 2nd St Glenarden MD 20706
4	Robin K. Rice O.D	Robin K. Rice	1518 2nd Street Glenarden MD 20706
5	Jermaine Hodges	Jermaine Hodges	8627 Glenarden Pkwy 20706
6	Michelle Creech	Michelle Creech	6617 Tan St. New Carrollton MD 20784
7	Michelle Togi Taylor	Michelle Togi Taylor	11650 Parklawn Place Bowie MD
8	Harry B. Smith	HARRY B. SMITH	9016 - GLENARDEN PKWY (20706) GLENARDEN MD 20706
9	Quindora Simmons	Quindora Simmons	1431 9th St Glenarden MD 20706
10	Rhonda Reed	Rhonda Reed	9021 Glenarden Pkwy, Glenarden MD 20706
11	Shake Gray	Shake M. Gray	1428 10th St. Glenarden MD 20706
12	Annie A. Smith	Annie A. Smith	9016 Glenarden Pkwy Glenarden MD 20706
13	Wilhemina Dorsey	Wilhemina Dorsey	1516 3rd St Glenarden Md. 20706
14	Tammy D. Prince	Tammy D Prince	3506 Gary Court Glenard MD 20706
15	MICHAEL MALLOY	Michael Malloy	9206 GARY LANE SPRINGDALE MD 20706
16	Cory Lee	Cory Lee	9115 Gary Ln Glenarden MD
17	Colleen B. Thomas	Colleen B. Thomas	3510 TYRA DR. GLENARDEN, MD 20704
18	Elton Price	Elton Price	3634 Cousins Dr. Glenarden MD
19	Miles Simmons	Miles Simmons	3630 Cousins Dr Springdale MD 20774
20	Dredder R. Carter	Dredder R. Carter	3606 Cousins Dr Glenarden, MD 20704
21	FRANCES CARTER	Frances J. Carter	" " " "
22	MICHAEL E. BRYSON	Michael E. Bryson	3603 Cousins Dr GLENARDEN MD 20774-2600
23	Raymond L. Carter	Raymond L. Carter, Jr.	3510 JEFF Rd Glenarden, MD 20704-2610
24	Elizabeth Jones	Elizabeth Jones	7927 Cowtop Ave
25	Janice Jones	Janice Jones	7937 Cambridge Ave

We the Citizens of the City of Glenarden are strongly opposed to any route being proposed for the North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

10/6/2017

Print name	Signature	Address
1 Debbie Gray	Debbie Gray	1303 Bell Haven Dr
2 Linda Milhouse	Linda Milhouse	390 Possum Ct
3 Joan Smith	Joan Smith	354 Possum Ct Cyf Ln
4 ELLA WATTS	Ella Watts	4211 54th St. Bladensburg
5 Lottie Battle	Lottie Battle	10104 Westphalia Rd. Upper Marlboro, MD
6 John Battle	John Battle	11
7 Milton Williams	Milton Williams	6561 Hilmar Dr. Forestville MD
8 LARRY C REID	Larry C Reid	8806 Jenna Ct. Lutham, MD
9 Catherine Young	Catherine Young	3829 Thornwood Rd Hyattsville, MD 20799
10 LAVERNE CREEK	Laverne Creek	10195 Campus Way South Largo MD 20774
11 ANTHONY MELORFE	Anthony Melorfe	1816 Sycamore 1492 Ct. Prince Md 20724
12 MABEL MELORFE	M. Melorfe	4027 Glenarden Pkwy Glenarden MD.
13 Barbara Robinson	Barbara Robinson	7414 Parkside Hill Glen Park MD 20769
14 LORNA WAINES	Lorna Waines	729 CARRINGTON PI SEAT. PLEASANT MD 20743
15 Charles Williams	Charles Williams	1216 Farmington Ave Capitol Hill Md 20743
16 Reginald Gordon	Reginald Gordon	8723 McLean Ave. Glenarden, Md. 20706
17 Donald Taylor	Donald Taylor	800 CARRINGTON AVE SEAT PLEASANT MD 20743
18 Thelma Woodard	Thelma Woodard	3134 - 63rd Ave. Cheverly, MD 20785
19 Sandra Miller	Sandra Miller	6704 Rebeck Street Springdale Md 20784
20 Diann Austin	Diann Austin	114 Edenton Dr Largo, MD 20774
21 LORNA MACHIE	Lorna Machie	7511 arcade ct Hyattsville MD 20785
22 Jeanine Simms	Jeanine Simms	907 Pleasant Hill Ln., Bowie MD 20716
23 Carolyn Johnson	Carolyn Johnson	906 Highview Dr. Cap Hts MD 20716
24 Norma Hamall	Norma Hamall	5507 Sharon Elaine Dr New Carrollton, MD 20784
25 Alysia Cross	Alysia Cross	4204 NW Way Bowie MD

we the Citizens of the City of Glenarden are strongly opposed to any route being proposed for the MAGLEV Project
 PRELIMINARY ALTERNATIVES SCREENING REPORT
 North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

Date 10/7/2017

	Print name	Signature	Address
1	Catherine Smith		3104 Tyler Ct., Glenarden, Md 20702
2	Devonte Jones		3102 Tyler Ct, Glenarden MD 20702
3	Bita Jones		3102 Tyler Ct, Glenarden, MD 20702
4	Kim Jones		3101 Tyler Ct, Glenarden, MD 20702
5	Montel Neal		3101 Tyler Ct, Glenarden, MD 20702
6	Yvonne Jones		3101 Tyler Ct, Glenarden, MD 20702
7	Antonio Raynor		7915 Tyler St Glenarden MD 20702
8	Wesley Robinson		7915 Tyler St Glenarden MD 20702
9	Carrie		7918 Tyler St. Glenarden, MD 20702
10	Wanda Armstrong		7914 Tyler St, Glenarden, MD 20702
11	Rosangely		7914 Tyler St Glenarden
12	Tameeka Robotham		7904 Tyler St, Glenarden, MD
13	CORTEZ MARKS		7938 Polk St Glenarden, MD
14	Jollyn Marks		7938 Polk St Glenarden, MD 20702
15	MONA HILL		7937 POLK ST GLENARDEN, MD
16	Siva Powell		7931 Polk St Landover, MD
17	Maerwin Powell		7920 Polk St Glenarden, MD 20702
18	Jon Hunter		20706
19	KEITH REDMAN		7924 Polk St., Glenarden MD 20702
20	SULAIMAN KEITA		7922 Polk St. GLENARDEN, MD
21	Deborah Robinson		7920 Polk St. Glenarden, MD 20702
22	Charles L. Messie		7925 Polk St. Glenarden, MD
23	Raymond H Langford		7919 Polk St Glenarden, MD
24	Fheraldine Langford		7919 Polk St Glenarden, MD
25	Celestine E. Wilson		7906 Tyler St, Glenarden, MD 20702

PRELIMINARY PERMITS FOR THE CITY OF GLENARDEN ARE STRONGLY OPPOSED TO ANY ROUTE BEING PROPOSED FOR THE MAGLEV PROJECT

North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents of Glenarden

MORE SIGNATURES FORTH COMING

Date 10/9/2017

Print name	Signature	Address
1 Wynnon Kelley	Wynnon Kelley	4410 Allies Rd Suitland md 20746
2 Arthur Bucher	Arthur Bucher	7704 Allendale Dr
3 Betty Buchanan	Betty Buchanan	7704 Allendale Dr
4 Penelope	Penelope	12302 Longwater Dr 20721
5 Edith G. Butler	Edith G. Butler	8234 Dellwood Ct. 20706
6 SHARON BURKE	Sharon Burke	10033 Campers Way Largo MD 20782
7 Dollie Upchurch	DK	4602 Burlington Rd Hyattsville, md. 20781
8 Cynthia Miles	Cynthia Miles	803 Cypress Pt Cir Mitchellville, MD 20721
9 Patricia Vinson	Laticia Vinson	6402 Carrollton Court New Carrollton, MD 20784
10 JAMES KENNEDY	James Kennedy	12705 Cypress Rd Glenarden, md. 20767
11 Lessie Matthews	Leslie Matthews	7808 Johnson Avenue Glenarden, md 20706
12 Arrey Johnson	Larry Johnson	906 Highview Rd CAPITLT. 20749
13 Brent W. Clay	Brent W. Clay	3805 Clairton Dr. Bowie Md 2072
14 Sharon Taylor	Sharon Taylor	3414 Bramble Ln-Bowie
15 Wilbert Wilbert Jackson	Wilbert Jackson	2104 Woodvale Ln-Bowie
16 Melvin Fields	Melvin W. Fields	8628 Glenarden Pkwy
17 Carolyn Goss	Carolyn Goss	10400 Welshie Dr, Upper Marl MD 207
18 Cynthia A. Anderson	Cynthia A. Anderson	3604 Kingswood Dr., District Heights 207
19 DD BLUE	3714 A2nd AVE	Cottage City md 20722
20 Jenatha Smith	440723 pl	Temple Hills MD
21 DORIS ANDERSON	Doris A. Anderson	FORESTVILLE MD 20747 2560 OAKGLEN WAY
22 VICTOR THOMAS	Victor Thomas	918 FLORES STREET CAPITOL HEIGHTS MD 20743
23 Charles Thompson	Charles Thompson	5999 EMERSON ST APT 822 BLADENSBURG, MD 20710
24 Sharonda Wood	Sharonda Wood	8117 Allendale Dr, Landover, MD 20755
25 Angela Angela Walker	Angela Walker	401 Addison Rd So. Capitol Hgts. MD 20706

We the Citizens of the City of Glenarden are strongly opposed to any route being proposed for the
 PRELIMINARY ALTERNATIVES SCREENING REPORT Baltimore-Washington SCMagLEV Project
 North East Magnetic Levitating train passing through the City of Glenarden

We are also opposed to the North East Magnetic Levitating train passing through Prince George's County

The MagLev project would have a negative impact on both the homes and quality of life for the residents
 of Glenarden

MORE SIGNATURES FORTH COMING

Date 10/8/2017

	Print name	Signature	Address
1	Karen Mackay	Karen Mackay	4608 Brandon Ln, Beltsville, MD 207
2	Constance Pope	Constance Pope	PO Box 1352, Laurel, MD 20785
3	James E. CROSS III	James E. Cross	16013 DORSET RD, LAUREL 2070
4	Laurel S. CROSS	Laurel S. Cross	16013 Dorset Rd, Laurel, 20705
5	Michelle Snider	Michelle J. Snider	11808 N. Lincoln Ave. Belt MD 20705
6	Margaret Lear	Margaret S. Lear	4003 Hillwood Ct. Beltsville MD
7	Ann WAGNER	Ann Wagner	16116 Kent Rd Laurel Md 20707
8	Ellen Utley	Ellen Utley	7310 Good Luck Rd, Lanham, MD 20706
9	Elizabeth Wilkins	Elizabeth Wilkins	7074 Hanover Parkway APT C1, ^{Greenhair, MD} 20770
10	Baffour Owusu-Adjei	Baffour Owusu-Adjei	3401 JEFF Rd Glenarden 20774
11	Tu Little Camara	Tu Little Camara	6207 Gary Lane Glenarden 20774
12	Hawa Traore	Hawa Traore	4615 Cimmaron greenfield MD bowie
13	Agatha Owusu-Adjei	Agatha Owusu-Adjei	3401 JEFF Rd Upper Marlboro 20774
14	Barbara J Powell	Barbara J. Powell	8207 Bellvue Ct, Clinton MD
15	Kimberly Williams	Kimberly Williams	6314 Hard Bergeon Circle MD Indian Head MD 20640
16	Elizabeth A. Brown	Elizabeth A. Brown	5104 Woodford Ln ^{Upper MD} Marlboro 20772
17	Shirley V. Hill	Shirley V. Hill	6102 Brooke Lane Dr. Clinton, Md 20735
18	Vivian Bridges	Vivian Bridges	700 Parkside Pl. NE ^{Dos}
19	Matthew L. Williams	Matthew L. Williams	913 Larchmont Ave Capitol Hill
20	Ebony McKinley	Ebony McKinley	8409 Cinema Ct. Clinton MD 20735
21	Philip J. Russell	Philip J. Russell	10704 Heatherleigh Cheltenham MD 20615
22	Peggy White	Peggy White	9918 Lyndee Pl ^{Upper Marlboro} MD 20772
23	Joyce R. McKinley	Joyce R. McKinley	8409 Cinema Court Clinton MD 207
24	Rhonda ERWIN	R. Erwin	6205 Buckler Rd Clinton
25	Jaquelyn Garrison	Jaquelyn Garrison	9209 Gary Lane 20774

MORE SIGNATURES FORTH COMING



WE HAVE EACH SIGNED THIS PETITION FREELY AND WITHOUT PRESSURE

Signature	Name (Please Print)	Address	Home Phone No.
1. <i>Phyllis Cokney</i>	Phyllis Cokney	7903 Piedmont	
2. <i>JOS</i>	Jennifer Romero	7904 Johnson	
3. <i>James Hall</i>	James Hall	7817 Johnson	
4. <i>Armentia Jackson</i>	Armentia Jackson	7814 Johnson Ave	
5. <i>Rena Johnson</i>	Rena Johnson	3204 Johnson	
6. <i>Wanda R. Johnson</i>	Wanda R. Johnson	11 "	
7. <i>Daisy Chapman</i>	Daisy Chapman	7922 Johnson Ave.	
8. <i>Tamosh L. Simpkins</i>	Tamosh L. Simpkins	3204 Hayes Street	
9. <i>Eulalia C. Simpkins</i>	Eulalia C. Simpkins	3204 Hayes Street	
10. <i>Louis Smith</i>	Louis Smith	3202 Hayes St	
11. <i>Fred Ferguson</i>	Fred Ferguson		
12. <i>Noel Carino</i>	Noel Carino	306 Hayes	
13. <i>Wanda Gibson</i>	Wanda Gibson	7924 Johnson Ave	
14. <i>Debra Johnson</i>	Debra Johnson	7900 Johnson Ave.	
15. <i>Barbara McLog</i>	Barbara McLog	7909 Piedmont Ave	
16. <i>Jerry Colbert</i>	Jerry Colbert	7904 Piedmont Ave	
17. <i>Catherine Ford</i>	Catherine Ford	7904 Piedmont Ave	
18. <i>McPaula Creighton</i>	McPaula Creighton	7915 Piedmont Ave	
19. <i>Credon Spaxton</i>	Credon Spaxton	7922 Piedmont Ave	
20. <i>Michael Shannon</i>	Michael Shannon		
21. <i>Dwight</i>	Dwight	3202 Reed St	
22. <i>Michael Washington</i>	Michael Washington	320 Reed St	(240) 241-0057
23. <i>Charles Browne</i>	Charles Browne	3204 Reed St	

MORE SIGNATURES FORTH COMING



WE HAVE EACH SIGNED THIS PETITION FREELY AND WITHOUT PRESSURE

Signature	Name (Please Print)	Address	Home Phone No.
1. <i>[Signature]</i>	Royette Smith	7907 + 7909 MLK Jct	301-773-0140
2. <i>[Signature]</i>	Royette Smith	8733 McLain Ave	301-773-0140
3. <i>[Signature]</i>	Barbara Ford	7906 Piedmont Ave	301-322-2325
4. <i>[Signature]</i>	Andre Young	7226 E. Forest Rd	202-665-0119 Cell
5. <i>[Signature]</i>	Teresa Taylor	717 Chillum Rd Pineville MD	210-665-3519
6. <i>[Signature]</i>	Mickie Williams	6802 Deethland Rd	301-333-1793
7. <i>[Signature]</i>	Anzela Hobbins	14204 Denwood Valley	301-334-6651
8. <i>[Signature]</i>	CHRIS BURGESS	7932 PIEDMONT AVE	301-322-4183
9. <i>[Signature]</i>	DAYNE KENLEY	7932 PIEDMONT AVE	202 570 2406
10. <i>[Signature]</i>	Denise Kenley	7932 Piedmont Ave	301 322 4183
11. <i>[Signature]</i>	Briyana Mitchell	7932 Piedmont Ave	240-351-1690
12. <i>[Signature]</i>	Kenna Burgess	7932 Piedmont Ave	301-395-6731
13. <i>[Signature]</i>	Evelyn K... ..	7932 Piedmont Ave	301-322-4183
14. <i>[Signature]</i>	KEVIN BURGESS	4539 KINMOUNT RD	240-832-8744
15. <i>[Signature]</i>	JANOU GLOFF	5067 West Boniwood	301-773-1172
16. <i>[Signature]</i>	ONLEY MINNIS	8230 Dellwood Ct	301 773 1172
17. <i>[Signature]</i>	Gracie Minnis	8230 Dellwood Ct	301 773 1172
18. <i>[Signature]</i>	Folth Prutler	8234 Dellwood Ct	301 706 7243
19. <i>[Signature]</i>	Tiffany Hinson	8238 Dellwood Ct	202-468-7997
20. <i>[Signature]</i>	Paula Ray	8237 Dellwood Ct	301-772-5676
21. <i>[Signature]</i>	Kieran Cress	8225 Dellwood Ct	202-701-6503
22. <i>[Signature]</i>	Antoine Holman	8224 Dellwood Ct	202-253-5614
23. <i>[Signature]</i>	Trevor MITCHELL	8222 Dellwood Ct.	240-472-4896

September 13, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

Received
SEP 19 2017
Office of Planning

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mssrs. Smith and Trueschler,

We are formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community, and property, would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

I am almost 80 years old. I can't have this in my back yard. I want a safe place and a good environment for my two grandsons to visit and play in my back yard. You are forcing me to move from the only home I have ever had if this proposal goes through.

Regards,

Jules R. Cornet
7879 East Drive
Severn, MD. 21144
410-551-5525
hhmachine@aol.com

CC:

Governor Larry Hogan

100 State Circle
Annapolis, Maryland 21401
410-974-3901

State Senator Doug Peters

James Senate Office Building, Room 120
11 Bladen St., Annapolis, MD 21401
(410) 841-3631, (301) 858-3631
1-800-492-7122, ext. 3631 (toll free)
email: douglas.peters@senate.state.md.us
fax: (410) 841-3174, (301) 858-3174

Delegate Geraldine Valentino-Smith

House Office Building, Room 427
6 Bladen St., Annapolis, MD 21401
(410) 841-3101, (301) 858-3101
1-800-492-7122, ext. 3101 (toll free)
email: geraldine.valentino@house.state.md.us
fax: (410) 841-3850, (301) 858-3850

US Senator Chris Van Hollen

Washington, DC Office
110 Hart Senate Office Building
Washington, DC 20510
Phone (202) 224-4654
Fax: (202) 228-0629
TTY: (202) 224-1546

State Office

111 Rockville Pike
Suite 960
Rockville, MD 20850
Phone (301) 545-1500

US Senator Ben Cardin

Washington, DC Office
509 Hart Senate Office Building
Washington, DC 20510
Tel: (202) 224-4524
Fax: (202) 224-1651

Bowie Office
10201 Martin Luther King Jr.
Highway, Suite 210
Bowie, MD 20720
Tel: (301) 860-0414
Fax: (301) 860-0416

Congressman Steny Hoyer
Washington, DC Office
1705 Longworth House Office Building
Washington, D.C. 20515
Phone - (202) 225-4131
Fax - (202) 225-4300

Prince George's County Office
U.S. District Courthouse
6500 Cherrywood Lane, Suite 310
Greenbelt, MD 20770
Phone - (301) 474-0119
Fax - (301) 474-4697

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Mssrs. Smith and Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel
- Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City.

Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

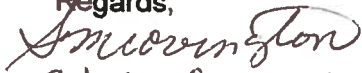
- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."

- 40 CFR §1501.7, Scoping states that "There shall be an early **and open process** for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action."
- 40 CFR §1506.6, Public involvement states that "Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
- The Council on Environmental Quality 40 Most Asked Questions (Q13) states "The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. **But that is only the minimum requirement..**" and scoping must have "**appropriate public notice** and enough information available on the proposal so that the public and relevant agencies can participate effectively."

In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

We await your response to these requests.

Regards,


Sylvia Covington
12214 Rolling Hill Ca
Bowie MD 20715

CC:

EPA Region 3
Advisory Council on Historic Preservation
Council on Environmental Quality
MD Historical Trust
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Prince George's County Executive Rushern Baker

Prince George's County Councilman Todd Turner
Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)
Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Trough (District 4)

PRELIMINARY ALTERNATIVES SCREENING REPORT

JAMES "ED" DEGRANGE, SR.

Legislative District 32
Anne Arundel County

Budget and Taxation Committee

Chair

Capital Budget Subcommittee

Chair

Public Safety, Transportation,
and Environment Subcommittee

Executive Nominations Committee

Vice Chair

Rules Committee

Senate Chair

Joint Committee on Legislative Ethics

Baltimore-Washington SCMAGLEV Project

James Senate Office Building
11 Bladen Street, Room 101
Annapolis, Maryland 21401
410-841-3593 · 301-858-3593
800-492-7122 Ext. 3593
Fax 410-841-3589 · 301-858-3589
James.DeGrange@senate.state.md.us



The Senate of Maryland
ANNAPOLIS, MARYLAND 21401

September 6, 2017

Received

SEP 12 2017

Office of Planning

John G. Trueschler/SCMAGLEV Project
Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21202

Dear Mr. Trueschler:

I am writing to request that the scoping period for the Super Conducting Magnetic Levitation Environmental Impact Study be reopened.

Recently, numerous constituents have reached out to me with concerns about the Super Conducting Magnetic Levitation (SCMAGLEV) scoping period, specifically that they did not have sufficient notification or time to provide comment. This potential project will have a tremendous impact on both Northern and Western Anne Arundel County, and my constituents have communicated to me that they wish for the scoping period for the SCMAGLEV Environmental Impact study to be reopened to provide them the opportunity to have their voices heard.

Because of the potential impact of SGMAGLEV or any alternatives, I believe it to be vital for citizens to be given extended opportunity to voice their opinions on the need for the project, any alternatives to the project, and the scope of the environmental studies and impact analyses.

Again, for the best interest of my constituents, I ask that you reopen the scoping period for the Super Conduction Magnetic Levitation Environmental Impact Study. Should you wish to discuss this matter with me personally, I can be reached by email at james.degrange@senate.state.md.us or by telephone at (410) 841-3593.

Sincerely,

A handwritten signature in black ink that reads "James Ed DeGrange, Sr." in a cursive script.

James "Ed" DeGrange, Sr.
State Senator, District 32

cc: Governor Lawrence Hogan, Jr.
Secretary Pete Rahn

September 13, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mssrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,



Mr. & Mrs. Wayne Fells
Hyattsville, Maryland (Woodlawn Sub-Division)

CC:
Congressman Steny Hoyer
County Exec. Rushern Baker
Senator Ben Cardin
Senate Delegate Alonzo Washington

Received
SEP 19 2017
Office of Planning

September 14, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mr. Smith and Mr. Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My Community and home will be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternative meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the concerns of the affected public.

Regards,



Joellyn Furmage
1209 Winer Road, Odenton, MD 21113

CC:

Lawrence J. Hogan, Governor
Christopher Van Hollen, JR., U.S. Senator
Benjamin L. Cardin, U.S. Senator
John P. Sarbanes, U.S. Representative 3rd Congressional District

Oct 2017

To Whom it may Concern,

I am writing this letter in regard to the SCMAGLEV train. PLEASE do not allow this train to take away my home of 35 years as well as the neighborhood of my childhood. My life is here and I am not able to relocate and start my life over. PLEASE, in the name of God Almighty see how devastating this would be to thousands of families and lives. In Odenton we already have a train [MARC], so this is non-sense to have this go thru beside an existing train. Why not run the train (if such a train is actually a necessity which I truly believe it is not) from Washington DC to NY down the medium of either BWI Parkway or I 95 corridor? This would make more sense if such a train is truly needed. This would be more cost effective and would not destroy so many lives.

I am just now hearing about this train. My neighbors and I never received any information on scoping meetings or were given the opportunity to comment on the EIS. I am formally requesting that the scoping period for SCMAGLEV be reopened. The impact of this SCMAGLEV is devastating to thousands, it harms more than it could ever benefit.

Jacqueline Gombrell
542 King Malcolm Ave.
Odenton, MD 21113

July 29, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Mr. Smith and Mr. Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

July 29, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 2 of 6

According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel
- Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City.

Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."

July 29, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 3 of 6

- 40 CFR §1501.7, Scoping states that “There shall be an early **and open process** for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action.”
- 40 CFR §1506.6, Public involvement states that “Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
- The Council on Environmental Quality 40 Most Asked Questions (Q13) states “The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. **But that is only the minimum requirement...**” and scoping must have “**appropriate public notice** and enough information available on the proposal so that the public and relevant agencies can participate effectively.”

In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

We await your response to these requests.

Regards,

Darcie Gingell-Manosa

12412 Rockledge Drive
Bowie, MD 20715

CC:

EPA Region 3
Advisory Council on Historic Preservation
Council on Environmental Quality
MD Historical Trust
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Prince George's County Executive Rushern Baker

Prince George's County Councilman Todd Turner
Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)
Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Trough (District 4)

Joseph M. Gorman
1200 Winer Rd
Odenton, MD 21113

October 10, 2017

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Ave. SE
Washington, DC 20590

I am writing to you to oppose the SCMAGLEV Project. The Maryland Department of Transportation has implemented the NEPA study with a lack of transparency and proper notification. Attached is the Maple Ridge Home Owners Association resolution to oppose this project. Thank you for your attention to this subject.

Thank You,
Joe Gorman

Peter & Edna Granahan
12102 Rustic Hill Drive
Bowie, Maryland 20715
301-262-9018
Cell 301-395-1624
pegranahan@verizon.net

July 31, 2017

Maryland Department of Transportation
Bradley Smith Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, C/O John G. Trueschier
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy and Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

Re: SC Maglev Scoping Report – Please reopen scoping process

To whom it May concern:

Until very late in June 2017 I, my neighbors and most Bowie land owners and residents had never heard about Maglev or the possibility it may come into our area.

There were no announcements, notifications, letters, posters, post cards, invitations to open house meetings, advertisements, scoping process, final scoping report or any other communications.

My wife and I have lived in Bowie for 51 years. Raised a family of 4 children and have 7 grandchildren. We have lived on Rustic Hill Drive in the “R” section for 40 years. It would be difficult for us in our late seventies to have to relocate. Our home is the center of family and extended family activities and holiday dinners for up to 30 people.

We have two small properties between our house and the WB & A trail. We have had a lot of inquiries to purchase our house and properties in the last two years, 3 in the last week. This project may explain that, almost all of them were from outside our area.

Now that I have received and read the 170 page Final Scoping Report of 5/17/2017, I understand why I and a whole lot of Bowie land owners had no knowledge of this project. We were never informed or included in the process in any way. Shameful! We were excluded and we may be the most affected by this project.

Website

Did not know website existed, therefor it was of no use to us.
We were excluded

Flyer Distribution Locations – EJ Outreach

No contact or distribution locations in Bowie, MD per Final Scoping Report, the largest city between Baltimore and Washington.
Only one distribution location in Anne Arundel County, a boys and girls club
Again, we were excluded.

Post Cards and letters

Never saw one. I don't know anyone who did.
Again, we were excluded.

Open House Presentations December 10–15, 2016

One location in Prince George's County – West Lanham fire house
18 attendees out of a population of 39,395 with a total of 2 comments
We did not know about this open house. No invitation.
No open house in Bowie.
Again, we were excluded.
In the 5 open houses from Baltimore to Washington there were only 152 sign-ins and total of only 57 comments or questions. This is a very poor representation of the study area.

Advertisements

Never saw one. I don't know anyone who did. Newspapers: the Bowie Blade and the Washington Post. Saw no television ads on the Washington channels.
Again, we were excluded.

Public Comments

In total there were only 75 comments and questions. 16 were from the website, 2 mail and 57 from open houses. The largest number by topic was 19 or 25% about alignment of routes. Most (15 to 17?) of these comments requested more information of a Maglev alignment. Was this information provided to them?
Answers not in the report and still not provided to us.

City of Bowie City Council Meeting – July 10, 2017

A presentation was made by David Henley, SCMAGLEV Project Director.
His presentation was primarily the technology and speed of the train, future possibilities, funding by Japan (5 billion dollars) and by the US Government (27

million dollars). No funding needed from Maryland. He did a good sales presentation.

Mr. Henley had been told not to comment on the alignment routes, impact or environmental issues. These were the primary reasons over 300 local citizens attended the council meeting.

The meeting overflowed the City Council Chambers and there were standing room only rooms set up with projection TVs to watch the presentations. The presentations were broadcast live on the city of Bowie TV station as well as scheduled rebroadcasts. I have no idea how many stayed at home and watched it on TV, my wife did. Most people in the room commented they just heard about the Maglev in the past 2 to 3 weeks by word of mouth.

24 citizens made presentations. A good number of them also provided their statements in writing to the Mayor. Mr. Henley was very impressed with every one of the presentations, as was I, and said he made a lot of notes to take back with him. He was also very apologetic and said he would work to make this process fair. He carefully made no direct guarantees. Each Council Member also made a brief presentation.

Scoping Process / Report Should Include

Opportunity for everyone to be informed, involved, submit comments and recommendations.

Include Mr. Henley's notes from the Bowie Council meeting on July 10, 2017.

The impact of alignment routes on homes and property, direct and property value.

Impact of alignment routes on churches, schools, recreation, parks and businesses.

City of Bowie report on council meeting of July 10, 2017 and other reports.

Notes and recommendations from the Citizens Against SCMAGLEV.

Bill Boone's analysis on the effect property on the yellow and green routes in Bowie, MD and Twin Rivers Retirement Community in Anne Arundel County

Pro forma or synopsis of pro forma based on revenue and ridership.

Projected ridership by Maryland citizens by; state regions, counties or city/communities for MAGLEV between Wash. BWI and Baltimore.

Overview of cost for Bowie, State of Maryland and the US

Overview of funding and future possibilities for liabilities

What are the terms of the 5 billion dollar loan from the Bank of Japan? Who is liable?

What is the status of all other of the SCMAGLEV proposal, approvals, projects, construction, operations and financial status worldwide?

Peter & Edna

Peter and Edna Granahan

Cc: David Henley

Bradley Smith
Chris Van Hollen
Steny Hoyer
Ben Cardin
Larry Hogan
Doug Peters
Rushern Baker
Fred Robinson
Bowie City Council Members

Peter & Edna Granahan
12102 Rustic Hill Drive
Bowie, Maryland 20715
301-262-9018
Cell 301-395-1624
pegranahan@verizon.net

August 20, 2017

SCMAGLEV Project, C/O John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Re: SC Maglev questions for the September 5, 2017 Bowie City Council meeting

Dear Mr. Trueschler:

- Questions:
 - **Who in Maryland would use this train between Washington, BWI & Baltimore?**
 - It would take too much time to travel to these stations to make it worthwhile for us to use SC MAGLEY, and the total travel time would be longer not shorter.
 - Better options and access are available now for Marylanders; Marc, Amtrak, Acela Express, Metro and improvements are planned.
 - I do not know anyone who would use SC MAGLEV.
 - **Is the protection of houses, businesses, religious centers, schools, communities, personal property, family lifestyles, recreational facilities, parks, green areas, family lifestyles and property values the top priority in selecting the route for this train, if it is going to be built?**
 - **If not, do not build it. The project has no true value to Maryland and potentially harms our state and its residents.**
 - **Why consider building this “TEST PROJECT” in Maryland and not in an area such as Philadelphia to New York where it should have more ridership?**
 - **Can we cancel this project and return any remaining funds from the \$27 million grant from the Federal Government, so it could be used for other needed purposes such as medical insurance or infrastructure repairs?**

- **What is the environmental (construction runoff) and financial resources impact of this train experiment on the Chesapeake Bay, removing the potentially disastrous backup of silt from the Conowingo Dam, restoring the Bay and the seafood industry in Maryland?**
 - The Chesapeake Bay should be a much higher priority for Maryland than this train.
- **Who is liable for the \$5 billion loan from the Bank of Japan and what are the terms of that loan?**
- **What is the expected cost of a trip from Washington to Baltimore?**
- **Why did most of us only hear about this proposed train 3 months after the 170 page Final Scoping Report was completed, with almost no participation from citizens?**
 - The Scoping process and report were fatally flawed, the report has no value, and the report should be discarded and scoping process done again after open honest disclosure of potential impacts of this project on citizens of Maryland.
 - We were not informed about this train project, meetings or the scoping process.
 - There were many ways to contact us if you wanted to: newspaper articles, newspaper inserts, mailing lists from state land records, religious institutions, community organizations, local government distributions, TV, radio announcements, handouts in the local communities, businesses, and etc.
- **What is the history and status of the SC Maglev proposals, projects, completions, financial successes, failures, cancelled proposals, cost overruns, projects bailouts by governments, medical concerns, legal concerns or any other problems or claims?**
 - This should also be part of the scoping report.
- **Have you released your Pro-forma evaluation / documents for this Washington, BWI and Baltimore project for review by independent third party analysts?**
 - If not, please release them as soon as possible to be able to review this project.
 - Also release the Pro-forma evaluation for the proposed plan for Washington to New York.

- **Who will; own the technology, manufacture the trains, Maglev operating components, operating systems, parts and jobs?**
 - What happened to build American, buy American and American jobs?
 - If this project expands in the future, who will design and build the trains, the operating components and parts?
 - Who would control this potentially major part of the United States transportation infrastructure, Japan or the United States?
 - Would the train system be compatible with other systems built in the US?
 - Could the US build high speed systems in other countries?

- **Who would pay for the more than \$100 Billion cost to build this system from Washington to New York?**
 - Japan? United States?
 - This rail system makes no sense if it does not travel long distances.
 - Is there any possibility this would be financially successful for the US?
NO!
 - The \$100 Billion cost estimate is probably very low. There is a very long building period with major tunnels and no revenue without trains operating.

- **Finally, why should we consider this test project in Maryland?**

Peter and Edna Granahan

Peter and Edna Granahan

Cc: President Donald Trump
David Henley, SCMAGLEV Project Director
Bradley Smith, Maryland Dept. of Transportation
Federal Railroad Administration
US Senator Chris Van Hollen
Congressman Steny Hoyer
US Senator Ben Cardin
Governor Larry Hogan
State Senator Doug Peters
County Executive Rushern Baker
Mayor Fred Robinson
Bowie City Council Members

July 27, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Msrs. Smith and Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, I am formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. I request to be included as a consulting party under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Even though the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within my community, no one in my community has been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. I have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. No one I have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor does it notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

Per the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. I was one of only 29 people to attend the open house at Arundel Middle School, and only 7

July 27, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 2 of 3

people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had my neighbors been notified about the meetings and that the community was in the cross hairs of this project, community members would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at the Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. ~~Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County).~~ In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie, Glenn Dale, Crofton, Odenton, or other affected communities.
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel.
- Post cards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Wilson Town, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City.

Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to attend). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."
- 40 CFR §1501.7, Scoping states that "There shall be an early **and open process** for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action."

July 27, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 3 of 3

- 40 CFR §1506.6, Public involvement states that “Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
- The Council on Environmental Quality 40 Most Asked Questions (Q13) states “The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. **But that is only the minimum requirement...**” and scoping must have “**appropriate public notice** and enough information available on the proposal so that the public and relevant agencies can participate effectively.”

In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks, and the destruction of our quality of life, I formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA can make informed decisions regarding the MAGLEV project. In addition, I request that I be included as a consulting party under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

I await your response to these requests.

Regards,



Ann Greenawalt
2720 Middle Neck Road
Odenton MD 21113

CC:

EPA Region 3
Advisory Council on Historic Preservation
Council on Environmental Quality
MD Historical Trust
Senator Chris Van Hollen
Senator Ben Cardin
Congressman John Sarbanes
Governor Larry Hogan

State Senator Jim Rosapepe
Delegate Joseline Peña-Melnyk
Delegate Ben Barnes
Delegate Barbara Frush
Anne Arundel County Council Member Andrew Pruski
Anne Arundel County Council Member Pete Smith
Anne Arundel County Executive Steve Schuh

July 25, 2017

Bradley Smith, Director of Office of Freight and Multimodalism
Maryland Department of Transportation
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Mr. Smith and Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of post cards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the post card shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

July 18, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 2 of 3

According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we or our neighbors been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel
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Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."

July 18, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 3 of 3

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- 40 CFR §1506.6, Public involvement states that “Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
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In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

We await your response to these requests.

Regards,



James and Coring Greig
4705 Redding Lane
Bowie, MD 20715

CC:

EPA Region 3
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Prince George's County Executive Rushern Baker
Prince George's County Councilman Todd Turner

Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)
Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Trouth (District 4)

August 30, 2017

SCMAGLEV Project
c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21076

Received
SEP - 5 2017
Office of the Secretary

Subject: SCMAGLEV Project

Dear Mr. Trueschler,

I live in Bowie, Maryland, and I did not receive timely information on the scoping meetings concerning the proposed SCMAGLEV project. The information I have gathered shows that the boundary of one of the proposed routes is in my front yard. Needless to say this is an alarming revelation. This proposed route would completely destroy my neighborhood in the Rockledge section of Bowie.

I have not been able to provide comments on the scoping document(s), the environmental impact statement, or even the document which would show the benefits of a 300 miles per hour train ride from Baltimore to Washington.

I am formally requesting that the scoping period for the SCMAGLEV project be reopened and that a more robust effort be undertaken to solicit input and comments from the affected communities and individuals.

Sincerely,



William A. Haddock
12225 Rolling Hill Lane
Bowie, MD 20715

PRELIMINARY ALTERNATIVES SCREENING REPORT

Director, Office of Freight and Multimodalism
Maryland Department of Transportation
7201 Corporate Center Drive
Post Office Box 548
Hanover, MD 21076

From
Baltimore-Washington SCMAGLEV Project
Dr. & Mrs. M. Haziq
1301 Jade Court
Odenton, MD 21113

SCMAGLEV Project
c/o Suhair Al Khatib
Maryland Transit Administration
6 Saint Paul Street
Baltimore MD 21202

Oct 7, '17

Office of Railroad Policy and Development
Federal Railroad Administration
1200 New Jersey Ave SE
Washington, DC 20590

Dear Bradley Smith and Suhair Al Khatib:

I am writing to formally request reopening of the scoping study for the environmental impact statement (EIS) for the Baltimore Washington Super Conducting Magnetic Levitation (SCMAGLEV) project.

Neither I nor my neighbors received any information about the project, how it might affect our neighborhood, or the public meetings held concerning the EIS scoping study. We were not given any meaningful opportunity to offer our comments on the scope of the EIS.

The outreach efforts of the project proponent and associated agencies—namely, sending out a few hundred postcards to addresses in Baltimore and Washington, DC (and none to residents living along the proposed alignments in Anne Arundel and Prince George's Counties) and allowing only a 30-day comment period, coinciding with the Christmas holiday season 2016—were woefully inadequate. In fact, this appears to have been a thinly disguised effort to suppress public participation, which must not be allowed to stand.

I am directly affected by the SCMAGLEV project, as my home lies adjacent to the WB&A bike trail, which coincides with one of the proposed alignments, indeed the most likely alignment for final selection. I am concerned about the impact SCMAGLEV would have on noise, the destruction of local greenspace, and the degradation of the bike trail, a much used and treasured community resource. The bike trail plays an integral role in promoting physical health and recreation, and it would suffer grievously by having the equivalent of a four-lane highway erected above it. I am concerned that this concrete monstrosity, having driven away joggers and bicyclists, will instead attract loitering teens and graffiti, adding further to the deterioration of the quality of life in our community.

An ample opportunity for public input into the EIS scoping study is needed to ensure that our concerns are fully taken into account, as required by the National Environmental Policy Act (NEPA). For instance, members of the affected public are strongly questioning the need for the proposed project. The ostensible objective of the project—shortening traveling time between Washington DC and Baltimore—could be achieved much more cost-effectively by upgrading existing rail infrastructure to accommodate increased speeds. This approach would also lessen risks that federal and/or Maryland taxpayers would end up bailing out an uneconomical new service with operating subsidies, as has been the experience with other maglev systems around the world.

Again, I formally request reopening of the scoping study for the EIS for the SCMAGLEV project. Thank for your consideration.

January 2018

Farima Haziq and Mohammed Haziq

Karen A. Holmes
730 Emerald Way
Odenton, MD 21113
karenholmes52a@gmail.com

September 15, 2017

Bradley M. Smith
Director, Office of Freight and Multimodalism
Maryland Department of Transportation
7201 Corporate Center Drive
Post Office Box 548
Hanover, MD 21076

SCMAGLEV Project
c/o Suhair Al Khatib
Maryland Transit Administration
6 Saint Paul Street
Baltimore MD 21202

Office of Railroad Policy and Development
Federal Railroad Administration
1200 New Jersey Ave SE
Washington, DC 20590

Dear Bradley Smith and Suhair Al Khatib:

I am writing to formally request reopening of the scoping study for the environmental impact statement (EIS) for the Baltimore Washington Super Conducting Magnetic Levitation (SCMAGLEV) project.

Neither I nor my neighbors received any information about the project, how it might affect our neighborhood, or the public meetings held concerning the EIS scoping study. We were not given any meaningful opportunity to offer our comments on the scope of the EIS.

The outreach efforts of the project proponent and associated agencies—namely, sending out a few hundred postcards to addresses in Baltimore and Washington, DC (and none to residents living along the proposed alignments in Anne Arundel and Prince George's Counties) and allowing only a 30-day comment period, coinciding with the Christmas holiday season 2016—were woefully inadequate. In fact, this appears to have been a thinly disguised effort to suppress public participation, which must not be allowed to stand.

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equivalent of a four-lane highway erected above it. I am concerned that this concrete monstrosity, having driven away joggers and bicyclists, will instead attract loitering teens and graffiti, adding further to the deterioration of the quality of life in our community.

An ample opportunity for public input into the EIS scoping study is needed to ensure that our concerns are fully taken into account, as required by the National Environmental Policy Act (NEPA). For instance, members of the affected public are strongly questioning the need for the proposed project. The ostensible objective of the project—shortening traveling time between Washington DC and Baltimore—could be achieved much more cost-effectively by upgrading existing rail infrastructure to accommodate increased speeds. This approach would also lessen risks that federal and/or Maryland taxpayers would end up bailing out an uneconomical new service with operating subsidies, as has been the experience with other maglev systems around the world.

Again, I formally request reopening of the scoping study for the EIS for the SCMAGLEV project. Thank for your consideration.

Yours truly,



Karen A. Holmes

cc: Governor Lawrence J. Hogan
US Senator Benjamin L. Cardin
US Senator Christopher Van Hollen, Jr.
US Representative John P. Sarbanes
State Senator James C. Rosapepe
Delegate Benjamin S. Barnes
Delegate Barbara A. Frush
Delegate Joseline A. Peña-Melnyk
Councilman Andrew Pruski

September 17, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mssrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,



Marymal L. Holmes, D.Mus.A
14706 Arabian Lane
Bowie, Maryland 20715

CC: Congressman Steny Hoyer
U.S Senator Ben Cardin
Governor Larry Hogan
County Executive Rushern Baker
Mayor Fred Robinson

September 13, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

Received
SEP 19 2017
Office of Planning

SCMAGLEV Project, c/o John G. Trueschler

Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Msrs. Smith and Trueschler,

We are formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community, and property, would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

We have a 3 year old son with development problems and another son on the way in two months. We want to provide the best life for our children and having this right in our backyard is not the answer. We will just have to move out of state if our opinion doesn't matter in this situation.

Regards,

Will and Heide Hostelley
1134 Thompson Avenue
Severn, MD. 21144
heidecch@gmail.com
443-790-7073

CC:

Governor Larry Hogan

100 State Circle
Annapolis, Maryland 21401
410-974-3901

State Senator Doug Peters

James Senate Office Building, Room 120
11 Bladen St., Annapolis, MD 21401
(410) 841-3631, (301) 858-3631
1-800-492-7122, ext. 3631 (toll free)
email: douglas.peters@senate.state.md.us
fax: (410) 841-3174, (301) 858-3174

Delegate Geraldine Valentino-Smith

House Office Building, Room 427
6 Bladen St., Annapolis, MD 21401
(410) 841-3101, (301) 858-3101
1-800-492-7122, ext. 3101 (toll free)
email: geraldine.valentino@house.state.md.us
fax: (410) 841-3850, (301) 858-3850

US Senator Chris Van Hollen

Washington, DC Office
110 Hart Senate Office Building
Washington, DC 20510
Phone (202) 224-4654
Fax: (202) 228-0629
TTY: (202) 224-1546

State Office

111 Rockville Pike
Suite 960
Rockville, MD 20850
Phone (301) 545-1500

US Senator Ben Cardin

Washington, DC Office
509 Hart Senate Office Building
Washington, DC 20510
Tel: (202) 224-4524
Fax: (202) 224-1651

Bowie Office
10201 Martin Luther King Jr.
Highway, Suite 210
Bowie, MD 20720
Tel: (301) 860-0414
Fax: (301) 860-0416

Congressman Steny Hoyer
Washington, DC Office
1705 Longworth House Office Building
Washington, D.C. 20515
Phone - (202) 225-4131
Fax - (202) 225-4300

Prince George's County Office
U.S. District Courthouse
6500 Cherrywood Lane, Suite 310
Greenbelt, MD 20770
Phone - (301) 474-0119
Fax - (301) 474-4697

ATTN: Bradley Smith
 Maryland Department of Transportation
 7201 Corporate Center Drive
 Hanover, MD 21076

September 17, 2017

SCMAGLEV Project
 ATTN: John G. Trueschler
 Maryland Transit Administration
 6 St Paul Street
 Baltimore, MD 21202

Received
 SEP 20 2017
 Office of Planning

Federal Railroad Administration
 Department of Transportation
 1200 New Jersey Avenue, SE
 Washington, DC 20590

TO WHOM IT MY CONCERN:
 RE: SCMAGLEV

I believe our community was left completely out of the loop on the Scoping Process, not just to keep us in the dark, but because of our lack of representation. The Maryland 4th Congressional district is one of the worst gerrymandered districts in the entire United States encompassing parts of Prince Georges County and a small part of Anne Arundel County. This district is carved out of the two counties. I live in the smaller section (Anne Arundel County) in the 4th Congressional district. I believe that is why we have had to get our information from the "grapevine" and not from any representatives. That is why I feel we need to reopen the Scoping Process and give the people affected a chance to input. That is the only fair way to do this.

I live in Four Seasons in Gambrills, very close to the Yellow Route. I have lived in Four Seasons for forty seven years. I raised my family here. Four years ago we moved from a 4 Bedroom Colonial in Four Seasons to a 3 bedroom rancher in Four Seasons to downsize to our senior living home with no steps. We thought we were set for our end times. Our daughters (and grandchildren) are located in Severna Park and Fairfax, VA so everyone was close and we loved this area. Imagine our shock to find out (from the grapevine) that Maryland Transportation Department wanted to bring a MAGLEV train just 1000 ft from my house. And on top of that the MAGLEV will not benefit our community (only elite rich who want to cut their travel time).

We live right next to the Dairy Farm (used to be Naval Academy Dairy Farm). It is farmed with organic crops. There are cattle and horses there. The 4-H is there to teach children about animal care. The Anne Arundel County extension office is there. They do a corn maze in the fall. How would a MAGLEV train affect the environment for the Dairy Farm and its animals?

Lastly, I am concerned about my property values. We are retired and don't want to move somewhere else. How will this affect our financial future?

Sincerely,



Louise H. Keister
 875 Frost Valley Lane
 Gambrills, MD 21054

cc: GOV Lawrence J. Hogan
 US Senator Benjamin Cardin
 US Senator Christopher Van Hollen, Jr
 US REP Anthony Brown

Timothy J. Kenney
1412 Amethyst Court
Odenton, MD 21113
timkenney1@gmail.com

October 10, 2017

Bradley M. Smith
Director, Office of Freight and Multimodalism
Maryland Department of Transportation
7201 Corporate Center Drive
Post Office Box 548
Hanover, MD 21076

SCMAGLEV Project
c/o Suhair Al Khatib
Maryland Transit Administration
6 Saint Paul Street
Baltimore MD 21202

Office of Railroad Policy and Development
Federal Railroad Administration
1200 New Jersey Ave SE
Washington, DC 20590

Dear Bradley Smith and Suhair Al Khatib:

I am writing to formally request reopening of the scoping study for the environmental impact statement (EIS) for the Baltimore Washington Super Conducting Magnetic Levitation (SCMAGLEV) project.

I have never received any information about the project, how it might affect our neighborhood, or the public meetings held concerning the EIS scoping study. I was not given any meaningful opportunity to offer our comments on the scope of the EIS.

In January, 2013, I took a job at NSA and moved to Odenton where my wife and I found a perfect place to retire after my wife retires from Rochester NY in 2019 to live with me. She still lives in Rochester, NY until she finished her Ph.D studies. Our condo is located only a few feet away from Washington, Baltimore and Annapolis Trail where I walk or cycle almost every day and appreciate the beauty of the woods. It would be a perfect retirement for us. Since I moved here, I have seen more and more people coming out to walk on the trail and every one looks so friendly and shows their smile face all the time. Note my wife and I are deaf and there are a lot of deaf people living in my area. Now I have trouble sleeping and wondering whether or not I should put up the condo for sale now. The value might be gone down by now because of the publication. We worked very hard for this retirement. We almost put our house in Rochester up for sale, but now we are holding it until I hear more from a meeting on October 14th at Arundel High School in Gambrills.

I don't think it's really necessary to have MAGLEV here since we have Amtrak that is fast enough and would save all of us and environment. MAGLEV has no benefits for us and Maryland and will get us suffer.

Again, I formally request reopening of the scoping study for the EIS for the SCMAGLEV project. Thank for your consideration.

Yours truly,

A handwritten signature in blue ink that reads "Timothy J. Kenney". The signature is fluid and cursive, with a long horizontal stroke extending to the right.

Timothy J. Kenney

cc: Governor Lawrence J. Hogan
US Senator Benjamin L. Cardin
US Senator Christopher Van Hollen, Jr.
US Representative John P. Sarbanes
State Senator James C. Rosapepe
Delegate Benjamin S. Barnes
Delegate Barbara A. Frush
Delegate Joseline A. Peña-Melnyk
Councilman Andrew Pruski

SCMAGLEV Project
C/O John G. Trueschler
Maryland Transit Administration
6 St. Paul Street
Baltimore, MD 21202

Received
SEP 19 2017
Office of Planning

RE: The Proposed MAGLEV between Baltimore and Washington DC

To John Trueschler,

I am writing to today to express my concern for our historical structures, for our environment and finally the administrative process that followed that can loosely be defined as "scoping".


On the planned routes lies vast stretches of mature vegetation containing thousands of animals. On the proposed route, lies many structures that could be defined as historical structures.

It is on most concern for me that a decision of this magnitude.--A decision affecting my family and countless families like mine took place without me being informed of the plan. It came as a surprise to myself and my neighbors that the scoping process had opened and closed without our knowledge during a holiday week in 2016. We were not given notice of the Scoping Meeting or an opportunity to comment. I believe that the timing and limited notification to the affected parties (people like myself) was an attempt to circumvent due process. I ask that the scoping period for SCMAGLEV EIS be re-opened, allowing for true due process to occur.

On a personal note, I cannot fully comprehend how a project whose price makes it destined for failure followed by decades of state and local subsidies all at the tax payer's expense would be considered in the first place. I question whether the devastation that this project would cause to the hard working families of Maryland directly in the line of MAGLEV and the families whose property values would be greatly diminished due to the proximity of their property(effectively eliminating the vast majority of their life savings) to the MAGLEV was considered. I question whether the property taxes caused by the reduction in property values was considered.

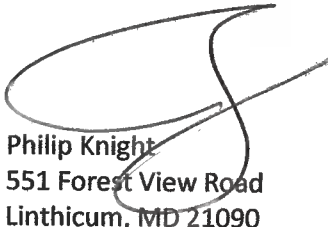
I question why a project with no redeemable financial incentive whose building would do immeasurable harm would even be considered.

Thank you for your consideration.


Greg Klingler
737 Seneca Drive
Odenton, MD 21113

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

We were recently made aware of the SCMAGLEV Project, and its proposed route to go through the Linthicum Heights neighborhood. This is very disturbing to not only my family, but also the numerous families in and around the Linthicum Heights area. At no point have any of us been officially notified of this project nor had an opportunity to voice our concerns or get hard facts. I could find some information on the internet about the project, but no real facts. For example, there are numerous routes shown going through the Linthicum Heights area but there is no information on which route has been chosen. Several of the routes are shown to run straight through the Lindale Middle School property and others through the Linthicum Ferndale Youth fields. Is the intent to relocate these facilities? What about the numerous houses in Linthicum that would have to be demolished to install this system. What has been put in place to notify these families and how will they be compensated for the loss of their property investment? On that same note what about the other residents who would remain with the rail obstruction running past their homes and through their remaining neighborhood. How would they be reimbursed for their loss of property value? None of the residents of Linthicum Heights have been made aware of any environmental study which has been done to determine the physical, ecological, and fiscal impact this project would have on this community. When will this study be completed and when will the residents be given the opportunity to review the report and comment? What about the historical significance of Linthicum Heights to the state of Maryland, has that been taken in consideration? Additionally, there has been rumors that the SCMAGLEV project could possibly tunnel underneath Linthicum Heights. Again, has a study been done and when will the residents of Linthicum Heights be made aware of the findings? Will the project or State do a structural assessment of all the homes in the area prior to commencement of this construction? Will continuous monitoring be done of the homes possibly affected during this operation? Will this type of construction have the possibility of damage to the properties or sink holes? If so what procedures will the project or the State put in place to ensure this doesn't happen and if it does, what compensation would the resident receive if this unfortunate event would take place? Have any of the existing rail systems been explored to see if their use is a more feasible option? Has the State looked to any other routings that don't drastically affect our community or other communities? Is there the possibility of radiation or any other health effects that possibly could occur due to this system running through our neighborhood? As you can see there are hundreds of questions that remain unanswered and a historical community in shock and uninformed about the project. We, the residents of Linthicum Heights polity ask you as our elected official, to stop this project from moving forward until all the residents of Linthicum are made fully aware of its consequences to our community.



Philip Knight
551 Forest View Road
Linthicum, MD 21090

Angela Korwek
1503 Sapphire Court
Odenton, MD 21113
LaKorwek@aol.com

October 5, 2017

Bradley M. Smith
Director, Office of Freight and Multimodalism
Maryland Department of Transportation
7201 Corporate Center Drive
Post Office Box 548
Hanover, MD 21076

SCMAGLEV Project
c/o Suhair Al Khatib
Maryland Transit Administration
6 Saint Paul Street
Baltimore MD 21202

Office of Railroad Policy and Development
Federal Railroad Administration
1200 New Jersey Ave SE
Washington, DC 20590

Dear Bradley Smith and Suhair Al Khatib:

I am writing to formally request reopening of the scoping study for the environmental impact statement (EIS) for the Baltimore Washington Super Conducting Magnetic Levitation (SCMAGLEV) project.

None of my neighbors or I received any information about this project, how it might affect our neighborhood, or the public meetings held concerning the EIS scoping study. We were not given any meaningful opportunity to offer our comments on the scope of the EIS.

The outreach efforts of the project proponent and associated agencies—namely, sending out a few hundred postcards to addresses in Baltimore and Washington, DC (and none to residents living along the proposed alignments in Anne Arundel and Prince George's Counties) and allowing only a 30-day comment period, coinciding with the Christmas holiday season 2016—were woefully inadequate. In fact, this appears to have been a thinly disguised effort to suppress public participation, which must not be allowed to stand.

My husband and I are directly affected by the SCMAGLEV project, as our home lies adjacent to the WB&A bike trail, which coincides with one of the proposed alignments. I am concerned about the impact SCMAGLEV would have on noise, the destruction of local greenspace, and the degradation of the bike trail, a much used and treasured community resource. The bike trail plays an integral role in promoting physical health and recreation, and it would suffer grievously by having the equivalent of a four-lane highway erected above it.

An ample opportunity for public input into the EIS scoping study is needed to ensure that our concerns are fully considered, as required by the National Environmental Policy Act (NEPA). For instance, members of the affected public are strongly questioning the need for the proposed project. The ostensible objective of the project—shortening traveling time between Washington DC and Baltimore—could be achieved much more cost-effectively by upgrading existing rail infrastructure to accommodate increased speeds. This approach would also lessen risks that federal and/or Maryland taxpayers would end up bailing out an uneconomical new service with operating subsidies, as has been the experience with other maglev systems around the world.

Again, I formally request reopening of the scoping study for the EIS for the SCMAGLEV project. Thank for your consideration.

Yours truly,



Angela D. Korwek

cc: Governor Lawrence J. Hogan
US Senator Benjamin L. Cardin
US Senator Christopher Van Hollen, Jr.
US Representative John P. Sarbanes
State Senator James C. Rosapepe
Delegate Benjamin S. Barnes
Delegate Barbara A. Frush
Delegate Joseline A. Peña-Melnyk
Councilman Andrew Pruski
Delegate Michael Malone

John A. Lawson
12514 Rambling Lane
Bowie, Maryland 20715
301-395-2407
August 15, 2017

Mr. Bradley Smith
MD Department of Transportation
Director, Office of Freight and Multimodalism
7201 Corporate Center Drive
Hanover, MD 21076

Subject: SCMagLeV Project, AKA Super Conductivity Magnetic Levitation Train Project Is Projected to Destroy Our House in the Rockledge R Section of Bowie, Maryland Including Another 200 to 300 Homes in the R and W Sections of Bowie Plus Thousands of Homes and Businesses in Anne Arundel, Prince George's County, and Baltimore County

Dear Mr. Smith:

Please reopen the scoping period for the environmental impact statement for the SCMagLeV train project. Before two months ago, no one in the R and W sections of Bowie, those sections that will be demolished by the construction of SCMagLeV, knew anything about this planned SCMagLeV project. I also know from our meetings that other communities in Prince George's, Anne Arundel and Baltimore counties knew nothing about this SCMagLeV project. Meanwhile, the scoping and primary alternatives portions of the environmental impact statement were completed in 2016 and the beginning of 2017. Whatever outreach there was to residents, cities and counties was impoverished and generally abysmal since no one here in Bowie knew anything about any of this. The environmental impact on the residents of Bowie is massive and depressing and has no mention in the scoping or primary alternatives portions of the environmental impact statement because no opportunity was given to the residents of Bowie to allow them to state their grievances with this SCMagLeV project which will lay waste to their homes, businesses and lives.

I learned recently that the SCMagLeV Project, AKA Super Conductivity Magnetic Levitation Train Project, will begin construction in 2019. I learned this from a news article in the Bowie Blade newspaper, dated Thursday, August 3, 2017, in which it was stated that Governor Larry Hogan came to Bowie and met with the mayor and the city council and listened to the concerns expressed by them about the SCMagLeV Project. The same article stated the construction start date is 2019.

More than mere concerns are looming about the catastrophic consequences to us in Bowie who know about the SCMagLeV and very, very few people know anything about the dreaded SCMagLeV, because outreach to the 200 or 300 homes in the Bowie R and W sections that will be affected was never made. From the environmental impact statement on the SCMagLeV website, the most favored route, colored yellow, is Alternative G (WBA – 2017) on the Screening Level 1 Summary. Screening Level 2 Preliminary Initiatives Alternatives map, also colored yellow, runs directly through the R (Rockledge) and W sections of Bowie where my house is located along with 200 to 300

Stop the SCMagLev Train

August 15, 2017

Page 2

other houses that will be demolished for the SCMagLev. Here are the Summary and Alternatives pages:

Screening Level 1 Summary

Alternative Name	Screening Level 1									
	Is a viable alternative with the Generalist?	Does not require the use of 'Clear Right-of-Way'?	Significant Impacts from Land Use?	Designated (Urban?)	Meets local needs?	Significant Impacts to Airport Facilities?	Significant Impacts to Airport Runways?	Environmental Impacts	Construction	Cost/Value
No Build Alternative	N/A	Yes	N/A	N/A	N/A	Low	Low	Person	Established baseline	
Interoperable System Improvements	N/A	Yes	Low	No	No	Low	Low	Do not retain	Does not meet project description/travel time	
Alternative A (B-95 - 2003)	No		Medium	Yes	No	Medium	High	Do not retain	Does not meet project description/travel time	
Alternative B (B&A Parkway - 2003)	No		Medium	Yes	No	Medium	High	Do not retain	Does not meet project description/travel time	
Alternative C (Amtrak - 2003)	No		High	Yes	No	Low	Medium	Do not retain	Does not meet project description/travel time	
Alternative D (Baltimore - 2003)	No	No	Medium	Yes	No	Low	Medium	Do not retain	Does not meet project description/travel time	
Alternative E (Amtrak Corridor - 2017)	Yes	Yes	High	Yes	Yes	Medium	Medium	Person	Constructability is a major issue due to interferences with Amtrak operations	
Alternative F (B&P - 2017)	Yes	Yes	Medium	Yes	Yes	Medium	High	Person	Post Service impacts	
Alternative G (WB&A - 2017)	Yes	Yes	Low	Yes	Yes	Medium	Medium	Person	Urban form not corridor	

Note: High = negative, Medium = neutral, Low = positive
 *Project description
 - Construction duration

Screening Level 2 Preliminary Initial Alternatives

- No Build
- Alternative E (Amtrak Corridor)
- Alternative F (BW Parkway)
- Alternative G (WB&A)
- Alternative H (WB&A/Amtrak Hybrid)
- Alternative I (Amtrak/WB&A Hybrid)
- Alternative J (BW Parkway Modified)

BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT
 Preliminary Alternative Assignments and Station Zones

You can imagine that those Bowie residents who know (there are very few) about this impending catastrophe to their lives are completely miserable. This is stark stuff.

Stop the SCMagLev Train

August 15, 2017

Page 3

What is making Bowie residents unhappy and miserable?

1. **SCMagLev will destroy our homes.** If this SCMagLev train is built through Bowie, my house and 200 or 300 houses in the R and W sections will be demolished.
2. **No one knew.** Bowie residents did not know about the SCMagLev project. No outreach was made to them. We are beginning to contact residents and making them aware. There will be fervent outcries and vigorous opposition to the SCMagLev disaster project by not only Bowie residents, but also all those in surrounding residential and business locales that are projected to have their properties and lives demolished. Residents and business owners in Bowie, Glen Arden, Piney Orchard and all others in the path of SCMagLev in Baltimore, Anne Arundel and Prince George's counties, once they learn of what is happening, will be stridently opposed to its construction.
3. **No real estate transactions possible.** Now that we know, there can be no real estate buying or selling, because to sell or buy a house full disclosure must be made and with this SCMagLev project that is pending, no one who is sane is going to buy a house in this section of Bowie. Also, those homes near the project route are affected too, because who wants a house next to or near a train track especially a MagLev train track. Additionally, investors will seek to purchase our houses at a ridiculously low price to make a profit when government comes through with eminent domain offers.
4. **Eminent domain will make residents poor.** If the project is approved to begin in 2019 as projected, then government will offer eminent domain prices for our houses which I can imagine will not be at market value. Even given market value for the houses, this is a forced selling of these homes and nothing is considered about the living plans of the residents. For example, my spouse and I put a lot of money into our house to have it as our retirement home. Within the last 5 years alone, we installed a 50-year roof, a new water heater, a new furnace and air conditioner. Ten years ago, we did a total renovation of the house at a cost of \$200,000. We can well itemize these costs. In addition, we paid off our \$210,000 mortgage in the last ten years. We are ready to have a pleasant retirement in Bowie, which is a lovely place to live. But now, the SCMagLev disaster is upon us and we will become poor, because we will have to accept the eminent domain offer, incur moving costs, find a new place to live most likely other than Maryland (my spouse moved to Maryland in 1967), get another mortgage of about \$150,000 (outrageous!), and live at a level much below the standard of living we have now. My spouse is 67 and I will be 70 in September. Even with another mortgage, which is ridiculous at our ages, we will survive this catastrophe, but there are residents of Bowie and other counties that, I'm sure, will not survive this hit on their lives. Instead of promoting the general welfare (preamble, Constitution

Stop the SCMagLev Train

August 15, 2017

Page 4

of the United States) of the Bowie residents, this move will impoverish current Bowie residents. Absolutely awful!

Some general comments about SCMagLev project:

- 1. Is SCMagLev needed?** Why is it necessary to have a new high-speed train for the Washington-BWI-Baltimore route? I note on the Acela schedule that the current travel time from Washington to Baltimore for the Acela is 37 minutes. I understand that an upgraded version of the Acela will be ready soon. So why is the SCMagLev needed? In addition, this SCMagLev train is going only from Washington DC to Baltimore and the Acela goes all the way to New York. The SCMagLev train is cutting off maybe only 15 minutes of travel time from Washington DC to Baltimore. Shaving off a few minutes and traveling only locally. Strange stuff. Why is it needed? Looks like it isn't needed. And, good golly, is taking a little more travel time to get from Washington DC to Baltimore so terrible. If this is being done in the name of progress, that is the wrong label for this SCMagLev project. Instead this SCMagLev project is destructive and no tangible benefit for any of the communities and governments along its route will be derived. If anything, the state of Maryland will end up in debt because of the SCMagLev project and show nothing for its investment. Quite amazing!
- 2. Will local governments and communities derive benefits from SCMagLev?** Will Bowie, Glen Arden, Piney Orchard and all the other communities and businesses in the path of the SCMagLev derive any benefit from the destruction of our homes and businesses to construct SCMagLev? I don't know, since I'm not an insider, but I doubt that any benefit monetary or otherwise will accrue to local government and communities through which SCMagLev will plow. In fact, local governments and county governments will be losing a significant tax base because of the destruction of homes and businesses in the path of SCMagLev. Crazy stuff!
- 3. Why is the construction of this SCMagLev train going through excessively populated home and business areas?** Thousands of homes and businesses are on the construction route of the SCMagLev train. This is wrong. If it has to be built, and that is doubtful, then pick a route that is free of homes and businesses, maybe along Interstate 95 or along the Baltimore-Washington Parkway, but not crashing through peoples' lives, homes and businesses.

In summary, there are more than concerns that the residents of Baltimore, Anne Arundel and Prince George's counties are facing. They and us in the R & W sections of Bowie, through which the SCMagLev train will be constructed, are facing a permanent destruction of our businesses, homes and lives. As this destructive gamut builds to construct a SCMagLev train right through the heart of Bowie, Maryland, I will continue to write, email, call and contact project managers, government officials, news

Stop the SCMagLev Train

August 15, 2017


Page 5

organizations and social media to oppose this SCMagLev train. We in Bowie seek the "No – Build Alternative" decision evident on the Screening Level 1 Summary of the environmental impact statement which will conclude this initiative with no construction of the SCMagLev train through the very heart and life of Bowie, Maryland.

I respectfully request that you take this matter to heart and help us, Bowie residents, find a way to stop this SC MagLev train. Thank you.

Please contact me for any additional information you require. My cell number is 301-395-2407 and the home number is 301-464-9473.

Sincerely,

A handwritten signature in black ink, appearing to read "John A. Lawson". The signature is fluid and cursive, with the first name "John" being particularly prominent.

John A. Lawson
12514 Rambling Lane
Bowie, Maryland 20715
301-395-2407 or 301-464-9473

September 26, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mr. Smith and Mr. Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,

Gene and Leigh Lovelace
1240 Colonial Park Drive
Severn, MD 21144

CC:

Lawrence Hogan
Christopher Van Hollen, Jr.
Benjamin Cardin
John Sarbanes
James Rosapepe
Barbara Frush
Andrew Pruski
Benjamin Barnes
Joseline Pena-Melnyk
Julie Hummer
Stacy Korbela

September 25, 2017

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

Dear officer of the Office of Railroad Policy & Administration,

I am writing you in your capacity as representative for the Federal Railroad Administration for the Baltimore-D.C. MAGLEV project.

My family's home, our neighborhood, and my place of work will be greatly and adversely impacted by the project's construction and operation if either of two proposed routes taking it near/through Odenton are selected. Furthermore, it will not accomplish any great social good in exchange for serious disruption to affected communities and opportunity cost to taxpayers. I would think that upgrades to the existing Amtrak capacity would be simpler and less costly and more likely to improve corridor movement.

As I understand things, the project should have been raised to all potential stakeholders for input, discussion, and resolution of concerns before any decisions were made to proceed. I would have expected very substantial marketing of learning and input opportunities and I would have expected very significant opportunity to express my concerns and to have received substantive and considered response.

Since neither I, nor any of my neighbors, were aware of the proposed project until quite recently I conclude efforts to engage us per the intent of the scoping process were weak at best.

I formally request that the scoping period for the SCMAGLEV EIS be reopened.

I also request that your office notify me in writing of the proper procedures I should follow in order to ask my specific questions and to receive written response either as part of a reopened scoping process or separately from that process.

My address is 527 Maple Ridge Ln, Odenton, MD 21113.

Sincerely,


Beverly I. Maliner

Joseph M. Gorman
1200 Winer Rd
Odenton, MD 21113

October 10, 2017

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Ave. SE
Washington, DC 20590

I am writing to you to oppose the SCMAGLEV Project. The Maryland Department of Transportation has implemented the NEPA study with a lack of transparency and proper notification. Attached is the Maple Ridge Home Owner's Association resolution to oppose this project. Thank you for your attention to this subject.

Thank You,
Joe Gorman

RESOLUTION OF THE MAPLE RIDGE HOMEOWNERS' ASSOCIATION, INC.

WHEREAS, in accordance with the Constitution and By-Laws of The Maple Ridge Homeowners' Association (MRHOA), particularly ARTICLE VI, the following Resolution has been decided:

The routing of any proposed Magnetic Levitation Rail Line ("MAGLEV") through any of the common property of Maple Ridge or through any of the routes that are located close to the property, such as the yellow and red routes, would be devastating to the community for numerous reasons, including, but not limited to: the quiet enjoyment of the residents in their home, neighborhood, and wider community; grossly altered traffic flows close to Maple Ridge which would make getting into and out of the community and the City of Odenton difficult; substantial decrease in property values and perhaps a near total loss to some residents; increase in taxes throughout Maryland; health hazards, known and unknown, from the building and operation of the MAGLEV; destruction of nearby historic buildings lessening the enjoyment and heritage of the City of Odenton, interference with longstanding growth plans of the City of Odenton, and no benefit accruing to the Maple Ridge community or to Odenton.

Accordingly, the MRHOA, acting on behalf of the Maple Ridge community, resolves that the President of the MRHOA, or any other person he may delegate on the board or in the community, may represent to any interested party—Federal, State, County, or other officials, organizations, agencies, associations, or individuals—that the MRHOA opposes such routing and project. Furthermore, that neither the MRHOA or individual residents who are impacted by the proposed MAGLEV project were properly notified of the scoping meetings and therefore were never afforded an opportunity to learn about or voice objections to the project, nor, at any time, offered the ability to comment on the scope of the Environmental Impact Study. Furthermore, the whole process was flawed from the beginning because alternative transportation options were never considered, the only question raised was where the MAGLEV would best be located. It is the position of MRHOA that the scope of any such study which fails to look at alternative transportation options and/or fails to register the impact on the Maple Ridge community and the City of Odenton is fundamentally flawed and deficient, and therefore, MRHOA is hereby declaring that it is opposed to the MAGLEV project and any and all proposed MAGLEV routes which would impact our community, directly or indirectly.

FURTHER RESOLVED: The President and other officers of MRHOA and are hereby authorized and directed to take such additional actions, execute and deliver such additional instruments, and make such further determinations as may be necessary or appropriate to carry into effect the foregoing resolution.

ATTEST:

Joseph Gorman
Joseph Gorman, President

The Maple Ridge Homeowners' Association, Inc.

date 10-2-2017

Juli Mai

date 10/2/17

July 31, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Dear Mssrs. Smith and Trueschler,

As there has been grossly insufficient public notice about the MAGLEV project, we are formally requesting that the scoping process be reopened, that appropriate public notice be sent to all citizens along all of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of environmental studies and impact analyses.

In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act. We request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy green space, parks, and historic sites within our community, we nor anyone in our community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. We have spoken to residents in communities along the existing Amtrak line and along the WB&A trail. We have spoken to stakeholders such as the Washington Area Bicycle Association. We have spoken to small business owners, some of whom are of minority ethnicity, along the proposed lines and they had no idea about the impact of the Maglev project on their businesses. No one we have spoken with has been provided any notice of the project or the public meetings. Rather than hearing from MTA, we learned about the MAGLEV project from a neighbor who learned about it from a friend. Citizen word of mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely send thousands of postcards to all affected citizens prior to scoping and alternatives meetings. MTA has failed to adequately notify citizens in this manner.

In addition, the copy of the postcard shown on the MAGLEV website makes no mention of Section 106 of the National Historic Preservation Act nor notify citizens that they will be able to comment on historic resource issues as part of the scoping process.

July 31, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act

Page 2 of 3

According to the May 2017 scoping report (issued 6 months after the meetings and AFTER alternatives meetings were held), a cumulative total of 117 people attended the 5 scoping meetings that were held. Only 7 people attended the one meeting held in Prince George's County. This poor attendance for a project that will affect literally thousands of residents is proof that outreach for these meetings was utterly insufficient. Had we, or our neighbors, been notified about the meetings and that our community was in the cross hairs of this project, we would have been present and very vocal. Instead, the meetings were held without adequate notice during the holiday season resulting in MTA failing to hear the issues and concerns of the affected public.

The sparse attendance at Prince George's County public meeting is further evidence that low-income and minority populations located along the rail lines affected have been particularly excluded from the NEPA/EIS process.

Prince George's County has a whole has been largely ignored in the MAGLEV NEPA/EIS process. The May 2017 scoping report, pages 20 and 21, lists the agencies that were invited to participate in the project and invited to the agency scoping meeting. Not a single Prince George's County agency is included (though the Howard County Planning Department was included, but the project does not affect Howard County). In addition, there is no mention of the Prince George's County Council, local city councils or departments, or other stakeholder groups that must be given an adequate voice in the project.

- Flyer locations listed in the May 2017 scoping report do not include any locations in Bowie or Glen Dale, Crofton, Severn, Odenton, or other affected communities
- No notices were placed in newspapers serving much of the project area including the Washington Post, the Bowie Blade, the Capital Gazette, or the Sentinel
- Postcards were not distributed to communities directly affected by the proposed action including, but not limited to, Old Bowie, Rockledge, Northridge, Saddlebrook, Saddlebrook West, Two Rivers, Piney Orchard, Jason's Landing, Andorick Acres, or Pioneer City.

Close to 200 people attended the Bowie City Council meeting on July 10th to hear from MTA (a meeting MTA failed to show up to). The public outcry demonstrated at this meeting is further evidence that the public's concerns have not been heard by MTA. The overriding sentiment at this meeting was that citizens vehemently oppose the MAGLEV project and that the citizens of our communities have not received notices about this project from MTA nor been provided the opportunity to comment on the scope of the alternatives and the scope of the Environmental Impact Statement being prepared.

Public involvement is the cornerstone of the National Environmental Policy Act, and MTA has completely failed to provide notice of this project and the opportunities to comment on the scope of the project. Specifically:

- 40 CFR §1500.2, Policy (d) states that agencies must: "**Encourage and facilitate public involvement in decisions** which affect the quality of the human environment."

July 31, 2017

SCMAGLEV – Compliance with the National Environmental Policy Act and Section 106 of the National Historic Preservation Act


Page 3 of 3

- 40 CFR §1501.7, Scoping states that “There shall be an early **and open process** for determining the scope of issues to be addressed and for identifying the significant issues related to a proposed action.”
- 40 CFR §1506.6, Public involvement states that “Agencies shall: (a) Make **diligent efforts to involve the public** in preparing and implementing their NEPA procedures. (b) **Provide public notice of NEPA-related hearings, public meetings, and the availability of environmental documents** so as to inform those persons and agencies who may be interested or affected.
- The Council on Environmental Quality 40 Most Asked Questions (Q13) states “The regulations state that the scoping process is to be preceded by a Notice of Intent (NOI) to prepare an EIS. **But that is only the minimum requirement...**” and scoping must have “**appropriate public notice** and enough information available on the proposal so that the public and relevant agencies can participate effectively.”

In closing, for your failure to properly notify the public that will be the most affected by this action, resulting in loss of homes, greenspace, and parks; and the destruction of our quality of life, we formally request the FRA and MTA reopen the public scoping process to allow for public comment as afforded under the National Environmental Policy Act, so that FRA and MTA are able to make informed decisions regarding the MAGLEV project. In addition, we request that we be included as consulting parties under Section 106 and be informed of all Section 106 activities and meetings associated with this project.

We await your response to these requests.

Regards,


John and Kathleen McGinnis
12105 Raritan Lane
Bowie, MD 20715

CC:

EPA Region 3
Advisory Council on Historic Preservation
Council on Environmental Quality
MD Historical Trust
US Senator Chris Van Hollen
US Senator Ben Cardin
Congressman Steny Hoyer
Governor Larry Hogan
State Senator Doug Peters
Delegate Geraldine Valentino-Smith
Prince George's County Executive Rushern Baker

Turner
Bowie City Mayor Fred Robinson
Bowie Councilman James Marcos (At Large)
Bowie Councilman Henri Gardner (At Large)
Bowie Councilman Michael Estève (District 1)
Bowie Councilwoman Diane Polangin (District 2)
Bowie Councilwoman Courtney Glass (District 3)
Bowie Councilman Ike Truth (District 4)

Prince George's County Councilman Todd

September 20, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Re-Open the Scoping Period

Dear Mr. Smith and Mr. Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,



Rebecca McHugh
1282 Ava Road
Severn, MD 21144

CC:

Delegate Mark Chang
Representative John Sarbanes
Senator Chris Van Hollen
Senator Ben Cardin

Federal Railroad Administration
Department of Transportation
1200 New Jersey Avenue, SE
Washington, DC 20590

Maryland Department of Transportation
7201 Corporate Center Drive
Hanover, Maryland 21076

To Whom It May Concern:

On November 25, 2016, the Federal Railroad Administration published a Notice of Intent (NOI) to prepare an EIS for the Baltimore-Washington Superconducting Magnetic Levitation (SCMAGLEV) Project. This notice provided for thirty day period to provide comments in writing and four public meetings. The NOI's Purpose and Need statement was to "increase capacity, reduce travel time, and improve both reliability and mobility options between Baltimore and Washington."

In May of 2017, a Final Scoping Report was completed to capture the results of the initial scoping effort. The document states that 57 comments were submitted at public meetings, 16 comments were submitted via project email, and two comments were submitted via mail for a total of 75 comments.

The proposed project is multi-billion infrastructure development that initially identified six possible routes that transect numerous private lands in multiple jurisdictions. Despite repeated efforts to engage the project proponent, we have been unable to garner additional information on the exact proposed routes and even documents justifying the purpose and need of the project. In fact, the project manager indicated that they identified the proposed routes through computer simulations and had not even walked area under consideration. Now the website shows two possible routes without any explanation as to the narrowing of options or engagement with impacted communities.

Many of us would agree that MAGLEV technology offers benefits in terms of reduction harmful emissions and the current traffic congestion plaguing our region. However, a project of this magnitude deserved more public engagement than 30 day during the holiday season. Moreover, there needs to be more discussion regarding the purpose and need of this project given existing infrastructure rail needs along the Northeast Corridor and within the DC Metro area.

We respectfully request that you work with the project proponents to **re-open the Scoping Process and provide for more meaningful engagement with landowners**, local governments, elected officials, and federal partners before moving forward with the selection and analysis of alternative to consider in the full Environmental Impact Statement. Further, we would appreciate more direct engagement and information related to the purpose and need for the proposed project.

While we appreciate that the Scoping process is the first in a series of opportunities to engage, it is critical for landowners and communities who are potentially impacted by the review of routes impacting

their lands. Other major infrastructure projects in the DC Metro area have indicated that the failure to productively engage communities and landowners results in a process mired in litigation. As landowners, community members, and Maryland taxpayers, we don't feel that this scoping process was adequate.

Thank you, in advance, for your consideration of this request and we look forward to your response.

Sincerely



Citizens Against the SCMAGLEV (CATS) in Bowie
Keep Our Community Beautiful
(8306 Laurel Bowie Road
Bowie, MD 20715)

Cc: Maryland Congressional Delegation
Prince Georges County Elected Officials
Council on Environmental Quality
Advisory Council on Historic Preservation

Placement of a structure such as this thru a residential neighborhood would cause property values to plummet for landowners whose life savings is in their homes.

August 27, 2017
1338 Chapelview Drive
Odenton, MD 21113

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mr. Smith,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,

LeslieAnn Nataro

LeslieAnn Nataro
leslieann427@msn.com

CC: US Senator Benjamin Cardin
US Senator Chris Van Hollen, Jr.
US Representative John Sarbanes
MD Senator James E. DeGrange, Sr.
MD Delegate Pamela G. Beidle
MD Delegate Mark S. Chang
MD Delegate Theodore J. Sophocleus

826 Knob Court
Odenton, MD 21113
jnauroth@gmail.com
August 12, 2017

Maryland Department of Transportation
Bradley Smith
Director of Office of Freight and Multimodalism
7201 Corporate Center Drive
Hanover MD 21076

Dear Mr. Smith,

I am writing in protest of the SCMaglev train that is proposed to link Washington DC with New York City. The negative effects on the communities and environment that it passes through will be significant. As a 15 year resident and homeowner in Odenton MD, a town that may be in the path of this train, my family and I are strongly opposed to the development of the SCMaglev in Odenton and anywhere in MD. The tracks, which are 46 feet wide and elevated would pass through predominantly developed areas with housing, schools, parks and industry.

Maryland is a highly developed state and no matter the path of this train, some community will be affected and the environment impacted. We do not feel that decreasing travel time between Washington DC and NYC is justification for the expense and negative effects that the SCMaglev will bring. Odenton is already home to the MARC/Amtrak train, which can be seen and heard running through our community daily. Hikers and bikers on the BW "nature" path in Odenton can see and hear the trains running today. Please stop the development of this train in Odenton and Maryland. Money is better served improving roads, schools, and saving the limited natural areas that we have for our families.

Sincerely,



Dr. Julie M. Nauroth, Ph.D.

August 29, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight & Multi-modalism
7201 Corporate Center Drive
Hanover, MD 21076

Dear Mr. Smith,

As there have been grossly insufficient public notice about the MAGLEV Project, I am formally requesting that the scoping process be re-opened, that appropriate public notice be sent to all citizens along ALL of the potential alternative alignments, and that citizens, as required under the National Environmental Policy Act, be afforded the opportunity to comment on the purpose and need for this project, the alternatives to be considered, and the scope of environmental studies and impact analysis.


In addition, the FRA and MTA have failed to publicize citizens' rights to participate under Section 106 of the National Historic Preservation Act.

Despite the fact that the MAGLEV project will require the demolition of homes and destroy greenspace, parks and historic sites within our communities, neither I nor anyone in my community have been notified in any manner of the proposed project or the scoping and alternatives meetings that have taken place. Seemingly, no one has been provided any notice of the project or the public meetings. Rather than hearing from MTA, I learned about the project through an online community forum. Citizen word-of-mouth has been the only outreach on this project.

Other agencies including the Maryland State Highway Administration routinely sends thousands of post-cards to all affected citizens prior to scoping and alternatives meetings. The MTA has failed to adequately notify citizens in this manner.

For your failure to notify the public that will be most affected by this action resulting in loss of homes, greenspace & parks, and the destruction of our quality of life, I formally request the FRA & MTA re-open the public scoping process to allow for public comment as afforded under NEPA, so that the FRA & MTA are able to make informed decisions regarding the MAGLEV. In addition, under Section 106, I request to be informed of all activities and meetings associated with this project.

Regards,


Caroline Pancotti, RN
520 Dogwood Road
Linthicum, MD 21090

CC:

US Senator Chris Van Hollen
US Senator Ben Cardin

August 8, 2017

To Bradley Smith,

This letter is to request the scoping process for the Maglev project be reopened. There has been grossly insufficient public notice to all citizens along the potential routes. According to the National Environmental Policy Act we should be given the opportunity to comment on the purpose and need for this project, the alternatives, and the outcome of the environmental studies and impact analyses.

I live in Bowie and my house would be subject to be destroyed if the "yellow/ WB&A trail" path is chosen. I only found out about this train potential reading the Bowie Blade 2 weeks ago. I did attend the meeting at Bowie town hall on August 3rd. There supposedly been prior meetings but I was never informed.

My concerns are not only to the Bowie residents who stand to lose their homes but to the remaining residents. The demolition of the 263 houses required in Rockledge section would require the expensive remediation of asbestos or expose remaining neighbors and school children to asbestos.

I am also concerned that if these houses were to be destroyed and the city of Bowie would lose this yearly tax revenue, it would no doubt have a critical impact on the city over each year in the future.

Then there is the loss of property value from now on. As this Maglev project potentially occurring in Bowie, property owners are already having difficulty selling their homes because they must disclose this potential. We are forced to wait until a decision has been made for which line is chosen or take a major loss of value in our homes. Then, if this Bowie line is chosen, we have to wait for the eventual payout resulting from eminent domain. A no win situation for the Rockledge of Bowie homeowners.

Prince Georges County has nothing to gain from the Maglev project. The Prince Georges county residents have no access to ride this train in Prince Georges County. We will only be bothered by the demolishing process, the building process and the noise and electromagnetic field it will pose to the remaining residents of Rockledge and school children of Rockledge Elementary school.

Again, I request the scoping process for the Maglev project be reopened, and to have more information in the mail, by phone and at the city meetings. I look forward to your response to this request.

Thank you,



Kathryn Paolucci

4808 Reston Lane

Bowie, MD 20715

(301)404-0113

SEP 14 2017

Office of Planning

September 11, 2017

Maryland Department of Transportation (MDOT)
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Misters Smith and Trueschler,

I was surprised to learn recently from my HOA, the Colonial Park Homeowners Association, about this project impacting our community. I was further disappointed to learn during further online searches that this project has been considered on various levels for many years with the Final Scoping Report being published recently in May 2017.

The report states that the public notification portion of the project (and the NEPA process) began in November 2016. While I see that the Notice of Intent was published in the Federal Register, it is not a publication that I regularly check and I venture to guess that many other residents impacted by this project do not check or are not familiar with this publication either. Also, did your group consider posting public notice signs along the potential routes? If not, why not? If this notice, notification to public officials and a limited mailing to select communities were the only tactics used to elicit public feedback, then effort to obtain an appropriate sampling of feedback has fallen short.

Such insufficient outreach efforts denied me and many other homeowners the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. As such, I therefore formally request that the scoping period for the SCMAGLEV Environmental Impact Study be reopened.

I hope that MDOT, the Federal Railroad Administration, Northeast MAGLEV, Baltimore Washington Rapid Rail (BWRR) and others associated with this project move forward in a more transparent way in working with the communities that will be severely impacted by this project.

Your consideration of my comments is appreciated.

Sincerely,



Jack Penafiel

CC:

Senator Benjamin Cardin
509 Hart Senate Office Building
Washington, DC 20510

Delegate Michael E. Malone (District 33)
House Office Building, Room 154
6 Bladen St., Annapolis, MD 21401

Senator Christopher Van Hollen
110 Hart Senate Office Building
Washington, DC 20510

Delegate Tony McConkey (District 33)
House Office Building, Room 163
6 Bladen St., Annapolis, MD 21401

Representative John Sarbanes
Arundel Center, Suite 349
44 Calvert St., Annapolis MD 21401

Delegate Sid A. Saab (District 33)
House Office Building, Room 157
6 Bladen St., Annapolis, MD 21401

State Senator Edward R. Reilly (District 33)
James Senate Office Building, Room 316
11 Bladen St., Annapolis, MD 21401

The Honorable John Grasso
Anne Arundel County Council
Arundel Center
44 Calvert St., Annapolis, MD 21404 - 2700

John Trueschler Baltimore-Washington SCMAGLEV Project
MTA
6 St. Paul St.
Balt. MD. 21202

4/5/2017



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

Received

MAY -8 2017

Office of Planning

DATE 4.5.2017

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Bonnie Jean Roberts

Address: 13111 Martha's Choice Circle

City: Bowie State: MD Zip: 20720

Telephone: 301-538-7726 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Mr. Trueschler, I have emailed you, but am following up with these comments as well. This process is flawed and rushed. The train is not even needed and will not be affordable for the majority of people to use, just business people and tourists. It absolutely should not be built anywhere along the WBTA trail, this trail provides recreation + and is a sensitive habitat for rare birds. Birders come from miles around to see birds. It is unconscionable to remove that habitat. There are homes + historic horse farms on the trail. It would alter the history, look, tradition and feel of Bowie. These train will negatively affect Wifi + other internet + people with pace-makers: No animals will feel safe with the train passing 30 x per day. It makes Zero sense to build a demonstration train UNLESS the pants →

4/5/2017

Comments: to NYC + Boston were built simultaneously.

This 10 year project will become 30 years +
be out of date.

I spend time on the WBTA. Families walk
out from their backyards. These days it's
all too rare to have a safe outdoor
recreational trail, and this one has such
historic value.

If any train is built, it should not go through
the WBTA. Perhaps the parkway, but I hear
that is eliminated. That leaves the
Amtrak corridor. It will split Bowie
into pieces and destroy Old Town Bowie.

Please avoid the WBTA trail.

Sincerely, Bonnie Roberts
301 538 7726

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project
c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21202

Comments also can be submitted via e-mail: info@BaltimoreWashingtonSCMaglevProject.com



Received

SEP 19 2017

Office of Planning

September 13, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mr. Smith and Trueschler,

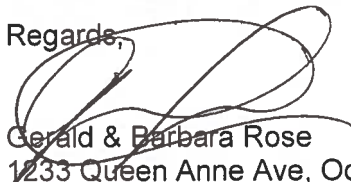
I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only recently learned of it by word of mouth from neighbors.

I consider myself a fairly informed citizen and resident of the King Heights community of Odenton. I am amazed that a project of this scope and the possible affects it would have on me and my community could have taken place without my knowledge. We are elderly citizens and 50 years residents of Odenton and do not relish the thought or uprooting or changing of our neighborhood and surrounding support system being changed or destroyed without my knowledge or input.

I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. From what I have discovered, notification and outreach for these meetings was almost negligible, I would have been present and very vocal about a proposed project that might such a severe impact on my life.

I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,



Gerald & Barbara Rose
1233 Queen Anne Ave, Odenton, MD 21113

EMAIL: JerryRose1233@comcast.net

CC:

US Senator Chris Van Hollen Washington, DC Office,
110 Hart Senate Office Building Washington, DC 20510

US Senator Ben Cardin Washington, DC Office,
509 Hart Senate Office Building Washington, DC 20510 -

Governor Larry Hogan
100 State Circle Annapolis, Maryland 21401

Barbara Rudnick, NEPA Team Leader, EPA Region 3
1650 Arch St, Philadelphia, PA 19103

Steve Shue, Anne Arundel County Executive
44 Calvert Street, Annapolis, MD 21401

SASSO REALTY INVESTMENT, LLC.
Maria Delores Sasso BROKER/OWNER
1240 Duke Lane
Odenton, MD. 21113
410-991-8488
maria@sassorealtyinvestment.com

Received
SEP 19 2017
Office of Planning

September 15, 2017

SCMAGLEV Project
c/o John G. Trueschler
Maryland Transit Administration
6 Paul Street
Baltimore, MD. 21202

Dear Mr. Trueschler;

I completely OPPOSE the construction of the SCMAGLEV in our neighborhood or any neighborhood as I truly feel it is not functional as it is limited in length, scope and service. I OPPOSE for the following reasons:

1. My neighborhood and my office NEVER received any notification or information on the meetings or even its study. I heard of it from a friend at Piney Orchard, and when I studied it, I saw that my office located at the above address would be one of the properties that would DISAPPEAR if the yellow or purple lines considered were in the final decision.
2. You have not given the community the opportunity to comment on the devastating scope of the construction IF it affects our community and my business.
3. The financial cost (\$27.8 MILLION) of the study is an utterly federal WASTE of our taxpayers' money as the cost of the final project for the train for such a limited mileage is utterly ridiculous! I am speaking as a FORMER COMMISSIONER for the HUNTER MILL TRANSPORTATION DISTRICT OF Northern Virginia (Tyson Corner, McLean, Vienna, Reston).
4. This project will clearly affect my \$450,000 – 2016 investment in construction for my office. If this project was already financed for study BEFORE that day, all residents of this development should have been appraised of its magnitude and financial detriment to any home-owner; business owner's further financial construction investment in their property, if eminent domain would eventually take over their land and investment.
5. I am a Real Estate Broker with financial investment and roots in this area, and my livelihood and investments are at stake.
6. I am 68 years old with my life time investments sitting right in the path of your destruction where SCMAGLEV is planned; and with NO input from ME or the community? Are we living in a DEMOCRACY?
7. A train with this capacity and speed has no place in this area. Let us FIX our infrastructure, add to the present needs with REASONABLE train service and stops, and stop wasting taxpayers \$\$\$\$ in ridiculous studies that only benefit those in power and with political contacts.
8. CAN YOU PLEASE LET ME KNOW WHO (THE NAME OF THE PERSON) OR WHAT ENTITY (THE NAME) received the "grant" to do the study?.....Wasn't this study done before? AND WHO (the Federal Government Division) who gave/allocated the money for the study?
9. I WILL FIRMLY STAND WITH STOP THIS TRAIN!

Sincerely,


Maria Delores Sasso



SASSO REALTY INVESTMENT, LLC
Your Bridge to Real Estate Opportunities in Greater Baltimore
www.SassoRealtyInvestment.com

MARIA D. SASSO
RESIDENTIAL & COMMERCIAL BROKER

410.991.8488
1240 Duke Lane, Odenton, MD 21113
maria@sassorealtyinvestment.com

Scan to access my business card on your smartphone



August 30, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

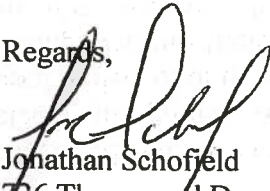
RE: SCMAGLEV – Request to Reopen the scoping period

Dear Msrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

I am a federal employee with over twenty years experience serving our country, both domestically and overseas. I moved my family to Odenton a year ago as it seemed an ideal place to raise my family, including my daughters, aged seven and three. That this project is being contemplated without appropriate local outreach beggars belief and reflects not the ideals of an open and fair society but a closed, repugnant system anathema to the values I have pledged my career to. I ask that you do the right thing and let appropriate discussion ensue. If indeed your plan is the best option, all you face is a slight delay.

Regards,


Jonathan Schofield
726 Thornwood Dr.
Odenton, MD 21113

CC:

Congressman Steny Hoyer
1705 Longworth House Office Building
Washington, D.C. 20515
Fax: (202) 225-4300

U.S. Senator Chris Van Hollen
110 Hart Senate Office Building
Washington, DC 20510
Fax: (202) 228-0629

2017-09-01

To whom it may concern,

Thank you for taking the time to read my letter. I am formally requesting that the scoping period for the Super Conducting Magnetic Levitation (SCMAGLEV) Environmental Impact Study be reopened. My community would be severely impacted by this project, yet I only learned of it yesterday by means of Facebook. Do you think that a project of this magnitude should be discovered while scrolling through Facebook? Is that the appropriate way to inform the general public? I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient! Most of my neighbors have been living in their homes since this community was established in 1969 and have no idea about this proposal. I am requesting that I be given the opportunity to comment on the purpose for the project, need, alternatives to be considered, the scope of the environmental studies and the overall impact analysis. Currently, the Maryland Transit Administration (MTA) is failing to hear the issues and concerns of the affected public.

I recently bought my first home in hopes of the great American dream and raising my young family in a quiet and safe neighborhood. I am now facing this massive train project a half of a mile outside of my home. This sleepy older neighborhood will be destroyed. The park I take my 3 year old daughter to learn to ride her bike is just outside of the yellow line cross-hairs. Surely, you know what the typical look and stigma outside of a train line is. It is generally associated with low property values, gangs, drugs, violence and basically slums that everyone wants to avoid. This train line will not bring prosperity to our area and will only take away communities and families from the affected counties. I would have never purchased this home knowing this train project was in the works. Unfortunately, this project is driving me to consider selling my home and leaving the state if either the red or yellow lines are picked. I am very disappointed and upset with how this project was hidden from the public. I am requesting it to be reopened and seriously reconsidered. Countless families will lose their homes, and communities that were established generations ago will be completely removed in the development of this project. There are other options, lets discuss them. Thank you for your time.

Regards,

Eric and Kendra Short

2219 Dairy Farm Road Gambrills, MD 21054

2017-09-01

Received

SEP -5 2017

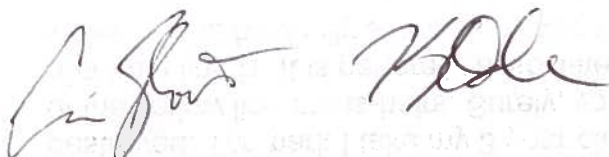
Office of Planning

To whom it may concern,

Thank you for taking the time to read my letter. I am formally requesting that the scoping period for the Super Conducting Magnetic Levitation (SCMAGLEV) Environmental Impact Study be reopened. My community would be severely impacted by this project, yet I only learned of it yesterday by means of Facebook. Do you think that a project of this magnitude should be discovered while scrolling through Facebook? Is that the appropriate way to inform the general public? I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient! Most of my neighbors have been living in their homes since this community was established in 1969 and have no idea about this proposal. I am requesting that I be given the opportunity to comment on the purpose for the project, need, alternatives to be considered, the scope of the environmental studies and the overall impact analysis. Currently, the Maryland Transit Administration (MTA) is failing to hear the issues and concerns of the affected public.

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Regards,



Eric and Kendra Short

2219 Dairy Farm Road Gambrills, MD 21054

July 27, 2017
Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and
Multimodalism
7201 Corporate Center Drive,
Hanover, Md 21076

To Whom It May Concern,

On November 25, 2016, the Federal Railroad Administration published a Notice of Intent (NOI) to prepare an EIS for the Baltimore -Washington Superconducting Magnetic Levitation(SCMAGLEV) project. This notice provided for a thirty day period to provide comments in writing and four public meetings. The NOI's Purpose and Need statement was to "increase capacity, reduce travel time, and improve both reliability and mobility options between Baltimore and Washington".

In May of 2017, a Final Scoping Report was completed to capture the results of the initial scoping effort. The document states that 57 comments were submitted at public meetings, 16 comments were submitted via project e-mail, and two comments were submitted via mail for a total of 75 comments,

The proposed project is a multi-billion infrastructure development that initially identified six possible routes that transect numerous private lands in multiple jurisdictions. Despite repeated efforts to engage the project proponent, we have been unable to garner additional information on the exact proposed routes and even documents justifying the purpose and need of the project. IN fact, the project manage indicated that they identified the proposed routes through computer simulations and had not even walked area under consideration. Now the website shoes two possible routes without any explanation as to the narrowing of options or engagement with impacted communities.

Many of us would agree that MAGLEV technology offers benefits in terms of reduction of harmful emissions and the current traffic congestion plaguing our region. However, a project of this magnitude deserved more public engagement than the 30 days during the holiday season. Moreover, there needs to be more discussion regarding the purpose and need of this project given existing infrastructure rail needs along the Northeast Corridor and within the DC Metro area.

We respectfully request that you work with the project proponents to re-open the Scoping Proess and provide for more meaningful engagement with landowners, local governments, elected official and federal partners before moving forward with the selection and analysis of alternatives to consider in the full Environmental Impact Statement. Further, we would appreciate more direct engagement and information related to the purpose and need for the proposed project.

While we appreciate that the Scoping process is the first in a series of opportunities to engage, it is critical for the landowners and communities who are potentially impacted by the review of routes impacting their lands. Other major infrastructure projects in the DC Metro area have indicated that the failure to productively engage communities and landowners results in a process mired in litigation. As landowners, community members, and Maryland taxpayers, we don't feel that this scoping process was adequate.

Thank you, in advance, for your consideration of this request and we look forward to your response.

Sincerely

Anne Simonetta
adsimon@verizon.net
410-721-0936

Citizens Against the SCMAGLEV (CATS)
Keep Our Community Beautiful
98306 Laurel Bowie Road
Bowie, MD 20715

August 15, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive,
Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

Dear Messrs. Smith and Trueschler,

As a resident of Glenn Dale, MD, I am a citizen whose community will be severely affected by any SCMAGLEV route through Glenn Dale or surrounding areas. **I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened.** I learned only recently, through word of mouth, of the SCMAGLEV project and that community meetings had already taken place.

I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place.

Clearly, the outreach and notification process for these meetings was insufficient since my community and other communities affected by this project are only learning of it after the closure of the process. Had I been notified, I would have been present and able to give my views on this project.

I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Sincerely,



Elizabeth Sloan
6008 Bell Station Rd
Glenn Dale, MD 20769

CC:

County Executive Rushern Baker
14741 Governor Oden Bowie Dr.
Upper Marlboro, MD 20772-3050

County Councilman Todd Turner
County Administration Building, 2nd floor
14741 Governor Oden Bowie Drive
Upper Marlboro, MD 20772 - 3070

12109 Round Tree Lane
Bowie, MD 20715
July 28, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive
Hanover, MD 21076

Dear Mr. Smith:

My home and community are in the path of the MAGLEV project and I have not received any information from MTA regarding the project. Neither I or my neighbors have been given the opportunity to comment on this project or participate in the NEPA compliance process.

I am formally requesting that MTA reopen the Public Scoping Period for this project!

Please respond ASAP!!!!!!!

STOP THE MAGLEV PROJECT!!!!!!!

Sincerely,

A handwritten signature in black ink that reads "Rose Smith". The signature is written in a cursive style with a large, sweeping "R" and "S".

Rose Smith

12109 Round Tree Lane
Bowie, MD
August 6, 2017

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21202

Dear Mr. Trueschler:

I AM HIGHLY OPPOSED TO THE ENTIRE MAGLEV PROJECT but the purpose of this letter is to express my concerns and obtain answers about the huge amount of asbestos within the area designated as yellow in the City of Bowie. Neither I or my neighbors were notified of the scoping period for this project. I formerly request that the scoping period be reopened so these concerns can be heard and addressed!!!!!!!!!!!!!!!!!!!!!!

I am highly concerned about the environmental impact of the asbestos that will be dislodged in the demolition path for the Maglev train coming through Bowie as identify as the yellow area.

I am the original occupant/owner of one of the 263 homes in Bowie within the yellow path that would be taken by Eminent Domain if you proceed with this fiasco.

All our homes built in 1967 by Levitt and Sons have asbestos shingles on three sides, with some or all asbestos shingles on the fronts. Additionally, every square inch of floor tile throughout our homes are asbestos floor tile with the exception of the small areas of ceramic tile in the bathrooms.

Many of our homes were covered with beautiful siding which was attached over the asbestos shingles so as not to disturb the asbestos that now lies under the siding.

What are you going to do to mitigate the asbestos from each of our homes?

How and where are you going to dump this enormous amount of toxic asbestos waste?

What are your plans to mitigate the asbestos and prevent it from spewing into the atmosphere as our homes crumble in ruin after eminent domain while awaiting demolition?

STOP THE MAGLEV PROPOSAL!!!!!! Its a ridiculous amount of destruction to the entire corridor to save a mere 15 minutes on a commute from Baltimore to Washington. It is of no benefit to the City of Bowie. There is no rationale for imposing this horrendous financial boondoggle on the taxpayers of Maryland for a project of no benefit to Marylanders!

Sincerely,



Rose Smith

cc:

David Henley, SCMAGLEV PROJECT, BRRR

✓ Mr. Bradley Smith, MD Dept. of Transportation

U. S.Senator Chris Van Hollen

U. S. Senator Ben Cardin

Congressman Steny Hoyer

Governor Larry Hogan

State Senator Doug Peters

State Delegate Geraldine Valentino-Smith

Prince George County Executive Rushern Baker

Prince George County Councilman Todd Turner

Bowie Mayor Fred Robinson

Bowie Councilman Michael Esteve (District 1)

September 20, 2017

Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Msrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,

Kermit and Barbara Taylor

CC: *Governor Larry Hogan*
Del. Pamela G. Beidle
Del. Mark B. Chang
Del. Theodore J. Sophocleus
Sen. James E. DeGrange, Sr.
Congressman John Sarbanes
Sen. Chris Van Hollen
Sen. Benjamin L. Cardin
Councilman Andrew C. Pauski
Councilman John J. Drasso
County Executive Steve Schuh
The Office of Planning and Zoning

To:

Bradley Smith

Received
SEP 19 2017
Office of Planning

In Regard to SCMAGLEV Project:

My concern is to make sure you are aware of the seriousness of this situation and how it will affect the upcoming voting process in coming elections.

This proposed Scmaglev train was unknown to me and my neighbors. The scoping meetings were not given enough exposure for me to make any appearance or decision.

Until recently I have not been given any opportunity to comment on the scope of the EIS or to the officials that are/were heading this venture.

With this writing, I am formally requesting that the scoping process be reopened and more information be given to the general public. Especially to the neighborhoods that are directly involved.

On a personal note the effect that this will have on housing values, established suburban neighborhoods and "green space" will have a disastrous impact on our migrating birds and/ or endangered species that these green spaces help to serve.

Please get involved and Stop the Scmaglev project.

I remain sincerely,

Paul J Teal

My neighborhood is:

King Heights Dmsm

533 Queen An Ave

Odenton MD 21113

Dear Process Owner/Decision Maker-

We are Steven and Jean Van Rees and live in the "R" section of Bowie, MD.

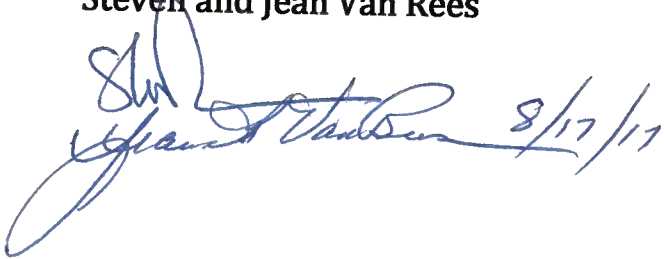
Very frankly, the SCMAGLEV Project makes no sense for anyone, especially property and home owners in one of the proposed paths of this "train to nowhere." It will destroy the environment, cost taxpayers billions of dollars, and provide no real "public" service. We understand the need for transportation planning to meet future demands but the MAGLEV is not a viable option. Tax payer dollars can and must be spent on projects that benefit human kind, not the handful of elite commuters with deep pockets.

Specifically, to start with, the Scoping Process for the EIS is flawed and must be reopened since:

- Neither we nor our neighbors received notification of scoping process
- We have not been given the opportunity to comment on the scope of the EIS

We have lived in our home for almost 45 years, love our community and want no part of the destruction and turmoil that the MAGLEV will bring at, of course, no benefit to the citizens of Bowie and Maryland.

Steven and Jean Van Rees



Handwritten signature of Steven and Jean Van Rees dated 8/17/11.

August 3, 2017.

Dear Mr. Bradley Smith,

My home and community are in the path of the MAGLEV project, and I have not received any information from the MTA regarding the project. Neither I nor my neighbors have been given the opportunity to comment on the project or participate in the NEPA compliance process. I am formally requesting that MTA reopen the scoping period for this project.

Sincerely,

Kim Vejarano

Kim Vejarano

8612 Fluttering Leaf Trail
Unit 307
Odenton, MD 21113-4055

12 Sep 2017

Received

SEP 19 2017

Office of Planning

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Adm.
6 Saint Paul St
Baltimore, MD 21202

Dear Mr Trueschler and successors:

I only just this week heard about the MAGLEV project that looks like it will rip apart my community. I was not informed of the scoping process and am requesting that it be re-opened because you need to take into account the voices of the community you plan to rip apart. The entire project is absurd as we already have rail between Baltimore and Washington. It is called AMTRAK. Works pretty well. The \$\$ spent to shave a few minutes off this trip is absurd. I don't think you have done a detailed ~~economic~~ economic study. Have you considered the losses

of tax revenue. Our community has no interest in supporting this project as we will derive no benefit, only detriment. Perhaps this is why you registered the program the day after Thanksgiving when folks were away, recovering from the election, etc.* I certainly did not hear about it until this week and I know I am not alone. You closed the scoping process without our input. Please re-open this process and give us the notice we deserve.

Sincerely,

Elissa Weidaw

Elissa Weidaw

* As explained to me today by a concerned party.

September 20, 2017

✓ Maryland Department of Transportation
Bradley Smith, Director of Office of Freight and Multimodalism
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, c/o John G. Trueschler
Maryland Transit Administration
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy & Development
Federal Railroad Administration
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: SCMAGLEV – Request to Reopen the scoping period

Dear Mssrs. Smith and Trueschler,

I am formally requesting that the scoping period for the SCMAGLEV Environmental Impact Study be reopened. My community would be impacted by this project, yet I only learned of it recently. I did not receive any notification of the proposed project or of the scoping and alternatives meetings that took place. Outreach for these meetings was insufficient if communities in the crosshairs of this project are only learning of it now. Had I been notified, I would have been present and very vocal. I request that I be given the opportunity to comment on the purpose and need for the project, the alternatives to be considered, and the scope of the environmental studies and impact analyses. Currently, MTA is failing to hear the issues and concerns of the affected public.

Regards,

Linda Witt

CC:

Governor Larry Hogan

DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Drusilla Yorke
 Address: 3804 Meadowkill Rd
 City: Springdale State: MD Zip: _____
 Telephone: 301-502-1564 Email: _____

Join our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: Do not bring this train into
our communities and disrupt our
home and environment where we live
with our families. I don't even see
the need for this - we have Amtrak,
so why another train. Go thru
the waters - like England + France did -
there are other options. Don't come
thru MY community! we own our
home for over 25 years.
- Do better letting everyone know
what's going on.
- As a former environmental employee -
please do not destroy our communities!



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

*We are our
community alone!
Go elsewhere*



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: LINDA McMANON

Address: 4909 ROCKVUE PASS

City: BOWIE State: MD Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

* This project is NOT for the
greater good.

Even a Japanese Group protested
its presence in their community

This is a hair-brained idea!

NO MAGLEV



DATE 10-17

bwmaglev.info

COMMENT FORM

Name: Patricia Williams

Address: 3616 Tyrone Drive

City: Stenandow State: Md Zip: 20774

Telephone: 301-807-3854 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: NO - NO - NO



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Maxine Phifer

Address: 9010 Glenarden Parkway

City: Glenarden State: MD Zip: 20706

Telephone: 301-341-1751 Email: maxine.phifer@verizon.net

Join our mailing list? [X] Yes [] No

Preferred method of communication? [X] Email [] Mail

Comments: I'm not in favor of the maglev Train because it is not good for PG. County. When it fails the taxpayers will have to foot the Bill for its upkeep. P.G. County is one of the most affluent counties in Maryland. This train will destroy the Economy of our County. No matter where it is built in P.G. we all will pay dearly. NO MAGLEV Train in P.G. County.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: M. Douglas

Address: _____

City: _____ State: _____ Zip: _____

Telephone: 202 689 3718 Email: douglas.m@aljazeera.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am primarily interested in knowing the routes chosen for transit should the project go forth.

Also of concern is the immense amount of noise created by the train. Although it is pitched as being a quiet form of transportation, it is not so.

I have ridden the train in Japan and the noise is a huge complaint.



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10-

bwmaglev.info

COMMENT FORM

Name: Carolyn Harrison

Address: 7929 CANKER AVE

City: Glenarden State: MD Zip: 20704

Telephone: 240-459-4145 Email: PeacefulCarolyn@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Don't want it. "Disruptive"



DATE 10/19/17

bwmaglev.info

COMMENT FORM

Name: J. Stevens
 Address: 6908 Decatur Pl
 City: Hyattsville State: MD Zip: 20784
 Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Please find another location
for this train. To have been in
my home for 42 years I can not
afford to move or to have my foundation
affected by by vibration of the train
and noise



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Gilbert & Bobbie Clark

Address: 1412 - 2nd St

City: Glenarden State: MD Zip: 20706

Telephone: 301-341-5819 Email: sweetbobbie@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Please do not come our way. We
have lived in Glenarden for pretty
years and love our town. We
are happy with our city. No No
you are not NEEDED



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Keith Tapscott

Address:

10702 Wymfield Ct.

City:

Glenn Dale

State:

MD

Zip:

20769

Telephone:

Email:

tapscott_k@bls.gov

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Comments:

Scrap the plan !!

Widen B/W Pkwy



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Pamela Gaines

Address: 2104 Parkside Drive

City: Bowie State: MD Zip: 20721

Telephone: 301-249-0774 Email: pamtrips@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This project should not impede
Bowie State University one of the
first Historically Black Colleges
in Maryland; built in 1865.

My family has attended this college
we are legacy. My grandchildren
will attend.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Jul Hochman

Address: 6202 Gideon St

City: Bowie State: MD Zip: 20720

Telephone: ~~301 261 1111~~ Email: hochmanjd@earthlink.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ① What actual factors are considered when making a final route selection?

② Why were those directly impacted never individually notified like those in the Baltimore area?

③ How was the purpose & need developed, when, by whom, & what purpose does it serve to destroy existing communities?

④ Why would this huge investment of funds not be better put to use building other -- more needed -- transportation infrastructure

DO NOT DO THIS



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: NAOMI CRELLIN

Address: 12900 10TH STREET

City: ROWIE State: MD Zip: 20720

Telephone: 781 632 4975 Email: HELLO.NAOMICRELLIN@GMAIL.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ① PROCESS HAS NOT BEEN TRANSPARENT
② PROCESS HAS NOT BEEN COMMUNICATIVE W/ HOMEOWNERS
DIRECTLY AFFECTED BY LINE PLACEMENT.
③ THIS PROCESS IS ALREADY IMPACTED/IMPACTING MY
HOME VALUE - IF I WANTED TO SELL, I CANNOT NOW
UNTIL THE STUDY IS COMPLETED, WITHOUT LOSS OF VALUE
④ EARLIER OPEN HOUSE PROMISED THAT HOMEOWNERS
DIRECTLY AFFECTED BY SELECTED ROUTES WOULD BE
CONTACTED; WE HAVE RECEIVED NO COMMUNICATION.
⑤ STUDY NEEDS TO QUICKLY PROVIDE PROPERTY OWNERS
WITH DETAILS ON PLANS FOR REMEDIATING ECONOMIC
+ LEGAL IMPACT. BOTH THOSE PROPERTY OWNERS THAT WOULD
HAVE LAND SEIZED / LOSE HOMES TO "EMINENT DOMAIN"
AND THOSE PROPERTIES ADJACENT TO THE PROPOSED ROUTE:
WE WOULD HAVE THE LINE AT OUR PROPERTY LINE
IT WILL DEGRADATE THE VALUE OF OUR HISTORIC HOMES



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Concerned Citizen

Address: _____

City: _____ State: MD Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I believe that this project should be stopped. Especially if you ~~can~~^{can't} find a route that wouldn't disrupt a community. If you do build it in an area where there are homes, businesses, schools, and churches, are you going to reimburse fully those citizens who may lose their property? A lot of people may not be able to afford starting over again, so you should fully compensate everyone who loses property. Also, what about the animals that may be displaced? Are you going to relocate them? They deserve to live in peace as well.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Concerned Citizen

Address: _____

City: _____ State: MD Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We do not need faster transportation.
What we do need is more love in our hearts.

God Bless You!



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: MARIA TAYLOR

Address: 6904 DECATUA PLACE

City: HYATTSVILLE State: MD Zip: 20784

Telephone: 301-322-7458 Email: LADYNBLUE5@YAHOO.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: DO NOT WANT THE MAGLEV TRAIN
TO IMPACT THE WOODLAWN AREA BECAUSE
THE TRAIN IS TOO EXPENSIVE, DON'T KNOW
IF THE TRAIN IS SAFE AND IT FOR THE
LATE PERSON, I FEEL THAT THE TRAIN
WILL NOT MAKE MONEY

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: LATRICIA HARRISONAddress: 7832 FISKE AVECity: GLENARDEN State: MD Zip: 20706Telephone: 240-460-5623 Email: latriciasix9@verizon.netJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: I AM AGAINST THE MAGLEV PROJECT COMING TO
GLENARDEN. MANY RESIDENTS OF GLENARDEN HAVE BEEN
THERE FOR MANY YEARS, INCLUDING MYSELF. ~~AND~~ I
AM TOTALLY ~~AGAINST~~ AGAINST UPROOTING RESIDENTS,
MANY OF WHICH ARE SENIOR CITIZENS AND TELLING
THEM THEY HAVE TO RELOCATE. THIS WOULD CAUSE
A GREAT BURDEN ON THE SENIORS IN THE COMMUNITY
AS WELL AS THOSE WHO ARE NOT SENIORS. I AM
A PROUD RESIDENT OF GLENARDEN OF 40+ YEARS,
I DO NOT WISH TO MOVE, OR BE BOUGHT OUT!
I THINK THAT IT IS A SHAME THAT THE RESIDENTS
HAVE JUST BEEN MADE AWARE OF THE VARIOUS MTGS
IN REF TO THIS PROJECT. AGAIN I AM AGAINST, AGAINST
AGAINST THIS MAGLEV PROJECT COMING TO GLENARDEN!
THANK YOU!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Paulette Davis

Address: 12106 Woodwind Ln

City: Mitchellville State: MD Zip: 20721

Telephone: 301 249-2152 Email: davis.paulette.20721@

Join our mailing list? Yes No

Preferred method of communication? Email Mail

yahoo.com

Comments: _____

Please provide copy of Powerpoint presentation on website. A larger space is needed for next meeting; auditorium was full.



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Beverly McCloud

Address: 6215 Lee Place

City: Capitol Heights State: MD Zip: 20743

Telephone: 301 925 4854 Email: McCloud648@AOL.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: P.G. sit on a swamp how will the noise & sound affect the homes? Will our homes be worth it's full value? Will you give us what you want and not it's full value and why? How will the sound be controlled where the homes exist? Who will pay for all this? If homes are affected by the sounds how will you correct it? If homes are damaged how will you cover the home owners? P.G. also sit on a spring where water sit under homes daily. How will that spring water sitting affect the homes with the noise & sound of the train?



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: Candace & Lawrence Tucker

Address: 8402 Triple Crown Rd

City: Bowie State: MD Zip: 20715

Telephone: 301-352-5298 Email: adtk41@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: E1 Option - We have concerns that in this proposed plan the above ground structures needed in the 564 Corridor will be negatively impactful to the business corridor that sustains the community. The obstructions that will occur due to the build will decrease the open space and open air integrity off the community. The need to maintain such business w/o disruption in the community.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: CHARLES GEORGE

Address: 4003 CORDELL CT

City: Bowie State: MD Zip: 20715-1540

Telephone: - Email: JGeorge@MSN.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Though your recent route changes have eased the impact on my neighborhood, I'm still opposed to this project. I feel it'll impose a huge burden on ^{local} taxpayers with little benefit. I do not think the project can produce sufficient income to pay off any interest or debts incurred. Thus, I believe subsidies will ultimately be required.

With only a PWT stop, it provides little practical use for PG & G residents. It basically duplicates services that BMTARB now provides (admittedly faster but w/ less access) and seems, it seems to be too costly, too disruptive to warrant continuation. Maybe it'd be better to use these resources on the transportation assets now in place.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Emily Shelter

Address:

City:

Landrum

State:

MD

Zip:

20706

Telephone:

Email:

estelter24@gmail.com

Join our mailing list? Yes

No

Preferred method of communication? Email

Mail

Comments:

You could at least have your maps lined up properly!
If sheet 8 matches w/ sheet 9, why is sheet 9 next to it? Or is this another one of your deceptive ideas?
It's obviously no concern of yours how you will affect those people whose houses you will destroy!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Partington's

Address: 6805 Willow Creek Rd

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: partington.ed@gmail

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We don't need the Maglev train
in the city of Bowie. It will
disrupt the environment. Who is the
genius who decided what area
the train will go thru? Please
cancel the train project.

Thank-you.
The Partington



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: Theresa R Groves

Address: 1011-60th Avenue

City: Fairmount Heights State: MD Zip: 20743

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: It will not benefit prince Georges County at all. I was not inform in writing about this Train and the. It was not inform by mail or postcard. It will destroy our Homes, so I say please stop it for our Homes Benefit Thank you.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Daisy Capers

Address: 6003 Lee Pl

City: Fairmount Heights State: MD Zip: 20743

Telephone: 301 925-2114 Email: DaisyCapers@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Thanks, first for removing the route of
Fairmount Heights, MD from your travel routes. Second,
your purpose was made clear, who was involved,
study process and timeline, project study area, key
environmental considerations all good info.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Bill Mann

Address: 6205 Bidean Street.

City: Bowie State: MD Zip: 20720

Telephone: 240.676.7374 Email: homescope4u@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: 1.) Where is the elevation study?
2.) For the 295 route is expanding lanes also
being taken into account for the study?
3.) The millions of dollars spent on this project
could be better served on other projects that
would benefit a wider community base.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: GERALD M FELDMAN

Address: 3509 MASIE LAWE

City: BOWIE State: MD Zip: 20715

Telephone: 301-262-9062 Email: gfldm@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This is a boondoggle! Enormous cost, extensive environmental damage, years of disruption to homes & businesses. Existing rail service between D.C. & Balt. takes about 40 minutes. This would cut that in half. NOT WORTH IT! NO BUILD!



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Karen PARKER

Address: 9211 Myrtle AVE

City: Bowie State: MD Zip: 20720

Telephone: 2407316273 Email: KarenPARKER88

Join our mailing list? Yes No @gmail.com

Preferred method of communication? Email Mail

Comments: I am against this project.
Too many homes will be
destroyed.

Amtrak & Marc trains ARE
Great options between
DC & Baltimore.

Use current infrastructure

Ridership does NOT exist
to justify destroying
Peoples LIVES.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Nancy Egan

Address: 6810 Willow Creek Rd

City: Bowie State: MD Zip: 20720

Telephone: 301-262-8978 Email: noon1be@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: How is this train going to help the
citizens of Bowie? We would still have to
drive to DC or BWI to get the train - then
pay for parking in addition to paying for the train.
finding a parking space in addition to the drive
~~in addition to the time spent to find a~~
~~parking space.~~
would be more time spent + more money than just
driving your car to DC or BWI. What about the
Noise factor? Are there vibrations? Communities
like Bowie don't need something like this because
it really doesn't provide us with any service it
would just disrupt our community. Have
environmental impact studies been done?
What are the results?

~~Nancy Egan~~



DATE _____

bwmaglev.info

COMMENT FORM

Name: GLORIA TURNER

Address: 3311 BARROFF DR.

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We have enough hi transportation going to Baltimore from Wash.

1. Amtrak

2. Penn Marc

3 Acela

what else do you need

Enough already!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Erma H. Anderson

Address: 9814 Ridge Street

City: Lanham State: md. Zip: 20706

Telephone: 301-794-7123 Email: anderma
anderma42@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I don't feel the train will serve the
needs of our community. We need regular bus
service and metro. We don't need to have
our homes taken to set up this train. This
project sets back programs that were in
play for better transportation for all of the county.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Pat Hughes

Address: 13401 Katrinka Drive

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I'm concerned about taxpayer financial commitment, neighborhood impact and environmental impact. Will the electrical energy used be from sustainable sources such as solar?



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Francis Anderson

Address: 7708 Hanover Pkwy #104

City: Greenbelt State: MD Zip: 20770

Telephone: (301) 747-2361 Email: ciscocastille@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ~~It~~ If it's to be done, then the "E1" has my vote. But,
Pub. costs will doesn't have the ^{public} transportation system to support this
project. Thank!

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Julie Kelly

Address: 12512 Windover Turn

City: Bowie State: MD Zip: 20715

Telephone: 301352 3385 Email: jellyrace@comcast.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am in opposition to the Maglev project.
First as a citizen I was not given an
opportunity to comment on the scope of the
project and the alternatives until recently
The Acela train is an alternative ~~that it is~~
The environmental impact both in destroying
communities in its path and the long range
health issues from the running of the trains
need to be weighted more heavily than
the proposed benefit to an elite group
of travelers. This train provides no benefit
to my community.



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: Rusetta M. Greer
 Address: 7907 Glenarden Pkwy
 City: Lanham State: MD Zip: 20706
 Telephone: 301-772-0092 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My concern is structural. Will tunneling under ground may my home has different cracks? Moving to another area will NOI be helpful to me. I just retired; still paying mortgage, and can't think about another home somewhere that I don't have public transportation.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name:

Joyce Gross

Address:

14700 Stallion Ct.

City:

Bowie

State:

MD

Zip:

20715

Telephone:

~~3~~

Email:

jawagross@yahoo.com

Join our mailing list? Yes

No

Preferred method of communication? Email

Mail

Comments:

I am in favor of the "No Build" option. Let's continue to upgrade Amtrak. This is unnecessary for travel to Baltimore, Philadelphia, Newark & north.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Margaret Abney
 Address: 6407 CROSBY RD
 City: LANDOVER HILL State: MD Zip: 20784
 Telephone: 301-327-4883 Email: msabney1@MSN.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I personal do not wanting this
Super Train in my area (Landover-Cheverly)
It doesn't benefit me or my Family
I think its a shame what's going on.
Let the citizen No at the last minute.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Wayne E Fells

Address: 6913 Emerson St

City: Hypattsville State: MD Zip: 20784

Telephone: 301-322-1918 Email: Wayne.Fells@Verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Strongly against Any Homes + Property Loss
Due to this TRAIL. Even with tracks under ground
there will be vibration and such. to much of a
unknown.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: HARVEY GROSS

Address: 14700 STALLION CT.

City: BOWIE State: MD Zip: 20715

Telephone: 301 805 9026 Email: —

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I SUPPORT — THE "NO BUILD OPTION"
I SEE NO NEED FOR ANOTHER RAIL LINE
IN THIS TRANSPORTATION CORRIDOR.
IT'S REDUNDANT!
FIX/IMPROVE AMTRAC FACILITIES BEFORE
CONSIDERING ALTERNATE RAIL LINE.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Laurel Maduro

Address: 15115 Roving Wood Drive

City: Bowie State: MD Zip: 20715

Telephone: 301-464-1480 Email: La.Maduro@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My family has lived in the Saddlebrook West community of Bowie for almost 16 years. Both of my children attended PGCP's public schools and graduated and went on to college. Bowie is a wonderful, family friendly community. Why would our Governor think it is a good idea to displace thousands of families in order to allow a train to pass through the community?

DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: David + Eleanor ManuelAddress: 1514-3rd StreetCity: Glenarden State: Maryland Zip: 20706Telephone: (301) 341-0646 Email: davidmanuel1025@gmail.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: We were told that the MAGLEV train will not be coming thru Glenarden. What happens if they change their mind. The town of Glenarden (6,000 pop.) is made up of mostly seniors. If forced to move, will we get the proper value of our homes vs. fair market value. As stated before the majority of citizens in Glenarden are seniors. Think about it: Who is going to give anyone in their 70's or 80's money for a new mortgage? Also with what's been going on in recent months how come nothing has appeared on the news media? People watch the news and could have shared their opinions months or years ago. Also it would have been nice to see our state representatives attend the public meetings!!!



DATE October 14, 2017

bwmaglev.info

COMMENT FORM

Name: Nathaniel Pollard

Address: 8614 Glenarden Pkwy.

City: Glenarden State: Md. Zip: 20706

Telephone: 301-219-0158 Email: nathan.pollard@pgcps.org

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This entire concept is the craziest nonsense I've ever heard. I have resided at my home nearly 40yrs. and enjoy the comfort and pleasure it provides for both my wife and myself. At this stage in life, I am worried about where I'm going to purchase a German shepard puppy - not where I'll have relocate. At 70 years of age, cutting grass, keeping my home and the surrounding area neat and presentable and totally enjoying my home are honorable marks for me. This proposed project has made me extremely angry and its disrupted my health. This project needs to be discontinued.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: B. Clemens

Address: 6823 Furman Pkwy

City: Riverdale State: MD Zip: 20737

Telephone: 301(395-3470) Email: rossbarbie@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I was never informed in writing



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Corey Gaines

Address: 2104 Parkside Dr

City: Bowie State: MD Zip: 20721

Telephone: 301-467-9899 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Concern about Bowie State University
is a Historically Black College
1865. Make sure this does not
interfere with the school and
impede with the school and surrounding
land.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Cynthia Johnson

Address: 8621 Leslie Ave.

City: Glenarden State: MD. Zip: 20706

Telephone: 301-777-6444 Email: Cynjohn6@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am a resident of Glenarden, MD.

Glenarden^{which} is a black historic community w/a rich history. This community has been disrupted twice because of transportation needs. This latest potential disruption will impact many residents especially senior residents who are living on a fixed income.

I oppose this project because of lack of timely information. I was not properly notified about this project, which may ultimately affect my home and community, in writing.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Shirley Pollard

Address: 8614 Glenarden Parkway

City: Glenarden State: MD Zip: 20706

Telephone: _____ Email: loveya256@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

Stop and discontinue this ~~the~~ project. It will effect my community tremendously - in a negative way. We live in a honorable black neighborhood that enjoys a settled way of life. This proposed plan appears to be another example of a White America takeover. This plan attacks a pocket of black families that could have a lifetime of negative consequences. Stop this entire thought process!!! If this rail system is a must, establish it in a predominantly white neighborhood and watch the consequences.

Thank you



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Cynthia Johnson
 Address: 8621 Leslie Ave.
 City: Glenarden State: MD. Zip: 20706
 Telephone: 301-772-6444 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I understood that there will be jobs resulting from this project.
- How will these jobs impact the Seniors who are past the job market age and the young adults who are starting families and probably love job security w/ their current employers?
- Will these jobs provide an income to allow me an opportunity to sustain myself w/ new housing in the event that I lose my house due to this project? Please respond IN WRITING to the address provided above.

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Pam Jenkins

Address:

12326 Lanham Severn Rd

City:

Bowie

State:

MD

Zip:

20720

Telephone:

301-728-1005

Email:

pjenkins728@gmail.com

Join our mailing list?

 Yes No

Preferred method of communication?

 Email Mail

Comments: I strongly oppose the SCMaglev
concept not only for my neighborhood
(I live on 564 in direct path of
what is now called the orange option). I am
all for improving transportation, but do not believe
this is a viable solution especially w/intent of
going through populated areas. I do not
want increased taxes to support something
that I have no access to. I am also strongly
concerned about the health & safety impacts
of this type of technology and as a
real estate agent the property values decrease
even if my home is not taken. NO TO SCMAGLEV



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Beverly Wilks-McFadden

Address: 8614 Johnson Avenue

City: Glenarden State: MD Zip: 20706

Telephone: _____ Email: beverly.gray@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am most concerned and outraged that any consideration is being given to a route that will detrimentally impact Bowie State University in any form or fashion. As a Historically Black College/University, Bowie State has serviced many of the disenfranchised citizens of P.G. County and the State of Maryland. This University has grown with millions of dollars of improvements and growth. It services a wide group of citizens at an affordable cost. SAVE MY UNIVERSITY and the future of many young people in the State.

NO TRAIN NEAR Bowie State.



DATE 10/15/2017

bwmaglev.info

COMMENT FORM

Name: Nancy Johnson

Address: 13307 Testh ST

City: Bowie v State: MD Zip: 20715

Telephone: 571-230-0612 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We Do not need A TRAIN where you MUST
Drive to DC to board. This Trains will be a
Train to nowhere.

Why are the other meetings STARTING at 5:00 PM
during the week when people are not home from
WORK. ~~Someone~~ Who will paid for the train
when you RUN out of money.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Margaret T. Dade

Address: 8714 McLean Ave

City: Glenarden State: MD Zip: 20706

Telephone: 301-772-7097 Email: dade4809@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: After reviewing the current alignment
I am please to see the routes will not destroye
communities that have been in existance
for 50 years or more



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Catherine Epps
 Address: 8603 Hamlin Street
 City: Landover State: MD Zip: 20785
 Telephone: _____ Email: blunt0842@msn.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My family does not want this
project coming through our community?
(Short and Sweet)



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Regina & Louis Carroll

Address: 7933 Bellwood Ave

City: Glenarden State: MD Zip: 20706

Telephone: 301873-5529 Email: regina.carroll@ymail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I want to keep updated with information
I do not want to ~~the~~ lose my home in
Glenarden. I feel we do not need this
MAGLEV trains I have wrote the people
in District 4 and have expressed how I feel
about my family and others loosing their
property. Thank you



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: Vivica Jones

Address: 7937 Campbell Ave

City: Menard State: MD Zip: 20726

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am hearing from your Maglev
representatives that the City of Menard
is no longer being considered as point of
movement for your train. If that is the
truth "Great" but if not let me take this
opportunity to say "No Absolutely No To
this Train."



DATE 14 October 2017

bwmaglev.info

COMMENT FORM

Name: John Allen

Address: 14206 Pleasant View Dr

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: _____

Join our mailing list? Yes No Already on

Preferred method of communication? Email Mail

Comments: This project will not benefit the vast majority of people in Maryland. Any purported economic benefits are based on trickle down effects from big companies that can afford the high costs of using the train over other means of transportation.

Having a single source of equipment will result in high costs due to lack of competition between suppliers. There is tremendous risk that the Japanese company providing the unique SCMAGLEV technology will go out of

business or choose to no longer support the project.

DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Stephen W. Swartz
 Address: 12605 Hillmeade Station Dr
 City: Bowie State: MD Zip: 20720
 Telephone: 301-262-8031 Email: csswartz@verizon.net

Join our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: Stop This Train! What a foolish project to start! Sure the project has been promised money from Japan, but that will not cover the cost of building said train. Then will the people of MD end up "footing the bill" for the rest of the cost. If you think the Federal government will "bail MD out," I don't think so. The present administration is looking to cut cost not increase them. This train will have little benefit to the people of MD. People in P.G. county are better off just driving to DC or MD. Same for the majority of people in the State. If the governor thinks this will be his "legecy," hit will be in a negative contex about the increased taxes this project cost MD.



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE Oct 14, 2017

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COMMENT FORM

Name: Berlett Warden

Address: 1420 4th Street

City: Glenarden State: MD Zip: 20706

Telephone: 301-341-5572 Email: bwarden@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Residents should have been informed
of this project long ago. Why the
secrecy? Please send information
especially as to the route this train
will take.

Thank you.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Suzanne Johnson

Address: 8610 Leslie Ave

City: Glenarden State: MD Zip: 20706

Telephone: 301-322-2162 Email: suzyteaches@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I understand the need for progress, but
I don't understand why this progress needs to
affect mostly historically African American
communities. African American homes, churches,
Colleges & Cemeteries, always seem to be the
1st considerations



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Phil Van Sickle

Address: 7509 OLD CHAPEL DRIVE

City: BOWIE State: MD. Zip: 20715

Telephone: 301-464-3150 Email: PVANSECKLE@AOL.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This project will, in the long run, benefit potentially 500-1000 people per day. It will negatively impact several thousand people for the rest of their lives. People will be removed from homes they have been in for 20-40 years, many like myself in a home for 30 years that I should be able to live my life out in. And, we were not informed in writing of this project until 3 months ago. That is a requirement by law!



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: CATHERINE SWARTZ

Address: 12605 Hillmeade Station Drive

City: Bowie State: MD Zip: 20720

Telephone: 301-262-8031 Email: CSSWARTZ@Verizon-net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We are definitely opposed to the Maglev Project!!
The cost to ALL MD Taxpayers is UNCONSCIONABLE.
The benefit to those affected is negligible and is only
going to benefit the elite who can afford it. It is not
even accessible to those affected since there are only
3 access points, none of which are near us/them.
This is a pet project of Gov. Hogan to be on
the leading edge and for his legacy. His "No New
Taxes or increase in taxes" is negated with this
project and will impact all MD taxpayers for years.
STOP THE TRAIN!!


**BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/14/17

bwmaglev.info

COMMENT FORMName: Bella VanSickleAddress: 7509 Old Chapel Dr.City: Bowie State: MD. Zip: 20715Telephone: 301-464-3150 Email: BMVanSickle@gmail.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: This train is ruining our lives. We have lived in our house for 38 years. We are ready to retire and move my 86 year old mother in. If our home is destroyed we will not be able to afford to move. We were never informed in writing about this train. We never received any information about the MAGLEV. When I heard about it "through the grape vine" I told my neighbors. No one in my neighborhood was ever informed in writing or any other means about this project.



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: Patty Green

Address: 2708 Babbitt Lane

City: Bowie State: MD Zip: 20715

Telephone: 301-262-1885 Email: patgreen05@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

I am opposed to the MAGLEV project

- I was not informed in writing about this train

① not informed by writing, did not receive a mailing or ^{any} online communication

- This project will not benefit Prince George's County

- I am concerned about the environmental impact and the lack of information

- This is not a commuter train

- ~~the~~ Our area does not have the resources to maintain the train after ~~it~~ launch.

- HAS NOT WORKED ANYWHERE ELSE

- It is private venture ~~to benefit~~ with no benefit to our communities!



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Phyllis Johnson

Address: 4910 Lake Ontario Way

City: Bowie State: MD Zip: 20720

Telephone: 301-805-5158 Email: phylliscjohnson229@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Beverly Powell

Address: 4421 LANCEFIELD LANE

City: Bowie State: MD Zip: 20720

Telephone: 3016029236 Email: bipowell49@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am extremely appalled that our
~~political~~ Political officials have allowed a discussion
of ~~something~~ maglev to get this far. ^{Bowie} ~~it~~ should
NEVER have been considered as an option for
this train. It offers PG nothing and even if it
did its a horrible proposition. Our leaders
aren't considering the negative affect this will
have on its constituents. I repeat this should
never have been considered. Is it just because PG
is a minority community and they don't ^{CARE} ~~give a~~
about us. Just money. Shame on you Rushern Baker



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: T. Greedy

Address: _____

City: Bowie State: MD Zip: 20721

Telephone: _____ Email: tommgreedy@comcast.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Please eliminate the proposed AMTRAK Alternative (E1).



DATE 10/14/07

bwmaglev.info

COMMENT FORM

Name: WILLIAM WASHINGTON Sr.

Address: 1510-4th STREET

City: GLENARDEN State: LA. Zip: 30706

Telephone: _____ Email: WSMECHANIC@VERIZON.NET.

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This is a no no. Please ~~don't~~ bypass this project
My family benefit or gets nothing in return in taxes or
money tree value. To take my home and destroy my families
for a short ride for others to travel to work to New York.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Rosa Washington
 Address: 1510 4th St.
 City: Glenarden State: MD Zip: 20706
 Telephone: _____ Email: WSmechanic@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This building of this train will destroy the investments of many citizens. I realize that we live in a society that only sees green which amounts to nothing but greed. Have a heart and do the right thing. Build communities and relationships. We need a stronger America not a faster train.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: MARIE HYMAN

Address: 3027 Tyrol Dr.

City: Glenarden State: MD Zip: 20774

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: NO, NO, NO MEGLEV in
Glenarden!

NO, NO, NO MEGLEV in State
of Md.

No, No, No To you Larry Hagar



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Jerry Hardy

Address: 1501 2nd St

City: Blenardern State: MD Zip: 20706

Telephone: 301-341-1355 Email: Kidcixcd@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This train will not benefit the County!
It will displace many families and seniors
there is no need for it. The existing trains
and planes are fast enough.
I can not afford to lose my home.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Takisha James (Mayor of the Town of Bladensburg)

Address: 5507 Doris Court

City: Bladensburg State: MD Zip: 20710

Telephone: cell-240/392-5237 Email: tjames@bladensburg.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I'm disappointed that this project is being given serious consideration. This project will negatively impact Bladensburg residents and our home values. After having dialogues with various staff at today's open house, I'm concerned that all of the alignments that have been proposed will impact my town. Much progress seems to have been made on this, even though we are given the appearance that this train is going through initial planning and research phases. I've reviewed the streets that would be impacted in my community and this is unacceptable. I urge you on behalf of my residents to stop this project.



DATE 10/15/17

bwmaglev.info

COMMENT FORM

Name: Alain Fournier, Cheryl Graewe

Address: 13217 Mockingbird Ln

City: Bowie State: MD Zip: 20720

Telephone: 301 464 5740 Email: graFou21@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: - Surely it would be less expensive to invest in ACELA tracks for which we already have train (which might need some updates.) but buying both tracks & trains for MAGLEV now would be wasteful.
- Impact on Patuxent Wildlife refuge? on wildlife for both Magnetic Field & Wind Motion.
- Same questions as above but for humans?
- How about property value along routes?



DATE _____

bwmaglev.info

COMMENT FORM

Name: KIM L. FRAZIER
 Address: 6403 Woodpointe Drive
 City: Alenn Dale State: Md. Zip: 20769
 Telephone: 301 221-3492 Email: DANCER560@GMAIL.COM

Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: Unacceptable. We will fight you to the end. All Black Neighborhood (Zip Codes) No Leadership in PG. No previous success in the country with trains of this kind. No attempts to improve the existing infrastructure. What guarantees will we have about the value of our homes; because housing values will fall - no one will want homes w/ 2 or 3 trains. There are no successful train projects in this area including California which is a disaster. Again we will fight this effort to the end.



DATE 10 / 11

bwmaglev.info

COMMENT FORM

Name: THOMAS Wedge
 Address: 3913 92nd AVENUE
 City: Springdale State: MD Zip: 20774
 Telephone: 301-661-6262 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I Live in Springdale I'm 86 years old
this would put a hardship on me I live in Springdale
since 1978 this is not accept in my mind please
consider the Elderly



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: James + JoAnn Bailey

Address: 1001-60th AVE.

City: Fairmount Heights State: MD Zip: 20743

Telephone: 301-925-4027 Email: Jo7231@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We have been living in Fairmount Heights for over
40 years our children all grew up in this town. Our home
is a historic home. Fairmount Heights is a growing
Comm. of All people. We are not trying to find
a new home. ~~NO~~ ~~Brain~~ ~~NO~~ ~~Train.~~

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Thomas Ferguson
 Address: 1409 3rd St.
 City: Glenarden State: Md Zip: 20706
 Telephone: (301) 386-9767 Email: TLAKERMAN@AOL.COM

Join our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: I am very concern that my community is being consider for the Maglev train. My community is a historic community and even if my house is not directly impacted, the noise and vibration will definitely impact our quality of life. Other concerns I have is related to the fair market value for my house. I just finish a full renovation and do not believe the money that would be offered is going to fairly/justly compensate us. How ~~with~~ long in advance will we be notified if my community is selected to be taken over, what plan is in place to transition folks who are retired on fix income and cannot afford the extra expense associated with relocating.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Pam Jenkins
 Address: 12326 LANTHAM SEVERN RD
 City: Bowie State: MD Zip: 20720
 Telephone: 301 728 1005 Email: pjenkins728@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The new route that came out on 10/13/17 which comes out route 564 will totally take out my home and neighborhood that my now deceased husband built as a legacy for the family and I have no intentions of selling. I do not want this train in my neighborhood or state and strongly oppose the SCMaglev project.



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: M. GUARIS

Address: 7900 Echols Ave.

City: Glenarden State: MD Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: No maglev through Glenarden!!
We are proud of our city, too much history
and blood sweat + tears to lose!!!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Timothy & MARIAN Mcintosh

Address: 1902 PARKSIDE DRIVE

City: Mitchellville State: MD Zip: 20721

Telephone: 301-249-2297 Email: nccueg@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This issue has not been properly
exposed to the communities with proper
notices for citizens objections. We are
TOTALLY OPPOSED TO it!!

Timothy & Marian McIntosh



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Kevin Greeley
 Address: 4200 Huntchase Dr.
 City: Bowie State: MD Zip: 20720
 Telephone: _____ Email: kgkgreeley@outlook.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ELIMINATE alternative E-1 which has
 the most impact on residential properties of
 the remaining alternatives. This option
 will cause disruption of existing traffic
 patterns; longer commutes into DC; lower
 property values in the Bowie area; and
 displacement of residential communities.
 If the project is approved at all, it should
 be limited to routes where there are existing
 rights of way - the BW Pkwy Routes.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: James Martin
 Address: 1000 60th Ave.
 City: Fairmount Heights State: MD Zip: 20743
 Telephone: 301-925-8435 Email: martinjd1@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We do not want A Super Train coming
Along our community. Many of our residents
Are seniors and disabled. As a community, we
were not notified and our government was not
given a notice. We need All information and
plans.

Stop the Train!!!
We Are A historic Community



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Kathy + Scott McIndoe

Address: 13214 Pine Rd

City: Bowie State: MD Zip: 20720

Telephone: 240 206-6144 Email: ~~rs mchen-kathy~~ 1204 splineworld.us
rs mcindeoe@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ① We are concerned about the financial/economic impact this train will have on our state/our counties. Many homes are "underwater" already. The value of our homes will decrease, making difficult to sell. We will lose a significant amount of money. The decreased value of our homes will also impact the property taxes and the revenues our counties gain from them, thus impacting the ^{economy} of the counties + State of MD.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Janet Merkel

Address: 9306 Merkel Farms Rd

City: Bowie State: MD Zip: 20715

Telephone: 301-262-6587 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I know noone wants this mode of transportation
to come near their property and I also know we
need to accept change. With this proposal, I sincerely hope
that you will consider the impact this will have on
people's homes and lives. Please realize the homes that
will be lost because of this train. People have saved
and worked for many years to have their homes.
Please consider the people that
will be affected by this.
And communicate, communicate



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Rouana Beckford

Address: 7403 Hoodland, Landover, Md 20785

City: Landover, Md State: Md Zip: 20785

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This project does not justify the interruption the families that will be affected.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Michelle Shelton

Address: 8618 Leslie Ave

City: Glewarden State: MD Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My home is my family legacy. And
the possibility that that can be lost
because some company from Japan and
the idiot governor of Maryland wants
millionaires from Washington to get to
billionaires in New York in 25 mins is
totally absurd.

As an African American the only wealth
we have is in our homes and if that is
taken the foundation for our future
generations is stopped dead in it's tracks.
We will not allow that to happen.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Linda Watson

Address: 12405 Hillmeade Sta Dr

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: As I walked around the room
looking at all the boards, I noticed
that much of the train will be
above ground. The trains in
Japan are 90% underground.
Why aren't we giving that same
courtesy. The part of the
train that is above ground
cuts Prince George's county in
half. It too should be
underground.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Marjorie B. Tanner

Address: 12309 Melody Turn

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I firmly support the No Build option. Maglev
will not benefit many people ~~except~~ for the
investors. The money (federal & state) could
be used for projects that will benefit the
communities.

I'd like to know why the Federal RR
Commission was willing to give millions of
dollars to study environmental effects.
I resent my tax dollars being used for such
a project that will benefit so few.

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Geoffrey Holdridge
 Address: 13111 Martha's Chorus Circle
 City: Bowie State: MD Zip: 20720
 Telephone: 301-464-5046 Email: geoff.holdridge@gmail.com

Join our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: The State of Maryland accepted a
\$28 million grant from the FRA to study this
proposed maglev project. The original FRA Federal
Register notice announcing the availability of
these funds includes a requirement that
the study include an assessment of the
economic viability of the project, including
its ability to be a self-sustaining,
revenue-generating transportation service.
So far all I have seen is a study of the
potential environmental impact, per NEPA.
Where is the economic viability study?
I am concerned that this project is going to need
a big bailout from the taxpayers.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: CARL ABILDSD

Address: 14902 Arabian Lane

City: Bowie State: MD Zip: 20715-3357

Telephone: (301)262-4474 Email: clabildsd@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Fix the (METRO) first before
investing any money in this new
project. The METRO system has a
deficit in the millions (\$\$) and
needs to be upgraded. Funds from
this project could also be applied to
the Purple line which is now under
construction. Maintain what you have
before starting a new fantasy project.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: VICTOR KIRK, SR.

Address: 6117 SEABROOK ROAD

City: LANHAM State: MD Zip: 20706

Telephone: 301-429-3345 Email: pastorkirk@SHARONBIBLE.ORG

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: WILL RESIDENTS BE MISPLACED FROM THEIR HOMES?

- HAS A CRASH FEASIBILITY STUDY BEEN CONDUCTED?
- ARE THERE HEALTH CONCERNS TO CONSIDER (RADIATION, INCREASED HEAT, ETC)
- IS THE TRAIN RIDE AFFORDABLE? WHAT'S THE COST TO TRAVEL?
- HOW MANY JOBS WILL THIS PROJECT PROVIDE TO THE COMMUNITY?
- WILL THIS EVENTUALLY REPLACE AMTRAK?
- WHAT OTHER CITIES ~~DOS~~ HAVE THIS SUPERCONDUCTING TRAIN? WHAT'S THE SUCCESS RATE/STORY FOR THESE CITIES?
- WHERE IS THE FUNDING FOR THIS PROJECT COMING FROM? WILL THE CONSUMER HAVE TO PAY ADD'L TAXES TO PAY FOR IT?
- WHY ARE SO MANY PEOPLE AGAINST THIS PROJECT, ESPECIALLY IF IT'S GOOD FOR THE ENVIRONMENT?

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Donna D. KlemAddress: 13301 KATRINKA DR.City: BOULE State: MD Zip: 20720Telephone: 301-464-2565 Email: pt4baby@gmail.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: I AM AGAINST CONSTRUCTION OF SC MAGLEV ANYWHERE IN THIS AREA FOR THE FOLLOWING REASONS -

1. I DON'T BELIEVE IT CAN BE SELF SUPPORTING AND AT SOME POINT TAXPAYERS WILL BEAR THE FINANCIAL RESPONSIBILITY.

2. IT WILL NOT RELIEVE COMMUTER CONGESTION - CENSUS DATA INDICATES THAT FEWER THAN 1% OF THE POPULATION COMMUTES B/W THE TERMINAL POINTS.

3. THIS AFFECTS HOMEOWNERS IN COUNTIES THAT RECEIVE NO BENEFIT FROM IT.



DATE OCT 14, 2017

bwmaglev.info

COMMENT FORM

Name: A. Reed
 Address: 12107 Long Ridge LN
 City: Bowie State: MD Zip: 20715
 Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: It would have helpful to have printouts of the "posters" and maps as handouts. There are many senior citizens who are impacted who do not have computers or know how to use them.

There are too many impacted citizens who are unaware of the Maglev project. There should be a better way to reach everyone - perhaps via newspapers (Balt. Sun) (Washington Post). How about a bulk mailing?

There are other numerous concerns in all areas of this project. These comments will be mailed.



DATE _____

bwmaglev.info

COMMENT FORM

Name: RICHARD GAEDSMOE

Address: 4909 ROCKVUE PASS

City: BOWIE State: MD Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: BRR Did not do a good job
notifying the Public in advance

Please look into this!
and publish your results

thank-you

Also for parking facilities
at these 3 stops — There is no
room at Union Station for sure
— LOOK INTO THAT!!



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Romaine Bradford

Address: 4711 Lake Ontario Way

City: Bowie State: MD Zip: 20720

Telephone: 301-262-3162 Email: Romaine Bradford @ G Mail. com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

DATE 10/14/17

bwmaglev.info

COMMENT FORMName: Craig SheltonAddress: 8618 Leslie AveCity: Glenarden State: MD Zip: 20706Telephone: 301 806 6664 Email: _____Join our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: I made a trip to the Harriet Tubman
Museum a conductor on the Underground
Railroad. It was a very moving experience.
A film that is shown at the exhibit is
of Gov. Larry Hogan giving a speech at
the grand opening. I find it to be really
ironic that in one moment Hogan
praised the movement of the Underground
Railroad and the next moment he goes
to Japan, comes back to Maryland and wants
to run a railroad through our homes.

Hogan must go!!!



DATE 14 OCT 2017

bwmaglev.info

COMMENT FORM

Name: Helene merkel

Address: 9425 merkel Road

City: Bowie State: MD Zip: 20715

Telephone: (301) 262-4462 Email: helene.merkel@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

Purpose + Need: Purpose is well defined, but you have not demonstrated the need for this project. Why would people in Prince Georges + Anne Arundel County drive in the opposite direction to catch a train to go north to Baltimore? While this is a high speed train, it wouldn't save time. Furthermore, the economic data isn't there to show that this project would be economically viable.

Segmentation - why is this project being segmented from the larger picture? This project is obviously part of the larger NE corridor + should be presented as such.



DATE 10/13/17

bwmaglev.info

COMMENT FORM

Name: Faye Owens

Address: 8623 Leslie Ave

City: Glenarden State: Md Zip: 20706

Telephone: 478 550-8349 Email: adamfaye@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We are not trying to impede progress, WE ARE PRESERVING OUR HISTORICALLY BLACK COMMUNITY. We don't need MAGLEV. It does not support Glenarden and we don't support it.
DEMOGRAPHIC

- 1) We don't need this mode of travel recurrently
- 2) Our incomes do not support use of this transportation
- 3) IT IS NOT CONVENIENT
- 4) It disrupts our historically black community
- 5) Fair market pricing would not allow CLEAR PURCHASE of comparable housing with regard to EMINENT DOMAIN
- 6) Glenarden is history. Glenarden is legacies
- 7) There has not been sufficient studies w/ regard to health risks



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/13/17

bwmaglev.info

COMMENT FORM

Name: Faye Owens
 Address: 8623 Leslie Ave
 City: Glenarden State: Md Zip: 20706
 Telephone: 478 550-8349 Email: adam1faye@gmail.com
 Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments:

I am voting against MAGLEV as I see no benefit to my community. This train disrupts my low crime neighborhood by taking our homes. Our neighborhood has more than a handful of Centurians and many seniors. My mom lives on her own (self sufficient) and is the youngest of her local friends at 90 years old. Forcing her out of her home and into a rent or mortgage situation is neither an affordable nor viable solution. The value of her property would also be reduced! On top of that recouping costs of this train probably falls back on us and certainly no revenue comes to Glenarden.



DATE 10/13/17

bwmaglev.info

COMMENT FORM

Name: Anita Simmons

Address: 8623 Leslie Ave

City: Glenarden State: Md Zip: 20706

Telephone: 301 772-1811 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Glenarden is a historically black community
and we have built our city up little by little through
hard work during a time when things were not afforded us
on equal terms. It is through hard work and perseverance
that we fought to ensure our city progressively SUPPORTED
OUR NEEDS and allowed OUR PEOPLE comparable benefits
of useful measure and preserved our history. This train
does not stop in our neighborhood, is an unnecessary
acomodation and is not feasible for most of us



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Irene Kaplanis

Address: 2916 Brierdale Lane

City: Bowie State: MD Zip: 20715

Telephone: 240-375-6510 Email: irene.kaplanis@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: There are absolutely no positive benefits of this project to citizens/residents in Bowie or surrounding areas. Baltimore + Washington are basically equidistant - so we drive 20 min to one or the other in order to ride the train? It has many negative impacts to Bowie + surrounding areas - decimation of people's homes, is an eye sore, not practical, environmental impacts + possible destruction of historical properties

THIS MUST NOT GO FORWARD - ITS A COLLOSSAL WASTE OF TIME + MONEY



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Linda Soaper

Address: 1283 AUA Rd

City: Severn State: MA Zip: 21144

Telephone: _____ Email: lmssoaper@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I think the impact of this project is already being felt. Home sales are up in proposed areas, contracts on houses "sold" are being withdrawn. So lives are affected. Where is the electricity going to come from to cool the magnets. What happens when Helium leaks & yes all gases leak. - physics my friend? The world has real issues and Balto/Washington areas have real people concerns that will not be helped by taking Federal, State money. You are selling speed at a high cost for a select few. Are you clear on your mission. Does it benefit the people, community, state for doing what is right or from money, greed, & destruction. Thank you



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Meghanne Schrieber

Address: 3100a wipkeg Ct.

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: Meghanne.Schrieber@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I don't like the Maglev train all it is going to do is destroy homes. I think it is a waste of money. It will increase taxes the maglev train is just a amusement ride with no purpose. You can already get to Baltimore in 15 min with a regular train anyways. Do not make the orange train it is going to increase noise pollution people can't sleep. It has no purpose!!! DON'T MAKE THE TRAIN!!!



DATE _____

bwmaglev.info

COMMENT FORMName: Edwin ReeseAddress: 12500 Markgraf DrCity: Bowie State: MD Zip: 20720Telephone: 301 919 5005 Email: erdaar@verizon.netJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: _____

I recommend that ^① the project be cancelled. It has no benefit to Bowie residents. I recommend ^② that your future meetings indicate how many homes will be removed along each route. ^③ Please inform the community as to whether or not they have an option to sell their property to you if they feel that the line is too close to their home for their comfort. ^④ My current commute is approximately 1 hour long into DC. What would prevent a person from NY displacing DC workers since their commute will be approximately 1 hour as well.

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Johnnay Schrieber
 Address: 9106 Wipkey Ct
 City: Bowie State: MD Zip: 20720
 Telephone: _____ Email: johnnaydonaldson@hotmail.com
 Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: I do not support the building and implementation
of the SCMaglev Train. I do not believe this project
will bring any benefit to my community. Instead,
it will destroy my community, increase my taxes
and reduce my property value. There is no
benefit from this project considering one can
already travel to Baltimore Penn Station in 20
minutes. Adding a reduction of 5 ^{minutes} ~~minutes~~ with
all the collateral damage is simply not justifiable.
In addition, there has been no studies to show
the magnetic fields will not harm individuals
who live near this train. This project will destroy
Bowie and our community with absolutely no
benefit to us.

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Darrell Leeks
 Address: 3605 Jeff Rd
 City: Springdale State: MD Zip: 20774
 Telephone: 301-322-5992 Email: darrell-leeks@verizon.net
 Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: I will like to say that I am in agreement with ~~a~~ high speed transportation, but I donot agree with something that does not benefit a majority of the people it will effect in building the system. This will effect communities, environments, and the financial stability of people in the area. Families and individuals will lose their homes. The digging for the underground tunnels can cause shifts in the ground structure, ~~in this~~ It will cause the increase of rodents in the community. In addition, this may cause an increase in county and state taxes.



DATE 10/15/17

bwmaglev.info

COMMENT FORM

Name: Sanovia Peterson

Address: _____

City: Bowie MD State: MD Zip: _____

Telephone: _____ Email: Sanovia.Peterson@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____



DATE 14 Oct 17

bwmaglev.info

COMMENT FORM

Name: Irene Richmond
 Address: 7933 Fiske Ave
 City: Glenarden State: MD Zip: 20706
 Telephone: 301-322-2191 Email: _____

Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: Since we already have the ACELA TRAIN and Air transportation, I see no need to uproot thousands of people to install a high speed train fm D.C. to N.Y. The train will be of no benefit to the residents of P.G. County since it won't even stop there. Plus most of the residents in Glenarden are seniors and most houses are paid for so where would they possibly find housing affordable to them once they are uprooted. I'm all for progress but this proposal is utterly ridiculous. If Hagan liked the train in Japan so much, let him go there and ride. We don't need it!



DATE 14 Oct 2017

bwmaglev.info

COMMENT FORM

Name: Kathy B. McKenzie

Address: 5302 59th Ave

City: Riverdale State: MD Zip: 20737

Telephone: 301-559-8037 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: For days I'm hearing & seeing things about
Maglev & when checking the street lay out
I see all the streets around me but
not 59th Ave & Carter's Lane. Will my
Street be effected by this proposed transportation
move? What & how will we know if
we have to move & relocate?

My name is Mary McDonough — *Mary Mac 600 @ AOL.com*
301-262-2199 - 301-529-7761

I have been living in High bridge Estates for 61 yrs. My husband developed the original part of Highbridge Estates and my house was the first he built there. I have raised my family here and have enjoyed the comfortable and peaceful way of life here.

I HAVE SEVERAL QUESTIONS TO ASK:

First of all are the questions and answers in these meetings being recorded and made available to everyone?

Second Why is the focus of this project centering on taking private home and businesses and neighborhood parks and places of worship especially as this is not even proven to be a successful venture, and there are other routes that would cause far less destruction of people's lives, such as abandoned railroad beds, highway space, the Agriculture farm. Parks etc.

Third May we have a copy of Federal and State regulations and their authority in this matter and the names of those persons at the top of this group?

Fourth Has the cost of the following been arrived at? 1. The cost of purchasing all of the area from Union Station to Baltimore .2.the cost to consumers to use this rail. Compared to the need and necessity and compared to the service already rendered to us by Amtrak, with no stops between Baltimore and D.C. and probably high fares and has been stated in previous meetings not even a sure thing. What if it fails after affecting so many people's livelihood, way of life, sense of belonging and security?

Fifth Has a toll been taken to give an actual number to how many homes, businesses, schools and playgrounds will be affected? Why haven't we been informed to these numbers if as stated at the last meeting you plan to start construction within two to three years?

Sixth what relocation plans and benefits are being offered and by whom and who is financing this Project?

Seventh Are you planning to take our property as eminent domain? Is this for Benefit of a private Corporation or thru Fed or state authority?

Eminent Domain includes an acquisition and a separate relocation allowance for the property it is two pieces the relocation benefit may not be part of the relocation acquisition.

What about those who may be "underwater " on their mortgage?

May we have the name of the corporation involved in this venture and the names of its directors?

Within the last year property along the BW parkway was transferred from the Department of the interior to the State supposedly with the purpose of supporting the construction of the train.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Sandra McNeil

Address: 3112 DERRICK ST

City: Upper Marlboro State: MD Zip: 20774

Telephone: _____ Email: ms sandydee@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I have elderly parents and inlaws in Landover Hills and Springdale who would be negatively impacted by this project and have NO benefit to them or their communities where they have lived for 50 years. This project will negatively impact home sales from this day forward impacting their ability to get the real value for their property at a crucial time in their lives where they may need those funds.



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: ALBERT BAUMANN

Address: 12811 BRUNSWICK LANE

City: BOWIE State: MD Zip: 20715

Telephone: (301) 503-5877 Email: stringtrio@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The fact that this project has progressed this far is an outrage!

What a waste of money!
This project will benefit an elite few in Wash. or Balt. It has no value to any local residents.

Put funds for this in road, bridge and railroad improvement that will benefit our communities.

Stop this crazy project immediately!



DATE 5/14/17

bwmaglev.info

COMMENT FORM

Name: Steve Devine

Address: 13211 Mockingbird Ln

City: Bowie State: MD Zip: 20720

Telephone: 301 262 3574 Email: STEVE BOB DEVINE AOL.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Why isn't MARC or Acela enough
to satisfy rider ship - will maglev
provide a better and economical
ALTERNATIVE?



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: MAINTAINANCE FACILITY IN THE COUNTY
 ARE YOU KIDDING! NO WAY
 THERE IS NO BENEFIT TO
 Residents of the County from
 a transportation STAND POINT!
 NO STOP — NO TRAIN — NO
 MAINTANENCE !!!

PRICE NEEDED TO BE AFFORDABLE
 OR THE RIDERSHIP WILL NOT
 BE ABLE TO PAY FOR THE PROJECT



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Jeanette Harris
 Address: 14802 Royal Crossing
 City: Bowie, Md. State: Md. Zip: 20715
 Telephone: 301-262-8040 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I don't want to see our neighborhood
tear apart. Only the elite will be able
to use this mode of transportation.
Fix and add to what we already have
in place for transportation! How dare
you try to do this! One more time
"money talks and the "people" are
ignored." What impact does it have
on our taxes?!" Use money to do improve-
ments!!!

Please just go away!!



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: *dutch 444 @ msn.com*

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: *Your Display shows +1500 jobs MD,
 +1500 Baltimore is this the same 1500 jobs
 AND ARE you double counting*

Maglev Comments

October 14, 2014

to:
Concerned Persons.

I am responding to the Maglev project, with another viewpoint.

We live in the Creator God's world implemented by principles, laws, commandments and precepts. Man is God's greatest creation, made to function in a prescribed environment. There comes a time when people need to know when to quit and submit to the principles and plans of our creator God.

We are commanded to treat our neighbors as we would treat ourselves. Are you OK with uprooting your home and family of 40-50+ years to find another home? What about the impact of comparable value, surroundings, businesses, churches, schools and undue pressure of starting over at mature ages of 65-97+? What about the younger persons who have recently purchased homes and are raising families? You would be OK in this situation?

When Jesus left this earth, He commanded us to love one another as he loved us. God loves us. Is this fast train showing love for one another when the end product affects the quality of many lives? Jesus Christ's purpose is to manifest his glory that is inside every human being.

What you may call growth, God calls his glory exposure—having the right attitude and perspective in every situation; and being a blessing to everyone we meet. God gives man gifts and talents to be a blessing to Him and his creation. Be sure your path is not one of fame and fortune or pleasure and power.

{ Be mindful that we reap the seeds that we sow, whether good or bad. God says "woe to those who call evil good, and good evil. "Woe to those who make unjust laws – who issue oppressive decrees etc."

What will you do on the day of reckoning when disaster comes from afar? "to whom will you run for help? "To whom will you leave your riches? All these questions are asked by Isaiah in Chapter 10. "Come, let us reason together!"

{ God has given the earth to man to dwell together in unity. I believe we can do just that! He has given man wisdom and knowledge. Let's not let each other down, or God. We need one another and we certainly need God!

May the God ^{of} Peace continue to shower us with His Blessings and Love.

Lionella Houser
Lionella Houser
3502 Gary Ct -
Glenarden, MD 20774
Springdale



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: JAMES BAILEY SR

Address: 1001- 60th AVE.

City: Fairmount Heights State: MD Zip: 28743

Telephone: 301-925-4027 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: NO TRAIN, NO TRAIN, NO TRAIN.

NO TRAIN

⊙ ⊙



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Jeanette Walters

Address: 4400 Huntchase Dr

City: Bowie State: MD Zip: 20780

Telephone: _____ Email: 4400hcd@hoanetworks.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ~~The~~ Do Not want Maglev At All!!
There is no demand especially with
more and more people working from
home!



DATE _____

bwmaglev.info

COMMENT FORM

Name: GREG

Address: ROCKINGHAM LANE

City: BOWIE State: _____ Zip: _____

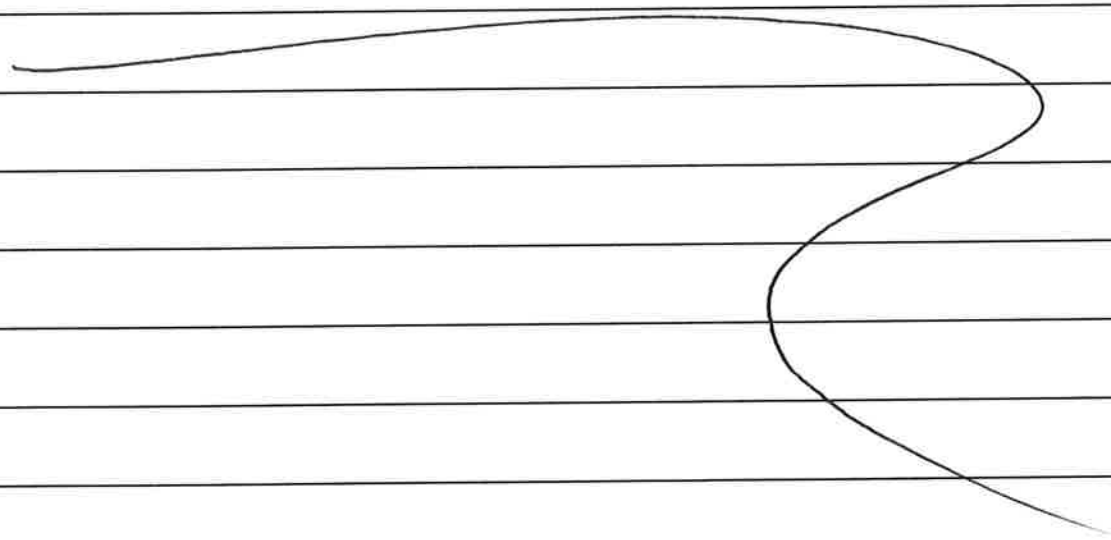
Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

NOT THRU OUR NEIGHBOORHOOD





DATE _____

bwmaglev.info

COMMENT FORM

Name: James Duckett

Address: 7913 Piedmont Ave

City: Glenarden State: md Zip: 20706

Telephone: 240-731-0087 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Stop try to taking homes.
We need ~~low~~ homes. Think about people.
~~we~~ We need a place to live in stead
of the maglev Train



DATE 14 Oct 2017

bwmaglev.info

COMMENT FORM

Name: Melvin W. Fields

Address: 8628 - Glenarden Pkwy.

City: Glenarden State: Md. Zip: 20706

Telephone: 301-341-9388 - Home
301-233-3056 - Cell Email: Key9824@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The historical community of Glenarden have
Twice endured The Transit developement through our
community. The first was George Palmer Highway (now -
Martin L. King Highway), and The second was Interstate
495 (Capital Beltway) which divided our Community.
Many homes were uprooted which included businesses.
The propose route of the Maglev Train would Totallly
wipe out our historical black community. Schools,
Churches, Government Buildings, and many ~~businesses~~
will be uprooted. Three strikes and we are out.

signed,
Melvin W. Fields



DATE Oct. 14, 2017

bwmaglev.info

COMMENT FORM

Name: Jelena Markovic

Address: 4906 Reston Lane

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: jkannula@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

People are concerned about their style of life. This was not represented.

1) For each root (E, J) and J) what is the better area, area where NO work, or housing will exist, people who'd have to leave, businesses move? This is a general question, not tied to a particular root, and therefore can and should but were NOT answered. Stud HDOT does refer to a similar trains in Japan and Germany, so there IS already impact studies available. So I say NO to it,

because I do not see PEOPLE interest to be taken into account



DATE _____

bwmaglev.info

COMMENT FORM

Name: Kim Frazier

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____
- Need to have opposing viewpoints at meetings.

- Need to have a Q&A session for participants

- Need to talk about successes of other like projects in "this country!"

- This is an environmental justice issue



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Kevin Walters

Address: 4400 Huntchese DR

City: Bowie State: MD Zip: 20720

Telephone: 301 262 3165 Email: KWALT@HONANETWORKS.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

This does NOT appear to be necessary!

- no real defined need
- too many ALTERNATED TRANSP. options
- APPEARS DRIVEN BY A BANKRUPT ~~USA~~ Japanese firm - currently exp. problems
- opposed by a majority of residents
- Why Prince Georges County?
- Why not Howard, Ann Arundel?
- No \$ should be expended for this!



DATE 10/14/19

bwmaglev.info

COMMENT FORM

Name: Letitia Carolina-Powell

Address: 8107 River Gate Ln

City: Bowie State: MD Zip: 20715

Telephone: 202-256-0262 Email: L@carolinapowell@msn.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am against this project. It
does not make financial sense. It will not
benefit the residents of Prince George's
County and will not reduce carbon emissions
as ^{local} commuters will not ride this train.
Amtrak is subsidized by the Federal
gov't and MARC is subsidized by the
state. How will this train support itself
without subsidies? This project is
not good for Maryland.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Dwain F Wright

Address: 4822 Dearwood DR

City: Capitol Hgts State: MD Zip: 204

Telephone: (301) 502-6122 Email: thcdw@comcast.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: NO - Not interested in this project



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Forbes

Address: ~~12345~~

City: Glenn Dale State: MD Zip: 20769

Telephone: 301-262-1958 Email: gforbes1925@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____
 Stop the Train, Save our properties, Save
 our Communities. This idea will cause
 land vibrations and disturbance to the
 entire area.



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: GAIL FORBES

Address: ~~XXXXXXXXXX~~

City: Green Dale State: MD Zip: 20769

Telephone: 301 262 1958 Email: FORBESGEE@Verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: STOP! the train. Do not want
the train. Does not make sense
to destroy properties + families for
this train. Do not take properties



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Pam Jenkins

Address: 12326 Lanham Severn Rd

City: Pooie State: MD Zip: 20720

Telephone: 301 728.1005 Email: pjenkins728@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The new route that came out on 10/13/17 which comes down ^{Route} 564 will totally take out my home that my husband, no longer (Niz), built this home to be a legacy for the family and I have no desire to ever sell. For years 15+ we have had community outreach picnics to bring the communit and family together and now you want to totally totally destroy it. I strongly oppose not only on my neighborhood, but in state of MD - this has no benefit to our communit.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Michael F. McDermott

Address: 12303 Rockledge Drive

City: Bowie State: MD Zip: 20718

Telephone: 240-893-8136 Email: groveroue@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

- ① The upcoming Acela train will fill the need for high speed rail, MAGLEV is not necessary
- ② There is no proof that MAGLEV will be profitable and could leave MD taxpayers on the hook for the construction costs
- ③ The only funding available right now is a loan from Japan
- ④ No MAGLEV train in the world has been financially successful without 3rd party subsidies



DATE _____

bwmaglev.info

COMMENT FORM

Name: Donthry Duckett

Address: 7903 Piedmont Ave

City: Glenarden State: md Zip: _____

Telephone: 301-503-1986 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Thank about you are hurting.



DATE 10/19/17

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The Politicians in P.C. County
were unfair not to inform All
of the citizens of this eventual project.

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Joseph Lambert

Address: 3507 Tyrol Drive

City: Glenarden State: MD. Zip: 20774

Telephone: (301) 273-5504 Email: Lambert Joseph 03 @ Yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: As a Tax Paying Law abiding Citizen,
Let me First say I'm Appalled & Disappointed!!
Our Elected Officials have NOT done more
First to make us aware of this Project earlier!!
I and many of the Citizens of Glenarden/P.G. County,
Feel as if this Project is going through a
Back-door!! Meaning you're trying to keep the
Citizens out of the loop!! Especially the African-
American community! This is not Right!!
I am not opposed to Progress!! I am against
Sneaky, money-hungry elected officials!! Hopefully
our voices will be heard Today & Furthermore.
I would hope our wishes will be Respected



DATE _____

bwmaglev.info

COMMENT FORM

Name: Barbara Ford

Address: 7906 Piedmont Ave

City: Calverton State: md Zip: 20706

Telephone: 301-322-2325 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We need owl Home and Work
Place

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Frantz & Alicia DerenoncourtAddress: 12323 Quarterback CourtCity: Bowie State: MD Zip: 20720Telephone: 301-257-4895 Email: alicias18@gmail.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: This train talk has been a disaster from the beginning. Not once have I personally received information about this train via phone, email, or mail. All info received has been via neighbors. I am fully against this train and ~~any~~ any new construction it would bring including all the proposed routes. Bowie already has a train rail that can be utilized. Destroying neighborhoods, homes, schools, resources, schools and other neighborhood facilities is harmful to our way of life.

This train brings nothing to Bowie but destruction. We will not benefit from it.

I am fully opposed to this project.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Julie Kelly

Address: 12512 Windover Turn

City: Bowie State: MD Zip: 20715

Telephone: 301.352.3385 Email: jellyrace@comcast.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The potential maintenance yard location will disrupt the ecosystem of the Patuxent Research Refuge. The environmental impact on the surrounding communities will be detrimental.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Mayor Lillie Thompson Martin
 Address: 6100 Jost St,
 City: Fairmount Heights State: MD, Zip: 20743
 Telephone: 301-925-8585 Email: mayor@fairmountheightsmd.gov

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The Maglev Project is not for our community
We want to express the sentiment of our neighbors who
say send the train in another direction. We have entirely
too many seniors who live in historic homes that will be
shattered by disruption of their lifestyles and homes.

Stop the Maglev!!!!
Think of Our Historic Homes
And Seniors



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Bagarias

Address: 3801 Chapel Forge Drive

City: Bowie State: MD Zip: 20715

Telephone: 301-262-9409 Email: gfwb207@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Too expensive
not needed
fix Amtrak = Acela safety issues



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: Beth Moore
 Address: 110 Twin Oaks Road
 City: Linthicum State: MD Zip: 21090
 Telephone: 410-570-8387 Email: beth.moore022087@outlook.com
beth.moore022087@outlook.com
 Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: I've been a Linthicum Hts Resident for 20 years with my family. Linthicum was supposed to be a Historical District. How can you be ~~along~~ ^{allowed} to destroy a Historical District? We have the Light Rail that has brought CRIME to our neighborhood. We were Lied To Crime would not be an issue. We already have BW F the Bw Parkway & 695. The property value will drop. The family has lived and planned to live here forever. This Maglev will destroy the entire Linthicum, HTS are. How can you destroy people, families and childrens neighborhood. ENOUGH IS ENOUGH
Put the Linthicum residents to PUT UP WITH!
THANK YOU.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Ann Brennan

Address: 4802 Royal Crossing

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: annbrennan51@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This is a bad idea - It seems the train will
only be used by a few elite people
You will destroy + destroy Bowie neighborhoods
I have worked hard to buy my home and
I don't want to see it destroyed
Who needs to get to Baltimore in 15 min.
This is unreasonable & does not make sense
to the people who will be impacted.
DO NOT BUILD THIS TRAIN
SAVE BOWIE AND OUR HOMES!



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: EI- would directly impact my home,
my property value and my quality of life.
I plan on doing everything possible to
see that this project cancelled.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Lu Ellen Conti

Address: 6820 Decatur St

City: Hyattsville State: MD Zip: 20784

Telephone: 301-322-8010 Email: lconti6800@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: It is unfortunate that the
Woodlawn and Beacon Heights
communities were not informed
about MAGLEV until last month unlike
other communities that were informed last spring
I believe this is based on race and income
and is discriminatory.

We demand a meeting specifically
with the Woodlawn and Beacon Heights
communities immediately to inform us
about the impact and ramifications of
this project + allow Q+A.

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Diana Furey
 Address: 3309 Moylen Dr
 City: Bowie State: Maryland Zip: 20715
 Telephone: 301 706-9289 Email: nikkoskye.diana@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I Am Totally opposed to even the thought of this high speed train going thru not only Bowie but the whole project should not even be on the table for discussion. This Train has not been a success in any country that has built & tried it. It will not be a success in the U.S. either. We do not maintain the already trains, METRO roads, bridges that are existing. Why tear down communities. Just fly or drive or take the train that already exists. That money that it's going to cost could be put to use in much better ways than this train. This MAGLEV train is a REAL SHAME!
And I AM NOT FOR THIS PROJECT



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: VIKKI WELLS

Address: 6709 GREENLAND ST

City: Rwerdale State: MD Zip: 20737

Telephone: 301-731-5149 Email: vwells107@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We were not notified that this was happening in our neighborhood. No info has been put out. We found out by accident. This is wrong. This has a direct effect on our livelihood & those in charge of this project have not taken us into consideration at all. We are losing our homes & no one seems to care. Stop this train. It will not benefit anyone that may lose their home. We have a Historical Black Cemetery in our neighborhood which will be directly effected.



DATE 10/17/2017

bwmaglev.info

COMMENT FORM

Name: Rahnesha Mitchell

Address: 13302 Kelly Marie Court

City: Bowie State: MD Zip: 20740

Telephone: 73-368-7251 Email: rahnesha@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am totally against the SC Maglev. It will take away parks, historical sites, schools and some homes!! Move this train to another location (Outside PG County). I Am against the SC Maglev train!



DATE OCT. 14/17

bwmaglev.info

COMMENT FORM

Name: Kathleen Brockway
 Address: 13428 Yorktown Dr
 City: BOWIE State: MD Zip: 20715
 Telephone: _____ Email: Kathleen.Brockway@
 Join our mailing list? Yes No gmail.com

Preferred method of communication? Email Mail

Comments: CREATE captions for videos posted.
Set ASL Interpreters at ALL meetings and
post notification of "ASL interpreters available"
on every meeting post. Too much hassle
to track down DOT who to ask for
interpreters!

The video today was forgot to turn on captions!!

Not only Deaf but Senior citizens need it!

CAP TIONS/Interpreters!

Does blessings upon your doings unto others, as you would they do unto you!



DATE Oct. 14, 2017

bwmaglev.info

COMMENT FORM

Name: Lionella Houser

Address: 3502 Gary Ct.

City: Glenarden State: MD Zip: 20774

Telephone: (301) 773-7125 Email: —

Join our mailing list? Yes No
if possible

Preferred method of communication? Email Mail

Comments: God made this earth for man to dwell in
harmony. Why are we always in a rush to go to a
meeting/work. Take the route of least resistance.
— Are you ok with uprooting your family of 40-50 yrs?
— Growth should be in accordance with God's
glory, His principles and commandments.
— Love + treat your neighbors as you would yourself.
— Be mindful that we reap the seeds that
we sow. Our Bible says: "woe to those who
issue oppressive decrees and laws.
God gives man His wisdom. Find alternative ways
to fulfill needs. We can do this! Keep trying!
together we can do this and walk in love
and harmony.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: CHRIS BODIFORD

Address: 13305 COAMORAWT PL

City: BOWIE State: MD Zip: 20720

Telephone: _____ Email: cbodiford@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Concerned about how routes will
be prioritized if final 3 routes are
removed from consideration. If it comes
to competing interest of residents vs. Amtrak
vs federal government, the resident should
take priority. Amtrak is a business
so money can reconcile their concerns.
Fed govt should be easier to work with
concerning the BWF Parking options
Thank you



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Do the BWI routes. Its much less impactful on communities



DATE Oct. 14, 2017

bwmaglev.info

COMMENT FORM

Name: Letha Strothers

Address: 6608 Lisa Lane

City: Bowie State: MD Zip: 20720

Telephone: 301-809-9299 Email: letha.strothers@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I oppose the red and orange lines show on proposed maps as they would affect homes, property, parks, and schools in our community. While the train would not run through our neighborhood it affects our neighbors, friends, small businesses, parks, churches, and schools in our community. Route runs through/near Bowie State University, a historically Black University.



DATE Oct 14, 2017

bwmaglev.info

COMMENT FORM

Name: JAMES DUNCAN

Address: 6608 LISA LN

City: BOWIE State: MD Zip: 20728

Telephone: 301-809-9299/661-0741³⁰¹ Email: james.raymond.duncan@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ALTERNATIVE E1. DON'T LIKE THIS!
TOO CLOSE TO BOWIE STATE UNIVERSITY,
~~METAL~~ MY CHILDS SCHOOL HIGH BRIDGE ELEMENTARY
SCHOOL IS TOO CLOSE TO ROUTE

NO AMTRAK E1,

ALSO CONCERNED ABOUT OLD BOWIE WHICH LOOKS
LIKE E1 is going right through it,



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: Bowie State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Re: Today's sessions
Well organized, lots of staff, good signage

FRA rep made opening comments before 1st video
I couldn't hear them in the Open House room.
Disappointed they weren't being repeated w/
each showing of video.

Info being given out by state folks
is not consistent w/ what BWRR folks
were saying re: timeline, facilities,
etc. Different state reps also gave
conflicting info to some questions.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Gloria T. Pickett

Address: 4018 92nd Avenue

City: Springdale State: MARYLAND Zip: 20774-2505

Telephone: (301)322-9131 Email: pickettj1@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The train should be stopped if
it does not use existing rail lines.
We already have sufficient
transportation in our area that
is able to take folks up & down the
N.E corridor. An added train that
will have a negative impact on the
existing communities and the
environment is shortsighted.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Deborah Eason

Address: 2809 Red Oak Lane

City: Glenarden State: MD Zip: 20706

Telephone: 301.322.2181 Email: deborahason100@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ^① Can you confirm that all comments via website are included in your study. Even though the WB&A Route, which includes the City of Glenarden was removed from the proposed routes (today). ^② What assurance do citizens have that this route will be totally removed and no longer considered for this project? ^③ This project does not solve congestion (traffic problems). Waste of taxpayer money.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Barbara Parthasarathy

Address: 15101 Roving Wood Dr

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: be.parthas@comcast.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: only one choice:

NO BUILD!



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

I'm insulted that you thought that we were going to stir up so much trouble that you had to bring in so many police officers to placate the masses.

Shame On You



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: Daisy M Chadwick

Address: 6803 Emerson st

City: Riverdale State: MD Zip: 20737

Telephone: 301-577-6850 Email: ChadwDais@Aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The mag Lev train
What is The use For it? Train, we Can Driving
To New York, wht should I give up my house
To make it Better For u. I Live in Beacon High
that come through My Home I Live there 42 yr
And I am not Willing To give it up, and the Noise
disrupt,



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Pamela Wright

Address: 3508 Brightseat Rd

City: Landover State: MD Zip: 20781

Telephone: 301-772-6322 Email: Pamela.WRIGHT@em.doe.gov

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

I object to any project that removes
 uproots a family out of a home
 they have lived in for 50 yrs. This
 project is not wanted in the
 Steward area if that's the
 case.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Gael Swick

Address: 6305 Gabriel St.

City: Bowie State: MD Zip: 20720

Telephone: 301-464-8569 Email: gaelswick@comcast.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: 10-12 billion dollars ~~from~~ to build
a line from Baltimore to Washington
is much more than will ever be recovered.
This project is financially unsound and
will not serve the people who live
here.

Having an elevated train travel in
the middle of suburban neighborhoods
will destroy them. Would you like
a train going 300 mph in your front
yard?



DATE 10/4/17

bwmaglev.info

COMMENT FORM

Name: Curbean Family

Address: 1515 7th St.

City: Glenarden State: MD Zip: 20706

Telephone: (240) 770-5542 Email: tauche2002@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: We are totally in favor of establishing
the new railway system along the Baltimore
Washington Parkway corridor. There is a vast
amount of land available near the
Agriculture center in Beltsville as well as
along the Parkway which would not affect
the community as much



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Gwendolyn E. Wright

Address: 3508 Brightseat Rd I

City: Lanover State: MD Zip: 20785

Telephone: (301) 772-6322 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

Do NOT WANT IN MY area

Do not start This



DATE 10.14.17

bwmaglev.info

COMMENT FORM

Name: Boyer

Address: Adwell Woods

City: Bowie State: MD Zip: _____

Telephone: _____ Email: Yonna.Boyer@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

I LIVE in the community this is directly impacting, Old Town Bowie Historical Bowie - I AM NOT IN SUPPORT of this AND will be telling all my neighbors to protest this.

We Receive NONE of the benefits AND ALL the impact - you can count on a Fight.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Kenneth Aderotoye

Address: 8204 River Run dr

City: Bowie State: MD Zip: 20715

Telephone: 240-417-9685 Email: Ken_ade@hotmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Looking at the map and benefits that
comes with the train, I do support it.
However I will suggest that if the line or the
track ~~use~~ use BW parkway or I-95 corridor will
be more idea. In other words affected residential
area will be limited.

Thanks


DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Shavesha RutledgeAddress: 9119 Myrtle AvenueCity: Bowie State: MD Zip: 20720Telephone: 240472 6080 Email: shavesha@gmail.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: We do not desire to have this train in our city, in our county or in our state. Please consider building this train in an area that will not destroy or disrupt communities. Or consider not building the train at all. This train is not feasible. I do not know a single person that would pay to ride from ~~DC~~ DC to Baltimore. It is my understanding that this train will not work because it needs a straight line to travel. There are no planned paths that have a straight line. This train will be attempted to be built and will never be completed. Making all citizens of the State of Maryland responsible for the costs through our taxes. This is a bad idea!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: J Streeker

Address: 4403 Oakview Lane

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: yackie.streeker1984@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ~~It~~ It does not make sense for someone to drive from Bowie, Odenton, Glenarden, etc to either Wash DC or Balt. MD to take a train to the opposite city. Less than 10% of the commuting population live in one or the other of the two cities to make this a viable choice for their commuting needs. IF THEY COULD AFFORD THE COST



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Marilyn Feldman

Address: 3509 Mase Lane

City: Bowie State: md Zip: 20713

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: In addition to the environmental
impact, especially through the Patuxent
Refuge, we now have train service
from D.C. to Baltimore that takes
20 minutes. To go through all this
expense and construction to save 5 minutes
is absurd.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: JEAN KRIVAK

Address: 6211 GILFON ST

City: BOWIE State: MD Zip: 20720

Telephone: 301-262-1490 Email: QBKRIVAK@VERIZON.NET

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Why? Why? Why?
IT WILL CHANGE OUR WAY OF LIFE --
GETTING FROM POINT TO POINT (RAILROAD
TRACKS ETC)
WHY NOT SPEND MONEY WIDENING 295
AND OTHER ROADS TO MAKE IT QUICKER
GETTING TO BALTIMORE/WASHINGTON.
MAKES NO SENSE TO DISRUPT
HOUSES TO PROVIDE FOR A FAST WAY TO TRAVEL.
WHO IS PROVIDING THE MONEY TO
PAY FOR THIS?
I AM TOTALLY AGAINST THIS PROJECT!!



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Ida S. Williams

Address: 8901 Ardwick-Ardmore Rd

City: Springdale State: Md. Zip: 20774

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: would like to kept informed of latest developing concerning this project.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Louis & Gladys R. Horne

Address: 1512 1st Street

City: Glenarden State: md Zip: 20706

Telephone: 3/773-2787 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: this train will not benefit any home
owner at all in the areas in which it going to ^{be} remaining
be



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: JOHN PIETRAS

Address: 12606 QUAKING BRANCH CT.

City: BOWIE State: MD Zip: 20720

Telephone: _____ Email: JVPLISTS@COMCAST.NET

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ~~EVER THOUGH THE NO ISOLD OPTION IS STILL ON~~

I'm NOT SEEING STRONG ENOUGH JUSTIFICATION FOR THIS.
WHAT'S THE MARGINAL GAIN OF THIS OVER EXISTING MASS
TRANSIT BETWEEN DC, BWI, + BALTIMORE (AMTRAK, MARC)?
~~WHY ISN'T~~ IF THE EXISTING CAPABILITIES OF AMTRAK +
MARC AREN'T ADEQUATE, WHY NOT BOOST THOSE? OR FIX
THE TRACKS TO SUPPORT HIGH(ER) SPEED RAIL? WHAT WILL
BE THE DIFFERENCE IN TICKET PRICES TO JUSTIFY "ONLY 15 MINUTES"?
{ I.E., WHAT IS THE POTENTIAL MARKET? } I LIKE IN THE MD
SUBURBS + WANT TO GET TO BALTIMORE OR BWI, ~~THE~~ TODAY I'VE
GOT NEW CAROLTON + BSO TO CATCH A TRAIN. WITH ~~THE~~ THE MAGLEV
TRAIN, I'D HAVE TO GO TO DC FIRST.
THIS LOOKS LIKE THE 21ST CENTURY VERSION OF THE SST-



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Andre NAUDE

Address: 6910 Beacon Light Rd

City: Riverdale State: MD Zip: 20737

Telephone: 202 409 6938 Email: Mr Beachkitten @ AOL.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: No not in my neighborhood.

You are wasting money that can go for other purposes,
Improve Metro / Antrac.

This line will only serve ~~at~~ very few; and when
will it show A profit, And then who will
benefit.

So don't piss on my leg and tell me it
raining.



DATE 10.14.17

bwmaglev.info

COMMENT FORM

Name: Benique Muckelvene

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: mommienique@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The underground rails are projected to run under schools, PUBLIC SCHOOLS that have a majority of minority children attending them. These rails will also run through Cemeteries, what are you planning on doing with these schools and Cemeteries.

- Old Town Bowie?
- Seabrook Shopping Center?
- Bladensburg Waterfront Park area?
- Subsidized Housing?

I'm sure you can read my frustration in my handwriting.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Lena F. Peterson Naude

Address: 6910 Beacon Light Road

City: Riverdale State: MD Zip: 20737

Telephone: 301-580-6710 Email: BeachKitt@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Where are the ridership studies?! No one
will take this train. Maybe the RRT?
You cannot destroy neighborhoods, businesses
for a PRIVATE FOREIGN company!
I do NOT want a train even going
under my home.
Stop this project.
Watershed area & parks should not have
trains going through them.
'15 minutes to Baltimore?' The Acela Express
ALREADY does this in 20 minutes. Take that.
NO TRAIN THROUGH RIVERDALE!



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 15 Oct 2017

bwmaglev.info

COMMENT FORM

Name: William Dunn

Address: 14610 Arabian Lane

City: Bowie State: MD Zip: 20715

Telephone: 301-464-1781 Email: willhdunn@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: PG County is the "billpayer" for this project. How does it help PGC? More noise, loss of environment, ^{more} cost, and we can't even use it! Taxation without ^{accessibility} ~~representation~~ transportation. What is the need for this? Did the project do an analysis of alternatives? My opinion is that improving is a much wiser alternative because much of the infrastructure is already there. I use Metro daily and on a good day, it works great. But it is fraught with many problems - one of which is rising costs with less service. I can't imagine the escalating operation & maintenance costs with



DATE _____

bwmaglev.info

COMMENT FORM

Name: Robney M. Roberts

Address: 38 m Ridge Rd

City: Greenbelt State: md. Zip: 20770

Telephone: 240-375-4947 Email: Rmk38m@aol.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: _____

 very bad idea

 this project should be

 killed! it has no public

 benefit



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: William Proctor

Address: 1415 2nd St

City: Glenarden State: MD Zip: 20706

Telephone: 301 322-8680 Email: wgpinc@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I would like as much detailed information as soon as it becomes available to you.

→ I Am opposed to the construction of this maglev project.

* How will my house be impacted? *



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: William Proctor

Address: 7111 Maryland St

City: Londover Hills State: MD Zip: 20784

Telephone: 301 322-8680 Email: wgpiuc@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I would like as much detailed information as soon as it becomes available to you.

→ I Am opposed to the construction of this project. (Mag/Lev)

* How will my house be impacted?



BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Sarah A. Smith

Address: 8903 Bold St

City: Springdale State: MD Zip: 20174

Telephone: 301 322 1732 Email: ajajustin@msn.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My concern is regarding the impact on my community (District 5). It seems that the project will impose on private property. In addition, there are no stops in Prince George's County. The super train will not benefit residents in Prince George's County.
The train stops in Washington DC and Baltimore.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Marilyn Feldman

Address: 3509 Mase Lane

City: Bowie State: md. Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The cost of the project is more than any
benefits the people will get. There is already
sufficient service without adding the disruption
and expense this project would entail. I
strongly object to the project.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Pam Jenkins
 Address: 12326 Lanham Severn Rd
 City: Bowie State: MD Zip: 20720
 Telephone: 301.728.1005 Email: pjenkins728@gmail.com
 Join our mailing list? Yes No
 Preferred method of communication? Email Mail

Comments: The new route that came out on 10/13/17 which comes down Route 564 will totally take out my home that my now deceased husband built this home as a legacy for the family and I do not intend to ever sell. For years 15+ our home has had community outreach and on my 5+ acres and now my home is in direct path to be destroyed by the SCMaglev. I strongly oppose the SCMaglev



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: GERALD M. FELDMAN

Address: 3509 MASE LANE

City: BOWIE State: MD Zip: 20715

Telephone: 301-262-9062 Email: gfldm@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: UNNECESSARY PROJECT, EXTRAORDINARILY DISRUPTIVE
AND COSTLY. OPPOSED TO ENTIRE CONCEPT



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Robert Snyder

Address: 12-A Hillside Rd.

City: Greenbelt State: MD Zip: 20770

Telephone: 240-481-9190 Email: rsnyder@umd.edu

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I think more folks would benefit & use
(and thus support) the BW Mag line if there
were a stop/station just inside or outside
the Beltway, since there are a number of
state & federal facilities nearby, such as:
NASA Goddard, BARC, IRS (New Carrollton), Univ. MD,
possible FBI HQ in Greenbelt or Landover, etc.. So it
might be a 20 min. commute from downtown DC to
Balt. with 2 stops (BWI + Greenbelt/New Carrollton)
rather than the 15 min. one stop (BWI) plan.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Joyce Benjamin

Address: 750 Old Chapel Drive

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: Joycejbenjamin@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I want it to be known that
I was never informed in writing
about this project. The environmental
impact about this project was
never sent to me in any form.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Anna Rutledge

Address: 9119 Myrtle Avenue

City: Bowie State: MD Zip: 20720

Telephone: 2404726086 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I do not think this idea is the greatest because it will destroy ~~it~~ our ~~own~~ community. ~~I am~~



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: ANDREW SYKES

Address: _____

City: SPRINGDALE State: _____ Zip: _____

Telephone: _____ Email: ~~09576~~ ageri2882@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail NO TO THIS PROJECT!

Comments: What is the status of the other states

to this technology?

Use the \$85 M or whatever the price of this

Project is and upgrade the existing

MARC, AMTRAK, METRO & ACELA systems

& lines w/ safety & speed!

There is no benefit to Prince GEORGES

County. At least when metro was built

you have stops throughout D.C

which was the purpose of the lines.

It'll lines go under a cemetery in

the Riverdale AREA - That's a No

starter! no matter how deep nothing

should be under burial grounds!



DATE 10-14-2017

bwmaglev.info

COMMENT FORM

Name: Ms. Eleanor G. Jones

Address: 16018 Capital Blvd. B-313

City: Bowie State: MD Zip: 20716

Telephone: 240-716-1286 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: MDOT
line in Princess Anne's County
We do not MDOT Excalibrator Train
Stop it Now.

Eleanor G. Jones

DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Elizabeth + William Pocock

Address: 12610 Hillmeade Sta Dr

City: Bowie State: MD Zip: 20720

Telephone: 3014645684 Email: william.pocock@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Why are you building a line that will disrupt the lives of so many Marylanders? Most of these Marylanders will never even have access to this train. They would have to drive to DC or BALTO and then the cost to ride would be prohibitively high. What will happen to this structure when there are not enough riders to cover the cost to operate and maintain. Will the taxpayers of Maryland be the ones forced to pay this bill to keep it operating. We already have numerous ways for people to travel between Washington and BALTIMORE. This will ultimately become an eyesore for our state leaving a bitter taste to the Marylanders who have had their lives disrupted.

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: THOMAS FONTANAAddress: 9200 FIKKY COURTCity: BOWIE State: MD Zip: 20715Telephone: 301-464-8730 Email: fontat@aol.comJoin our mailing list? Yes NoPreferred method of communication? Email Mail

Comments: I understand that a final decision
won't be made until early to mid 2019 on
which ^{proposed} line to use. ~~Even~~ This delay also
has an impact on property values, and
also on putting one's house up for sale.
The delay in ^{the} decision will increase the
probability of people not interested in
your property or pushing down a potential
offer. Even a rumor that your house may
be impacted may ^{adversely} affect home sales or
drive down the price that you could receive.
Better to make a solid decision ^{right} ~~start~~ away
than later.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Mary Holt

Address: 2913 Braddock Lane

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: maroholt@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Concern regarding funding of project. Actual demand
for high speed transportation directly between Baltimore &
Washington. Concern for home values & impact of
construction on traffic.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Will attend future meetings

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The current concern is related to the sound generated from the displaced air.

Also what is the potential and impact of derailment at a train going this fast.



DATE _____

bwmaglev.info

COMMENT FORM

Name: DAVID MOZURKEWICH

Address: 9310 DUBARRY AVE

City: SEABROOK State: MD Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: TODAY I HEARD ~~PASS~~ ESTIMATED PASSENGER COSTS VARYING BY A FACTOR OF 5.

^{BIGGEST PROBLEM}
THE ~~ISSUE~~ WITH THIS SYSTEM IS NOT ENVIRONMENTAL IT IS PROGRAMMATIC - AFTER CONSTRUCTION, HOW DO YOU MAINTAIN THE RIDER BASE? HIGH COST AND FEW ACCESS POINTS IS A MAJOR CONCERN. WHAT HAPPENS IF THE ~~CON~~ SYSTEM GOES BANKRUPT? ETC

I WANT TO RECEIVE ^{YOUR} PRELIMINARY OPERATIONS PLAN.



DATE _____

bwmaglev.info

COMMENT FORM

Name: DAVID MOZURKEWICH

Address: 9310 DUBARRY AVE

City: SEABROOK State: MD Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: HOW DO YOU COOL THE MAGNETS
WITHOUT CREATING A SAFETY PROBLEM IN A TUNNEL.
- THE MAGNET ~~AI~~ FAILING WILL EVAPORATE THE LHe
DISPLACING THE OXYGEN.



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Lynette Fullerton

Address: 9310 Dubarry Ave.

City: Seabrook State: MD Zip: 20706

Telephone: _____ Email: brrdzenbeads@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I can't justify the expense, the disruption, the destruction that would be caused by this, for no benefit to anyone that would be affected. It's not needed - you talk about redundancy?



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: PATRICK AHERN

Address: 14809 ARABIAN LANE

City: BOWIE State: MD Zip: 20715

Telephone: 301-395-8550 Email: PATRICK.AHERN25@GMAIL.COM

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: ① DUE TO THE IMPACT ON HISTORIC OLD BOWIE, I WOULD DEFINITELY WANT TO SEE THAT ALTERNATIVE ROUTE ELIMINATED.

② WITH THE ADDITION OF AMTRAK'S UPGRADED ACELA TRAINS (160 MPH) COMING IN 2021, I BELIEVE THE ONLY REASONABLE APPROACH IS GO WITH THE NO BUILD OPTION. MY BELIEF IS THAT THE SCMAGLEV PROJECT IS NOT ECONOMICALLY SUSTAINABLE FROM A RIDERSHIP PERSPECTIVE.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: DENIS MCGURIN

Address: 6515 WOOD POINTE DR.

City: GLENN DALE State: MD Zip: 20769

Telephone: _____ Email: DMCGURIN@VERIZON.NET

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: THE TWO B-W PARKWAY ALTERNATIVES SHOULD
BE THE ONLY ONES CONSIDERED AS THEY APPEAR TO
IMPACT THE FEWEST COMMUNITIES AND INDIVIDUALS

ALTERNATIVE E1 SHOULD ONLY BE CONSIDERED
IF THE ALIGNMENT IS TUNNELED ALL THE
WAY ~~AND~~ FROM WASHINGTON TO PATUXENT
RESEARCH REFUGE.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Wade Woolfolk

Address: 4206 Windflower Way

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: WadeWoolfolk@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The Vista Gardens community is opposed to the Maglev Project coming through our community. Additionally, Bowie State University is a historically Black College and University (HBCU). Because it is historical land, you cannot disrupt it's surroundings.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Carolyn H. Dickerson

Address: 2809 Musclemore Ct

City: Glenarden State: MD Zip: 20706

Telephone: 301-775-4393 (C) 301-636-6297 Email: dcCarolyn430@gmail.com

Join our mailing list? [X] Yes [] No

Preferred method of communication? [X] Email [] Mail

TOTALLY AGAINST THIS PROJECT - HOPE
Comments: My concern revolve around simple questions. How

will this project benefit the people in Prince George County.

Will you be putting people out of homes with mortgages in order to ^{homeless} save 15 minutes to get to DC.

With no station in PG County there are no benefits to our County. yet we pay with our tax dollars & sacrifice to transport people from Baltimore to DC. (Jobs)

Another problem is this project may have already effected property values. I'm senior citizen who may not believe when this project is completed. I'm nurse without retirement

from a company but have managed to provide my own retirement plan part of my plan was to invest into home ownership knowing that property in this area is stable or increases as of

now January 2018 plan is in jeopardy as this will or has decreased property values



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: RAYMOND BOLLINGER

Address: 8200 HARNESS WAY

City: BOWIE State: MD Zip: 20715

Telephone: 301-503-2626 Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: TO ME THE BEST ROUTE WOULD BE TO TRANSFER OWNERSHIP OF
RT295 (B/W RAILWAY) FROM FEDERAL OWNERSHIP TO MARYLAND STATE OWNERSHIP.
RUN THE MAGLEV DOWN THE MIDDLE OF THE RAILWAY. THIS ROUTE WOULD WOULD
ELIMATE THE CONFISCATION OF EXISTING HOMES BY EMINENT DOMAIN. IT WOULD
ELIMATE THE DANGEROUS CONDITION OF RUNNING A TRAIN AT 311 MPH THROUGH
RESIDENTIAL AREAS.

THE PROPOSED NON-STOP ROUTE FROM DC TO BALTIMORE IS NOT
NON-STOP IF A STATION STOP IS LOCATED AT BWI AIRPORT.

THE MONETARY COST TO MARYLAND TAXPAYERS IS NOT KNOWN YET,
AND I SUSPECT THAT THE STATE OF MARYLAND WOULD HAVE TO CONTRIBUTE
TO THIS CORPORATION THAT WANTS TO BUILD THIS SYSTEM.

I THINK THIS PROJECT IS ~~A~~ A BAD IDEA FOR THE
CITIZENS OF MARYLAND AND THE CITIZENS OF THE COUNTIES THAT THIS WOULD

IMPACT



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Elsie M. Arkwright

Address: 12119 Quick Fox Lane

City: Bowie State: Md. Zip: 20720

Telephone: 301 352-0617 Email: arkwrem@outlook.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The alternative EI route proposed is not good for the neighborhood. It will impact too many homes. The alternative route JI (295) is the best one to use.



DATE 10.14.17

bwmaglev.info

COMMENT FORM

Name: Lisa Stator

Address: 9706 Maryland St

City: Lanham State: Md Zip: 20704

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Step 1 of this process commenced
on 11/25/2016. This was ~~inadequately~~ ineffectively
communicated w/ the public. Several
stakeholders in PG County were/are
unaware of the impacts concerning
this project.



DATE October 14, 2017

bwmaglev.info

COMMENT FORM

Name: Holland ~~Hargrove~~

Address: 4219 Lavender Lane

City: Bowie State: Maryland Zip: 20720

Telephone: 703-407-6084 Email: holland.hargrove@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: The Baltimore Washington Parkway alternative makes the most sense since the Governor has proposed to widen the lanes on Baltimore Washington Parkway anyway, this will be a better way to achieve the same goals of traffic alleviation along that corridor than new lanes and won't effect/interrupt private as many private home ~~gato~~ small businesses, or other commercial development.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Robyn Towles

Address:

4402 Huntchere DR

City:

Bowie

State: MD

Zip: 20720

Telephone:

301-928-3524

Email: towlesr@gmail.com

Join our mailing list? Yes

No

Preferred method of communication? Email

Email

Mail

Comments:

Ⓞ This project will impact significantly the property values and economy of residents in this area at the expense of another community to enrich a foreign developer. I AM opposed. This will create a cancer cluster. You know this but will not disclose.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Michelle Moo-Young

Address: 12110 Round Tree Ln

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: Angelique my @ msn.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: 1) Why is the vendor involved in the scoping process?

2) This is dated technology and there have been failures in other countries with this system and it seems we are buying an iPhone 4 when the iPhone 8 is out. This is not cheap and we should wait because we can develop a better less expensive system



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Mona Drexler

Address: 4200 Wheeler Way

City: Bowie State: MD Zip: 20715

Telephone: 301-262-8102 Email: monadrexler4200@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Don't want MAGLEV going thro + joining
communities, use AMTRAC rails. I have been
living in my house which is paid for a long time and
don't want to move.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Ms. Williams

Address: _____

City: Bowie State: Md. Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No already on the mailing list.

Preferred method of communication? Email Mail

Comments: E-1 (orange line on the maps) appears to be the most logical. Being good stewards as a company and giving back to the communities that will be impacted, It would be great if a partnership with Bowie State University could be developed and a SC Maglev educational center/department be built to educate students in this industry (all aspects), with employment and internship opportunities, especially since an operations center and rail yard appear to be planned in close proximity to the university. Bowie State is a part of the University of Maryland ~~system~~ system.

~~_____~~



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Patrice York

Address: 4410 Huntchase Drive

City: Bowie State: MD Zip: 20720

Telephone: _____ Email: patrice.york@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Against this project in Prince George's County!



DATE 10/13/17

bwmaglev.info

COMMENT FORM

Name: Laurence Winston

Address: 8632 Leslie Ave

City: Glenarden State: Md Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Do not build this high speed MAGLEV
train through Prince Georges County. We will not
benefit in any way. We will loss by it being
built. It will be too costly.



DATE 10/18/17

bwmaglev.info

COMMENT FORM

Name: Laurence Winston

Address: 8132 Leslie Ave

City: Glenarden State: Md Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: You have decided to not proceed with two of the routes suggested; we want you to NOT reconsider those routes.



DATE _____

bwmaglev.info

COMMENT FORM

Name: Laurence Winston

Address: 8632 Leslie Ave

City: Glenarden State: Md Zip: 20706

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: This train will disrupt our tax base
without sufficient return to be cost effective.



DATE 9/15/17

bwmaglev.info

COMMENT FORM

Name: Daryl Alston

Address: 12202 Raritan Lane

City: Bowie State: MD Zip: 20715

Telephone: 301-262-7823 Email: DKALS1@gmail.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am opposed to this project. I don't see any gains for our community. Homes will be lost and those that remain will be negatively impacted by the loss of value associated with the implementation of this train nearby. Communities will suffer from a loss of tax revenue which will cause the taxes of remaining homeowners to rise. There seems to be little if any concern ~~for~~ for the residents who have invested so much into their community only to be uprooted by a train that doesn't serve them. I do not see the benefits out weighing the negative impacts on people



DATE _____

bwmaglev.info

COMMENT FORM

Name: _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Fix Metro
Fix Amtrak, Don't do this
at all.
If you build it will they
come? Probably not!



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Marie Comstock

Address: 3829 Winchester Lane

City: Bowie State: MD Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Receiving information through local politicians
and other sources.

Do not see need for this project to continue
at this time. I am opposed to the MAGLEV
project and feel any of the proposed routes
would negatively impact our citizens. It
is extremely costly to tax payers - appalled
by the cost of the study alone.



DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: MARY ROMANELLI

Address: 12229 WYANMORE LA.

City: BOWIE State: MD Zip: 20715

Telephone: _____ Email: _____

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am totally opposed to this project.
It is extremely disruptive and costly.
I feel it will reduce our property values.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: BRUCE MILAM

Address: 8010 Laurel Bowie Rd

City: Bowie State: MD Zip: 20715

Telephone: 301, 809, 1923 Email: bmilam@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: If you make the BW Parkway (Rt 295)
A multi modal transportation artery with High
speed trains and at least 4 additional lanes
on each side I may support it. The current
approach will not succeed economically because
the ~~an~~ number of riders required to pay for
construction + maintenance exceeds the capacity
of the system and market. The Economic
analysis is flawed.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Bumi Akinfolarin

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Email: bunmi17@yahoo.com

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: My concern ~~about~~ is about how the residential homes will be impacted by this potential project.



DATE _____

bwmaglev.info

COMMENT FORM

Name: Celestine Wilson

Address: _____

City: Glenarden State: MD Zip: 20706

Telephone: _____ Email: cwilson@cityofglenarden.org

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: I am not in support of the SC maglev train.
It should not come through Glenarden or anywhere
in Prince George's County. It has not economical
benefit to any of the citizens. It will only destroy
neighborhoods and communities. Schools, churches,
businesses, libraries will be affected.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Bruce Milan

Address: 8010 Laurel Bowie Rd

City: Bowie State: MD Zip: 20715

Telephone: 301, 809, 1923 Email: b.milan@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: Private ventures for public transportation almost always fail:

1. The Dulles to Leesburg Toll Road Failed ~~because~~ ^{because} the tolls would not cover the cost of Building + maintenance, the state of VA is stuck with the road a long term expense.

2. The DC electric trolley is Failing because it is much more costly than the Metro and Buses on the same route.

3. Maglev in ~~China~~ Taiwan is Failing because the ticket sales will not cover the Building and maintenance cost.

Why would we continue with this project destined to fail.



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: MILUS FAMILY

Address: 7931 ECHOLS AVENUE

City: GLENARDEN State: MD Zip: 20706

Telephone: _____ Email: poohbaby1113@verizon.net

Join our mailing list? Yes No

Preferred method of communication? Email Mail

Comments: WE OBJECT TO ANY FURTHER CONSIDERATION OF THIS PROPOSAL TO BRING THE BALT-WASH SCMAGLEV TRAIN TO PRINCE GEORGES COUNTY. WE DON'T NEED NOR WANT THIS, SO STOP THINKING AND MAKING DECISIONS FOR WHAT YOU THINK OR ASSUME WHAT IS BEST FOR US!!!!

~~~~~





DATE 14 Oct 17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Angela Burriss

Address: 11913 Gcrason Lane

City: Bowie State: MD Zip: 20715

Telephone: 405-361-3604 Email: aburriss1@cox.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Risk - technology being considered  
is ~~approved~~ not currently being used  
any where else in the U.S. In order to  
maintain serviceable, there needs to be a  
service contract w/ vendor. Since it is a  
sole source, the company has MD over a  
barrel & can charge whatever they want.  
Are you buying tech data? I've heard you  
aren't. These life cycle costs need to  
be factored in.  
Plus - risk of being unserviceable.  
Risk of health issue due to magnets  
Risk to Ft. Meade.

What happened to buy American!





DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Curbean Family

Address: 1515 7th St.

City: Glenarden State: MD Zip: 20706

Telephone: (240) 770-5542 Email: tauche2002@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We are against the new rail system  
impacting the town of Glenarden, which a  
predominantly African-American community. It  
would be beneficial & cause less of an impact  
economically & environmentally to incorporate the  
new rail system with existing structures  
for Metro & Amtrak.



DATE 10-14-17

*bwmaglev.info*

**COMMENT FORM**

Name: Bonnie Lawson  
 Address: 12655 Heming Lane  
 City: Bowie State: MD Zip: 20716  
 Telephone: 301 379 8581 Email: bonnie-lawson@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am very against cutting a 100' wide swathe through Bowie for no gain.

This will trash our green spaces, lower our property values & ruin the friendly, walkable, family-oriented community that I am raising my family in.

If ~~we~~ we need another commuter solution - ~~we should~~ choose the hyper loop - most of which will be underground - we get to keep our green space - our pretty community. The county gets to keep its voters & its tax base.

the maglev will wipe out our neighborhoods. Its just wrong for Bowie



DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Gloria Brown

Address: 13800 Old Jericho Park Rd

City: Bowie State: MD Zip: 20720

Telephone: 301-262-0720 Email: browngja@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: NO TRAIN through Bowie  
Bowie State & Old Bowie are  
Historical sites

"NO" TRAIN by Bowie State

\* TAKE All track off the table

We have AMTRAK - what's the  
point do not put on  
AMTRAK line





DATE 10/14/17

bwmaglev.info

**COMMENT FORM**

Name: SUSAN Miller

Address: 6304 Gabriel St

City: Bowie State: MD Zip: 20720

Telephone: 301-502-0893 Email: succamiller@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The room where the presentation was held was filled to capacity and many people could not get in. This project benefits no one and is fiscally irresponsible. No maglev has ever made money and many have been abandoned. These 3 new routes proposed in October will run through 2500+ homes in Prince Georges & Anne Arundel counties. Stop this project!





DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: William F Bullock  
 Address: 3608 Burleigh Dr  
 City: Bowie State: Md. Zip: 20721  
 Telephone: 301 262-8462 Email: APPRAIS 34 @ VERIZON  
 \*IVET  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: Who in Prince Georges CTy, can ride this? What benefit is it to the citizens of this county.  
Why not take it out the BW Parkway. The center of the parkway is ideal for such. I'm sure it can be (BW parkway or the maglev) can be modified for such.  
Is this really necessary for whom?? Will we tax payers pay for this??



DATE 10/14/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: MICHAEL STEIN

Address: 7109 W. GREENDALE PKWY

City: CHEVY CHASE State: MD Zip: 20815

Telephone: 703 622-0624 Email: MICHAELJSTEIN1954@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do not see the need for this project and I am deeply concerned as a Maryland taxpayer that my tax dollars are being used for an unnecessary project when there are far more pressing infrastructure needs in the state

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DATE 10/14/17

bwmaglev.info

**COMMENT FORM**

Name: Howard Dobson

Address: 12203 Mackell Lane

City: Bowie State: MD Zip: 20715

Telephone: 301 809 1645 Email: HRD7892A@b.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1. The technology is unproven in the U.S.  
2. I think your tests for sound effects  
are in correct.  
3. If the line is built, it should follow  
existing rights of way and not use  
the proposed WB&A rail route





DATE 14 Oct 17

*bwmaglev.info*

**COMMENT FORM**

Name: Angela Burris

Address: 11913 Corason Lane

City: Bowie State: MD Zip: 20715

Telephone: 405-361-3604 Email: aburris1@cox.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Cost Benefit Analysis - As a MD homeowner and tax payer, I would expect MD to evaluate what benefits this provides to ALL or the majority of the taxpayers. This train will be running right next to existing infrastructure. True, it does not support what is being proposed but maybe this is the right technology. The people affected are reaping the benefit, and those who are receiving the benefit are paying nothing for it. I work at the Pentagon - why can't I have a stop here? Why not extend the Mark train? Why increase the footprint - reconstruct the Mark tracks, extend it to DC.





DATE 14 Oct 17

bwmaglev.info

**COMMENT FORM**

Name: Angela Burris

Address: 11913 Grason Lane

City: Bowie State: MD Zip: 20715

Telephone: 405-361-3604 Email: aburris1@cox.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Environmental - I Live in ~~Grason~~ <sup>the Grady's Walk area</sup> and see a great deal of wild life (deer, fox, ground hogs, snakes, rabbits, chipmunks, squirrels, and birds of all kinds. The traffic pattern, grazing, foraging habits of these animals needs to be taken into consideration. I know I'm far from the area being impacted but there are routes they travel that could include the area impacted by the proposed train

Also to be considered is the duplicated noise level. There are 2 trains running side by side - 2x the noise.

environmental impact is reramping the Mark, extending it, ~~and~~

It appears to me that the company and technology was selected prior to doing the analysis of alternatives. Not the way to do business



DATE 10/14/2017

bwmaglev.info

**COMMENT FORM**

Name: ROBERT J. RAPOZYNSKI

Address: 8706 MAPLE AVE

City: BOWIE State: MD Zip: \_\_\_\_\_

Telephone: 301-464-0588 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: WHO IS GOING TO PAY FOR UPGRADES TO POWER INFRASTRUCTURE?

HOW MUCH WILL FAIRS BE

WHO IS GOING TO PAY FOR LAND ACQUISITION

DO NOT BUILD



DATE 10.14.17

bwmaglev.info

COMMENT FORM

Name: Quondora Simmons  
 Address: 1431 9th Street  
 City: Lanham State: MD Zip: 20706  
 Telephone: \_\_\_\_\_ Email: QuondoraSimmons@icloud.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do believe in upper mobility and the next new thing. But I have a profound issue with this new adventure. This area consists of mainly working class, retired and we just lived day by day. To have such a big project in our backyard is not welcoming and we believe not any pro purporeal to our neighborhood. I am saying NO! to MAGLEV!

Peter & Edna Granahan  
12102 Rustic Hill Drive  
Bowie, Maryland 20715  
301-262-9018  
Cell 301-395-1624  
[pegranahan@verizon.net](mailto:pegranahan@verizon.net)

July 31, 2017

Maryland Department of Transportation  
Bradley Smith Director of Office of Freight and Multimodalism  
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, C/O John G. Trueschier  
Maryland Transit Administration  
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy and Development  
Federal Railroad Administration  
1200 New Jersey Avenue, SE  
Washington, DC 20590

Re: SC Maglev Scoping Report – Please reopen scoping process

To whom it May concern:

Until very late in June 2017 I, my neighbors and most Bowie land owners and residents had never heard about Maglev or the possibility it may come into our area.

There were no announcements, notifications, letters, posters, post cards, invitations to open house meetings, advertisements, scoping process, final scoping report or any other communications.

My wife and I have lived in Bowie for 51 years. Raised a family of 4 children and have 7 grandchildren. We have lived on Rustic Hill Drive in the “R” section for 40 years. It would be difficult for us in our late seventies to have to relocate. Our home is the center of family and extended family activities and holiday dinners for up to 30 people.

We have two small properties between our house and the WB & A trail. We have had a lot of inquiries to purchase our house and properties in the last two years, 3 in the last week. This project may explain that, almost all of them were from outside our area.



Now that I have received and read the 170 page Final Scoping Report of 5/17/2017, I understand why I and a whole lot of Bowie land owners had no knowledge of this project. We were never informed or included in the process in any way. Shameful! We were excluded and we may be the most affected by this project.

**Website**

Did not know website existed, therefor it was of no use to us.  
We were excluded

**Flyer Distribution Locations – EJ Outreach**

No contact or distribution locations in Bowie, MD per Final Scoping Report, the largest city between Baltimore and Washington.  
Only one distribution location in Anne Arundel County, a boys and girls club  
Again, we were excluded.

**Post Cards and letters**

Never saw one. I don't know anyone who did.  
Again, we were excluded.

**Open House Presentations December 10–15, 2016**

One location in Prince George's County – West Lanham fire house  
18 attendees out of a population of 39,395 with a total of 2 comments  
We did not know about this open house. No invitation.  
No open house in Bowie.  
Again, we were excluded.  
In the 5 open houses from Baltimore to Washington there were only 152 sign-ins and total of only 57 comments or questions. This is a very poor representation of the study area.

**Advertisements**

Never saw one. I don't know anyone who did. Newspapers: the Bowie Blade and the Washington Post. Saw no television ads on the Washington channels.  
Again, we were excluded.

**Public Comments**

In total there were only 75 comments and questions. 16 were from the website, 2 mail and 57 from open houses. The largest number by topic was 19 or 25% about alignment of routes. Most (15 to 17?) of these comments requested more information of a Maglev alignment. Was this information provided to them?  
Answers not in the report and still not provided to us.

**City of Bowie City Council Meeting – July 10, 2017**

A presentation was made by David Henley, SCMAGLEV Project Director. His presentation was primarily the technology and speed of the train, future possibilities, funding by Japan (5 billion dollars) and by the US Government (27

million dollars). No funding needed from Maryland. He did a good sales presentation.

Mr. Henley had been told not to comment on the alignment routes, impact or environmental issues. These were the primary reasons over 300 local citizens attended the council meeting.

The meeting overflowed the City Council Chambers and there were standing room only rooms set up with projection TVs to watch the presentations. The presentations were broadcast live on the city of Bowie TV station as well as scheduled rebroadcasts. I have no idea how many stayed at home and watched it on TV, my wife did. Most people in the room commented they just heard about the Maglev in the past 2 to 3 weeks by word of mouth.

24 citizens made presentations. A good number of them also provided their statements in writing to the Mayor. Mr. Henley was very impressed with every one of the presentations, as was I, and said he made a lot of notes to take back with him. He was also very apologetic and said he would work to make this process fair. He carefully made no direct guarantees. Each Council Member also made a brief presentation.

#### **Scoping Process / Report Should Include**

Opportunity for everyone to be informed, involved, submit comments and recommendations.

Include Mr. Henley's notes from the Bowie Council meeting on July 10, 2017.

The impact of alignment routes on homes and property, direct and property value.

Impact of alignment routes on churches, schools, recreation, parks and businesses.

City of Bowie report on council meeting of July 10, 2017 and other reports.

Notes and recommendations from the Citizens Against SCMAGLEV.

Bill Boone's analysis on the effect property on the yellow and green routes in Bowie, MD and Twin Rivers Retirement Community in Anne Arundel County

Pro forma or synopsis of pro forma based on revenue and ridership.

Projected ridership by Maryland citizens by; state regions, counties or city/communities for MAGLEV between Wash. BWI and Baltimore.

Overview of cost for Bowie, State of Maryland and the US

Overview of funding and future possibilities for liabilities

What are the terms of the 5 billion dollar loan from the Bank of Japan? Who is liable?

What is the status of all other of the SCMAGLEV proposal, approvals, projects, construction, operations and financial status worldwide?

Peter and Edna Granahan

Cc: David Henley

Bradley Smith  
Chris Van Hollen  
Steny Hoyer  
Ben Cardin  
Larry Hogan  
Doug Peters  
Rushern Baker  
Fred Robinson  
Bowie City Council Members

*Comments/Concerns/Issues*

*DENNIS BRADY*

*15916 PAISLEY LANE*

*Bowie, MD 20716-1647*

*Cell Phone: (301) 908-6903*

*e-mail: kathyndennis@verizon.net*

October 9, 2017

SCMaglev Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Dear Mr. Al Khatib,

The Citizens Against the SCMaglev sent a letter in June 2017 requesting that the NEPA scoping process be reopened due to lack of public notice on the part of the Maryland Transit Administration. To date, we have not received any response from MTA.

As we will not have our concerns ignored, we are providing the following scoping comments. The project scoping report must be amended to include these issues and they must be addressed in the EIS.

**PURPOSE AND NEED**

1. Provide the need for the SCMaglev project. While a purpose of the proposed project was provided at the April 2017 public meetings, the need for a SCMaglev train was not provided. The public has not been provided any information that leads to the conclusion that an SCMaglev is needed. We strongly believe that there is a need for regional transportation improvements in the Washington-Baltimore corridor that will serve all residents, visitors, and businesses in the region. However, we have seen no data that proves that a train that serves only Washington, BWI, and Baltimore is needed or will provide relief from the transportation challenges in the region.

2. The Notice of Intent, published on November 25, 2016 (the day after Thanksgiving), states that:

“The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent. Similarly, the demand on the transportation infrastructure between Baltimore and Washington will continue to increase along major roadways and railways including Interstate 95, the Baltimore-Washington Parkway (MD 295), US 29, US 1, and the Northeast Corridor (NEC) thereby decreasing the level of service, reliability, mobility, and potentially decreasing safety.”

This statement does not provide proof that there is a need for a transportation investment of this magnitude with significant environmental impacts that only serves DC, BWI, and Baltimore. Rather it demonstrates that there are transportation issues that extend throughout our region. Analysis of these needs will demonstrate that these issues will only be solved by intra-regional transportation investments like rapid bus transit, light rail, and roadway improvements that serve the entire traveling population.

Provide detailed analysis of transportation needs of the Baltimore-Washington area.



Page 2 of 12  
 October 5, 2017  
 SCMaglev Project  
 c/o Suhair Al Khatib

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3. The Notice of Intent makes the following claims:

“Without additional transportation improvements and capacity within the Baltimore-Washington area, economic development and growth opportunities will be restricted. As congestion increases on the NEC and on the region's highways, the demand for continued economic development will be impacted, including, for example, tourism.”

Provide documentation of this hypothesis. Specifically, provide documentation that the “transportation improvements... within the area” would be served by a rail system that only stops in DC, BWI, and Baltimore. Provide documentation that tourism, which is fueled by the great historical and governmental significance of the Washington, DC area would decline without a train that ONLY brings people from Baltimore.

4. Further the Notice of Intent quotes the NEC Future Program:

“To address these issues, in 2012 FRA launched the NEC FUTURE program to consider the role of rail passenger service in the context of current and future transportation demands and to evaluate the appropriate level of capacity improvements to make across the NEC. Through NEC FUTURE, FRA will determine a long-term vision and investment program for the NEC documented in a Tier 1 EIS and Service Development Plan. FRA published a Tier 1 Draft EIS in November 2015; however, the Draft EIS evaluated steel-wheel technologies as a way to serve the passenger rail needs of the region. It left open the possibility and did not preclude the study of and investment in advanced guideway and other new technologies, such as SCMaglev, to meet the transportation needs of the Northeast, including the Baltimore-Washington area.”

We note that this statement says that the NEC future EIS leaves open the possibility of studying the SCMaglev. While we concur, this statement does not support the need for the SCMaglev and calls into serious question why alternatives other than SCMaglev are not being studied in this EIS. See comments on Alternatives below.

5. Independent Utility/Segmentation - Provide detailed analysis proving that the proposed project has independent utility. Provide financial data showing how a train from DC to Baltimore can sustain short-and long-term viability without government funding and without an extension past Baltimore.

As BWRR is a subsidiary of The Northeast Maglev, LLC (TNEM), and TNEM has announced plans to build the SCMaglev from Washington, DC to New York, the entire SCMaglev project is a connected action. Separation of the project into pieces violates the prohibition on segmentation per the Council on Environmental Quality (CEQ) NEPA implementing regulations.

The CEQ NEPA regulations state that:

**§1502.4 Major Federal actions requiring the preparation of environmental impact statements:** Proposals or parts of proposals which are related to each other closely

Page 3 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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enough to be, in effect, a single course of action shall be evaluated in a single impact statement.

**§1508.25 Scope:** To determine the scope of environmental impact statements, agencies shall consider 3 types of actions, 3 types of alternatives, and 3 types of impacts. They include: 28 (a) Actions (other than unconnected single actions) which may be: (1) Connected actions, which means that they are closely related and therefore should be discussed in the same impact statement. Actions are connected if they: (i) Automatically trigger other actions which may require environmental impact statements. (ii) Cannot or will not proceed unless other actions are taken previously or simultaneously. (iii) Are interdependent parts of a larger action and depend on the larger action for their justification. (2) Cumulative actions, which when viewed with other proposed actions have cumulatively significant impacts and should therefore be discussed in the same impact statement. (3) Similar actions, which when viewed with other reasonably foreseeable or proposed agency actions, have similarities that provide a basis for evaluating their environmental consequences together, such as common timing or geography. An agency may wish to analyze these actions in the same impact statement. It should do so when the best way to assess adequately the combined impacts of similar actions or reasonable alternatives to such actions is to treat them in a single impact statement.

6. Provide data demonstrating the following required for financial viability:
  - Number of daily trains in each direction
  - Number of cars per train
  - Number of riders per train, per day, and per year
  - Cost per ticket
  - Cost to operate and maintain rail line
  - Any financial incentives (funding, grants, tax breaks, loans, etc) needed from governmental sources (Federal, State, or local) for construction and operation in the short-term and long-term
  - Provide information on foreign investment including required purchase of foreign-made rail components/trains
  - Provide examples of where the SCMaglev is currently operating in the world and provide financial data on that operation demonstrating that it does not require continual or intermittent government subsidies.

Page 4 of 12  
 October 5, 2017  
 SCMaglev Project  
 c/o Suhair Al Khatib

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7. Amtrak's annual ridership on the Northeast Corridor (from DC to Boston) for 2016 was 11.9 million. Further Amtrak estimates NexGen ridership in 2030 of 25 million for the ENTIRE Northeast Corridor (<https://www.amtrak.com/ccurl/214/393/A-Vision-for-High-Speed-Rail-in-the-Northeast-Corridor.pdf>).

In 2016, a total of 5,098,562 riders boarded Amtrak trains at the Union Station in Washington, DC, and a mere 1,030,161 riders boarded Amtrak trains in Baltimore <https://www.amtrak.com/national-facts>. Only 689,042 riders boarded/off-boarded at BWI in 2016. (<https://www.amtrak.com/ccurl/111/103/MARYLAND16,0.pdf>). It is inconceivable that most of these 6.8 million riders were solely traveling between Washington and Baltimore.

2016 ridership on the MARC Camden and Penn lines totaled approximately 1.3 million riders (<https://data.maryland.gov/Transportation/MTA-Average-Weekday-Ridership-by-Month/ub96-xxqw>), but MARC serves numerous stations and communities in between Baltimore and Washington – commuters that would not be served by the SCMaglev. In addition, in June 2016, MARC reported ridership DECLINES: “Currently, the MARC service is facing ridership declines, according to Erich Kolig, director, MARC train and commuter bus service. “They are rather slight, only about a percent down from last year,” he explains. “But we are not experiencing the 3% growth that we have been maintaining for the last 10 years.” (<http://www.metro-magazine.com/rail/article/713751/maryland-s-marc-railroad-upgrades-fleet-service-to-bolster-ridership>)

In its filing with the State of Maryland, BWRR claimed that “Annual ridership on the SCMaglev system connecting Washington to Baltimore would be between 10.2 million and 15.4 million annual passengers in 2030 (42,200 one-way trips per day on average), while still assuming continued Amtrak and MARC service.”

**Provide proof and solid data on how the SCMaglev will achieve 10 times the current Washington/Baltimore station onboard/offboard riders and 41 to 62% of the projected 2030 riders on the entire Amtrak Northeast Corridor in the short line from DC to Baltimore while “still assuming continued Amtrak and MARC service”! Include detailed origin and destination data for riders, type of riders (commuters, business travelers, tourists).**

See **Environmental Impacts** for further issues regarding Amtrak.

8. If the need for an SCMaglev train from DC to Baltimore cannot be defined, and independent utility for this short segment of rail cannot be established and sustained over the short- and long-term, the EIS must be revised to include the entire corridor (whether to Philadelphia, New York, or Boston) needed to sustain the financial viability of the project in accordance with 40 CFR 1502.4a and 40 CFR 1508.25.

Page 5 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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## ALTERNATIVES

The structure of this EIS based solely on studies for the SCMaglev, precludes all other possible decisions including the decision to construct Amtrak NextGen trains. As such, an EIS that does not study these other alternatives is not in compliance with the National Environmental Policy Act.

### As stated by the Council on Environmental Quality (40 Most Asked Questions):

Question 2a. Alternatives Outside the Capability of Applicant or Jurisdiction of Agency. If an EIS is prepared in connection with an application for a permit or other federal approval, must the EIS rigorously analyze and discuss alternatives that are outside the capability of the applicant or can it be limited to reasonable alternatives that can be carried out by the applicant?

A[nswer]. Section 1502.14 [of the CEQ regulations] requires the EIS to examine all reasonable alternatives to the proposal. In determining the scope of alternatives to be considered, the emphasis is on what is "reasonable" rather than on whether the proponent or applicant likes or is itself capable of carrying out a particular alternative. Reasonable alternatives include those that are practical or feasible from the technical and economic standpoint and using common sense, rather than simply desirable from the standpoint of the applicant.

2b. Must the EIS analyze alternatives outside the jurisdiction or capability of the agency or beyond what Congress has authorized?

A. An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Section 1506.2(d). Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies. Section 1500.1(a).

**In addition, Section 4(f) of the Department of Transportation Act prohibits the use of Section 4(f) property unless there is no feasible and prudent avoidance alternative to the use of land from the property. If the SCMaglev project uses public parkland, wildlife or waterfowl refuge land, or historic resources, it may not be constructed if there is a prudent and feasible alternative. It should be noted that the WB&A Trail is public parkland – NOT a railroad right-of-way, and as such is protected under Section 4(f).**

**Lastly, as noted in Amtrak's scoping comments for this EIS, the NEC Future EIS and Record of Decision, selected improvements to the existing rail alignments and "discarded the new alignment alternative."**



Page 6 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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1. NexGen – The Amtrak NexGen technology is a “reasonable” alternative to Maglev and therefore, in accordance with NEPA and the CEQ implementing regulations (40 CFR 1502.14), must be studied in the EIS. ADD INFORMATION ON NEXGEN – SPEED, TIMING, COST, ETC. While outside of the Congressional legislation, the NexGen technology is a “reasonable alternative as it is practical and feasible from the technical and economic standpoint and must be included using common sense, rather than simply desirable from the standpoint of” BWRR. In addition, as construction funding is not available at this time, it is prudent to provide Congress with an equal analysis of all alternatives to facilitate informed decision-making.

Lastly, the Amtrak NexGen is a prudent and feasible alternative to the SCMaglev and would avoid the use of public parkland (including the WB&A Trail), wildlife and waterfowl refuges, and historic properties. Therefore, the Amtrak NexGen must be studied as an alternative to the SCMaglev.

2. Elon Musk and SpaceX have proposed constructing an underground Hyperloop railroad to travel between Washington, DC and New York. As with NexGen, this technology must be analyzed alongside the SCMaglev to provide decision makers with information on the environmental impacts of each technology prior to making a decision.
3. Detail on alternatives must include design details including but not limited to: limits of disturbance; right-of-way; staging areas; station areas and impact areas for these; connections between stations and existing transportation hubs (i.e. BWI); maintenance yards and impact areas for construction of these; utility connections; security features including fencing; haul routes for construction equipment, materials, construction debris, and mining spoils; and disposal areas for construction debris and fill materials.

### **Environmental Impacts**

1. Environmental Justice – Provide a detailed analysis on the disproportionate impacts that low-income and minority populations in Washington, DC; Prince George’s County; and Baltimore City will suffer under the SCMaglev plan including but not limited to loss of property values and housing opportunities, loss of parkland and greenspace, increases in noise levels, and impacts to health and safety. Specifically, the above ground portion of the project will bisect Prince George’s County while not providing ridership opportunities to the citizens of the County. Provide analysis of the EPA EIScreen demonstrating the disproportionate impacts already suffered by the residents of the County and the impact that a SCMaglev train would have. A baseline EPA EIScreen along the WB&A Trail shows an EJ score of over 90% compared to 50 to 60% in communities in Montgomery and Howard counties.

Page 7 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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2. Property Acquisition – Provide information on all properties to be acquired and displaced as a result of the proposed action including:
  - a. Number, types, and locations of all of acquisitions/displacement of residents, parks, businesses, schools, churches, etc;
  - b. Method by which properties will be acquired including information on eminent domain and what entity will exercise eminent domain;
  - c. How compliance with the uniform relocation act will occur including analysis of the availability of comparable housing of equal price, value, and setting
  - d. Impact to remaining homes including loss of home values, changes in setting, visual and noise impacts. Provide information on how close a residence can be from the rail line
  - e. Impacts to remaining businesses including change of access and resulting loss of business.
3. Economic Impacts – Provide thorough, quantitative analysis on the negative economic impacts associated with the proposed alternatives including:
  - a. Loss of property values (especially home values) from the proximity of a high-speed train where there is currently more compatible, highly valued land uses such as parks, trails, forested areas, and residential areas
  - b. Loss of tax base from the loss of properties and from the devaluation of remaining properties
  - c. Impacts to Amtrak ridership and revenues in the Washington/Baltimore corridor, the Northeast Corridor, and nationally as many of Amtrak's lines outside urban areas count on revenues generated in areas like the Northeast Corridor
  - d. Impacts to viability of NexGen Acela as it is probable that there is not sufficient ridership/need for both technologies to operate in the same market
  - e. Impact to Marc ridership, revenues, and viability with the proposed loss of riders to the high-speed train; this assessment should include any additional State, local, or Federal funds that will need to be expended to keep the Marc system viable. If the Marc system is no longer viable or must raise fares to maintain viability, the impact to commuters that board the train at locations between Baltimore and Washington must also be analyzed
  - f. Analysis of permanent job creation with SCMaglev vs. job losses at Amtrak and Marc
4. Parks and Recreation/Section 4(f) – Provide impacts to parks and recreation areas in Prince George's and Anne Arundel Counties including the loss of access to these critical community features. Impact analysis should include changes to these facilities including changes to park settings, park features, park noise levels, and the overall changes in the user's experience. Note – the WB&A Trail is public

Page 8 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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parkland; owned and operated by the Maryland National Capital Park and Planning Commission and the Anne Arundel Recreation and Parks Department. The trail is a critical component of the East Coast Greenway and the American Discovery Trail.

5. Historic Resources – Provide impacts, including visual, noise, and vibration impacts, to historic resources including archaeological features, historic sites such as the Glen Dale Hospital, the Marietta Mansion, the Bowie Racetrack, private historic residences and farms, and historic districts including the Odenton historic district.
6. Visual Resources - As the only renderings provided by MTA thus far show a train in the middle of a forest with no residences nearby, provide more accurate renderings of the train line. Specifically:
  - a. Provide before and after renderings of the proposed rail line and train in ACTUAL locations showing the view to and from historic properties, parks, and residential properties that will remain after construction of the train.
  - b. Renderings to and from historic properties, parks, and residential properties should include day and night views and views with and without leaf cover on trees.
  - c. Provide information on the visual intrusion of graffiti into neighborhoods that may occur on the elevated rail line.
7. Topography – Provide details on changes to topography including cut and fill; provide detail on whether the proposed train and track system will cut through hills (creating environmental impacts) or rise up over them creating visual impacts to large numbers of communities and citizens.
8. Water Quality – Provide quantitative data on the increases in impervious surface created by the track, stations, maintenance yards, and parking.

Provide quantitative data on the resulting stormwater runoff and impacts to erosion, sedimentation, and TMDLs in the already degraded Patuxent River and its tributaries including Horsepen Branch.

Provide information on the chemicals to be used for snow removal and deicing and the impact runoff of these chemicals will have on the Patuxent River and its tributaries.
9. Bird Strikes – Provide data and analysis on the impact of bird strikes and how they will be prevented from a train going over 300 mph. There are nesting bald eagles on the Patuxent Wildlife Refuge in close proximity to the WB&A Trail. Bald eagles also frequent the Patuxent River between Prince George's and Anne Arundel counties. In addition, there are numerous other bird species including barred owls, pileated woodpeckers, and red-tailed hawks nesting along the WB&A Trail and along the river.
10. Bats - Provide data and analysis on the impact to bats and how strikes will be prevented from a train going over 300 mph. Provide detailed studies of bat habitat, bat populations, and presence of white-nose bat syndrome.

Page 9 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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11. **Wildlife Displacement** – Provide data and analysis on wildlife displacement from construction of the proposed train. Habitats in the vicinity of the proposed alternatives are at capacity and any loss of habitat will force wildlife further into developed areas including residential areas. Detailed wildlife surveys must be conducted to document species, numbers, health of population (including over population), and capacity of habitat.
12. **Vegetation** – Provide data on number, species, size, and locations of all trees to be removed for construction of the rail line. Provide types of vegetation to be removed. Provide information on how the project will prevent invasive species from being introduced into areas where forest are bisected and native vegetation is removed. Provide mitigation including where and what types of vegetation will be replanted, how reforestation areas will be monitored, and how the rail line will be screened after vegetation is removed.
13. **Noise** – Provide 24-hour noise analysis for all types of properties in the vicinity of the proposed alternatives (residences, schools, churches, libraries, historic sites, parkland, etc.). Provide existing noise levels and noise modeling when the train is in operation. Noise models should take into account the loss of existing vegetation and buildings that would be removed and modeling should show day and night noise levels.
14. **Electromagnetic Fields** – Provide information on electromagnetic fields including health effects from long-term exposure to an SCMaglev train. Provide health information from populations currently exposed to SCMaglev trains.
15. **Light** – Provide information on how the rail line, parking areas, rail yards, stations, etc. will be illuminated at night. Provide analysis of light intrusion into residential areas, parkland, and historic sites.
16. **Energy Usage** – Provide information on the energy needed to operate the SCMaglev train including the source of the power. Provide locations of any new substations or other utility upgrades needed to power the SCMaglev. Provide information on how power interruptions would affect train operation.
17. **Utilities** – Provide information on temporary and permanent utility disruptions to residences, businesses, and other property owners. Provide information on the location and type of new utility lines needed to serve the proposed project. Provide information on impacts to the power grid and other utilities.
18. **Community Cohesion** – Provide information on impacts to neighborhoods that will be bisected or will be cut off from neighboring communities by the SCMaglev Train.  
  
Provide information on the effects to communities such as Glenarden, Glen Dale, Bowie, Piney Orchard that will be bisected by the proposed train. Include analysis of the cumulative effects of bisection by transportation projects to communities such as Glenarden.



Page 10 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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19. Land Use and Land Use Planning – Provide detailed analysis of changes in land use that will occur due to undesirable proximity to the train and from the economic development that BWRR claims will accompany the train. Provide detail on where this development will occur and how it will affect sprawl in the Baltimore-Washington Area
20. Community Services –
  - a. Provide analysis on effects to police, fire, and EMS services including responding stations, personnel levels, training (how much it will cost, who will provide it, and how often it will occur). Provide information on BWRR provided police services and a comparison of those services to those provided by Amtrak police. Provide information on how terrorist threats will be mitigated at stations and on the rails.
  - b. Provide information on how other community services will be impacted including schools, hospitals, churches, etc. Include analysis of cumulative impacts based on the economic growth projected to occur solely because of the SCMaglev construction (as based on BWRR claims).
21. Safety and Security – In addition to the police information requested above, provide information on how the SCMaglev rail line will be secured such that people cannot climb onto the line resulting in injury or death and/or vandalize the line. Provide renderings of any fences or other barriers that will be placed along the rail line, rail and maintenance yards, parking areas, etc. Provide information on the distance fence lines will be placed from the line, height of fences, and type of fencing.
22. Construction Impacts – Provide information on the construction related activities and impacts including but not limited to:
  - a. Limits of disturbance for construction of each of the alternatives – temporary and permanent land use
  - b. Areas of cut and fill
  - c. Staging areas
  - d. Tunneling/mining locations
  - e. Tunneling technology
  - f. Haul routes for construction equipment and disposal of tunneling spoils
23. Traffic – Provide impacts from:
  - a. Construction vehicles traffic
  - b. Temporary and permanent roadway closures including travel delay times, creation of land-locked properties, changes in access to properties

Page 11 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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- c. Additional vehicular traffic, especially at Union Station, BWI, and in Baltimore generated from train riders accessing the rail line; analysis should include intersection level turning movements and delays
- d. Traffic from economic development that BWRR claims will accompany the SCMaglev; the Notice of Intent states that “The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent.” If this growth will occur without the SCMaglev, and the SCMaglev claims it will radically increase development in the region, the cumulative impact analysis must quantitatively analyze the traffic that will accompany this level of development and explain how the rest of the regional transportation network will handle this development or where levels of service will decrease to the point of complete gridlock.

24. Airline/Plane Traffic – As part of BWRR’s economic and ridership “promises” include increased use of BWI and development around BWI, increases in air traffic must be analyzed in the EIS.

This analysis must also include resulting increases in noise from increased air traffic. It should be noted, that Governor Larry Hogan recently instructed the State Attorney General to sue the FAA for noise violations at BWI.

25. Union Station –

- a. Provide information on the impacts to the historic Union Station in Washington, DC
- b. Provide information on how the SCMaglev Train will impact current renovations taking place at Union Station
- c. Provide analysis of how a SCMaglev Train will affect the Washington Union Station’s 2nd Century Plan which is being developed to accommodate the Amtrak NexGen train.

26. Cumulative Impacts – A project of this magnitude requires a comprehensive analysis of cumulative impacts including quantitative analysis of the significant harmful impacts transportation projects have had and continue to have on communities in Prince George’s and Anne Arundel counties. Included in this cumulative impact analysis must be the impacts of the supposed development that SCMaglev claims it will generate. In addition, the cumulative impact analysis must analyze any track widening or realignments planned by Amtrak.

The Notice of Intent also provides grossly inaccurate information on the study area of the proposed project. The NOI states that the study area is bounded by the former Washington-Baltimore & Annapolis Electric Railroad alignment while in reality, the study area extends 3 miles to the east of this alignment. Maps of preliminary alternatives have been provided by MTA which MTA now states should not be relied on. BWRR CEO Wayne Rogers, as recently as September 14, 2017, stated in an interview with ABC 7 news that ““We’re very early in the process. They’re called initial preliminary screening alternatives. They’re not even to the preliminary to the alternatives to the preferred alternatives, so

Page 12 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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we're years away," when in fact alternatives have been submitted to FRA for approval. These examples of misinformation by the MTA project team along with abysmal public notice on the scoping process for the project, demonstrate that this project should be restarted to provide meaningful opportunity for public input and decision-making.

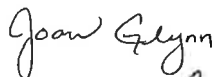
As MTA has not responded to repeated requests to reopen the scoping period for this project, we request that you provide a formal reply to this letter and an affirmation that the issues outlined herein will be thoroughly analyzed in the EIS.

You may reach us at:


Citizens Against the SCMaglev  
PO Box 669  
Bowie, MD 20715

Sincerely,

Citizens Against the SCMaglev



On behalf of:



Dennis Brady, Chair  
Dawn Wampler, Vice-Chair  
Aviva Nebesky, Vice-Chair  
Breanne Reynolds, Secretary  
Joan Glynn, Secretary

Cc:

EPA Region 3  
US Senator Chris Van Hollen  
US Senator Ben Cardin  
Congressman Steny Hoyer  
Governor Larry Hogan  
State Senator Doug Peters  
Delegate Geraldine Valentino-Smith  
Delegate Marvin E. Holmes, Jr.  
Delegate Del. Joseph F. Vallario, Jr.

Prince George's County Executive Rushern Baker  
Prince George's County Councilman Todd Turner  
Bowie City Mayor Fred Robinson  
Bowie Councilman James Marcos (At Large)  
Bowie Councilman Henri Gardner (At Large)  
Bowie Councilman Michael Estève (District 1)  
Bowie Councilwoman Diane Polangin (District 2)  
Bowie Councilwoman Courtney Glass (District 3)  
Bowie Councilman Ike Truth (District 4)

**Peter & Edna Granahan**  
12102 Rustic Hill Drive, Bowie, MD 20715  
301-262-9018 pegrarahan@verizon.net

SCMAGLEV Project, C/O John G. Trueschler  
Maryland Transit Administration  
6 Saint Paul Street, Baltimore, MD 21202

Re: SC Maglev questions for the September 5, 2017 Bowie City Council meeting

Dear Mr. Trueschler:

- Questions:
  - **Who in Maryland would use this train between Washington, BWI & Baltimore?**
    - It would take too much time to travel to these stations to make it worthwhile for us to use SC MAGLEV, and the total travel time would be longer not shorter. And it would cost more.
    - Better options and access are available now for Marylanders; Metro, Light Rail, Subway, Marc Train, Amtrak, Acela Express, and others. Plus improvements are planned.
    - I do not know anyone who would use SC MAGLEV.
    - I would have no reason to use this MAGLEV train.
  - **Is the protection of homes, businesses, religious centers, schools, communities, personal property, recreational facilities, parks, green areas, family lifestyles and property values the top priority in selecting the route for this train, if it is going to be built?**
    - **If not, do not build it. The project has no true value to Maryland and potentially harms our state and its residents.**
    - **How Many homes and other facilities would be torn down if the train ran through Bowie and other communities on the same route?**



- **Why consider building this “TEST PROJECT” in Maryland and not in an area such as Philadelphia to New York where it should have more ridership?**
- **Can we cancel this project and return any remaining funds from the \$27 million grant from the Federal Government, so it could be used for other needed purposes such as medical insurance or infrastructure repairs?**
- **What is the environmental impact (construction runoff) and financial resources impact of this train experiment on the Chesapeake Bay, removing the potentially disastrous backup of silt from the Conowingo Dam, restoring the Bay and the seafood industry in Maryland? Recreational fishing? Crabs, oysters & rockfish?**
  - The Chesapeake Bay should be a much higher priority for Maryland than this train.
- **Who is liable for the \$5 billion loan from the Bank of Japan and what are the terms of that loan?**
  - We were told in the July <sup>10, 2017</sup> ~~12, 1971~~ by Mr. Henley that the funding will be provided by Japan and the US Federal Government and no funds were needed from Maryland. I do not believe that.
  - What are the Maryland possible costs?
- **What is the expected cost of a trip from Washington to Baltimore on this train?**
- **Why did most of us only hear about this proposed train 3 months after the 170 page Final Scoping Report was completed, with almost no participation from citizens?**
  - The Scoping process and report were fatally flawed, the report has no value, and the report should be discarded and scoping process done again after open honest disclosure of potential impacts of this project on citizens of Maryland.

- We were not informed about this train project, meetings or the scoping process.
- There were many ways to contact us if you wanted to: newspaper articles, newspaper inserts, mailing lists from state land records, religious institutions, community organizations, local government distributions, TV, radio announcements, handouts in the local communities, businesses, and etc.
- **What is the history and status of all of the SC Maglev proposals, projects, completions, financial successes, failures, cancelled proposals, cost overruns, projects bailouts by governments, medical concerns, legal concerns or any other problems or claims?**
  - This should also be part of the scoping report.
- **Have you released your Pro-forma evaluation / documents for this Washington, BWI and Baltimore project for review by independent third party analysts?**
  - If not, please release them as soon as possible to be able to review this project.
  - Also release the Pro-forma evaluation for the proposed plan for Washington to New York.
- **Who will; own the technology, manufacture the trains, Maglev operating components, operating systems, parts and jobs?**
  - What happened to build American, buy American and American jobs?
  - If this project is successful and expands in the future, who will design and build the trains, the operating components and parts?
  - Who would control this potentially major part of the United States transportation infrastructure, Japan or the United States?
  - Would the train system be compatible with other systems built in the US?
  - Could the US build high speed systems in other countries?
- **Who would pay for the more than \$100 Billion cost to build this system from Washington to New York?**
  - Japan? United States?

- This rail system makes no sense if it does not travel long distances.
  - Is there any possibility this would be financially successful for the US? NO!
  - The \$100 Billion cost estimate is probably very low. There is a very long building period with major tunnels and no revenue without trains operating.
- **Finally, why should we consider this test project in Maryland?**

Peter and Edna Granahan

Cc: President Donald Trump  
David Henley, SCMAGLEV Project Director  
Bradley Smith, Maryland Dept. of Transportation  
Federal Railroad Administration  
US Senator Chris Van Hollen  
Congressman Steny Hoyer  
US Senator Ben Cardin  
Governor Larry Hogan  
State Senator Doug Peters  
County Executive Rushern Baker  
Mayor Fred Robinson  
Bowie City Council Members



DATE 10-14-17

bwmaglev.info

**COMMENT FORM**

Name: DARLEAN HALL-BYRD

Address: 6707 GREENLAND MD

City: RIVERDALE State: MD Zip: 20737

Telephone: 240 667 2288 Email: dhallbyrd@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

*THIS NOT TAKING PEOPLE LIVES OR FUTURE into thought. What about the elder. Where are they to go. What about the homes that people have worked all their lives for. What about the community? The stores, schools, gas stations? Where are we suppose to live with the vibration of the train. Who is going to use something this fast. Do you really realize what you're doing to people lives. We were never notified this was going to People are all and have no where to go but where we are. Where will our grand children and great grand go from*



Comments: *hear. Can you send us information  
As to why this is happening where is  
the money coming from? You're taking  
our money and moving us out. why?  
why not improve what is here now  
instead of spending millions of dollars on  
something new.*

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

**SCMAGLEV Project**  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10-14-17

bwmaglev.info

**COMMENT FORM**

Name: Genevieve H. Johnson

Address: 3622 Jeff Road

City: Glenarden State: md. Zip: 20774

Telephone: 301 773 8813 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have lived in this house for 54 years,  
I raised one family, and 1 grand family and  
hope to raise a great grand family in this same  
house. I want some one to tell me personally,  
why would I sell my house after 54 years. I  
tought school in this county for 38 yrs. I am comfortable  
where I am. Please leave us alone as I am  
79 years old and do not want to move.  
In addition, why can't these new train lines  
hook up with metros, The Mark train and or  
Amtrak line, this seems to be more cost  
effective. Moreover I stayed awake all night  
to get my comment on this (MAGLEV) train  
I would like an answer from someone and  
over

Comments: Comments that would ever affect Glenarden  
in my lifetime.

Genevieve G. Johnson

3622 Jeff Road

Glenarden Md 20774

301-773-8813

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Shirley Conley  
 Address: 9008 Whipkey Ct.  
 City: Bowie State: MD Zip: 20720  
 Telephone: 301-262-4656 Email: slconley500@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am opposed to the Superconducting MAGLEV train disrupting our communities of Bowie and Glenarden. The disruptive nature of a train to our homes, schools, churches and businesses will be astronomical to Prince George's County.

The residents of Prince George's County will not benefit from a fast speed train. The train will disrupt our livelihood, potentially causing residents to have to relocate creating a financial burden for those who have invested in establishing their homes and businesses in Prince George's County.

January 2018 Could someone tell me how MAGLEV will benefit residents of Prince George's County.



Comments: I do not understand why it is such a priority to build a high speed train from Washington, D.C. to Baltimore, Maryland. Who does this train benefit? I vote to STOP the train!!!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/14/2017

bwmaglev.info

**COMMENT FORM**

Name: Jim & Debbie McGowan

Address: 7308 Westwind Drive

City: Bowie State: MD Zip: 20715

Telephone: \_\_\_\_\_ Email: deb.jim@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments:

① No one I have met at any of the multiple large meetings received a postcard prior to impact study being done. This despite the fact that the hundreds of people in these meetings were potentially impacted. If you want valid input on impact then you have to ask those who might be impacted. If you want to check off the box on study completion then, that is a different story

② No one in the negatively affected areas will benefit from this train blowing through their community. There are no stops in Glen Arden, Bowie, Piney Orchard. Any speed advantage of

The train for these people is negated by

Comments: a 30 minute Trip To D.C. To catch The  
Train

~~3) Sure is A~~

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10-14-17

bwmaglev.info

COMMENT FORM

Name: Denise Barone

Address: 12107 Raritan Lane

City: Bowie State: MD Zip: 20715

Telephone: 240 401 6716 Email: Gasper d1 FAST@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Maintenance Yards - <sup>why</sup> So large near Bowie Univ? What will occur here?

→ I received no notification via postcard mailings regarding the scoping or any open house. This is not being transparent AS I found out from neighbors.

→ What are the noise levels when standing near train passing at 300mph?

→ What will ticket price be for Washington, DC to Baltimore, MD?

→ What will total project cost be + how much will be incurred by MD TAXPAYERS?

→ Why hasn't requests for re-opening of the scoping process been addressed?

→ What is the effect of this type of electromagnetic fields tech. on peoples health?

→ What is the current safety record of this train in other countries?

→ If homes are impacted by eminent domain how will asbestos be environmentally handled during the home demolition?

→ If homes are impacted to eminent domain, how will the current property owner be justly compensated?

→ How will you handle noise accumulation? where will the runoff be directed + how will the runoff be directed?

→ How will debris on tracks be handled?



Comments: Who will be in charge of onboard security?

- What will be the 'working/running' hours of this train?
- What is the source + amount of energy to ~~to~~ operate this train?
- What is the source + amount of energy to operate this train?
- How many feet/miles does it take for train to reach top speed?
- How many feet/miles ~~does~~ <sup>does</sup> it take for train, traveling at top speed, to slow to a complete stop?
- Who handles maintenance of train + electric substations supplying the power?
- IF the train loses power, while running, how will Passengers be safely evacuated when in tunnels + on the elevated guides?
- How will ~~the~~ wetlands + Floodplains be protected?
- What are effects of wildlife when coming in contact with the train guides?
- What is the length of the Construction Phase?
- Homes impacted by eminent domain → how will new home buyers be compensated when they owe a 30 year note to a Bank?

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE \_\_\_\_\_

bwmaglev.info

**COMMENT FORM**

Name: Anne Simonetti

Address: 12811 Roundtree Lane

City: Bowie (≠ hope) State: MD Zip: 20715

Telephone: 410-721-0936 Email: adsimon@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

- Comments: it is apparent that people in this area dont want to lose their property. this fact has already been established. Alternatives need to be found because
- it can be afforded to elite travelers - do we have enough elite travelers that will use this train to offset the cost?
  - it will not help relieve the traffic congestion in this area.
  - not reduce greenhouse gases (EPA)
  - this train will only cost Bowie with absolutely no advantages for this area → I feel like we are a speed bump in your agenda.
  - the man behind this doesn't live in USA!



Comments: Beef up the transportation route that we already have - that would be wonderful + economical to boot -

\* Causes less revenue to state + city

it is a no-win situation all around for this area no matter how you look at it.

what about NEX GEN Goela?

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DATE \_\_\_\_\_

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**COMMENT FORM**

Name: Anthony E. Powell

Address: 7918 Piedmont Avenue

City: Glenarden State: MD. Zip: 20706

Telephone: 202-288-7663 Email: PowellAnthony@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The state of Maryland's main source of income is personal property taxes. A lot of the houses along the proposed routes were built in the 1950s, 1960s and the 1970s. A high-speed train going near these neighborhoods four times an hour will cause "cracks" in the foundation of these houses forcing the residents to move. This will cause a loss of personal property taxes and force "property values" to go down. A drop in property values will cause long-term residents' net worth to drop. Also, I attended two "open" forums and no one testified in favor of the project. The Federal Government is promoting a project that is not



Comments: wanted by the general public. There is  
no evidence that a lot of people will ride  
the train.

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DATE 14 OCT 2017

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**COMMENT FORM**

Name: ARTHUR C. TANNER  
 Address: 12309 MELODY TURN  
 City: ROWIE State: MD. Zip: 20715  
 Telephone: 301-464-2667 Email: aseatanner@yahoo.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: One of the questions the sponsors of today's "open house" are offering answers to is "Why study MAGLEV?". The reason to me is clear: The federal govt. has made \$27.8 million available.  
There is no need to cut the travel time between D.C. and Baltimore to 15 min. when it already stands at less than 30 min. And AMTRAK is increasing its maximum Acela speed to 160 mph.  
This project might make sense between NYC and San Francisco, but it makes absolutely ~~no~~ sense for this short route except to prove a concept and provide a rationale for building from Baltimore to NYC. The cost for this project would be stupendous, and ~~it~~ would not significantly increase ridership because only the very affluent could afford it, and most people would continue to take the marginally slower but significantly cheaper

transit options.

Comments: And the environmental impact would also be stupendous, regardless of which route is selected. The project would disturb routine life along its path for decades.

I could go on with many other reasons why this project should be killed, but I will simply say, the ONLY OPTION for this project is NO BUILD. How many people here today who do not have financial interests in this project are in favor of it? In my opinion, VERY few. And as you are aware, hundreds have turned out. BOWIE does not want this project. Please KILL IT!

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DATE 10/14/17

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COMMENT FORM

Name: Van Black  
 Address: 3322 Hayes Street  
 City: Glenarden State: MD Zip: 20706  
 Telephone: 202-754-6247 Email: vonblack@rocketrail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am a 4<sup>th</sup> generation resident of  
Glenarden, Maryland. This is a very unique  
town. During a time Blacks in America  
was fighting civil rights and the right to  
migrate to suburban communities in and  
around Washington, D.C. This was not the  
case with Glenarden. Both my father's parents  
and my mother's parents purchased their homes  
brand new amidst other affluent Blacks  
without conflict or harassment. They founded  
the city and to this day serve on the council.  
Glenarden is our home and an American  
heritage with no "Dark racial" history.  
To obliterate such a beacon of American values,



on

Comments: would be a tragedy of monumental proportions and a blatant disregard of a people's right to legacy. 5<sup>th</sup> even 6<sup>th</sup> generation family members frequent the home of my Grandparents to this day on both sides of my family.

Glenarden is all I've ever known.

Glenarden is one of the only towns founded by us, ~~for~~ for us, and successfully managed by us in the country. At least in Maryland.

What a tragedy to uproot a single family from the "historic" parts of Glenarden it would prove to be.

We're for progress and believe the train is needed, but not at the expense of our scarce legacy!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)

1004



DATE 4 Oct. 2017

bwmaglev.info

COMMENT FORM

Name: Rhonda Ferguson  
 Address: 1409 3rd St  
 City: Glenarden, MD State: MD Zip: 20706  
 Telephone: \_\_\_\_\_ Email: traferg@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This project needs to be stopped until they have completely explained the end to end process to the impacted and surrounding communities. It is imperative that those impacted have a clear understanding (and the proposed benefits (which there don't appear to be any)). The fact that this train is going through so many communities where people who have worked hard to build homes for and community for their families, and they won't be able to even afford to use the train. It will tear down what most Americans want to live and enjoy life, be productive citizens in the communities they live, and work, worship, shop etc. It is a true blaten disera. disera that

this could even be considered a knowing the impact



2014

Comments: this will have on the residents of not only  
Prince Georges County but the entire state of MD.  
I believe there are a number of areas that have  
not been developed that could be used to help  
this project w/out impacting families and businesses.  
I beg as a resident that this project be STOPPED.  
PE The residents wint be able to afford to use the train.  
The health issues w/ adding this train (electricty etc)  
what is the impact of this?  
Who funds this? who will be taxed once its  
completed? The state of MD doesnt need to have this  
train, we need things that will enable this area  
to keep thriving, our schools will also be impacted  
This idea needs to be re thought, the fact that it's  
stated the project started in 1962, and was proposed  
to keep moving is not enough, nothing has been done  
or said in this area for people to oppose.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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3 of 4



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE \_\_\_\_\_

bwmaglev.info

COMMENT FORM

Name: R. Ferguson C

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Continued: when where the people in the communities poled to see if it was something they wanted? where are the data points that this is needed or wanted.

If developers were not allowed to continue building ~~and~~ <sup>we</sup> wouldn't have the need to have this train. We need to have archetives/<sup>engineers</sup> to design better ways to lay out the current structures/highways, Road/Rail ways..

Please stop this Project,  
you are not only impacting the low income  
you are not only impacting the elderly, you

are impacting americans who have worked and served these communities for years



4064

Comments: and it will only put America in horrible shape. We beg of you to STOP the project!

Be transparent! Be open.

Think about the people. Think about the impact of communities and families.

Please dont allow Government to come into our ~~communities~~ communities and tear down what we have built.

Work in partnership w/ Amtrak, Marc train to improve their infrastructure Dont impact the ~~communities~~ communities

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DATE 14 Oct 2017

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COMMENT FORM

Name: Valerie Boshare  
 Address: 7856 Dogwood Blossom Rd.  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-969-6375 Email: ValrieB10@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \* This is an unnecessary project!!

1. Amtrak can be upgraded to go 200 mph using the Acela technology + train.
2. Historic properties, parklands, and sensitive environmental areas will be adversely affected in PG + AA counties.
3. Hundreds of homes and businesses, and thousands of Maryland citizens will be displaced, and negatively affected personally and financially.
4. Notification was NOT given to all those within 1/2 or the proposed routes or a mile on either side of the proposed routes.
5. This proposed project is in direct competition with Amtrak. Amtrak already exists!

6 Ridership projections are incorrect - the numbers



Comments: are far too high for this mode of transportation,

- 7. current Maglev projects throughout the world are NOT turning a profit, and are heavily subsidized by taxpayers.
- 8. Again, this is a handout that will be primarily for the benefit of wealthy business people and politicians.
- 9. Why was the scoping study not audited?
- 10. Why are the MDOT and MTA supporting this?
- 11. What are the payment terms for the \$5 billion loan from Japan?
- 12. Will the taxpayers of Maryland be on the hook for subsidizing this if it is built, and when it loses money?

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 Baltimore, MD 21202

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DATE 10-14-17

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**COMMENT FORM**

Name: Darcie Gingell  
 Address: 12412 Rockledge Drive  
 City: Bowie State: MD Zip: 20715  
 Telephone: 570-417-9639 Email: lilysmi@aol.com

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I am a very concerned citizen of Bowie, Maryland.  
I received info about project in July 2017 after Scoping process  
was completed from Nov-Dec of 2016. I was not notified of process  
and should have. The Maglev train will be a waste of tax payer  
money and here are some other reasons why I disagree with it.  
Based on research only 6% of DC commuters are from Baltimore and  
3% of Baltimore commuters are from D.C. If I had been notified of  
scoping process, the idea of asbestos contamination could have been  
brought up. Most Levitt houses contain asbestos which will pollute  
the environment when they're torn down. Most areas of the world  
have few passengers who ride high speed trains. For instance only  
6% of passengers in Europe use railways. Another example  
is a high speed train from the Shanghai airport to downtown  
Shanghai only fills 20% of its seats with passengers.



Comments: Most people in Shanghai don't like the train because it doesn't take them where they want to go. High speed trains cost millions of dollars and yet few (in Europe/Shanghai) use the rails. Our current transportation options could use money for improvement instead of a new MAGLEV. The new plan for Acela trains will be the most energy efficient in the world compared to MAGLEV. Also the current Acela train can get me from D.C. to Baltimore in 20 min. The MAGLEV will get me from D.C. to Baltimore in 15. That is not much of a difference. Five minutes ~~of~~ difference is not worth the tearing down of schools, churches, businesses and homes. One plan for a high speed train (Connecticut/Rhode Island) bypass was dismantled because of the effect on businesses and homes. I hope this will be the case for Bowie and its surrounding areas.

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/2017

*bwmaglev.info*

**COMMENT FORM**

Name: Leslie Ann NATARO

Address: 1338 Chapelview Drive

City: Odenton State: MD Zip: 21113

Telephone: 301-906-2886 Email: leslieann427@msn.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

① How is the Forest Conservation act being addressed? Many trees will need to be cut for the track line & maintenance yard.

② How will this train reduce traffic in the DC-Balt. area. Most traffic is going from suburbs to other suburbs or office developments - not DC or Baltimore

③ How will the Chesapeake Bay be protected from the runoff of wastewater off the tracks after rain or the "warming" process in the winter.

④ What jobs will be created by this train on a permanent basis?

⑤ Where are the ridership studies posted?

⑥ How much vibration is set up by the train in the tunnels and what is the effect on the buildings above?

Comments: This train, in my opinion, is nothing more than a "toy" for a few elite people. The cost of a ticket is too much for the average commuter. How many people, other than Kevin Plank, live in DC and work in Baltimore?

By the time it is built and then goes belly-up, I will not be living here, but my children & grand-children will be and will have to foot the bill to pay back the loans.

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 Baltimore, MD 21202

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DATE 10/14/17

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COMMENT FORM

Name: EARL JONES

Address: 12114 SWINBURNE PL.

City: GLENN DALE State: MD Zip: 20769

Telephone: \_\_\_\_\_ Email: ejdesigns94@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The proposed line along the W&A trail runs directly through my neighborhood in Glenn Dale, MD. The construction of this train line would completely wipe out our community, multiple others, schools, parks, historic districts and churches. We have raised generations of families in this area and do not wish to be pushed out and forced to move because of rich individuals who wish to get richer. This train serves no purpose to the residents of P.G. county as it only runs from Baltimore to D.C. with no stops. Please stop trying to feed us the lies about the benefits, when there are none.

We lose our homes, and communities, while you make money off of a train that serves no purpose.



Comments: The only purpose is that you reap the benefits of becoming wealthier, while we are forced to rebuild our lives and try find somewhere else to live. I'm sure you wouldn't care for this if this was happening to you and your family! The other thing is with terrorism as it is in today's landscape... **this MAKES IT VERY EASY FOR A TERRORIST TO GET ON A TRAIN AND RUN A BOMB RIGHT INTO WASHINGTON DC, D.C.**

Also the scoping process was a joke. We were never notified publicly by your organization of the meetings prior to completing your scoping process. We were told you held meetings in ~~the~~ secret and barely no one showed up because of where you posted your ads. You have been under-handed, slick, extremely dishonest, and not forth coming, regarding this entire project to this point... in an effort to get to where you are now.

**DO NOT BUILD IN OUR COMMUNITIES. YOU DON'T DESERVE OUR LAND AND OUR FUTURES!!**

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**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/14/2017

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**COMMENT FORM**Name: Naomi WambleAddress: 3006 Berrywood LaneCity: Springdale State: Maryland Zip: 20774Telephone: 240-281-3811 Email: njwamble@comcast.netJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This train is not necessary in our community just for some rich business people to get to another location a few minutes sooner, buy a plane. This devastation will destroy communities, homes, and the lives of many of the residents in Prince George's county. ~~This~~ This train will not benefit our county because it will not stop in this location. We have so many needs in this country, invest your money somewhere else. There is not a great demand for high speed transportation, people around here have a tendency to be home bound. There is more to life than making money, the life of people and their basic needs is more important. No to this train!!!



Comments: Send maps and let us know if other communities, cities, or state are saying NO to this construction project.

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Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 6/14/17

bwmaglev.info

COMMENT FORM

Name: Bonnie Roberts

Address: 13111 Martha's Choice Circle

City: Bowie State: MD Zip: 20720

Telephone: 301-464-5046 Email: bonnie@hddridge.org

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Now that you have narrowed the routes down to 2 options, what is your plan for notifying people directly in the path? We were just knocking on doors of businesses in Old Town Bowie and none of the people we spoke with were aware that their businesses are in the path of one of the 2 preferred alignments. Only one of them was even aware of the SCMaglev project at all.

Why don't you go door to door? There is a much smaller number of people involved now. Mailings + newspaper ads are not enough!



Comments: \_\_\_\_\_

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→ It is not appropriate for this private  
 company's logo to be on the same  
 page with MDT. Suggests you are colluding!!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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DATE 10/14/17

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**COMMENT FORM**

Name: Natasha Jones

Address: 12114 Guinevere Pl

City: Glenn Dale State: MD Zip: 20169

Telephone: \_\_\_\_\_ Email: nwhite112@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The scoping process was closed by the time we were made aware that one of the proposed routes came right into our neighborhood. The means of communication used by the group was insufficient and intentionally left constituents in the dark. Most of the communication provided to the public and our neighbors came from community groups opposed to the project. Project leads have not been transparent and therefore are not trusted. How will increased communication occur as this project moves forward? How can we be certain that the communities impacted by the project all kept informed? Additionally why are

Comments: We not considering cheaper and less environmentally impactful means of improved transportation infrastructure? How will the school system account for the students who are impacted by closing schools due to this project? What are the environmental impacts of the work needed to get this train up and running? Why have other Maglev projects been abandoned?

There needs to be increased communication, increased transparency and increased accountability to the persons who live and work in these communities.

Maglev is not the answer to the transportation needs in Prince George's County. Who will benefit by the loss of this community?

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DATE 10-14-17

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COMMENT FORM

Name: Jim ANDREWS

Address: 4800 RAEMORE LANE

City: Bowie State: MD Zip: 20715

Telephone: 301 262-5811 Email: james-cpm@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1. Other alternatives should be considered such as AMTRAK NextGen.

2. MAJOR impact on MD tax revenues due <sup>to</sup> eminent domain properties being removed from ~~the~~ tax rolls.

3. Future projected revenues did not factor in more desirable AMTRAK Next gen ridership option

4. Fed + State funding directly or thru loans (if defaulted - who pays - state + fed)

5. Screening was rigged to only allow consideration of maglev

6. Ridership taken away from

AMTRAK will lose. It increased



Comments:

7. Public comment opportunities were totally inadequate AND purposely rigged to substantially reduce public input.

A. 68 post cards sent out during holiday period ~~to~~ Thanksgiving to Christmas.

This whole process should be started over, officially, to reinstate lost time for public input - irrespective of recent efforts.

8. Stop maglev completely - ANY route will harm MD and its citizens/businesses irreparably.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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DATE 14 OCT 2017

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COMMENT FORM

Name:

JOSEPH S. MROZ *J. Mroz*

Address:

1706 JONAFREE COURT

City:

ODENTON State: MD Zip: 21113-3961

Telephone:

410-305-0658 Email: js.mroz@VERIZON.NET

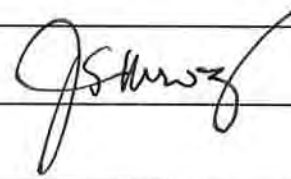
Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: MORE INFORMATION IS NEEDED ABOUT THE STATUS OF THE ENTIRE PROJECT TO CONSTRUCT A HIGH-SPEED SCMAGLEV TRAIN SYSTEM BETWEEN WASHINGTON, DC AND NEW YORK CITY, AND THEN TO BOSTON, MA. IT IS NOT LIKELY THAT THERE WOULD BE ENOUGH PASSENGERS TO SUPPORT <sup>BOTH</sup> AMTRAK AND A RAPID RAIL SYSTEM.

THE OTHER SEGMENTS IN THE OVERALL WASHINGTON, DC TO BOSTON, MA PROJECT SHOULD BE IDENTIFIED AND A SUMMARY OF THE STATUS OF EACH SHOULD BE PROVIDED. IN ADDITION, THE LOAD ON THE MARYLAND POWER GRID SHOULD BE PROVIDED FOR THE WASHINGTON TO MARYLAND STATE LINE SEGMENTS, AND, IF THERE IS A NEED FOR NEW POWER STATIONS, THIS SHOULD BE MADE KNOW. (JSM CONTINUED)

Comments: FROM MY PERSPECTIVE, THERE IS A SIGNIFICANT RISK THAT, IF ONLY A BALTIMORE TO WASHINGTON RAPID RAIL SYSTEM WAS BUILT, THERE WOULD NOT BE ENOUGH PASSENGERS USING IT TO KEEP THE SYSTEM PROFITABLE. IF BWRR DECLARES BANKRUPTCY, THEN WHAT HAPPENS TO THE ABANDONED RAPID RAIL SYSTEM?  
 I AM CONCERNED THAT IT COULD BECOME A HUGE EYE-SORE!



PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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DATE 10/14/17

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AGAINST IT! COMMENT FORM NO SCMAGLEV!

Name: Ramona L. Thompson

Address: 6814 EADS STREET

City: CAPITOL HGTS, State: MD Zip: 20743-2162

Telephone: CELL 301-613-0314 Email: \_\_\_\_\_

Join our mailing list?  Yes  No I'm definitely Against System being

Preferred method of communication?  Email  Mail Built here on East Coast

Comments: ① I'm concerned about my home being taken from me as a retiree. I'm 62 where would I go at my age with an elderly husband and 85 yr old mother living off RT 704. Both my mom, other family members live off RT 704 from Bowie, MD Area to the DC Line in Seat Pleasant, MD.

② I'm concerned about the magnetic field and our health impact. My home is NOT able to sustain the heavy vibration of ground shaking and shifting the train would produce. IT'S falling Apart now!

③ I'm concerned about the animals, wildlife + vegetation being destroyed. What about our historical areas and Parks + Recreation areas.

④ I don't think we can afford the cost to ride the train, nor pay for it in our US Taxes, MD + AGES OR TOWN TAX bills. Please fix what we currently have Now!

⑤ The USA is broke as it is - We owe China, Japan + everybody



Comments: I AM TOTALLY AGAINST THIS PROJECT ALTOGETHER.

I'T WILL NOT IMPROVE ANYTHING FOR US NOT THE POLLUTION, AIR QUALITY NOW OR IN THE FUTURE.

WE NEED OUR ROADS + OTHER PROJECTS FIXED FIRST.

IT'S ALL ABOUT THE MONEY!  
STOP PICKING WITH PRINCE GEORGE'S COUNTY MD. THAT PROJECT WILL AFFECT ALL OF PRINCE GEORGE'S CO. RESIDENTS AND WORKERS, EMPLOYEES AND VISITORS. THIS PROJECT AFFECTS OUR LIVES AS WELL AS ALL  
≡≡≡  
THE CHURCHES IN THE COUNTY.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: WALDO W. N. JOHNSON

Address: 8621 LESLIE AVENUE

City: GREENARDEN State: MD Zip: 20706

Telephone: (301) 772-6444 Email: wwnjohns@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: YOU HAVE SUDDENLY TAKEN GREENARDEN OFF THE  
MAP FOR CONSIDERATION OF HAVING YOUR TRAIN COME THROUGH  
OUR CITY. I FEEL THIS IS A GUISE TO HAVE US BE  
NON-RESPONSIVE TO FUTURE MEETINGS, ETC. SO, WHEN A  
FINAL DETERMINATION IS MADE, GREENARDEN IS CHOSEN  
BECAUSE WE DID NOT RESPOND IN NUMBERS SUFFICIENT TO  
GARNER YOUR ATTENTION. I SHALL ALWAYS BE  
WATCHFUL AT WHAT'S GOING ON, AND, SHALL ATTEND ALL  
MEETINGS, TO SEE IF SUBTLE DIFFERENCES/CHANGES ARE  
MADE IN YOUR PRESENTATIONS TO CLOUD OR VELL YOUR  
TRUE AND ACTUAL MOTIVE(S) IN GETTING THIS DISASTEROUS  
TRAIN PROJECT OF YOURS, OFF THE GROUND.

January 2018 ADDITIONALLY, YOU HAVE SKIRTED THIS COMMUNITY FOR Page C-521

UPWARD OF A YEAR BEFORE FINALLY GETTING THE WORD OUT

Comments: ABOUT THIS PROJECT. I STILL HAVE NOT RECEIVED ANY OFFICIAL NOTIFICATION, (IN WRITING!!!), ABOUT THIS PROJECT AS IS PROPER... DECENT AND IN ORDER.

REV. WAUDD W. N. JOHNSON

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DATE 10-14-17

bwmaglev.info

## COMMENT FORM

Name: Edward Wysocki  
 Address: 4804 Rocky Spring Ln  
 City: Bowie State: MD Zip: 20715  
 Telephone: 301-805-1839 Email: edthefed@verizon.net.

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: <sup>①</sup> MDOT has done a very poor initial job of communicating with the people of MD. The scoping phase in Dec. of 2016 was unknown to the people of Bowie. No one in Bowie that I know was aware of the SC Maglev project until May. MTA did not come to the Bowie City Hall meeting in July. Now finally MTA is having meetings and is finding out that the people are not in favor of having their neighborhoods ruined by an expensive, unproven technology. <sup>②</sup> MTA is also working way to closely with the SC Maglev people. There should be a wall between MTA + SC Maglev.



Comments: <sup>(3)</sup> MARC and Amtrak provide sufficient service between D.C., Prince George's County, BWI and Baltimore. It is inexpensive and reliable service. 40,000 people use MARC. Is the Mag Lev going to provide cheaper more reliable service. NO. The current study is not considering the existing rail line as a viable alternative to the Mag Lev. The Mag Lev has to be considered as a viable transport between D.C. and Baltimore. Not as part of a D.C. to NYC route. As a viable transport it does not work between D.C. and Baltimore. It cannot be as inexpensive as MARC or Amtrak. If it would be built without a NYC. end, it would be a nonviable rail route and the citizens of MD would be picking up the bill for SC Mag Lev.

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Baltimore, MD 21202

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DATE 10/14/17

bwmaglev.info

## COMMENT FORM

Name: WARREN W. JOHNSONAddress: 8621 LESLIE AVENUECity: GREENARDEN State: MD Zip: 20706Telephone: (301) 772-6444 Email: wwjohns@hotmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I received a card indicating the massive reduction of greenhouse gasses and how the train will be an environmental benefit to the areas through which it travels. My question comes from my failure to comprehend the basis of your statements on the environment. How will the air quality improve in my area (Greenarden, MD)? Those using the train will be those ~~to~~ who historically use the airplane. This is based on the similar costs in ticket pricing. This will do NOTHING to reduce vehicle traffic down 70% or 495/95. Those who drive, even those going to New York, currently drive because the plane costs too much. Soooo! The train will be the same cost as the plane, which will be out of their reach. Thus, how will the air quality in my area decrease when there is NO reduction in vehicle traffic, but an increase in pollution,

Comments: from whatever the train emits! Seems like your  
train will make the air quality worse for us in Glenarden,  
NOT BETTER!!

Rev. Walter W. N. Johnson

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 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10/14/2017

bwmaglev.info

COMMENT FORM

Name: Amy Bogan

Address: 13926 Old Stage Road

City: Bowie State: MD Zip: 20720

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am against the Maglev train through the Bowie area as well as other areas in Maryland.

- Amtrak + Acela already exist between Baltimore + D.C. More discussion is needed regarding the purpose, infrastructure and cost to ~~the~~ residents in the impact area. What about improving existing lines?

- I have concerns about how the project will impact the environment, noise level, health impacts, agriculture of nearby communities.



Comments:

I don't feel that the majority of MD citizens will benefit from the project but may be paying for it with increased taxes as well as dangerous impacts to the community.

Please halt the project and examine other alternatives.

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 14 Oct 17

bwmaglev.info

COMMENT FORM

Name: PAUL HUBAWKI

Address: 12207 RARITAN LN

City: BOWIE State: MD Zip: 20715

Telephone: 301-404-7682 Email: paul@mindstar.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Thank you for dropping the BWA Trail Yellow Line that would have bulldozed 3000+ houses & destroyed many neighborhoods.

This maglev Train is <sup>Not</sup> ~~not~~ needed -- there are already high speed trains.

How wide are the routes

where in the world is a maglev operating underground

Why do you need a 270 acre maintenance yard

we need details on building & activities in maint yard.

This is crazy to destroy homes & communities for

the rich elite biz traveler.

What happened to "buy american" this is making

japanese comp. rich.

Tax \$ should be spent to improve repair existing

Comments: Tell us who benefits from this?

Not enough riders to ~~make~~ pay for the build cost. This will need a bailout eventually & fall on taxpayers.

Is the "maintenance yard" really a maglev construction factory complex? Details

Why is BWRR & NEMaglev so secretive  
Why are meetings & info not announced.  
Scoping period was not well advertised & too short.

What are ~~the~~ health impacts ~~from~~ from high levels of magnetic energy

Health impacts from high levels of sound?

This won't run in snow ice

What is the impact on property values mid county where the train goes (massive loss of value)

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DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Loretta F. Jones

Address: 4204 DAY Lily Drive

City: Bowie State: MD Zip: 20720

Telephone: 240-615-8071 Email: lorettaj376@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: SC Maglev Project should be cancelled for the following reasons: (1) Federal, state, and local monies should be use to fix up our current roads, Highways, and Bridges; (2) Homes, Parks, and the little natural environment will be impted by the development; (3) Residened ~~Peop~~ People in ~~the~~ the affected areas will not have use for this service unless they goto DC or Baltimore, therefore it does not reduce traffic along the BWI, 495, 95, 295 ~~and~~ routes; (4) ~~the impact~~ This invest ment is only desiged to make the rich and Stakeholders reacher off the backs of Lower-middle income retirees and

Finally! Turn over



Comments: This project really needs to be cancelled and taking off the table permanently. Our Federal, state, and local monies invested in this project ~~she~~ must be used to take care of the people who so desperately needs it.

Suggestion for the upcoming meetings:  
Please have ~~some~~ representatives from MD State come and speak about their process of buying/paiding for residences homes/property that may be taking ~~to~~ taken for this development.

Respectfully,  
Loretta F. Jones

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DATE ~~14~~ Oct. 2017

bwmaglev.info

**COMMENT FORM**

Name: Deanna E. Todd Spindler

Address: 13419 Overbrook Ln.

City: Bowie State: MD Zip: 20715

Telephone: 301-946-1631 Email: Todd.Spindler@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: • Concerns about maintenance yard - pollution? industrial zone?  
Particularly industrial zones over Pax Refuge. Industrial waste  
near the river + water zones. Environmental impact statement  
only covered noise + vibration? what about the birds + animals?  
 • It does not serve the local community. Yet will our taxes  
pay for it? NO! we get all the impacts & no benefits.  
 • Acela already goes btw DC + Balto in 20 min. Do  
you really expect local commuters to pay ~\$100 one way?  
Estimates of CO2 reduction & Miles travelled are misleading.  
 • AMTRAK is already investing ~150 Billion Dollars into  
track/line/train infrastructure. Our taxes already paying for this.  
What does MagLev bring to improve our lives? nothing

January 2018 increases our real estate values, assuming we don't Page 0533

bring our homes & communities to the rail.

Comments:

- Technology transfer - does the "training" include the actual technology or just maintenance & operations. Do we just get/buy a "black box" that we have to maintain, buying more Japanese components from a single source.
- Zero official information or notification to local community until now. All we heard was from local contacts.
- Maglev has failed in many places - even Germany. What are the chances it could actually work here, where people are NOT used to even ride-sharing. We would welcome a mass transportation that would actually work for the local community.
- The Japanese maglev is underground in all residential areas. This one is above ground for ~1/3 of the line - along residential areas. The <sup>effect of</sup> EM radiation from passing trains on sensitive electronics such as pacemakers & other medical equipment has not been addressed. This neighborhood is mostly older houses with elderly & retirees.

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DATE 10/14/17

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COMMENT FORM

Name: Stephanie Berry

Address: 822 Nalley Rd

City: Landover State: MD Zip: 20785

Telephone: 301-322-4255 Email: goforObama08@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My concerns are

① Parking - there isn't much parking available around current Amtrak/metro stops/buses. Need to be able to get to

② Access to transportation - <sup>Maglev stations</sup>

Will need to be able to move people to surrounding areas

~~#~~ <sup>current</sup> Metro + buses will reduce the benefits of Rapid <sup>rail</sup> ~~rail~~

③ Potential to build beyond Baltimore to

~~NY~~ NY, BOS + Philly. I support MAGLEV DC

because it will ~~be~~ go beyond Baltimore at some point.

Summary, WIFM's are still not clear. Need to be

able to benefit from the reduced commute time

w/ access to parking + transportation that takes

commuters to workplaces, meetings + most importantly



Comments: Businesses + Tourist spots in DC, Prince  
Georgis County + Baltimore).

Thank you for the opportunity to  
comment.

*[Handwritten Signature]*

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DATE 10/14/17

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**COMMENT FORM**

Name: Michele Vance

Address: 6926 Hanover Pkwy #302

City: Greenbelt State: MD Zip: 20770

Telephone: 301-717-1140 Email: Kittybutt\_20772@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I believe we already have many modes of public transportation that need to be fixed and upgraded and people are not using them. We don't need another one that's going to be too expensive for people to use and not feasible for the locals. The project will add more debt, more destruction, and more clutter. We already have abandoned railways that have been left to take up environmental space.

We are running out of open environment space and clean air. This will destroy more land and add more to the environmental

destruction. Many, many people use the WPA Page C-537

to it daily. ~~It~~ It is safe and well

Comments: maintained. It increases property values because people want to live near it. I use it weekly as I keep my horse at a farm that backs up to the trail. Many farms are being destroyed and land taken to development. Where are those farms and business owners of these farms supposed to go?

The Bowie route would displace many homes, but also over 30 people that keep their horses at the surrounding farms alone. Where are we supposed to go?

Bottom line, it's not worth it, it will be too expensive, too destructive, and inconvenient. Ridiculous!!!

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Janice Uthe  
 Address: 7525 Old Chapel Dr.  
 City: Bowie State: MD Zip: 20715  
 Telephone: 301-352-0957 Email: janice.uthel@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am concerned as a taxpayer and citizen on this project. I do not understand this huge investment and how it will positively affect my community. The government has failed in supporting our existing infrastructure; i.e. Amtrak and Metro, both of which have commuting benefits. Why are we not investing in technology that truly impacts traffic and commuting? What is in this for Bowie? It doesn't stop here and will not improve the commute for our citizens. I am also concerned about the long-term impact of maintenance. Will this fall on our shoulders? (over)



Comments:

Many people have homes and a quality of life that will be impacted if this goes through. Also, there is the impact to nature and wildlife.

This technology is not proven if you look at Germany and Japan. I am concerned about safety and health concerns related to this for both riders and areas near the tracks.

This appears to be solely for the purpose of money, not for the residents living along the area. Who is going to ride this? What will be the costs to ride? Is this just for the purpose of attracting industry such as Amazon at the expense of the residents and taxpayers?

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DATE 10/14/17

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**COMMENT FORM**

Name: Peter Crellin

Address: 12900 10<sup>th</sup> St.

City: Bowie State: MD Zip: 20720

Telephone: \_\_\_\_\_ Email: ptcrellin@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

- The proposed lines shown on the map do not show the reality of community disruption that will be caused by the train line or ~~its~~<sup>the</sup> construction process. The proposed routes need to include information about what land will be taken by eminent domain and the basis on which people will be compensated for loss of their homes.

- Until a final route is selected, home values are already being impacted by the uncertainty of







Comments:

possible proximity to a bullet train line. With a final decision still nearly two years away, I expect not to be able to sell my home for years to come. This process has already caused me and my neighbors significant financial hardship.

- The proposed line through old Bowie (Amtrak Alignment) will be destructive to a historic community and a precious wildlife refuge. I am astonished that it is still being considered.
- At the April meetings, we were promised that potentially impacted homeowners would be notified. That has not happened, and many of my neighbors are still unaware of this project.

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

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COMMENT FORM

Name: Patricia A. WEBB

Address: 1018 60<sup>th</sup> AVE.

City: Fairmount Hgts State: MD Zip: 20743

Telephone: 301-925-9117 Email: patriciawebb51@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am opposed to the Maglev Project  
and due to the detrimental impact it will have  
on our communities. Fairmount Hgts is ~~from~~ the  
home to primarily elderly, retired people who will  
find it next to impossible to relocate.

I was not informed in writing about this  
train and the potential impact on my community.  
I was not notified by mail, email, flyer or  
postcard. This train will not benefit  
PG or Anne Arundel County. Stop the  
Maglev train AND the underhanded  
way in which the public is being informed  
about this issue. It is appalling that even  
# of municipalities were not informed by state govern



Comments: Many, many low income residents and the elderly will be displaced with little opportunity to find suitable housing that is affordable.

STOP THIS TRAIN!

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DATE 10/14/12

bwmaglev.info

COMMENT FORM

Name: Lydia Darr

Address: 5105 Baltimore Lane

City: Lanham State: MD Zip: 20706

Telephone: \_\_\_\_\_ Email: LDdarr@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am against the maglev train project ~~with~~ in general and certainly as it has been proposed thus far. One of the proposed routes literally is in my back yard following the walking trail (old train track). My issues:

1) Where is the benefit in ~~the~~ <sup>alleviating</sup> traffic? It appears that much of the traffic along the beltway <sup>is</sup> between Baltimore & Washington ~~is~~ comprised of people working in various hubs around the beltway <sup>along the parkway</sup>. This train will not help Laurel to DC or back; Ft. Meade, Columbia, etc. It does nothing to deal with

Comments: This traffic/these drivers.

2) All along the proposed ~~old~~ train route are protected wetlands and at least two historic preservation sites/homes. These environmentally preserved places & historic places should remain that way.

3) Why build a "new" routes?  
Follow the existing train track utilized by MARC Train & ~~Amtrak~~, Amtrak, or #5.

4) Proposed speed train between Balt & DC does "save time". ~~there~~ <sup>Start up</sup>  
↓ slow down speeds will not allow the train to get to & from the two cities in "15" minutes.

5) Use the B-W Parkway if serious about alleviating traffic.

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kathryn Ells  
 Address: 3910 Meadow Hill Road  
 City: Springdale State: MD Zip: 20774  
 Telephone: 202-445-0495 Email: Kac833@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have deep concerns about the efficacy of this project and I'm angry about the lack of public participation in determining whether to do this at all. From what I have read, this was in the works since 2015, and now, after receiving a grant, Maryland residents are being consulted. I am concerned about the safety of this high speed train, the noise, and other environmental impacts, not to mention the displacement of Maryland residents. I am not convinced that the ~~benefits~~ <sup>potential benefits</sup> of building this



Comments:

particularly for residents of Prince Georges county, without a train stop. My vote at this point is ~~the~~ the "NO BUILD" alternative.

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Rose Smith

Address: 12109 ROUND TREE LANE

City: Bowie State: MD Zip: 20715

Telephone: \_\_\_\_\_ Email: Rosematch2@gmail.com

Join our mailing list?  Yes  No

does no good

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

I want to be notified of MAGLEV mtgs & impact. I've yet to receive any notification what so ever. !!!

Questions:

- (1) How much helium do you need to run the Maglev?
- (2) Where ~~is~~ is the source of the helium?
- (3) How is the water run off going to affect the chesapeake bay? Will we get ice below Maglev & on roads from ~~to~~ near the Maglev?
- (4) if the price of Helium is market dependent how will this affect the ticket price?
- (5) This train has been proven to be financially

Comments:

unsustainable, so how much are my personal taxes in Maryland going to increase?

(6) How much is the cost of Maglev going to take away from existing transportation projects in Maryland?

Please respond to my concerns  
Thank you for addressing these issues

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

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**COMMENT FORM**

Name: Vijay Parameshwaran

Address: 12 Crescent Road, Apt. B

City: Greenbelt State: MD Zip: 20770

Telephone: 650-336-4994 Email: vijay.pwn@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

① This study is presented as a "Maglev study," and from a specific Congressional budget program, but it is better served as a "transportation study" of transit in the area, with auto, train, Metro, light rail, and the best way to expand. The results of that, given to the public, will really better inform us.

② DC and Baltimore are only 30-40 miles apart. Even though the Maglev can run at a high speed, what time are commuters really gaining by using this as compared to the MARC? Maybe 10-15 minutes? When I asked, the answer given was a vision of DC to NYC Maglev, which makes more sense, as it is a long distance (like the CA high-speed rail). Maybe this should be presented as a vision of that.

③ January 2016 proposal shows that the terminus station in DC is either the Met  
page 6:551

Verman station of NoMa / Gallaudet which are both very dense areas. How can



Comments: the area be cleared so that this Maglev can be built, especially here? What Amtrak has done is to have "satellite" stations in Alexandria and New Carrollton, which can serve as conduits into the city through the MetroBus & MetroRail. It begs the question, "can we think of a more holistic way of integrating all public transit, with timings and hub stations, such that a person can travel easily across the city by many means without waiting long times or being stranded?" For example, New York has the Port Authority to manage all this.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)

DATE 10/14/2017

bwmaglev.info

## COMMENT FORM

Name: MARLENE SAVAGE  
 Address: 4310 LAVENDER LANE  
 City: BOWIE State: MD Zip: 20720  
 Telephone: 443-864-8677 Email: MJThomasph@yahoo.com

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: We Don't want your TRAIN. Why do you target  
Neighborhoods that have a majority of people of color.  
You come into our communities, displace us, lower  
our property value & still won't meet us face to face  
to address our issues. Why won't you take the  
train to the white communities and leave us alone.  
It is UNFAIR to us. Please don't run your  
TRAIN thro Lottfords Vista Rd. Why don't you  
take your TRAIN into Montgomery County, Loudoun  
County, VIENNA VA, TYSON CORNOR, ANN ARUNDEL  
County why the BLACK community LEAVE our  
communities alone

Comments: We have the right to RAISE our children  
 have the American Dream of Homeownership Skip without  
 caring about losing our home that we worked so  
 hard for. You don't have the right to force  
 me out. Leave BAWIE alone. Leave us alone

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 c/o Suhair Al Khatib  
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 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10/14/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: REGINA A Smith

Address: 5434 Taylor Street

City: Bladesburg State: MD Zip: 20710

Telephone: 301-887-1952 Email: Amalas Magic@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Community

Comments: I DO NOT SUPPORT THE SC Maglev Train  
AND WILL VOTE AGAINST ANY POLITICIAN WHO DOES.  
THE TRAIN COMES THROUGH MY COMMUNITY AT THE  
BLADESBURG WATERFRONT AND HAS POTENTIAL TO IMPACT  
THE WATERFRONT PARK, WILDLIFE, THE ANACOSTIA RIVER,  
AND OUR HIKING/BIKING TRAIL. THE PROPOSED BUS  
ROUTE ALSO HAS THE POTENTIAL TO EFFECT THE BUSINESS  
COMMUNITY ALONG ROUTE 450; MY COMMUNITY  
DEPENDS HEAVILY ON THESE BUSINESSES. THAT INTERSECTION  
AT ROUTE 450 AND ROUTE 202 ALSO IMPACTS THE PUBLIC  
PLAYHOUSE, OUR LOCAL THEATER. THE TRAIN (ABOVE OR  
BELOW GROUND) RUNS THROUGH BLADESBURG HIGH SCHOOL,  
SOUTH HIGH SCHOOL, THE BLADESBURG COMMUNITY  
COLLEGE AND 2 ELEMENTARY SCHOOLS.

OVER →



PERSONAL

Comments:

THE PROPOSED BLUE ROUTE WILL RUN THROUGH MY STREET IN BLADENSBURG. MY HOUSE, AS WELL AS MY NEIGHBORS' HOMES, WERE BUILT IN 1947 - ALL FRAME HOUSES. I JUST SPENT A CONSIDERABLE AMOUNT OF MONEY HAVING MY HOME WATERPROOFED WITH ABOUT A DOZEN STEEL BEAMS DRILLED INTO PLACE TO FURTHER STEADY MY HOME, PIPES LAID AT THE FOUNDATION, AND AN ADDITIONAL SUMP PUMP TO DRAIN WATER AWAY FROM MY HOME. I HAD A CEMENT PATIO LAID, NEW ROOF, VINYL SIDING, AND SOLAR PANELS PUT IN PLACE. I BELIEVE CONSTRUCTION ABOVE OR BELOW, WILL HAVE AN EFFECT ON ALL THE RECENT CONSTRUCTION TO MY HOME ... THE DICKINS, THE DRILLING, HEAVY EQUIPMENT + CONSTRUCTION VEHICLES, AND THE FINAL PRODUCT - A MAGNETIC TRAIN.

I REPEAT, I AM AGAINST THE CONSTRUCTION ON THE SCMAGLEV TRAIN AND I WILL VOTE AGAINST ANY POLITICIAN WHO SUPPORTS SAID CONSTRUCTION.

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Baltimore, MD 21202

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DATE Oct 14, 17

bwmaglev.info

**COMMENT FORM**

Name: Eddimae G. Tisdale  
 Address: 5706 Umber PL  
 City: Bowie State: MD Zip: 20720  
 Telephone: 301 464-2538 Email: eddimae@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have attended all of the presentations, however, I am still not convinced of the benefits of MAGLEV to Prince George's County.

(1) The train does not stop in our county. The distance between Baltimore City & BWI Marshall is far shorter than BWI Marshall & New Carrollton.

(2) What employment opportunities are there for us once the train is operational?

(3) The # of homes and families which would be displaced and impacted is far too great and seems to be given little or no consideration.

(4) This project has targeted the Afro American

groups, ~~and~~ and the older adult population, however the benefits for us are very, very limited.

Comments: We consider the proposal has made our community, ~~our community~~ our county a short cut without benefits for us.

Let's improve Amtrak & Acela!

?? How did the project get this far without the public being informed??

Everyone in this county should have been ~~not~~ notified "BY MAIL" at the onset of this project! Your first study should have been mailed ~~it~~ to everyone in the study area.

Everyone does not have internet access, but we do have a postal mailing address.

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/2017

[bwmaglev.info](http://bwmaglev.info)

COMMENT FORM

Name: Karen Wheat

Address: 10112 Morning Glory Way

City: Bowie State: MD Zip: 20720

Telephone: 443-975-5599 Email: BKLYNGR

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I totally oppose this project regardless of the route. This train will run through the Black community (PG County) instead of Annapolis, Loudon & the like because our communities are "dispensable". This will displace, disrupt & destroy our way of life to cater to the elite white community that will use it. This project will, in no way, benefit or serve the Black & Brown community in Prince Georges. The funds would be better spent improving our current transit system. Adela which is a high speed train, is under utilized by our community. This will be worse. ~~Even if there~~ will be little more to this train & cost.



Comments:

will also be a petrochemical factory  
 You are using our community as a throw away -  
 the ~~white~~ folks want to use our neighborhood  
 like a shortcut through the parking lot &  
~~the~~ our homes will become the  
 parking lot. Even if some parts are  
 underground, there will be massive construction  
 to make our quality of life unbearable.  
 Furthermore, historically, these trains have not  
 been really successful - having to be shut down or  
 pulled out by the govt. I am concerned  
 about the magnet field & its affect on our health.  
 The vibration destroying the structure of my home.  
 Also, the very fact that you have avoided addressing  
 us as a community is a reflection of how you  
 see us. You do not want to address us together -  
 just show a video & avoid confrontation

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 Baltimore, MD 21202

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DATE 10/14/17

bwmaglev.info

**COMMENT FORM**

Name: Lorenzo Hy Horn Jr  
 Address: 9317 Hobart Street  
 City: Springdale State: MD Zip: 20774  
 Telephone: 301 952 3307 Email: LRHORN2@CO.PG.MD.US  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: Please Note my following concerns:

- ① Why would BWSMP proposed alternative routes in contrary to MDOT and Governor Hogan's understanding that proposed routes would only utilize existing right of ways?
- ② The review process of alternative routes, and eliminated routes; whose to say eliminated routes cannot be reconsidered?
- ③ PRICE: Who is going to fund the remaining 5 billion and who is going to fund the ongoing maintenance. Is it sustainable under the States CIP Program
- ④ Market: in regards to Price who will be targeted to utilize MAGLEV
- ⑤ Environment domain: concerns in regards to just compensation

Comments: Conclusion : ~~Raw~~ Routes should have  
the least Homeowner Impact, sustainable in  
regards to funding excluding tax payers dollars.

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

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COMMENT FORM

Name: Deborah Helms

Address: 8010 Laurel Bowie Rd

City: Bowie State: MD Zip: 20715

Telephone: \_\_\_\_\_ Email: hlmdb8010@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I agree that our transportation needs improvement, but I would prefer we focus on improving the existing trains and other available methods. I am open to new technology but I question the affordability, ridership and methodology this uses. I also don't ~~think~~ see a successful program in those that Maglev currently has existing. The ridership is just not there, proving affordability to be at question. Also once the train is run, if the ridership and cost requirements arnt met, we the citizens are ultimately going to bear the burden of the remaining cost. Good



Comments: *I just dont see this working to anyone's benefit, short or long term.*

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Baltimore, MD 21202

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10.14.17

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COMMENT FORM

Name: Beverly Brown  
 Address: 10411 Cleary Ln  
 City: Bowie State: MD Zip: 20721  
 Telephone: Email: msbev.brown@yma12.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

① Comments: THE SIGNS ADDRESS IMPACT OF VIBRATIONS & NOISE ON PEOPLE. THEY DO NOT ADDRESS IMPACT OF VIBRATIONS & NOISE ON ① ground stability, ② animals, ③ structures in the vicinity.

⑤ "electromagnetic field exposure meets" STANDARDS. STANDARDS ARE THE lowest possible level to meet before damage. It is a FINE line & CITIZENS will NOT be fooled by this STATEMENT. It has been used too OFTEN in the PAST to push through projects which are later proven to be HARMFUL.

ALL

Comments: ③ We need to see the data behind the statements & particularly on the "need". IF I TAKE A TRAIN TO NY TODAY I drive 7 mins to a train station. With this project I drive 45 mins through heavy traffic. I need to see that your data accommodates reality.

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DATE 10-14-2017

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**COMMENT FORM**

Name: Princess Pringle  
 Address: 8004 Dellwood Ave  
 City: Glenarden State: MD Zip: AD 706  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Why were residents that's being affected in the proposed area, not notified earlier in the making of this project. What the leaders of the project need to know is that lives are being affected, the effects from this move will devastate all the home owner in the area. Families that have work hard all their life to secure dwelling for retirement and comfort, to just lose it because life & liberty ~~and~~ memories will be gone. When the state & the Federal Government and those rich can actually destroy the less <sup>fortunate</sup> ~~fortunate~~ way of living and with those being affected not benefiting from these changes Prince George county is being affected and not becoming able to benefit from this project, but will ~~continue to~~ contribute to its purpose.



Comments: *Not only will PG County not benefit, this project will become too expensive for local people to afford, it will cost more to take this ride than to take a plane*

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DATE 10-14-17

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**COMMENT FORM**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

*potholes*

Comments: First of all I'm very upset<sup>↑</sup> to just be brought to our attention for such a big Project that effects so many families. For people that have worked hard most of their lives and now are at a place where they can enjoy the rest of their life doing the things that make them happy have to worry about where do we go from here. This is so unfair for big money maker to come in and move people out of their homes, there should be other ways to promote your Project than the ones you have pick out. We need to be happy with what we have, no need

Comments: To have a faster Train even with  
building this train you will still have  
people driving, because we have new  
roads being build as we speak, and  
there will always be cars on the  
roads. We dont want you all here.  
Go somewhere else and upset peoples  
Lives.

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Baltimore, MD 21202

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50 DB



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE OCT 14, 2017

bwmaglev.info

COMMENT FORM

Name: JOHN LAWSON

Address: 12514 RAMBLING LANE

City: BOWIE State: MD Zip: 20715

Telephone: 301-~~464~~-9473 Email: JALENTERPRISES

Join our mailing list?  Yes  No I ALREADY HAVE - NEVER

Preferred method of communication?  Email  Mail SEE ANYTHING FROM MAGLEV

Comments:

① PLEASE COMMUNICATE - THE OUTREACH TO CITIZENS AFFECTED WAS AND HAS BEEN IMPOVERISHED AND ~~ABYSMAL~~ - NOBODY ~~KNOW~~ KNEW ABOUT THIS IN BOWIE

② SO THERE ~~IS~~ <sup>THESE</sup> MORE TUNNELING WITH ~~THIS~~ NEW ALIGNMENTS - SO 32 FT BELOW IS ~~50 DB~~ 50 DB VIBRATIONS ON THE SURFACE AND YOU SAY YOU'LL TUNNEL AT ~~SOFT~~ SOFT - PEOPLE DON'T WANT ANY TUNNELING BECAUSE THEY

~~THE~~ ALREADY KNOW ~~THE~~ ABOUT ~~THE~~

THE SUPPOSED MINIMAL VIBRATIONS →



~~AND~~ AND THOSE VIBRATIONS ARE CURRENTLY MORE THAN FELT AND ARE UNHAPPILY LIVED WITH. SO THE VIBRATION ARGUMENT

Comments:

AS BEING NO BIG DEAL IS SURELY A LIE.

③ SO HOW IS THIS ~~PROJECT~~ PROJECT GOING TO SUSTAIN ITSELF FINANCIALLY IN ~~THE~~ THIS ONE LEG WASH-BWI-BALT-BWI-WASH?

~~NO PROBABLY~~

SURELY IT WILL HAVE TO BE PROPPED UP BY STATE AND FEDERAL FUNDS. AMTRAK, METRO CAN'T SUPPORT

ITSELF THROUGH RIDERSHIP AND ~~SO~~ SO THIS ONE WILL ABSOLUTELY NOT BE ABLE TO SUSTAIN

ITSELF. I DON'T ~~WANT TO~~ ~~WANT TO~~

~~PAY FOR~~ → MIND PAYING

STATE AND FED TAXES, BUT NOT FOR UNSUSTAINABLE PROJECTS THAT COST US IN MORE TAXATION AND TOLLS. IF THIS PROJECT IS NOT MONETARILY SUSTAINABLE, THEN DO NOT BUILD - I WANT TO SEE A REALLY

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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ANALYTICAL  
DETAIL REPORT  
ON FINANCIAL  
CAPABILITY OF  
THIS PROJECT.

I AM SURE THAT ANALYSIS WILL SHOW





DATE 10/14/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Anna J. Lawson

Address: 12514 Rambling Lane

City: Bowie State: Md. Zip: 20715

Telephone: 301-464-9473 Email: ajklsister@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① Need? Is there a need for this high speed train at the huge financial cost? Using the current "Amtrak" system, their next generation, trains makes more economical sense. we need improved infrastructure that makes sense and is good for all of the people.

② The environmental impact! Not only would thousands of people lose homes & businesses, churches, etc., we would lose our green space, wild life, SAFE environment.

③ Bowie, a wonderful city, would be split - separating neighbors & communities. How does this construction impact the watershed to the Chesapeake Bay?

④ Health risks - what is the impact on health from the magnet force field? Pacemakers, etc.

⑤ Sound/Noise - Living next to or near this train. Vibration How is one's health affected. It surely won't be pleasant and quite possibly HARMFUL. The air/environment belongs to

houses have asbestos. How ~~could~~ would that be handled safely? -> over A lot of homes in Bowie are built on older slabs. Page 1573



Comments: ⑥ Affordability/Ridership: It is quite feasible that the majority of people will not be able to afford to use this train. We need transportation that is accessible and affordable to everyone.

⑦ Ridership: If you live in a city between Baltimore and DC you will have to drive and park in DC to get the train to Baltimore. ~~And vice versa.~~ Does not make sense.

⑧ Jobs: What specific jobs? Reading about the Germain train, I didn't see where the # of jobs created comes anywhere close to what BWRR is proposing/stating.

⑨ Loss of Tax Revenue: If this is not a profitable venture, the citizens of the state of Maryland will be responsible, along with the federal government (through taxation) to pay for this un-needed train. Everyone's taxes will increase.

⑩ The state of Maryland, the counties affected will lose a tremendous amount of tax revenue when so many (thousands) of homes, businesses, churches, etc. are demolished.

*(\$ millions annually)*



⑪ SCOPING Process: Last but not least. No one in Bowie or Glen Arden (and other areas) were notified. We NEVER had the opportunity to attend public hearings. Under the NEPA process, it is required that the public must be adequately notified. This did not happen. Sending out about 650 postcards to Wash. DC and Baltimore, did nothing to notify all the cities and towns in between DC & Baltimore.

This is the United States of America, we have a right to know about such monumental projects.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

⑫ Who is going to benefit?

My personal opinion SCMagLEV Project is the BWRR, the Japanese Government or Central Japan R.R. and all of the investors. *(Government Bailouts)*

c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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If Japan Central can sell their technology to other countries, they can try to recoup some of their financial losses!







DATE 10-14-17

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**COMMENT FORM**

Name: Mrs. Mary L. Rapczynski

Address: 8706 Maple Ave

City: Bowie State: MD Zip: 20720

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No I think we are on your list.

Preferred method of communication?  Email  Mail

Comments: It is too expensive to build & maintain and then to use. We will not benefit from it in any way, but we will be paying through taxes and government subsidies, and what happens if some part fails or goes bankrupt? Then we live with heights of unfinished construction or seized property that deteriorates just sitting there.

What about all the lives affected living their homes or sitting next to the loud or towering Maglev train and construction? We live near the <sup>old</sup> AMTRAC station in Old Bowie in an old, historic home.

January 2018 I don't want to lose it or live beside the construction Page C-575

or the height if it is completed, I don't think we need it.



Comments: *Use the money to improve the transportation systems we have!*

*Do not build is the best option.*

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Metta M. Hunter

Address:

6807 Comuser Street

City:

Greendale

State:

Md.

Zip:

20737

Telephone:

301-577-8392

Email:

Sexyida6AOL.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

both

Mail

Comments:

This project is a disgrace, it will take our homes, decrease home value, and it will tear down our schools and our african american historical cemetery. This is a ill conceived wasteful project. We have three trains. There is not going to be no stops in Prince Georges County. Why should it come to Prince Georges County. You people have not notified our community Beacon Heights about this train coming through our neighborhood. What is the

Comments: purpose of this Maglev.

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DATE 10/14/17

bwmaglev.info

**COMMENT FORM**

Name: Beverly Wills-McFadden

Address: 8614 Johnson Avenue

City: Glenarden State: MD Zip: 20706

Telephone: \_\_\_\_\_ Email: beverly.gray@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Even though, as of this date, it has been stated, that most of the routes that would impacted a large number of homes, have been taken out of consideration for this project (including GLENARDEN, MD), I do not trust that the promise will be kept down the road later in this process. I will continue to protest this project -- there is no ~~entire~~ economic justice in this project for most of the communities along any of the proposed routes. How can a community benefit if the train doesn't offer a stop for them. I won't benefit for example, if I wanted to take



Comments: a flight from BWI - ~~to~~ I literally would have to travel to Washington, DC (when I live in Maryland) to catch the train to BWI -- there is little sense in that scenario.

Finally I will always oppose a project that will uproot/displace people from their homes. Many people have been in their homes for 20, 30, 40 and more years -- what are they supposed to do after all these years.

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DATE 10-14-2017

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**COMMENT FORM**Name: KENNETH PERRYAddress: 5003 PATUXENT RIDING LANECity: BOWIE State: MARYLAND Zip: 20715Telephone: \_\_\_\_\_ Email: PERRYKC48@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I believe in being progress to better the way of life for the community, the county, the state and my life. However, what I have seen, heard and read on the MAGLEV Project will not benefit my community and family. There is the possibility that I may loose my home or my community will be destroyed due to advance Transportation progress. If the MAGLEV will travel from Washington, D.C. to New York City and Boston, MA might work if I travel to those locations weekly. So why will MAGLEV make stops at BWI, Baltimore, Philly etc. when we have Amtrak and the Ocela Train doing the same thing. The funding for this project has already in place from Japan. So who will be this money back to Japan, why can't America pay for American Projects. Who is profiting from the Project, the states, the Govenors, Senators etc.

Comments: I am satisfied where I live and have worked hard to be in the perfect home, and Perfect Location with good value to my property. If I lose my home because of the MAGLEV Project I am too old to start over.  
This project will not benefit the communities that this Project will destroy. My question is who will profit and what happen to the great lost.  
Let's be truthful to the people of this state and county. We all need to be on the same page of progress. I see what has happen to other Mass Transportation Projects, such as Metro Transit, Anitak and other Local Transportation to relieve the congestion of Travel on Major Highway both on the ground and underground. I am not pleased with this project and how it is being presented to the community. Money talks and every body else will suffer. If my people will call on the Lord Jesus Christ as our to stop this major effort. Thanks for letting me speak through writing.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 14 OCT 2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: VANDY WELCH

Address: 8119 MOUNT AVENTINE ROAD

City: SEVERN State: MD Zip: 21144

Telephone: 443 831 8901 Email: vwelch@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Why were residents of affected properties along proposed paths not notified of earlier scoping meetings? Reportedly residents of Anne Arundel and Prince Georges counties that could be affected were NOT notified. This does NOT generate trust.

Once (and if) a path is chosen and construction begins, how wide a path will the project require for both construction and operation? How will tunneling affect property owners above the tunnel?

Where is Maglev technology operating at a profit?

Where else is SCMaglev technology being studied within the US?

Where is SCMaglev operating underground?

January 2018 Will there be a market for expensive, fast transportation between Baltimore and DC? Currently a MARE Train ticket costs



from obverse side

Comments: \_\_\_\_\_

→ \$8 or \$16 roundtrip. Acela costs \$75. I've heard SCMaglev will cost more. If so please demonstrate where the ridership ~~is~~ come from for fast, but expensive travel. I don't believe it exists.

The closest alternative is Acela which only takes 20 minutes between Baltimore and DC. I don't see any significant advantage of SCMaglev, especially due to costs and disruption of numerous homes, businesses, parks, etc. Also Acela is being upgraded over the next few years. This is another reason SCMaglev does not make good sense.

Based on current information I am against building SCMaglev between Baltimore and DC based on

1. Current alternatives,
2. Disruption to local homes ~~and~~ businesses, and parks during construction and operation, and
3. Costs. The construction will cost too much. The tickets to ride will cost too much.

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6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10-14-17

bwmaglev.info

**COMMENT FORM**

Name: Lynn Beiber

Address: 5000 Rochester Lane

City: Bowie State: MD Zip: 20715

Telephone: 301-262-1269 Email: LMB0258@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I've done my homework on this technology and the pay to play politics behind the SCMaglev in particular. The time for this technology has come and gone as it is way too expensive and will not solve traffic congestion problems. It will serve a few elite travelers and end up costing us taxpayers just like every other techno boondoggle promising so much. It will be transformative alright, but not in a good or useful way. The health effects of EMF are just now being studied more for cumulative long term biological effects. We don't know



Comments: yet, so the precautionary principle must prevail.

I'm against this particular and all maglev trains no matter whose neighborhood it destroys.

Shame on you for spreading so much propaganda and for trying to interfere with progress towards better solutions to our traffic nightmares.

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6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10-14-2017

bwmaglev.info

**COMMENT FORM**

Name: Mike Lange

Address: 15022 RUNNING PARK Ct.

City: Bowie State: MD Zip: 20715

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: NO BUILD

Autonomous cars are a couple years  
away and will remove congestion on  
road ways per studies. Elon Musk is  
building a UNDERGROUND HYPERLINK FROM  
DC to AIRPORT TO New York. MAGLEV  
WILL NOT BE USED. IMPROVE MARC  
TRAIN & AMTRAK

ONLY ONE OPTION  
NO BUILD!!!



Comments:

ONLY

ONE OPTION

NO BUILD!

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name:

Metta L. Hunter

Address:

6807 Amerson Street

City:

Greendale

State:

md.

Zip:

20737

Telephone:

301-577-8392

Email:

Sexyida@AOL.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Comments:

Why have the Community of Beaker Heights and Woodlawn have not received any information by mail or e-mail about Maglev? Why has this been such a hush? You want to bring this train through our community but we do not have information concerning the Maglev. When will our community get information regarding the Maglev project? When will they let the community know that their homes are in





No mag lev in our Communities  
Beacon Hgts



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE Oct 14, 2017

bwmaglev.info

We were notified at all

COMMENT FORM

Name:

Roberta I Cox

Address:

5119 Trinidad St

City:

Riverdale

State:

Md

Zip:

20737

Telephone:

301 459-6161

Email:

Nemlnck@aol.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Both

Comments:

This mag-lev train "IS NOT NECESSARY." Do not disrupt our Communities for a "pist full of dollars". Benefits no one in our County except for the "Money grabbers". We will not be moved. (Financial disaster) there are two trains leaving DC Amtrack / Arcelo Train. Isn't this fast enough. This problem is not necessary. I am / We are against this move.

It is not a financial gain for P.G. County (No train) -lover



Comments:

We do not wish our homes to be taken. We have been established in our community for many years, and this is a rough, disrupter. Where do you expect older people, who are retired, comfortable, expect to go & move just like a piece of paper. We are 70-80 yrs old, we cannot just pick up and move. Acquiring expense and problems that is very costly. We just can't do it. You will cause many to have health problems and some may pass away from stress.

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 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Keilow King

Address: 6713 Greenland St

City: Riverdale State: MD Zip: 20737

Telephone: \_\_\_\_\_ Email: Keilow@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1. Communication is lacking; I live in Beacon heights that encompasses 2 routes. I have yet to receive any correspondence and neither has any of my neighbors

2. I would like to see more specific plans as to how deep would tunnels be, the effect of the tunnels on existing land, we have a historic cemetery what would the impact be on that cemetery.

3. I would like to know what are the revenue generation impacts and what happens if the revenue to sustain the Maglev is not met. (Do taxpayers have to fund this project through subsidies).

4. Can someone from the Transportation authority schedule a meeting in ~~Beas~~ Beacon Heights to explain the impact on the two proposed routes through our neighborhood

TURN

Comments: 5. What is the impact of the magnetic field on the community and the long-term impact of both the magnetic fields and the vibrations on the soil on ground

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Baltimore, MD 21202

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DATE 10-14

bwmaglev.info

COMMENT FORM

Name: K E G Owen

Address: 4918 ROCKINGHAM LANE

City: BOWIE State: MD Zip: 20715

Telephone: 301 809 6659 Email: Katieo.geo@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

I signed up for your email list in April. First email from you was Yesterday.

Pictures drawn of the above land tract look intentionally deceptive. Or is your artist that bad at proportions? Some thing with "air displacement."

Single source supplier. Passenger trains already serve these destinations.

Yellow Route: You moved it, but you're still making a liar of the governor. Too many homes affected!



Comments: Passenger trains in the US aren't profit  
generators. I expect it to be thrown at the  
state to fund after they're done using it  
as a demo for more routes. We don't need  
to be subsidizing "elite business travelers."

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: J Timothy Looney

Address: 13313 Ketrinka Drive

City: Bowie State: MD Zip: 20720

Telephone: 301-805-1352 Email: JTLooney@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

1) Please provide a draft outline of the chapters that will be in the EIS.

2) Will the EIS look at the socio economic impacts of the project?

3) Will the EIS look at the economics of the proposed project?

4) Please provide a break down of the project cost in the EIS. It should provide of

Comments: construction of project and cost  
land providing access to ~~suburban~~ land  
once the train is built.

5) prior to issuance of the DEIS  
please provide ~~and~~ white paper  
for <sup>document</sup> ~~provi~~ that gives the details of  
how the economic's of project will  
be looked at.

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6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Mrs Gilbert  
 Address: 8503 triple crown Rd.  
 City: Bowie State: MD Zip: 20715  
 Telephone: 410 551 2599 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This line is not needed! Fix what is already in place. if you build this you will take out Assisted living homes Schools businesses and houses. All from people who cant afford to move, don't want to move if you make them move you affect mental & physical health. Also for every house you knock down that is a property tax that the rest of us have to make up for. I can not afford that rise in tax. So there for I would have to move out of Bowie also. If we lose these houses then we will lose the money for the services like  
parts & rec. Also will you build a new Elementary



Comments: School once you knock down the existing one.

This whole process has been handled on the quiet side. When it comes to the companies and officials putting their intent out there. There would not be this turn out if it was not for the citizens word of mouth. Also I have had to spend material come to my house after I requested it!

We dont need this train line the majority of people who live at either end wont be able to afford the tickets.

I am NOT for this line.

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DATE 10/14/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Lynn Vosburgh

Address: 14610 Arabian Lane

City: Bowie State: MD Zip: 20715

Telephone: 301-464-1781 Email: Vosburgh1@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Even though the ~~B+A~~ B+A alternative is currently off of the table, the Amtrak alternative still affects my quality of life and the value of my home. Amtrak has recently announced new trains that are going to accomplish the same thing as SCMAGLEV and we will not have to spend the extra money. I do not want this train going by my neighborhood period!! I am not going to be able to use (does not stop in Bowie) and ~~and~~ I would never pay the amount of money for the cost of a ticket. This train is only for the rich or the powerful but they are not the ones who will be living near it. Spend the money on what we have and make it better!

Comments: ~~Q~~ I will not vote for any federal or state representative who is in favor of this project. Gov. Hogan I think you are doing great and I am 100% behind you but if you vote for this project, I will not vote for you!

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 6 Saint Paul Street  
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DATE 10/14/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kristi Valleau  
 Address: 931 Nanticoke Run Way  
 City: Odenton State: MD Zip: 21113  
 Telephone: (410)960-9820 Email: kristi.valleau@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

1.) This technology has not proven to be economically viable in other parts of the world, and other commercial implementations have failed and required bail-outs. How is this SCMAGLEV project anticipated to be economically viable? Who will cover the cost if it fails?

2.) It has been noted/alleged that this will revolutionize commuting in Maryland and help relieve current congestion. However, per the Census Bureau's County to County Commuting flow 2009-2013, only 4937 of 797,044 DC commuters (0.6%) come from Baltimore City, and only 1248 of 365,921 Baltimore City commuters (0.3%) come from DC. This does not support a need for SCMAGLEV.



Comments: particularly given the fact that those few who commute directly between those two cities can rely on current infrastructure for their commuting needs (eg. the Acela Express travel time between DC-Balt. is only 20 minutes) There is no indication that this project will benefit the majority of Maryland commuters, who are in dire need of improvements to our existing infrastructure.

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6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)

DATE October 13, 2017

bwmaglev.info

**COMMENT FORM**

Name: Abry Anna Henderson

Address: 14103 Gamma Ct

City: Bowie State: Maryland Zip: 20710-1747

Telephone: 240-319-1555 Email: abryanna.henderson@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am 16 years old and attend Bowie  
highschool. I came to this meeting because I  
don't agree with this train being built. Building  
a train through several counties and not  
having it stop in turn is not beneficial  
to any of us yet we have to deal with  
the consequences of it such as environmental  
problems and possible accidents. How are you  
going to tear up homes and schools and  
community centers for something like this. We  
are people, not just numbers on a spread sheet  
or some statistics. In the letter you all sent  
out it states that you're seeking the least  
disruptive alignments but it says that there

Comments: have been several conceivable routes have been proposed that cut through existing residential areas, which makes it seem as though you all aren't trying to save the community at all. I want someone representing my views on the issue because I don't agree with this construction. Also my school was affected by the starting school in September instead of late August to bring in more revenue for the state and I don't agree with that as well. As a student we barely have enough time as it is to learn enough in school and taking those days away makes it harder. We need to be going to school earlier so that we can obtain more information, because education is more important than money.

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**Subject:** more for SCMAGLEV Open House  
**From:** Horsepen Hill Farm (horsepenhill@verizon.net)  
**To:** horsepenhill@verizon.net;  
**Date:** Saturday, October 14, 2017 8:34 AM

Are these alternatives final? Will we be notified if these change yet again?  
How wide are the routes?  
What kind of tunneling techniques will be used?  
Where will mining sites be?  
How will properties be affected by vibrations from tunneling and operation of the train?  
Where in the world is a maglev operating underground?  
Why aren't public meetings being held in the communities that will be affected by the above ground train?  
If the Amtrak line is used, will Maglev replace Amtrak?  
What have the Federal agencies whose land is in the path said about this project? (Ft Meade, Patuxent Wildlife, National Park Service)

Aviva Nebesky  
[www.horsepenhill.com](http://www.horsepenhill.com)

Aviva Nebesky  
8306 Laurel Bowie Rd  
Bowie, MD 20715





Good evening. My name is Aviva Nebesky and I live in Bowie. Thank you to the City Council for allowing me to speak. Thank you to Mr Trueschler for being here. Thank you to this great crowd for coming out to hear about this issue. How many of you here are doing your jobs by being here? Mr Trueschler is. The City Council is. Any reporters who may be here. The majority of this huge crowd is here because we are worried about this SCMAGLEV and the impact on our community.

I am reminded of an old Janis Ian song....don't it always seem to go that you don't know what you got til it's gone. They paved paradise and put up a parking lot.

I live in paradise. My husband and I have a 16 acre horse farm. Not only is it beautiful and where I conduct my business but it has historical significance as well. We have pecan trees that were started as seedlings from Thomas Jefferson's plantation. Our house is 100 years old and we have both an old smoke house and an ice house. We are listed on the historic registry. We are surrounded by acres of pristine land and other horse farms

We all love the idea of decreasing traffic congestion and the time it takes us to get around. My question is how this project will do anything to help us. A train that stops in only three places- DC, BWI, and Baltimore- does nothing to help most of the commuters along the 40 mile line. The project documentation states that this project will serve the elite business traveler. Are any of us in this room the elite business traveler? Who is that? We are told it is for someone for whom time is more valuable than money. So how much is a ticket? No one seems to know the cost of a ticket but it is in the neighborhood of air fare according to the project documentation. So, air fare. Can you pay those kinds of prices for a daily commute? Do you know anyone who can? How many people will actually take this train? How many people NEED to take it to make it commercially viable? I keep asking this question but I get no good answers. Before sinking billions of dollars into something, shouldn't there be a needs assessment to give us answers about whether or not there is a need? The project has, so far, failed to do that. We are constantly told that the Draft EIS will give us our answers. OK. We are waiting....but....in the meantime let's consider.....

The track for the train can ride along the ground, above ground or under ground. Underground is very, very expensive. The track will be about 50 feet wide according to what we were told by David Henley from BWRR back in July. To give you some perspective, 50 feet wide is the equivalent of a 4 lane highway. FOUR LANES. That decimates an incredible path whether it is the WB and A Trail, a pasture, wetlands, or your house. How much damage will there be to the tributaries to the Bay from construction? Along the ground it creates a barrier that will effectively separate one side of Bowie from the other. OK- above ground....well it is only 14 feet above ground which is about the level of a two story house. Not so high. And 50 feet or a 4 lane highway wide. NOTHING will grow under that. The sun will be blocked out. You might as well be in a tunnel.

What does this do to our community? To our farms? Our wildlife? Our quality of life? For a train that does not serve our community?

As far as I can tell, so far there is no commercially viable SCMAGLEV train in operation anywhere in the world. Do we really want to be the guinea pig for untried technology? The problem with untried technology is that you don't know if it works. You don't have information about long term health or environmental consequences. Am I anti-technology? Maybe a little bit but I don't see the point of being a guinea pig for BWRR and TNEM for something that has no benefit to me or my community, has the potential for unintended negative consequences and seems to have no potential for financial viability. I don't want to see 50 foot tracks left to rot all along our community when this project fails nor do I want to bail it out with my tax dollars.

I oppose this train.

Thank you.

**Citizens Against the SCMaglev**

October 10, 2017

SCMaglev Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Dear Mr. Al Khatib,

The Citizens Against the SCMaglev sent a letter in June 2017 requesting that the NEPA scoping process be reopened due to lack of public notice on the part of the Maryland Transit Administration. To date, we have not received any response from MTA.

As we will not have our concerns ignored, we are providing the following scoping comments. The project scoping report must be amended to include these issues, and they must be addressed in the EIS.

**PURPOSE AND NEED**

1. Provide the need for the SCMaglev project. While a purpose of the proposed project was provided at the April 2017 public meetings, the need for a SCMaglev train was not provided. The public has not been provided any information that leads to the conclusion that an SCMaglev is needed. We strongly believe that there is a need for regional transportation improvements in the Washington-Baltimore corridor that will serve all residents, visitors, and businesses in the region. However, we have seen no data that proves that a train that serves only Washington, BWI, and Baltimore is needed or will provide relief from the transportation challenges in the region.
2. The Notice of Intent, published on November 25, 2016 (the day after Thanksgiving), states that:

“The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent. Similarly, the demand on the transportation infrastructure between Baltimore and Washington will continue to increase along major roadways and railways including Interstate 95, the Baltimore-Washington Parkway (MD 295), US 29, US 1, and the Northeast Corridor (NEC) thereby decreasing the level of service, reliability, mobility, and potentially decreasing safety.”

This statement does not provide proof that there is a need for a transportation investment of this magnitude with significant environmental impacts that only serves DC, BWI, and Baltimore. Rather it demonstrates that there are transportation issues that extend throughout our region. Analysis of these needs will demonstrate that these issues will only be solved by intra-regional transportation investments like rapid bus transit, light rail, and roadway improvements that serve the entire traveling population.

Provide detailed analysis of transportation needs of the Baltimore-Washington area.



Page 2 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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3. The Notice of Intent makes the following claims:

“Without additional transportation improvements and capacity within the Baltimore-Washington area, economic development and growth opportunities will be restricted. As congestion increases on the NEC and on the region's highways, the demand for continued economic development will be impacted, including, for example, tourism.”

Provide documentation of this hypothesis. Specifically, provide documentation that the “transportation improvements... within the area” would be served by a rail system that only stops in DC, BWI, and Baltimore. Provide documentation that tourism, which is fueled by the great historical and governmental significance of the Washington, DC area would decline without a train that ONLY brings people from Baltimore.

4. Further the Notice of Intent quotes the NEC Future Program:

“To address these issues, in 2012 FRA launched the NEC FUTURE program to consider the role of rail passenger service in the context of current and future transportation demands and to evaluate the appropriate level of capacity improvements to make across the NEC. Through NEC FUTURE, FRA will determine a long-term vision and investment program for the NEC documented in a Tier 1 EIS and Service Development Plan. FRA published a Tier 1 Draft EIS in November 2015; however, the Draft EIS evaluated steel-wheel technologies as a way to serve the passenger rail needs of the region. It left open the possibility and did not preclude the study of and investment in advanced guideway and other new technologies, such as SCMaglev, to meet the transportation needs of the Northeast, including the Baltimore-Washington area.”

We note that this statement says that the NEC future EIS leaves open the possibility of studying the SCMaglev. While we concur, this statement does not support the need for the SCMaglev and calls into serious question why alternatives other than SCMaglev are not being studied in this EIS. See comments on Alternatives below.

5. Independent Utility/Segmentation - Provide detailed analysis proving that the proposed project has independent utility. Provide financial data showing how a train from DC to Baltimore can sustain short-and long-term viability without government funding and without an extension past Baltimore.

As BWRR is a subsidiary of The Northeast Maglev, LLC (TNEM), and TNEM has announced plans to build the SCMaglev from Washington, DC to New York, the entire SCMaglev project is a connected action. Separation of the project into pieces violates the prohibition on segmentation per the Council on Environmental Quality (CEQ) NEPA implementing regulations.

The CEQ NEPA regulations state that:

**§1502.4 Major Federal actions requiring the preparation of environmental impact statements:** Proposals or parts of proposals which are related to each other closely

Page 3 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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enough to be, in effect, a single course of action shall be evaluated in a single impact statement.

**§1508.25 Scope:** To determine the scope of environmental impact statements, agencies shall consider 3 types of actions, 3 types of alternatives, and 3 types of impacts. They include: 28 (a) Actions (other than unconnected single actions) which may be: (1) Connected actions, which means that they are closely related and therefore should be discussed in the same impact statement. Actions are connected if they: (i) Automatically trigger other actions which may require environmental impact statements. (ii) Cannot or will not proceed unless other actions are taken previously or simultaneously. (iii) Are interdependent parts of a larger action and depend on the larger action for their justification. (2) Cumulative actions, which when viewed with other proposed actions have cumulatively significant impacts and should therefore be discussed in the same impact statement. (3) Similar actions, which when viewed with other reasonably foreseeable or proposed agency actions, have similarities that provide a basis for evaluating their environmental consequences together, such as common timing or geography. An agency may wish to analyze these actions in the same impact statement. It should do so when the best way to assess adequately the combined impacts of similar actions or reasonable alternatives to such actions is to treat them in a single impact statement.

6. Provide data demonstrating the following required for financial viability:

- Number of daily trains in each direction
- Number of cars per train
- Number of riders per train, per day, and per year
- Cost per ticket
- Cost to operate and maintain rail line
- Any financial incentives (funding, grants, tax breaks, loans, etc) needed from governmental sources (Federal, State, or local) for construction and operation in the short-term and long-term
- Provide information on foreign investment including required purchase of foreign-made rail components/trains
- Provide examples of where the SCMaglev is currently operating in the world and provide financial data on that operation demonstrating that it does not require continual or intermittent government subsidies.

Page 4 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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7. Amtrak's annual ridership on the Northeast Corridor (from DC to Boston) for 2016 was 11.9 million. Further Amtrak estimates NexGen ridership in 2030 of 25 million for the ENTIRE Northeast Corridor (<https://www.amtrak.com/ccurl/214/393/A-Vision-for-High-Speed-Rail-in-the-Northeast-Corridor.pdf>).

In 2016, a total of 5,098,562 riders boarded Amtrak trains at the Union Station in Washington, DC, and a mere 1,030,161 riders boarded Amtrak trains in Baltimore <https://www.amtrak.com/national-facts>. Only 689,042 riders boarded/off-boarded at BWI in 2016. (<https://www.amtrak.com/ccurl/111/103/MARYLAND16,0.pdf>). It is inconceivable that most of these 6.8 million riders were solely traveling between Washington and Baltimore.

2016 ridership on the MARC Camden and Penn lines totaled approximately 1.3 million riders (<https://data.maryland.gov/Transportation/MTA-Average-Weekday-Ridership-by-Month/ub96-xxqw>), but MARC serves numerous stations and communities in between Baltimore and Washington – commuters that would not be served by the SCMaglev. In addition, in June 2016, MARC reported ridership DECLINES: "Currently, the MARC service is facing ridership declines, according to Erich Kolig, director, MARC train and commuter bus service. "They are rather slight, only about a percent down from last year," he explains. "But we are not experiencing the 3% growth that we have been maintaining for the last 10 years." (<http://www.metro-magazine.com/rail/article/713751/maryland-s-marc-railroad-upgrades-fleet-service-to-bolster-ridership>)

In its filing with the State of Maryland, BWRR claimed that "Annual ridership on the SCMaglev system connecting Washington to Baltimore would be between 10.2 million and 15.4 million annual passengers in 2030 (42,200 one-way trips per day on average), while still assuming continued Amtrak and MARC service."

**Provide proof and solid data on how the SCMaglev will achieve 10 times the current Washington/Baltimore station onboard/offboard riders and 41 to 62% of the projected 2030 riders on the entire Amtrak Northeast Corridor in the short line from DC to Baltimore while "still assuming continued Amtrak and MARC service"! Include detailed origin and destination data for riders, type of riders (commuters, business travelers, tourists).**

See **Environmental Impacts** for further issues regarding Amtrak.

8. If the need for an SCMaglev train from DC to Baltimore cannot be defined, and independent utility for this short segment of rail cannot be established and sustained over the short- and long-term, the EIS must be revised to include the entire corridor (whether to Philadelphia, New York, or Boston) needed to sustain the financial viability of the project in accordance with 40 CFR 1502.4a and 40 CFR 1508.25.

Page 5 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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## ALTERNATIVES

**The structure of this EIS based solely on studies for the SCMaglev, precludes all other possible decisions including the decision to construct Amtrak NextGen trains. As such, an EIS that does not study these other alternatives is not in compliance with the National Environmental Policy Act.**

### **As stated by the Council on Environmental Quality (40 Most Asked Questions):**

Question 2a. Alternatives Outside the Capability of Applicant or Jurisdiction of Agency. If an EIS is prepared in connection with an application for a permit or other federal approval, must the EIS rigorously analyze and discuss alternatives that are outside the capability of the applicant or can it be limited to reasonable alternatives that can be carried out by the applicant?

A[answer]. Section 1502.14 [of the CEQ regulations] requires the EIS to examine all reasonable alternatives to the proposal. In determining the scope of alternatives to be considered, the emphasis is on what is "reasonable" rather than on whether the proponent or applicant likes or is itself capable of carrying out a particular alternative. Reasonable alternatives include those that are practical or feasible from the technical and economic standpoint and using common sense, rather than simply desirable from the standpoint of the applicant.

2b. Must the EIS analyze alternatives outside the jurisdiction or capability of the agency or beyond what Congress has authorized?

A. An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Section 1506.2(d). Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies. Section 1500.1(a).

**In addition, Section 4(f) of the Department of Transportation Act prohibits the use of Section 4(f) property unless there is no feasible and prudent avoidance alternative to the use of land from the property. If the SCMaglev project uses public parkland, wildlife or waterfowl refuge land, or historic resources, it may not be constructed if there is a prudent and feasible alternative. It should be noted that the WB&A Trail is public parkland – NOT a railroad right-of-way, and as such is protected under Section 4(f).**

**Lastly, as noted in Amtrak's scoping comments for this EIS, the NEC Future EIS and Record of Decision, selected improvements to the existing rail alignments and "discarded the new alignment alternative."**



Page 6 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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1. NexGen – The Amtrak NexGen technology is a “reasonable” alternative to Maglev and therefore, in accordance with NEPA and the CEQ implementing regulations (40 CFR 1502.14), must be studied in the EIS. ADD INFORMATION ON NEXGEN – SPEED, TIMING, COST, ETC. While outside of the Congressional legislation, the NexGen technology is a “reasonable alternative as it is practical and feasible from the technical and economic standpoint and must be included using common sense, rather than simply desirable from the standpoint of” BWRR. In addition, as construction funding is not available at this time, it is prudent to provide Congress with an equal analysis of all alternatives to facilitate informed decision-making.

Lastly, the Amtrak NexGen is a prudent and feasible alternative to the SCMaglev and would avoid the use of public parkland (including the WB&A Trail), wildlife and waterfowl refuges, and historic properties. Therefore, the Amtrak NexGen must be studied as an alternative to the SCMaglev.

2. Elon Musk and SpaceX have proposed constructing an underground Hyperloop railroad to travel between Washington, DC and New York. As with NexGen, this technology must be analyzed along side the SCMaglev to provide decision makers with information on the environmental impacts of each technology prior to making a decision.
3. Detail on alternatives must include design details including but not limited to: limits of disturbance; right-of-way; staging areas; station areas and impact areas for these; connections between stations and existing transportation hubs (i.e. BWI); maintenance yards and impact areas for construction of these; utility connections; security features including fencing; haul routes for construction equipment, materials, construction debris, and mining spoils; and disposal areas for construction debris and fill materials.

### **Environmental Impacts**

1. Environmental Justice – Provide a detailed analysis on the disproportionate impacts that low-income and minority populations in Washington, DC; Prince George’s County; and Baltimore City will suffer under the SCMaglev plan including but not limited to loss of property values and housing opportunities, loss of parkland and greenspace, increases in noise levels, and impacts to health and safety. Specifically, the above ground portion of the project will bisect Prince George’s County while not providing ridership opportunities to the citizens of the County. Provide analysis of the EPA EJScreen demonstrating the disproportionate impacts already suffered by the residents of the County and the impact that a SCMaglev train would have. A baseline EPA EJScreen along the WB&A Trail shows an EJ score of over 90% compared to 50 to 60% in communities in Montgomery and Howard counties.

Page 7 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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2. Property Acquisition – Provide information on all properties to be acquired and displaced as a result of the proposed action including:
  - a. Number, types, and locations of all of acquisitions/displacement of residents, parks, businesses, schools, churches, etc;
  - b. Method by which properties will be acquired including information on eminent domain and what entity will exercise eminent domain;
  - c. How compliance with the uniform relocation act will occur including analysis of the availability of comparable housing of equal price, value, and setting
  - d. Impact to remaining homes including loss of home values, changes in setting, visual and noise impacts. Provide information on how close a residence can be from the rail line
  - e. Impacts to remaining businesses including change of access and resulting loss of business.
3. Economic Impacts – Provide thorough, quantitative analysis on the negative economic impacts associated with the proposed alternatives including:
  - a. Loss of property values (especially home values) from the proximity of a high-speed train where there is currently more compatible, highly valued land uses such as parks, trails, forested areas, and residential areas
  - b. Loss of tax base from the loss of properties and from the devaluation of remaining properties
  - c. Impacts to Amtrak ridership and revenues in the Washington/Baltimore corridor, the Northeast Corridor, and nationally as many of Amtrak's lines outside urban areas count on revenues generated in areas like the Northeast Corridor
  - d. Impacts to viability of NexGen Acela as it is probable that there is not sufficient ridership/need for both technologies to operate in the same market
  - e. Impact to Marc ridership, revenues, and viability with the proposed loss of riders to the high-speed train; this assessment should include any additional State, local, or Federal funds that will need to be expended to keep the Marc system viable. If the Marc system is no longer viable or must raise fares to maintain viability, the impact to commuters that board the train at locations between Baltimore and Washington must also be analyzed
  - f. Analysis of permanent job creation with SCMaglev vs. job losses at Amtrak and Marc
4. Parks and Recreation/Section 4(f) – Provide impacts to parks and recreation areas in Prince George's and Anne Arundel Counties including the loss of access to these critical community features. Impact analysis should include changes to these facilities including changes to park settings, park features, park noise levels, and the overall changes in the user's experience. Note – the WB&A Trail is public

Page 8 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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parkland; owned and operated by the Maryland National Capital Park and Planning Commission and the Anne Arundel Recreation and Parks Department. The trail is a critical component of the East Coast Greenway and the American Discovery Trail.

5. Historic Resources – Provide impacts, including visual, noise, and vibration impacts, to historic resources including archaeological features, historic sites such as the Glen Dale Hospital, the Marietta Mansion, the Bowie Racetrack, private historic residences and farms, and historic districts including the Odenton historic district.
6. Visual Resources - As the only renderings provided by MTA thus far show a train in the middle of a forest with no residences nearby, provide more accurate renderings of the train line. Specifically:
  - a. Provide before and after renderings of the proposed rail line and train in ACTUAL locations showing the view to and from historic properties, parks, and residential properties that will remain after construction of the train.
  - b. Renderings to and from historic properties, parks, and residential properties should include day and night views and views with and without leaf cover on trees.
  - c. Provide information on the visual intrusion of graffiti into neighborhoods that may occur on the elevated rail line.
7. Topography – Provide details on changes to topography including cut and fill; provide detail on whether the proposed train and track system will cut through hills (creating environmental impacts) or rise up over them creating visual impacts to large numbers of communities and citizens.
8. Water Quality – Provide quantitative data on the increases in impervious surface created by the track, stations, maintenance yards, and parking.

Provide quantitative data on the resulting stormwater runoff and impacts to erosion, sedimentation, and TMDLs in the already degraded Patuxent River and its tributaries including Horsepen Branch.

Provide information on the chemicals to be used for snow removal and deicing and the impact runoff of these chemicals will have on the Patuxent River and its tributaries.

9. Bird Strikes – Provide data and analysis on the impact of bird strikes and how they will be prevented from a train going over 300 mph. There are nesting bald eagles on the Patuxent Wildlife Refuge in close proximity to the WB&A Trail. Bald eagles also frequent the Patuxent River between Prince George's and Anne Arundel counties. In addition, there are numerous other bird species including barred owls, pileated woodpeckers, and red-tailed hawks nesting along the WB&A Trail and along the river.
10. Bats - Provide data and analysis on the impact to bats and how strikes will be prevented from a train going over 300 mph. Provide detailed studies of bat habitat, bat populations, and presence of white-nose bat syndrome.

Page 9 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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11. **Wildlife Displacement** – Provide data and analysis on wildlife displacement from construction of the proposed train. Habitats in the vicinity of the proposed alternatives are at capacity and any loss of habitat will force wildlife further into developed areas including residential areas. Detailed wildlife surveys must be conducted to document species, numbers, health of population (including over population), and capacity of habitat.
12. **Vegetation** – Provide data on number, species, size, and locations of all trees to be removed for construction of the rail line. Provide types of vegetation to be removed. Provide information on how the project will prevent invasive species from being introduced into areas where forest are bisected and native vegetation is removed. Provide mitigation including where and what types of vegetation will be replanted, how reforestation areas will be monitored, and how the rail line will be screened after vegetation is removed.
13. **Noise** – Provide 24-hour noise analysis for all types of properties in the vicinity of the proposed alternatives (residences, schools, churches, libraries, historic sites, parkland, etc.). Provide existing noise levels and noise modeling when the train is in operation. Noise models should take into account the loss of existing vegetation and buildings that would be removed and modeling should show day and night noise levels.
14. **Electromagnetic Fields** – Provide information on electromagnetic fields including health effects from long-term exposure to an SCMaglev train. Provide health information from populations currently exposed to SCMaglev trains.
15. **Light** – Provide information on how the rail line, parking areas, rail yards, stations, etc. will be illuminated at night. Provide analysis of light intrusion into residential areas, parkland, and historic sites.
16. **Energy Usage** – Provide information on the energy needed to operate the SCMaglev train including the source of the power. Provide locations of any new substations or other utility upgrades needed to power the SCMaglev. Provide information on how power interruptions would affect train operation.
17. **Utilities** – Provide information on temporary and permanent utility disruptions to residences, businesses, and other property owners. Provide information on the location and type of new utility lines needed to serve the proposed project. Provide information on impacts to the power grid and other utilities.
18. **Community Cohesion** – Provide information on impacts to neighborhoods that will be bisected or will be cut off from neighboring communities by the SCMaglev Train.

Provide information on the effects to communities such as Glenarden, Glen Dale, Bowie, Piney Orchard that will be bisected by the proposed train. Include analysis of the cumulative effects of bisection by transportation projects to communities such as Glenarden.



Page **10** of **12**  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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19. Land Use and Land Use Planning – Provide detailed analysis of changes in land use that will occur due to undesirable proximity to the train and from the economic development that BWRR claims will accompany the train. Provide detail on where this development will occur and how it will affect sprawl in the Baltimore-Washington Area
20. Community Services –
  - a. Provide analysis on effects to police, fire, and EMS services including responding stations, personnel levels, training (how much it will cost, who will provide it, and how often it will occur). Provide information on BWRR provided police services and a comparison of those services to those provided by Amtrak police. Provide information on how terrorist threats will be mitigated at stations and on the rails.
  - b. Provide information on how other community services will be impacted including schools, hospitals, churches, etc. Include analysis of cumulative impacts based on the economic growth projected to occur solely because of the SCMaglev construction (as based on BWRR claims).
21. Safety and Security – In addition to the police information requested above, provide information on how the SCMaglev rail line will be secured such that people cannot climb onto the line resulting in injury or death and/or vandalize the line. Provide renderings of any fences or other barriers that will be placed along the rail line, rail and maintenance yards, parking areas, etc. Provide information on the distance fence lines will be placed from the line, height of fences, and type of fencing.
22. Construction Impacts – Provide information on the construction related activities and impacts including but not limited to:
  - a. Limits of disturbance for construction of each of the alternatives – temporary and permanent land use
  - b. Areas of cut and fill
  - c. Staging areas
  - d. Tunneling/mining locations
  - e. Tunneling technology
  - f. Haul routes for construction equipment and disposal of tunneling spoils
23. Traffic – Provide impacts from:
  - a. Construction vehicles traffic
  - b. Temporary and permanent roadway closures including travel delay times, creation of land-locked properties, changes in access to properties

Page 11 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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- c. Additional vehicular traffic, especially at Union Station, BWI, and in Baltimore generated from train riders accessing the rail line; analysis should include intersection level turning movements and delays
  - d. Traffic from economic development that BWRR claims will accompany the SCMaglev; the Notice of Intent states that “The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent.” If this growth will occur without the SCMaglev, and the SCMaglev claims it will radically increase development in the region, the cumulative impact analysis must quantitatively analyze the traffic that will accompany this level of development and explain how the rest of the regional transportation network will handle this development or where levels of service will decrease to the point of complete gridlock.
24. Airline/Plane Traffic – As part of BWRR’s economic and ridership “promises” include increased use of BWI and development around BWI, increases in air traffic must be analyzed in the EIS.
- This analysis must also include resulting increases in noise from increased air traffic. It should be noted, that Governor Larry Hogan recently instructed the State Attorney General to sue the FAA for noise violations at BWI.
25. Union Station –
- a. Provide information on the impacts to the historic Union Station in Washington, DC
  - b. Provide information on how the SCMaglev Train will impact current renovations taking place at Union Station
  - c. Provide analysis of how a SCMaglev Train will affect the Washington Union Station's 2nd Century Plan which is being developed to accommodate the Amtrak NexGen train.
26. Cumulative Impacts – A project of this magnitude requires a comprehensive analysis of cumulative impacts including quantitative analysis of the significant harmful impacts transportation projects have had and continue to have on communities in Prince George’s and Anne Arundel counties. Included in this cumulative impact analysis must be the impacts of the supposed development that SCMaglev claims it will generate. In addition, the cumulative impact analysis must analyze any track widening or realignments planned by Amtrak.

The Notice of Intent also provides grossly inaccurate information on the study area of the proposed project. The NOI states that the study area is bounded by the former Washington-Baltimore & Annapolis Electric Railroad alignment while in reality, the study area extends 3 miles to the east of this alignment. Maps of preliminary alternatives have been provided by MTA which MTA now states should not be relied on. BWRR CEO Wayne Rogers, as recently as September 14, 2017, stated in an interview with ABC 7 news that ““We're very early in the process. They're called initial preliminary screening alternatives. They're not even to the preliminary to the alternatives to the preferred alternatives, so

Page 12 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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we're years away," when in fact alternatives have been submitted to FRA for approval. These examples of misinformation by the MTA project team along with abysmal public notice on the scoping process for the project, demonstrate that this project should be restarted to provide meaningful opportunity for public input and decision-making.

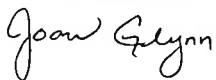
As MTA has not responded to repeated requests to reopen the scoping period for this project, we request that you provide a formal reply to this letter and an affirmation that the issues outlined herein will be thoroughly analyzed in the EIS.

You may reach us at:

Citizens Against the SCMaglev  
PO Box 669  
Bowie, MD 20715

Sincerely,

Citizens Against the SCMaglev



On behalf of:

Dennis Brady, Chair  
Dawn Wampler, Vice-Chair  
Aviva Nebesky, Vice-Chair  
Breanne Reynolds, Secretary  
Joan Glynn, Secretary

Cc:

EPA Region 3  
US Senator Chris Van Hollen  
US Senator Ben Cardin  
Congressman Steny Hoyer  
Governor Larry Hogan  
State Senator Doug Peters  
Delegate Geraldine Valentino-Smith  
Delegate Marvin E. Holmes, Jr.  
Delegate Del. Joseph F. Vallario, Jr.

Prince George's County Executive Rushern Baker  
Prince George's County Councilman Todd Turner  
Bowie City Mayor Fred Robinson  
Bowie Councilman James Marcos (At Large)  
Bowie Councilman Henri Gardner (At Large)  
Bowie Councilman Michael Estève (District 1)  
Bowie Councilwoman Diane Polangin (District 2)  
Bowie Councilwoman Courtney Glass (District 3)  
Bowie Councilman Ike Trough (District 4)



SENT VIA EMAIL

January 31, 2017

JANET CAMPBELL-LORENC  
Director, Business Development

Corporate Planning

Mr. Bradley M. Smith  
Director of the Office of Freight and Multi-modalism  
Maryland Department of Transportation  
7201 Corporate Center Drive  
Hanover, Maryland 21076

**RE: SCOPE COMMENTS / BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT EIS**

Dear Mr. Smith:

Amtrak is pleased to submit comments on the scope of the Federal Rail Administration's (FRA) and Maryland Department of Transportation's (MDOT) Environmental Impact Statement (EIS) for the Baltimore-Washington SCMAGLEV project.

Amtrak provides intercity passenger rail service over 21,000 route miles in 46 states, the District of Columbia, and Canada. Amtrak provides *Acela Express*, *Northeast Regional*, *State Supported*, and *Long Distance* rail services between Boston, New York City, Philadelphia, Baltimore and Washington, D.C. As majority-owner of the Northeast Corridor (NEC), Amtrak provides coordinated passenger and freight rail service planning for the NEC as well as infrastructure access and operational support to eight commuter rail authorities and four freight rail operators. Amtrak's experience as the U.S. high-speed rail operator and NEC end-to-end user provides a unique perspective and insight about the Baltimore-Washington passenger rail transportation network.

**Amtrak has several concerns with the scope of the Baltimore-Washington Superconducting Maglev (SGMAGLEV) project as described below.**

- 1. NEC FUTURE analyzed passenger rail transportation needs between Baltimore and Washington and discarded the new alignment alternative.**

The NEC FUTURE program has already addressed the mobility challenges of the Baltimore-Washington, DC travel corridor with a focus on the role of passenger rail in meeting those challenges. FRA evaluated future transportation needs and considered the capacity constraints of the total transportation system including rail, highway, and air and completed a programmatic EIS. The EIS focused on technology-neutral rail passenger technologies, and although a new alignment was considered, the new alignment option was ruled out. Instead, a preferred alternative focused on improving the existing rail alignment was selected.



Maryland Department of Transportation  
January 31, 2017  
Page 2 of 3



**2. The framework for passenger rail investment between Baltimore and Washington, DC is already in place.**

Amtrak, as owner of the NEC between Baltimore and Washington DC, works collaboratively with the FRA, the Northeast Corridor Commission, Maryland, MTA MARC, VRE, DDOT, WMATA and Virginia to solve problems, prepare plans and invest in passenger rail between Baltimore and Washington, DC. Together, the stakeholders have agreed upon a process to develop and implement multi-year investment plans with leadership by the NEC Commission. For longer range NEC investments, NEC FUTURE has already provided the framework for the Passenger Rail Corridor Investment Plan through 2040 and beyond. The proposed maglev scope is wholly contrary to the passenger rail investment framework that has been collaboratively developed by the region's stakeholders.

**3. The ability to evaluate the environmental consequences of Maglev is unclear.**

The maglev technology proposed is not a proven passenger rail technology. The technology has yet to be commercially proven. Data and experience are not yet available to evaluate the potential effects of maglev on the economy, transportation system, and the human and natural environment as is required in an EIS. Additionally, BWRR has clearly indicated that this is only the first segment of a SCMAGLEV line extending from Baltimore to Boston, Massachusetts to the north, and from Washington D.C. to Charlotte, North Carolina to the south, which indicates that the current maglev EIS scope does not provide true independent utility.

**4. Substantial investment in passenger rail transportation is already underway between Baltimore and Washington, DC.**

Amtrak questions the competing priorities between the Baltimore-Washington SCMAGLEV project – which calls for the construction of a separate maglev network with new guideway, stations and maintenance facilities, and anticipates funding from a mix of federal and private sources – and the NEC FUTURE's EIS to renew and modernize the NEC infrastructure between Washington, Baltimore, Philadelphia, New York City and Boston.

NEC FUTURE has confirmed the need for major passenger rail investments on the existing corridor between Baltimore and Washington DC including the replacement of the Baltimore & Potomac Tunnels, additional right-of-way and track segments, and modernization and expansion of Washington Union Station. These and other crucial NEC projects are already well along in the planning process, with several projects having completed environmental clearance and preliminary engineering. Over the next five to ten years, the cost to complete will require a substantial financial commitment from the Federal government, Amtrak and others, commitments that have the potential to be in direct competition with the plans for maglev.

BWRR has now openly stated that further public investment will be pursued for maglev. However, as noted above, major public passenger rail investment has already been committed and is underway. Public/private investment is also already underway. Amtrak has taken out a \$2.5 billion loan with the FRA to purchase new high speed trains and construct infrastructure needed to optimize high speed rail service between Baltimore and Washington DC.

Maryland Department of Transportation  
January 31, 2017  
Page 3 of 3



The completion of NEC FUTURE was a landmark achievement and key affirmation of Amtrak's long-held view that rebuilding and expanding the Northeast Corridor is essential for the growth and prosperity of the entire region. After four years of study by the FRA, which involved the use of significant financial and human resources, and the engagement of all stakeholders – the Federal government, state, cities, the railroads and the public – the recently-published Final EIS for NEC FUTURE recommends a planning and investment approach to address the NEC's current and future passenger rail needs. This report should remain the prevailing guide for outlining the pathway of passenger rail.

We look forward to addressing these concerns with MDOT and FRA.

Sincerely,

A handwritten signature in black ink that reads "Janet Campbell-Lorenc". The signature is written in a cursive, flowing style.

Janet Campbell-Lorenc, AICP  
Director, Business Development

cc: Stephen Gardner  
Karen Gelman  
Jeff Gerlach  
Thomas Moritz



Peter & Edna Granahan  
12102 Rustic Hill Drive  
Bowie, Maryland 20715  
301-262-9018  
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[pegrananan@verizon.net](mailto:pegrananan@verizon.net)

July 31, 2017

Maryland Department of Transportation  
Bradley Smith Director of Office of Freight and Multimodalism  
7201 Corporate Center Drive, Hanover, MD 21076

SCMAGLEV Project, C/O John G. Trueschier  
Maryland Transit Administration  
6 Saint Paul Street, Baltimore, MD 21202

Office of Railroad Policy and Development  
Federal Railroad Administration  
1200 New Jersey Avenue, SE  
Washington, DC 20590

Re: SC Maglev Scoping Report – Please reopen scoping process

To whom it May concern:

Until very late in June 2017 I, my neighbors and most Bowie land owners and residents had never heard about Maglev or the possibility it may come into our area.

There were no announcements, notifications, letters, posters, post cards, invitations to open house meetings, advertisements, scoping process, final scoping report or any other communications.

My wife and I have lived in Bowie for 51 years. Raised a family of 4 children and have 7 grandchildren. We have lived on Rustic Hill Drive in the “R” section for 40 years. It would be difficult for us in our late seventies to have to relocate. Our home is the center of family and extended family activities and holiday dinners for up to 30 people.

We have two small properties between our house and the WB & A trail. We have had a lot of inquiries to purchase our house and properties in the last two years, 3 in the last week. This project may explain that, almost all of them were from outside our area.



Now that I have received and read the 170 page Final Scoping Report of 5/17/2017, I understand why I and a whole lot of Bowie land owners had no knowledge of this project. We were never informed or included in the process in any way. Shameful! We were excluded and we may be the most affected by this project.

**Website**

Did not know website existed, therefor it was of no use to us.  
We were excluded

**Flyer Distribution Locations – EJ Outreach**

No contact or distribution locations in Bowie, MD per Final Scoping Report, the largest city between Baltimore and Washington.  
Only one distribution location in Anne Arundel County, a boys and girls club  
Again, we were excluded.

**Post Cards and letters**

Never saw one. I don't know anyone who did.  
Again, we were excluded.

**Open House Presentations December 10–15, 2016**

One location in Prince George's County – West Lanham fire house  
18 attendees out of a population of 39,395 with a total of 2 comments  
We did not know about this open house. No invitation.  
No open house in Bowie.  
Again, we were excluded.  
In the 5 open houses from Baltimore to Washington there were only 152 sign-ins and total of only 57 comments or questions. This is a very poor representation of the study area.

**Advertisements**

Never saw one. I don't know anyone who did. Newspapers: the Bowie Blade and the Washington Post. Saw no television ads on the Washington channels.  
Again, we were excluded.

**Public Comments**

In total there were only 75 comments and questions. 16 were from the website, 2 mail and 57 from open houses. The largest number by topic was 19 or 25% about alignment of routes. Most (15 to 17?) of these comments requested more information of a Maglev alignment. Was this information provided to them?  
Answers not in the report and still not provided to us.

**City of Bowie City Council Meeting – July 10, 2017**

A presentation was made by David Henley, SCMAGLEV Project Director. His presentation was primarily the technology and speed of the train, future possibilities, funding by Japan (5 billion dollars) and by the US Government (27

million dollars). No funding needed from Maryland. He did a good sales presentation.

Mr. Henley had been told not to comment on the alignment routes, impact or environmental issues. These were the primary reasons over 300 local citizens attended the council meeting.

The meeting overflowed the City Council Chambers and there were standing room only rooms set up with projection TVs to watch the presentations. The presentations were broadcast live on the city of Bowie TV station as well as scheduled rebroadcasts. I have no idea how many stayed at home and watched it on TV, my wife did. Most people in the room commented they just heard about the Maglev in the past 2 to 3 weeks by word of mouth.

24 citizens made presentations. A good number of them also provided their statements in writing to the Mayor. Mr. Henley was very impressed with every one of the presentations, as was I, and said he made a lot of notes to take back with him. He was also very apologetic and said he would work to make this process fair. He carefully made no direct guarantees. Each Council Member also made a brief presentation.

#### **Scoping Process / Report Should Include**

Opportunity for everyone to be informed, involved, submit comments and recommendations.

Include Mr. Henley's notes from the Bowie Council meeting on July 10, 2017.

The impact of alignment routes on homes and property, direct and property value.

Impact of alignment routes on churches, schools, recreation, parks and businesses.

City of Bowie report on council meeting of July 10, 2017 and other reports.

Notes and recommendations from the Citizens Against SCMAGLEV.

Bill Boone's analysis on the effect property on the yellow and green routes in Bowie, MD and Twin Rivers Retirement Community in Anne Arundel County

Pro forma or synopsis of pro forma based on revenue and ridership.

Projected ridership by Maryland citizens by; state regions, counties or city/communities for MAGLEV between Wash. BWI and Baltimore.

Overview of cost for Bowie, State of Maryland and the US

Overview of funding and future possibilities for liabilities

What are the terms of the 5 billion dollar loan from the Bank of Japan? Who is liable?

What is the status of all other of the SCMAGLEV proposal, approvals, projects, construction, operations and financial status worldwide?

Peter and Edna Granahan

Cc: David Henley

Bradley Smith  
Chris Van Hollen  
Steny Hoyer  
Ben Cardin  
Larry Hogan  
Doug Peters  
Rushern Baker  
Fred Robinson  
Bowie City Council Members

*Comments/Concerns/Issues*

*DENNIS BRADY*

*15916 PAYSLEY LANE*

*Bowie, MD 20716-1647*

*Cell phone (301) 908-6903*

*e-mail: kathyndennis@verizon.net*

October 9, 2017

SCMaglev Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Dear Mr. Al Khatib,

The Citizens Against the SCMaglev sent a letter in June 2017 requesting that the NEPA scoping process be reopened due to lack of public notice on the part of the Maryland Transit Administration. To date, we have not received any response from MTA.

As we will not have our concerns ignored, we are providing the following scoping comments. The project scoping report must be amended to include these issues and they must be addressed in the EIS.

**PURPOSE AND NEED**

1. Provide the need for the SCMaglev project. While a purpose of the proposed project was provided at the April 2017 public meetings, the need for a SCMaglev train was not provided. The public has not been provided any information that leads to the conclusion that an SCMaglev is needed. We strongly believe that there is a need for regional transportation improvements in the Washington-Baltimore corridor that will serve all residents, visitors, and businesses in the region. However, we have seen no data that proves that a train that serves only Washington, BWI, and Baltimore is needed or will provide relief from the transportation challenges in the region.

2. The Notice of Intent, published on November 25, 2016 (the day after Thanksgiving), states that:

“The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent. Similarly, the demand on the transportation infrastructure between Baltimore and Washington will continue to increase along major roadways and railways including Interstate 95, the Baltimore-Washington Parkway (MD 295), US 29, US 1, and the Northeast Corridor (NEC) thereby decreasing the level of service, reliability, mobility, and potentially decreasing safety.”

This statement does not provide proof that there is a need for a transportation investment of this magnitude with significant environmental impacts that only serves DC, BWI, and Baltimore. Rather it demonstrates that there are transportation issues that extend throughout our region. Analysis of these needs will demonstrate that these issues will only be solved by intra-regional transportation investments like rapid bus transit, light rail, and roadway improvements that serve the entire traveling population.

Provide detailed analysis of transportation needs of the Baltimore-Washington area.



Page 2 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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3. The Notice of Intent makes the following claims:

“Without additional transportation improvements and capacity within the Baltimore-Washington area, economic development and growth opportunities will be restricted. As congestion increases on the NEC and on the region's highways, the demand for continued economic development will be impacted, including, for example, tourism.”

Provide documentation of this hypothesis. Specifically, provide documentation that the “transportation improvements... within the area” would be served by a rail system that only stops in DC, BWI, and Baltimore. Provide documentation that tourism, which is fueled by the great historical and governmental significance of the Washington, DC area would decline without a train that ONLY brings people from Baltimore.

4. Further the Notice of Intent quotes the NEC Future Program:

“To address these issues, in 2012 FRA launched the NEC FUTURE program to consider the role of rail passenger service in the context of current and future transportation demands and to evaluate the appropriate level of capacity improvements to make across the NEC. Through NEC FUTURE, FRA will determine a long-term vision and investment program for the NEC documented in a Tier 1 EIS and Service Development Plan. FRA published a Tier 1 Draft EIS in November 2015; however, the Draft EIS evaluated steel-wheel technologies as a way to serve the passenger rail needs of the region. It left open the possibility and did not preclude the study of and investment in advanced guideway and other new technologies, such as SCMaglev, to meet the transportation needs of the Northeast, including the Baltimore-Washington area.”

We note that this statement says that the NEC future EIS leaves open the possibility of studying the SCMaglev. While we concur, this statement does not support the need for the SCMaglev and calls into serious question why alternatives other than SCMaglev are not being studied in this EIS. See comments on Alternatives below.

5. Independent Utility/Segmentation - Provide detailed analysis proving that the proposed project has independent utility. Provide financial data showing how a train from DC to Baltimore can sustain short-and long-term viability without government funding and without an extension past Baltimore.

As BWRR is a subsidiary of The Northeast Maglev, LLC (TNEM), and TNEM has announced plans to build the SCMaglev from Washington, DC to New York, the entire SCMaglev project is a connected action. Separation of the project into pieces violates the prohibition on segmentation per the Council on Environmental Quality (CEQ) NEPA implementing regulations.

The CEQ NEPA regulations state that:

**§1502.4 Major Federal actions requiring the preparation of environmental impact statements:** Proposals or parts of proposals which are related to each other closely

enough to be, in effect, a single course of action shall be evaluated in a single impact statement.

**§1508.25 Scope:** To determine the scope of environmental impact statements, agencies shall consider 3 types of actions, 3 types of alternatives, and 3 types of impacts. They include: 28 (a) Actions (other than unconnected single actions) which may be: (1) Connected actions, which means that they are closely related and therefore should be discussed in the same impact statement. Actions are connected if they: (i) Automatically trigger other actions which may require environmental impact statements. (ii) Cannot or will not proceed unless other actions are taken previously or simultaneously. (iii) Are interdependent parts of a larger action and depend on the larger action for their justification. (2) Cumulative actions, which when viewed with other proposed actions have cumulatively significant impacts and should therefore be discussed in the same impact statement. (3) Similar actions, which when viewed with other reasonably foreseeable or proposed agency actions, have similarities that provide a basis for evaluating their environmental consequences together, such as common timing or geography. An agency may wish to analyze these actions in the same impact statement. It should do so when the best way to assess adequately the combined impacts of similar actions or reasonable alternatives to such actions is to treat them in a single impact statement.

6. Provide data demonstrating the following required for financial viability:

- Number of daily trains in each direction
- Number of cars per train
- Number of riders per train, per day, and per year
- Cost per ticket
- Cost to operate and maintain rail line
- Any financial incentives (funding, grants, tax breaks, loans, etc) needed from governmental sources (Federal, State, or local) for construction and operation in the short-term and long-term
- Provide information on foreign investment including required purchase of foreign-made rail components/trains
- Provide examples of where the SCMaglev is currently operating in the world and provide financial data on that operation demonstrating that it does not require continual or intermittent government subsidies.

Page 4 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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7. Amtrak's annual ridership on the Northeast Corridor (from DC to Boston) for 2016 was 11.9 million. Further Amtrak estimates NexGen ridership in 2030 of 25 million for the ENTIRE Northeast Corridor (<https://www.amtrak.com/ccurl/214/393/A-Vision-for-High-Speed-Rail-in-the-Northeast-Corridor.pdf>).

In 2016, a total of 5,098,562 riders boarded Amtrak trains at the Union Station in Washington, DC, and a mere 1,030,161 riders boarded Amtrak trains in Baltimore <https://www.amtrak.com/national-facts>. Only 689,042 riders boarded/off-boarded at BWI in 2016. (<https://www.amtrak.com/ccurl/111/103/MARYLAND16,0.pdf>). It is inconceivable that most of these 6.8 million riders were solely traveling between Washington and Baltimore.

2016 ridership on the MARC Camden and Penn lines totaled approximately 1.3 million riders (<https://data.maryland.gov/Transportation/MTA-Average-Weekday-Ridership-by-Month/ub96-xxqw>), but MARC serves numerous stations and communities in between Baltimore and Washington – commuters that would not be served by the SCMaglev. In addition, in June 2016, MARC reported ridership DECLINES: "Currently, the MARC service is facing ridership declines, according to Erich Kolig, director, MARC train and commuter bus service. "They are rather slight, only about a percent down from last year," he explains. "But we are not experiencing the 3% growth that we have been maintaining for the last 10 years." (<http://www.metro-magazine.com/rail/article/713751/maryland-s-marc-railroad-upgrades-fleet-service-to-bolster-ridership>)

In its filing with the State of Maryland, BWRR claimed that "Annual ridership on the SCMaglev system connecting Washington to Baltimore would be between 10.2 million and 15.4 million annual passengers in 2030 (42,200 one-way trips per day on average), while still assuming continued Amtrak and MARC service."

**Provide proof and solid data on how the SCMaglev will achieve 10 times the current Washington/Baltimore station onboard/offboard riders and 41 to 62% of the projected 2030 riders on the entire Amtrak Northeast Corridor in the short line from DC to Baltimore while "still assuming continued Amtrak and MARC service"! Include detailed origin and destination data for riders, type of riders (commuters, business travelers, tourists).**

See **Environmental Impacts** for further issues regarding Amtrak.

8. If the need for an SCMaglev train from DC to Baltimore cannot be defined, and independent utility for this short segment of rail cannot be established and sustained over the short- and long-term, the EIS must be revised to include the entire corridor (whether to Philadelphia, New York, or Boston) needed to sustain the financial viability of the project in accordance with 40 CFR 1502.4a and 40 CFR 1508.25.

Page 5 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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## ALTERNATIVES

The structure of this EIS based solely on studies for the SCMaglev, precludes all other possible decisions including the decision to construct Amtrak NextGen trains. As such, an EIS that does not study these other alternatives is not in compliance with the National Environmental Policy Act.

### As stated by the Council on Environmental Quality (40 Most Asked Questions):

Question 2a. Alternatives Outside the Capability of Applicant or Jurisdiction of Agency. If an EIS is prepared in connection with an application for a permit or other federal approval, must the EIS rigorously analyze and discuss alternatives that are outside the capability of the applicant or can it be limited to reasonable alternatives that can be carried out by the applicant?

A[nswer]. Section 1502.14 [of the CEQ regulations] requires the EIS to examine all reasonable alternatives to the proposal. In determining the scope of alternatives to be considered, the emphasis is on what is "reasonable" rather than on whether the proponent or applicant likes or is itself capable of carrying out a particular alternative. Reasonable alternatives include those that are practical or feasible from the technical and economic standpoint and using common sense, rather than simply desirable from the standpoint of the applicant.

2b. Must the EIS analyze alternatives outside the jurisdiction or capability of the agency or beyond what Congress has authorized?

A. An alternative that is outside the legal jurisdiction of the lead agency must still be analyzed in the EIS if it is reasonable. A potential conflict with local or federal law does not necessarily render an alternative unreasonable, although such conflicts must be considered. Section 1506.2(d). Alternatives that are outside the scope of what Congress has approved or funded must still be evaluated in the EIS if they are reasonable, because the EIS may serve as the basis for modifying the Congressional approval or funding in light of NEPA's goals and policies. Section 1500.1(a).

**In addition, Section 4(f) of the Department of Transportation Act prohibits the use of Section 4(f) property unless there is no feasible and prudent avoidance alternative to the use of land from the property. If the SCMaglev project uses public parkland, wildlife or waterfowl refuge land, or historic resources, it may not be constructed if there is a prudent and feasible alternative. It should be noted that the WB&A Trail is public parkland – NOT a railroad right-of-way, and as such is protected under Section 4(f).**

**Lastly, as noted in Amtrak's scoping comments for this EIS, the NEC Future EIS and Record of Decision, selected improvements to the existing rail alignments and "discarded the new alignment alternative."**



Page 6 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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1. NexGen – The Amtrak NexGen technology is a “reasonable” alternative to Maglev and therefore, in accordance with NEPA and the CEQ implementing regulations (40 CFR 1502.14), must be studied in the EIS. ADD INFORMATION ON NEXGEN – SPEED, TIMING, COST, ETC. While outside of the Congressional legislation, the NexGen technology is a “reasonable alternative as it is practical and feasible from the technical and economic standpoint and must be included using common sense, rather than simply desirable from the standpoint of” BWRR. In addition, as construction funding is not available at this time, it is prudent to provide Congress with an equal analysis of all alternatives to facilitate informed decision-making.

Lastly, the Amtrak NexGen is a prudent and feasible alternative to the SCMaglev and would avoid the use of public parkland (including the WB&A Trail), wildlife and waterfowl refuges, and historic properties. Therefore, the Amtrak NexGen must be studied as an alternative to the SCMaglev.

2. Elon Musk and SpaceX have proposed constructing an underground Hyperloop railroad to travel between Washington, DC and New York. As with NexGen, this technology must be analyzed alongside the SCMaglev to provide decision makers with information on the environmental impacts of each technology prior to making a decision.
3. Detail on alternatives must include design details including but not limited to: limits of disturbance; right-of-way; staging areas; station areas and impact areas for these; connections between stations and existing transportation hubs (i.e. BWI); maintenance yards and impact areas for construction of these; utility connections; security features including fencing; haul routes for construction equipment, materials, construction debris, and mining spoils; and disposal areas for construction debris and fill materials.

### **Environmental Impacts**

1. Environmental Justice – Provide a detailed analysis on the disproportionate impacts that low-income and minority populations in Washington, DC; Prince George’s County; and Baltimore City will suffer under the SCMaglev plan including but not limited to loss of property values and housing opportunities, loss of parkland and greenspace, increases in noise levels, and impacts to health and safety. Specifically, the above ground portion of the project will bisect Prince George’s County while not providing ridership opportunities to the citizens of the County. Provide analysis of the EPA EJScreen demonstrating the disproportionate impacts already suffered by the residents of the County and the impact that a SCMaglev train would have. A baseline EPA EJScreen along the WB&A Trail shows an EJ score of over 90% compared to 50 to 60% in communities in Montgomery and Howard counties.

2. Property Acquisition – Provide information on all properties to be acquired and displaced as a result of the proposed action including:
  - a. Number, types, and locations of all of acquisitions/displacement of residents, parks, businesses, schools, churches, etc;
  - b. Method by which properties will be acquired including information on eminent domain and what entity will exercise eminent domain;
  - c. How compliance with the uniform relocation act will occur including analysis of the availability of comparable housing of equal price, value, and setting
  - d. Impact to remaining homes including loss of home values, changes in setting, visual and noise impacts. Provide information on how close a residence can be from the rail line
  - e. Impacts to remaining businesses including change of access and resulting loss of business.
3. Economic Impacts – Provide thorough, quantitative analysis on the negative economic impacts associated with the proposed alternatives including:
  - a. Loss of property values (especially home values) from the proximity of a high-speed train where there is currently more compatible, highly valued land uses such as parks, trails, forested areas, and residential areas
  - b. Loss of tax base from the loss of properties and from the devaluation of remaining properties
  - c. Impacts to Amtrak ridership and revenues in the Washington/Baltimore corridor, the Northeast Corridor, and nationally as many of Amtrak's lines outside urban areas count on revenues generated in areas like the Northeast Corridor
  - d. Impacts to viability of NexGen Acela as it is probable that there is not sufficient ridership/need for both technologies to operate in the same market
  - e. Impact to Marc ridership, revenues, and viability with the proposed loss of riders to the high-speed train; this assessment should include any additional State, local, or Federal funds that will need to be expended to keep the Marc system viable. If the Marc system is no longer viable or must raise fares to maintain viability, the impact to commuters that board the train at locations between Baltimore and Washington must also be analyzed
  - f. Analysis of permanent job creation with SCMaglev vs. job losses at Amtrak and Marc
4. Parks and Recreation/Section 4(f) – Provide impacts to parks and recreation areas in Prince George's and Anne Arundel Counties including the loss of access to these critical community features. Impact analysis should include changes to these facilities including changes to park settings, park features, park noise levels, and the overall changes in the user's experience. Note – the WB&A Trail is public

parkland; owned and operated by the Maryland National Capital Park and Planning Commission and the Anne Arundel Recreation and Parks Department. The trail is a critical component of the East Coast Greenway and the American Discovery Trail.

5. Historic Resources – Provide impacts, including visual, noise, and vibration impacts, to historic resources including archaeological features, historic sites such as the Glen Dale Hospital, the Marietta Mansion, the Bowie Racetrack, private historic residences and farms, and historic districts including the Odenton historic district.
6. Visual Resources - As the only renderings provided by MTA thus far show a train in the middle of a forest with no residences nearby, provide more accurate renderings of the train line. Specifically:
  - a. Provide before and after renderings of the proposed rail line and train in ACTUAL locations showing the view to and from historic properties, parks, and residential properties that will remain after construction of the train.
  - b. Renderings to and from historic properties, parks, and residential properties should include day and night views and views with and without leaf cover on trees.
  - c. Provide information on the visual intrusion of graffiti into neighborhoods that may occur on the elevated rail line.
7. Topography – Provide details on changes to topography including cut and fill; provide detail on whether the proposed train and track system will cut through hills (creating environmental impacts) or rise up over them creating visual impacts to large numbers of communities and citizens.
8. Water Quality – Provide quantitative data on the increases in impervious surface created by the track, stations, maintenance yards, and parking.

Provide quantitative data on the resulting stormwater runoff and impacts to erosion, sedimentation, and TMDLs in the already degraded Patuxent River and its tributaries including Horsepen Branch.

Provide information on the chemicals to be used for snow removal and deicing and the impact runoff of these chemicals will have on the Patuxent River and its tributaries.
9. Bird Strikes – Provide data and analysis on the impact of bird strikes and how they will be prevented from a train going over 300 mph. There are nesting bald eagles on the Patuxent Wildlife Refuge in close proximity to the WB&A Trail. Bald eagles also frequent the Patuxent River between Prince George's and Anne Arundel counties. In addition, there are numerous other bird species including barred owls, pileated woodpeckers, and red-tailed hawks nesting along the WB&A Trail and along the river.
10. Bats - Provide data and analysis on the impact to bats and how strikes will be prevented from a train going over 300 mph. Provide detailed studies of bat habitat, bat populations, and presence of white-nose bat syndrome.

11. **Wildlife Displacement** – Provide data and analysis on wildlife displacement from construction of the proposed train. Habitats in the vicinity of the proposed alternatives are at capacity and any loss of habitat will force wildlife further into developed areas including residential areas. Detailed wildlife surveys must be conducted to document species, numbers, health of population (including over population), and capacity of habitat.
12. **Vegetation** – Provide data on number, species, size, and locations of all trees to be removed for construction of the rail line. Provide types of vegetation to be removed. Provide information on how the project will prevent invasive species from being introduced into areas where forest are bisected and native vegetation is removed. Provide mitigation including where and what types of vegetation will be replanted, how reforestation areas will be monitored, and how the rail line will be screened after vegetation is removed.
13. **Noise** – Provide 24-hour noise analysis for all types of properties in the vicinity of the proposed alternatives (residences, schools, churches, libraries, historic sites, parkland, etc.). Provide existing noise levels and noise modeling when the train is in operation. Noise models should take into account the loss of existing vegetation and buildings that would be removed and modeling should show day and night noise levels.
14. **Electromagnetic Fields** – Provide information on electromagnetic fields including health effects from long-term exposure to an SCMaglev train. Provide health information from populations currently exposed to SCMaglev trains.
15. **Light** – Provide information on how the rail line, parking areas, rail yards, stations, etc. will be illuminated at night. Provide analysis of light intrusion into residential areas, parkland, and historic sites.
16. **Energy Usage** – Provide information on the energy needed to operate the SCMaglev train including the source of the power. Provide locations of any new substations or other utility upgrades needed to power the SCMaglev. Provide information on how power interruptions would affect train operation.
17. **Utilities** – Provide information on temporary and permanent utility disruptions to residences, businesses, and other property owners. Provide information on the location and type of new utility lines needed to serve the proposed project. Provide information on impacts to the power grid and other utilities.
18. **Community Cohesion** – Provide information on impacts to neighborhoods that will be bisected or will be cut off from neighboring communities by the SCMaglev Train.  
  
Provide information on the effects to communities such as Glenarden, Glen Dale, Bowie, Piney Orchard that will be bisected by the proposed train. Include analysis of the cumulative effects of bisection by transportation projects to communities such as Glenarden.



Page **10** of **12**  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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19. Land Use and Land Use Planning – Provide detailed analysis of changes in land use that will occur due to undesirable proximity to the train and from the economic development that BWRR claims will accompany the train. Provide detail on where this development will occur and how it will affect sprawl in the Baltimore-Washington Area
20. Community Services –
  - a. Provide analysis on effects to police, fire, and EMS services including responding stations, personnel levels, training (how much it will cost, who will provide it, and how often it will occur). Provide information on BWRR provided police services and a comparison of those services to those provided by Amtrak police. Provide information on how terrorist threats will be mitigated at stations and on the rails.
  - b. Provide information on how other community services will be impacted including schools, hospitals, churches, etc. Include analysis of cumulative impacts based on the economic growth projected to occur solely because of the SCMaglev construction (as based on BWRR claims).
21. Safety and Security – In addition to the police information requested above, provide information on how the SCMaglev rail line will be secured such that people cannot climb onto the line resulting in injury or death and/or vandalize the line. Provide renderings of any fences or other barriers that will be placed along the rail line, rail and maintenance yards, parking areas, etc. Provide information on the distance fence lines will be placed from the line, height of fences, and type of fencing.
22. Construction Impacts – Provide information on the construction related activities and impacts including but not limited to:
  - a. Limits of disturbance for construction of each of the alternatives – temporary and permanent land use
  - b. Areas of cut and fill
  - c. Staging areas
  - d. Tunneling/mining locations
  - e. Tunneling technology
  - f. Haul routes for construction equipment and disposal of tunneling spoils
23. Traffic – Provide impacts from:
  - a. Construction vehicles traffic
  - b. Temporary and permanent roadway closures including travel delay times, creation of land-locked properties, changes in access to properties

- c. Additional vehicular traffic, especially at Union Station, BWI, and in Baltimore generated from train riders accessing the rail line; analysis should include intersection level turning movements and delays
- d. Traffic from economic development that BWRR claims will accompany the SCMaglev; the Notice of Intent states that “The population in the Baltimore-Washington area makes up one of the largest and densest population centers in the United States. Over the next 30 years the population in the area is projected to increase by approximately 30 percent.” If this growth will occur without the SCMaglev, and the SCMaglev claims it will radically increase development in the region, the cumulative impact analysis must quantitatively analyze the traffic that will accompany this level of development and explain how the rest of the regional transportation network will handle this development or where levels of service will decrease to the point of complete gridlock.

24. Airline/Plane Traffic – As part of BWRR’s economic and ridership “promises” include increased use of BWI and development around BWI, increases in air traffic must be analyzed in the EIS.

This analysis must also include resulting increases in noise from increased air traffic. It should be noted, that Governor Larry Hogan recently instructed the State Attorney General to sue the FAA for noise violations at BWI.

25. Union Station –

- a. Provide information on the impacts to the historic Union Station in Washington, DC
- b. Provide information on how the SCMaglev Train will impact current renovations taking place at Union Station
- c. Provide analysis of how a SCMaglev Train will affect the Washington Union Station’s 2nd Century Plan which is being developed to accommodate the Amtrak NexGen train.

26. Cumulative Impacts – A project of this magnitude requires a comprehensive analysis of cumulative impacts including quantitative analysis of the significant harmful impacts transportation projects have had and continue to have on communities in Prince George’s and Anne Arundel counties. Included in this cumulative impact analysis must be the impacts of the supposed development that SCMaglev claims it will generate. In addition, the cumulative impact analysis must analyze any track widening or realignments planned by Amtrak.

The Notice of Intent also provides grossly inaccurate information on the study area of the proposed project. The NOI states that the study area is bounded by the former Washington-Baltimore & Annapolis Electric Railroad alignment while in reality, the study area extends 3 miles to the east of this alignment. Maps of preliminary alternatives have been provided by MTA which MTA now states should not be relied on. BWRR CEO Wayne Rogers, as recently as September 14, 2017, stated in an interview with ABC 7 news that ““We’re very early in the process. They’re called initial preliminary screening alternatives. They’re not even to the preliminary to the alternatives to the preferred alternatives, so

Page 12 of 12  
October 5, 2017  
SCMaglev Project  
c/o Suhair Al Khatib

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we're years away," when in fact alternatives have been submitted to FRA for approval. These examples of misinformation by the MTA project team along with abysmal public notice on the scoping process for the project, demonstrate that this project should be restarted to provide meaningful opportunity for public input and decision-making.

As MTA has not responded to repeated requests to reopen the scoping period for this project, we request that you provide a formal reply to this letter and an affirmation that the issues outlined herein will be thoroughly analyzed in the EIS.

You may reach us at:


Citizens Against the SCMaglev  
PO Box 669  
Bowie, MD 20715

Sincerely,

Citizens Against the SCMaglev



On behalf of:



Dennis Brady, Chair  
Dawn Wampler, Vice-Chair  
Aviva Nebesky, Vice-Chair  
Breanne Reynolds, Secretary  
Joan Glynn, Secretary

Cc:

EPA Region 3  
US Senator Chris Van Hollen  
US Senator Ben Cardin  
Congressman Steny Hoyer  
Governor Larry Hogan  
State Senator Doug Peters  
Delegate Geraldine Valentino-Smith  
Delegate Marvin E. Holmes, Jr.  
Delegate Del. Joseph F. Vallario, Jr.

Prince George's County Executive Rushern Baker  
Prince George's County Councilman Todd Turner  
Bowie City Mayor Fred Robinson  
Bowie Councilman James Marcos (At Large)  
Bowie Councilman Henri Gardner (At Large)  
Bowie Councilman Michael Estève (District 1)  
Bowie Councilwoman Diane Polangin (District 2)  
Bowie Councilwoman Courtney Glass (District 3)  
Bowie Councilman Ike Trough (District 4)

**Peter & Edna Granahan**  
12102 Rustic Hill Drive, Bowie, MD 20715  
301-262-9018 pegrarahan@verizon.net

SCMAGLEV Project, C/O John G. Trueschler  
Maryland Transit Administration  
6 Saint Paul Street, Baltimore, MD 21202

Re: SC Maglev questions for the September 5, 2017 Bowie City Council meeting

Dear Mr. Trueschler:

- Questions:
  - **Who in Maryland would use this train between Washington, BWI & Baltimore?**
    - It would take too much time to travel to these stations to make it worthwhile for us to use SC MAGLEV, and the total travel time would be longer not shorter. And it would cost more.
    - Better options and access are available now for Marylanders; Metro, Light Rail, Subway, Marc Train, Amtrak, Acela Express, and others. Plus improvements are planned.
    - I do not know anyone who would use SC MAGLEV.
    - I would have no reason to use this MAGLEV train.
  - **Is the protection of homes, businesses, religious centers, schools, communities, personal property, recreational facilities, parks, green areas, family lifestyles and property values the top priority in selecting the route for this train, if it is going to be built?**
    - **If not, do not build it. The project has no true value to Maryland and potentially harms our state and its residents.**
    - **How Many homes and other facilities would be torn down if the train ran through Bowie and other communities on the same route?**



- **Why consider building this “TEST PROJECT” in Maryland and not in an area such as Philadelphia to New York where it should have more ridership?**
- **Can we cancel this project and return any remaining funds from the \$27 million grant from the Federal Government, so it could be used for other needed purposes such as medical insurance or infrastructure repairs?**
- **What is the environmental impact (construction runoff) and financial resources impact of this train experiment on the Chesapeake Bay, removing the potentially disastrous backup of silt from the Conowingo Dam, restoring the Bay and the seafood industry in Maryland? Recreational fishing? Crabs, oysters & rockfish?**
  - The Chesapeake Bay should be a much higher priority for Maryland than this train.
- **Who is liable for the \$5 billion loan from the Bank of Japan and what are the terms of that loan?**
  - We were told in the July <sup>10, 2017</sup> ~~12, 1971~~ by Mr. Henley that the funding will be provided by Japan and the US Federal Government and no funds were needed from Maryland. I do not believe that.
  - What are the Maryland possible costs?
- **What is the expected cost of a trip from Washington to Baltimore on this train?**
- **Why did most of us only hear about this proposed train 3 months after the 170 page Final Scoping Report was completed, with almost no participation from citizens?**
  - The Scoping process and report were fatally flawed, the report has no value, and the report should be discarded and scoping process done again after open honest disclosure of potential impacts of this project on citizens of Maryland.

- We were not informed about this train project, meetings or the scoping process.
- There were many ways to contact us if you wanted to: newspaper articles, newspaper inserts, mailing lists from state land records, religious institutions, community organizations, local government distributions, TV, radio announcements, handouts in the local communities, businesses, and etc.
- **What is the history and status of all of the SC Maglev proposals, projects, completions, financial successes, failures, cancelled proposals, cost overruns, projects bailouts by governments, medical concerns, legal concerns or any other problems or claims?**
  - This should also be part of the scoping report.
- **Have you released your Pro-forma evaluation / documents for this Washington, BWI and Baltimore project for review by independent third party analysts?**
  - If not, please release them as soon as possible to be able to review this project.
  - Also release the Pro-forma evaluation for the proposed plan for Washington to New York.
- **Who will; own the technology, manufacture the trains, Maglev operating components, operating systems, parts and jobs?**
  - What happened to build American, buy American and American jobs?
  - If this project is successful and expands in the future, who will design and build the trains, the operating components and parts?
  - Who would control this potentially major part of the United States transportation infrastructure, Japan or the United States?
  - Would the train system be compatible with other systems built in the US?
  - Could the US build high speed systems in other countries?
- **Who would pay for the more than \$100 Billion cost to build this system from Washington to New York?**
  - Japan? United States?

- This rail system makes no sense if it does not travel long distances.
  - Is there any possibility this would be financially successful for the US? NO!
  - The \$100 Billion cost estimate is probably very low. There is a very long building period with major tunnels and no revenue without trains operating.
- **Finally, why should we consider this test project in Maryland?**

Peter and Edna Granahan

Cc: President Donald Trump  
David Henley, SCMAGLEV Project Director  
Bradley Smith, Maryland Dept. of Transportation  
Federal Railroad Administration  
US Senator Chris Van Hollen  
Congressman Steny Hoyer  
US Senator Ben Cardin  
Governor Larry Hogan  
State Senator Doug Peters  
County Executive Rushern Baker  
Mayor Fred Robinson  
Bowie City Council Members



DATE 10/16/17

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**COMMENT FORM**

Name: Dorothy Bartolotta

Address: 1603 Katherine Village Sq.

City: Annapolis State: MD Zip: 21409

Telephone: 410 349 9422 Email: 0902sep@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: this project is a step in the right  
directions to solve the massive traffic problem  
we have in this corridor. The money for this project  
is much better spent rather on the <sup>improvement of</sup> present  
infrastructure.





DATE 10/16/17

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**COMMENT FORM**

Name: Donna + Wesley Redd

Address: 1447 Maryland Avenue

City: Severn State: MD Zip: 21144

Telephone: 410-533-0019 Email: dcredd44@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: At this time, there is no need for this  
expensive train. It will not benefit the local communities  
that will be destroyed through this development. Stops at  
Baltimore, BWI and Washington DC will not ease the  
commute for locals. Those riding the train will need to  
commute to the stations — NO HELP! The money could  
be used to resolve other concerns in the communities —  
opiod addiction, heathcare, repair current infaastructure,  
mental heath. I could go on and on. The current proposed  
lines impact 2 elemantary schools; Ridgeway and  
Hebron-Harman and my home! Not acceptable!!



DATE 10/16/2017

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**COMMENT FORM**

Name: Michael Scaper

Address: 1283 AUA ROAD

City: SEVERN State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: MSCAPER1@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

1) PLEASE EVALUATE THE IMPACT OF THE TUNNELS ON WATER QUALITY IN THE AQUIFER, ALSO ANY IMPACT OF WATER QUALITY FOR THOSE ON WELLS.

2) PLEASE EVALUATE ANY IMPACT ON WILDLIFE IN "ENVIRONMENTAL/NATURAL AREAS"

3) SINCE THIS MAY BE THE FIRST LEG TO CITICUS MOUTH - OTHER MARYLAND COUNTIES SHOULD ALSO BE CONSIDERED FOR THEIR INPUT REGARDING IF THEY WOULD WANT A ~~MAGLEV~~ MAGLEV TRAIN THROUGH THEIR COMMUNITIES.


**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/17

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**COMMENT FORM**Name: Patricia BlakeAddress: 500 Saltoun AvenueCity: Odenton State: MD Zip: 21113Telephone: 410-672-1725 Email: \_\_\_\_\_Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I have resided in my home for 44 years and have raised my family here. Deciding to come thru our homes to build your train is not an option for all of the people you are going to impact. Looking for other land areas would be the best option for you to be looking at. Please consider more common sense options before jump in and start taking away what we as a community hold dear to our hearts. Thank You!!!




**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 16 Oct '17

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**COMMENT FORM**Name: MARIA EDERAddress: 515 QUEEN ANNE AVECity: ODENTON State: MD Zip: 21113Telephone: 410-674-7319 Email: N/AJoin our mailing list?  Yes  NoPreferred method of communication?  Email  MailComments: Why do we need this project in our area?And who is going to pay for it?It better not be our tax money. Most persons  
may not even use the — train.I think our roads should be fixed more  
than having the train. No more pot holes &  
rough roads, no patched up work. And repairs  
should be done quickly & done right.The public should be informed better. By sending out  
literature by mail or e-mail to who has access  
to e-mail.At this time, the proposed route is not thru my  
area, which is good.





DATE 10-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: William Ford

Address: 7899 Covington Avenue

City: Glen Burnie State: MD Zip: 21061

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am against this project, It will only help  
wealthy people commute faster, leaving the masses to  
foot the bill. Tax dollars should not be wasted  
in this manner, to only help a small few.



DATE 10/16/17

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**COMMENT FORM**

Name: Alan Redd President JRM Automotive  
 Address: 8272 Quarterfield Rd  
 City: Severn State: MD Zip: 21144  
 Telephone: 410 305 1190 Email: areddpost7@usa.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I am very concerned for the community disruption and potential loss of revenue. The viability of my business could be impacted in several ways. Either do impress/egress issues due to construction, or loss of potential clients due to housing loss. Most importantly with the line still not set 100% I am still concerned with loss of my property due to the train itself. I am STRONGLY AGAINST this project. If you can't use the current rail right of way then it is not necessary. NOT GOOD FOR BUSINESS, NOT GOOD FOR THE NEIGHBORHOOD, NOT GOOD FOR A.A. COUNTY.



DATE 16 Oct

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**COMMENT FORM**

Name: Jenn Carson

Address: 513 Maple Ridge Ln

City: Odenton State: MD Zip: 21113

Telephone: 443-694-0758 Email: Djinn.Carson@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Thank you for holding the open house. Your staffers were friendly and knowledgeable — cool under some unfortunate pressure. I wish there was more effort to debate and inform those who have already decided to oppose this project. There is information here that could be helpful in calming the hysteria — particularly the timeline (no — you won't wake up tomorrow to bulldozers in your yard). There is also room for more information to be presented ("my house is on a dotted line — if it's a tunnel, will I still have to leave my home?").

Thanks again!

Jenn Carson





DATE October 16, 2017

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**COMMENT FORM**

Name: Adam Gotz

Address: 170 Ellen Avenue

City: Severna State: MD Zip: 21144

Telephone: (443) 417-3718 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am entirely opposed to a MAGLEV proposal that requires eminent domain to displace residents from their homes. There is no justification for this train that is more important than the residences of these communities. This project should only move forward if it can do so without displacing or otherwise negatively impacting the residents in the train's proposed path.

\_\_\_\_\_

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DATE 10/16

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COMMENT FORM

Name:

Christopher DeManss

Address:

8010 Covington Ave.

City:

Glen Burnie State: MD Zip: 21061

Telephone:

443 557 8340 Email: cdemanss@aaccps.org

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: How do you reconcile the fact that the rail line will minimally serve the people w/in the communities most affected, namely those whose homes you will take via eminent domain? How do respond to arguments highlighting the cost (perhaps 2000 homes taken by eminent domain) so that you can charge an outlandish rate per fare that very little Marylanders can afford? How do you respond to arguments highlighting the likely difficulty to recoup the 28 or so billion dollars via fares nobody can afford? What about the fact that multiple Maglev projects have failed? Why keep pushing a failed project?

↑ STUPIDITY? ↑



DATE 10/16/17

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**COMMENT FORM**

Name: Therese ~~Bar~~ Barrett

Address: 513 Queen Anne Ave

City: Odenton State: MD Zip: 21143

Telephone: (410) 551-2271 Email: happyt1b@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Please do the right thing and do not  
displace thousands of people. Pick Route the  
affects the least amount of people. My home  
will be paid off in Feb. 2018 and I will be  
retiring in 2 years and cannot afford to buy  
another house, even with whatever money would  
be paid (if it is). Prices of housing is so high  
I could never afford it.

Thank you,



DATE 10-16-17

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**COMMENT FORM**

Name: Joyce C. Robinson

Address: 8010 Covington Ave

City: Glen Burnie State: MD Zip: 21061-4989

Telephone: 410 969 9057 Email: orcawolf@cable-speed.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail (either)

Comments: I feel this is only going to service wealthy  
travelers & will do nothing to help the middle or lower-  
middle class families. It is an over-priced  
project unsuited to this (planned) area. I do  
not believe it will enhance the quality of life  
for those in the immediate communities. The area is  
already over-developed. Enough with the  
destruction of our environment & quality of life.  
Respectfully, JRobinson





DATE 16 Oct 17

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**COMMENT FORM**

Name: Michael Young

Address: 1314 Somerset Rd

City: Sewern State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: looseduse@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am opposed to route E1 that would put a tunnel under my neighborhood,  
I would like to see the economic analysis for this project. We already have  
AMTRAC and MARC that can't pay for their own operation. Who expects  
enough economic growth to justify this?

The staff was very friendly at the open house





DATE 10/16/17

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**COMMENT FORM**

Name: Susan Grieve

Address: 120 Edelton Avenue

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: sgrieve4@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am concerned of the cost for this train.  
I also do not think the communities in Anne Arundel  
County will ~~not~~ benefit from this train. This  
area is very congested. Please do not put  
this train in MD.

Thank you.

DATE 10/16/2017

baltimorewashingtongscmaglevproject.com

**COMMENT FORM**

Name: FRANCES DAILEY  
 Address: 1363 ROSANNA DR  
 City: ODENTON State: MD Zip: 21113  
 Telephone: 40674-2583 Email: francesc.dailey@gmail.com

Comments: I AM totally opposed to this feshler plan. This train will not benefit the residents of MD in any way. Commuters go into Washington and Baltimore from the suburbs. A very small percentage of commuters travel between Baltimore and Washington. If the federal government has the money give it to MD to improve current commuter routes! The MARC station in Odenton needs a multi-level parking garage - it would cost pennies compared to the money being wasted on this effort.

Stop studying the AMTRAK route - it affects far too many neighborhoods! The most logical alignment is along the Parkway where it impacts the fewest people.



DATE 10/16/17

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**COMMENT FORM**

Name: Scott Grieve

Address: 120 EDELTON AVE

City: SEVERN State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: WHO IS EXPECTED TO RIDE THIS TRAIN? WHAT IS THE COST TO RIDE?

THE MOST CONGESTED AREA AROUND ME IS RT 32 NEAR NSA, THEY DOES NOTHING  
TO RESOLVE THAT TRAFFIC PATERN, I FEEL THE COST TO BUILD THIS TRAIN  
COULD BE BETTER SPENT TO UPDATE THE EXISTING TRAFFIC OPTIONS (AMTRAK, MARC,  
ADDITIONAL TRAVEL LANE @ RT 32 + NSA,



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/10/17

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**COMMENT FORM**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Where does Gov. Hogan Live?  
put the train through his place...





DATE 10/16/2017

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**COMMENT FORM**

Name: Wylie Donaldson

Address: 509 Maple Ridge Ln

City: Odenton State: Md Zip: 21113

Telephone: \_\_\_\_\_ Email: Wnd281@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Do we really need the MAGLEV? I say no!!  
I say no to going through any part of Odenton!  
It appears it will only benefit those traveling from  
Wash to New York! POLITICIANS???  
As a taxpayer I say no!!



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/17

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COMMENT FORM

Name: SUSAN S. DIXON

Address: 1308 JADE ST

City: ODENTON State: MD Zip: 21113

Telephone: 410 695 2048 Email: SUSAN.DIXON1@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Why weren't residents notified sooner  
to prepare for disruption. Have forested areas  
been considered / wildlife? I live in 55+  
community that was on proposed list to  
have train go through. Looks like that  
has changed - Thankfully - I worry about  
noise and construction disruption - dust, etc.  
Again concerned why so secretive. We have  
a right to know what is happening in our  
neighborhoods. We need time to prepare and  
learn and try to understand. Please  
keep neighbors informed.  
Thank you



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Barry + Kristin Hammen  
 Address: 8212 Lee Ave  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-551-1696 Email: barry.hammen@gmail.com

Comments: \_\_\_\_\_

As residents of Severn, MD along WB+A  
road we have serious concerns about  
the E1 route. The impact on residences  
in this area would be huge, while  
not having any benefit to them.  
We believe the money required to  
fund this project could be better  
used to improve existing infrastructure.




**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/17

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**COMMENT FORM**Name: Richard Barrett

Address: \_\_\_\_\_

City: Odenton State: MD Zip: 21113Telephone: \_\_\_\_\_ Email: reb2003@alumni.psu.edu

Comments: In no way shape or form will I, or my neighbors allow this to transit through or near Odenton and Piney Orchard. The noise will be unacceptable; the effects of vibration / seismic activity are undetermined; the effects of high powered radiated electromagnetic energy emitted along the route are under-researched.

I am by no means opposed to mass transit, our nation needs more of it. BUT, it must be done intelligently and balance the needs of consumers and parties affected along right-of-way. It is much more desirable to route this along less populated highway routes such as I-95 or I-295. While geometry may slightly degrade optimal performance, the disruption of population centers will be minimal.

// SIGNED //  
Richard E Barrett Jr, Capt, USAF  
Richard E Barrett





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/17

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**COMMENT FORM**

Name: Margaret S. Michaels

Address: 802 Seneca Dr

City: Odenton State: MD Zip: 21113

Telephone: 410-672-6820 Email: ptas2704@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am absolutely apposed to this  
Maglev train. There is no direct benefit  
to our community or the states.

This would impact many lives  
in our community by devaluing our property



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DATE 10-16-17

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COMMENT FORM

Name: R. S. ROBERTS

Address: 493 KING MALCOLM AVE

City: ODENTON State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① WHO WILL PAY FOR THIS!

② WHAT ABOUT EMF?

③ HOW MUCH VIBRATION WILL THERE BE!

④ WHO IS GOING TO BE RESPONSIBLE FOR DAMAGES ~~AFTER~~ ~~THE~~ OF PROPERTY AFTER ITS UP AND RUNNING

⑤ WHO IS GOING TO RIDE IT? NOT ANY ONE THATS LIVES HERE (JUST TOURIST - MONEY, MONEY, + MONEY)

DATE 10/16/17

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## COMMENT FORM

Name:

Susan Portis

Address:

2494 Amber Orchard Ct E. # 201

City:

Odenton

State:

MD

Zip:

21113

Telephone:

Email:

SHPORTIS@yahoo.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: Since Gov. Hogan may be interested in HOV to be added to the BW Parkway (295), the BW route would be best for the MAGLEV. I have seen trains in other states running parallel to major roadways (even in the median strip) + it was the least disruptive (during construction + when in operation). I am concerned about noise pollution, so running the Maglev parallel to an existing roadway makes sense + would be less disruptive as far as noise is concerned. Tunnelling under homes is unacceptable. I wonder how a train wreck would be handled - above + below ground. I wonder how the train would affect local health of people + wildlife. I really don't want this train.





DATE 10-16-17

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**COMMENT FORM**

Name: Judy Roberts

Address: 493 King Malcolm AVE

City: Odenton State: MD. Zip: 21113

Telephone: 410-674-2682 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

① Who will pay for the MAGLEV ?

② This train will do nothing for the residents of Odenton.

③ Where are they going to vent the tunnels ?

④ opposed to it in general







BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/2017

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Pauline Sands

Address: 8391 WB & A Rd

City: Severn State: MD Zip: 21144

Telephone: Email: pauline.sands@hotmail.com

Comments: Public (note) notified 2016 as stated on the video.

NEPA - 5 steps

significant co-ordinations,

(Step 1) 30 day comment period.

step 5-

Step A & D Final decision

NO GRAIN

(Big) discrepancy here; not notified till late 2017



DATE 10/16/2017

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COMMENT FORM

Name: Pauline Sands

Address: 8391 WB & A Rd

City: Severna State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: ~~Pauline.Sands@scmaglev.com~~  
Pauline.Sands@hotmail.com

Comments: \_\_\_\_\_

Video - safety & soundness of the  
cut most importance.

fires, explosions, weather events, terrorists.  
electro magnetic fields  
safety & construction.

First importance is the  
negative effects and  
~~and~~ a NO Build option  
is the decision to be made  
NO TRAINS



DATE 10/16/2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Pauline Sands  
 Address: 8391 WB & Road  
 City: Sewer State: MD Zip: 20144  
 Telephone: 443 623 4088 Email: Pauline.Sands@hotmail.com

Comments: tunnel is 80-100 ft so stated.

This is not safe. Many homes in Sewer  
are on sandy soil.

We will stand strong, stand together,  
we will fight against maglev with  
all our might.

End this monstrous project and  
give us our lives back, Raise our  
children. Many of us are seniors,  
we worked hard all our lives for  
our homes. This project is insane.

We can widen roads, get companies in  
Port Covington without a train.

Citizens knew nothing of this tree  
a few weeks ago. Maryland's secret  
kept for 2 years, now we get notified.  
millions of dollars on a train jets  
Seniors, let's suffer. Spend the money  
people Not Trains.





DATE 16 OCT 2017

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**COMMENT FORM**

Name: MAURER

Address: 507 BRENTWOOD AVE

City: SEVERNA PARK State: MD Zip: 21146

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: LOVED EVENT - ~~NO~~ PLEASE SCHEDULE MORE

DON'T SEE VIABILITY, PROFITABILITY, OR DEMAND FROM POTENTIAL  
RIDERS.

STAUNCHLY OPPOSED TO PROJECT AS A WHOLE. DISHEARTENED BY  
LOSS OF PROTECTED WETLANDS, AND POTENTIAL LOSS OF PROPERTY  
OWNED IN PINEY ORCHARD.



DATE 10/16/17

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COMMENT FORM

Name: NANCY C. BOSKEY

Address: 506 PRINCE CHARLES AVE.

City: ODENTON State: MD Zip: 21113

Telephone: 410-672-0331 Email: OBRIEN BOSKEY @ YAHOO.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am 71 yrs. I have lived here for 36 yrs. Where would I go at this period I am disabled and would have no place to go. This area 36 yrs. ago was a small town with wonderful people to live around. A place to let your children play & go to school. Please don't take this away from us seniors & are grand kids.

Nancy Boskey



DATE \_\_\_\_\_

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**COMMENT FORM**

Name: Patricia Wooten

Address: 2174 Commissary Circle

City: Odenton State: MD Zip: 21113

Telephone: (410)674-5851 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I feel that this project is largely unnecessary. Isn't the Acela enough?! And what about the airlines - other than Southwest, the others have been charging / increasing fees since Sept 2001 (they claim 9/11 lost them a lot of money, which should have been made up by this point). Enough business travel on this Maglev train, and we could see some airlines cease to exist. Since it won't be stopping in Odenton, we commuters won't benefit - still standing on the MARC platform. Everyone can't go to BWI airport station - this isn't feasible, and neither is this project.



DATE \_\_\_\_\_

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**COMMENT FORM**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Do Not Build at All.....

NO one Needs this  
CALL a UBER 😊





DATE 10-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: PAUL BEDJARIK

Address: 564 RITA DRIVE

City: ODENTON State: MD Zip: 21113

Telephone: (301) 325-7400 Email: PAULIE4FINGERS@YAHOO.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1) WHY IS THIS TRAIN REQUIRED WHEN IT GOES THE SAME PLACES AS THE MARC AND ACELA.

2) IF YOU ARE IN BALTIMORE, WHICH IS WHAT IS BEING "SOLD" IN OUR AREA, HOW DOES THAT GET PEOPLE TO NEW YORK (IN THE FUTURE) WHEN THE EXPRESS LINES ONLY STOP IN WASHINGTON, PHILADELPHIA AND NEW YORK.

3)



DATE 10/16/2017

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**COMMENT FORM**

Name: Bart Hewitt

Address: 1108 Tolman Lane

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: MUSICAL@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Number one: Don't Displace The Odenton Fire Department with The line. The Map look like you go right Through it. That is a really bad idea.

IF you really must Build This monstrosity, go up The Parkway underground all The way. Stay Away from As many homes as possible.

Number two: I don't want Any of my taxes going Anywhere near This. It won't benefit me at all from what I read & hear. IF Acela only takes 5 minutes more from DC to Baltimore, what good is saving 5 minutes for The Cost of This?

DATE 10/16/2017

baltimorewashingtongmaglevproject.com

## COMMENT FORM

Name: RITA WATSONAddress: 1301 GILL STCity: ODENTON State: MD Zip: 21113Telephone: 410 674 4955 Email: NO EMAILComments: ODENTON DOES NOT NEED THIS TRAIN.ODENTON HAS TRAIN SERVICE TO DC.THE ONLY PEOPLE THE TRAIN IS GOING TO BENEFIT IS PEOPLE IN DC + BALTIMORE.I AM STRONGLY OPPOSED TO THIS TRAIN GOING THRU ODENTON OR SEVERN OR GAMBRIEL'S. THIS TRAIN WOULD DISRESPECT THOUSANDS OF RESIDENTS.THE STATE OF MD DOES NOT NEED A FAST TRAIN. IF PEOPLE NEED TO TRAVEL IN LESS TIME THEY CAN USE THE PLANES.THIS PROJECT WAS POORLY ADVERTISED. THE FIRST I HEARD WAS A POST CARD + LAST WEEK THE SENATOR CALLED INFORMING ME OF THIS MEETING TONIGHT. I'VE LIVED IN THIS AREA 49 YRS. + I AM RETIRED + MY HOUSE IS PAID FOR. I DO NOT HAVE THE RESOURCES TO PAY A MORTGAGE. THIS TRAIN WOULD DISPLACE THOUSANDS OF PEOPLE.





DATE 8-16-17

bwmaglev.info

**COMMENT FORM**

Name: LINDA SCRIBA

Address: 1121 Thompson Ave

City: Severn State: md. Zip: 21144

Telephone: 410-551-7037 Email: catbabe8@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I'm opposed thoroughly to any  
route of the SC maglev. I think  
it's ridiculous. No one will be able to  
afford to ride it unless they were very  
wealthy. Maryland should be concentrating  
on the traffic jams at the bay bridge that  
are a ridiculous waste of gas & time.  
Stop wasting our money!!!





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/10/17

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**COMMENT FORM**

Name: Eileen Frazier

Address: 8116 Quarterfield Road

City: Severn State: Maryland Zip: 21144

Telephone: 410-799-5128 Email: eileenfrazier@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① If my house is where the tunnel is, does it affect my house?

(No in favor)



DATE 10/16/2017

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**COMMENT FORM**

Name: Scott Zenker  
 Address: 8334 Dubbs Dr  
 City: Severn State: MD Zip: 21144  
 Telephone: \_\_\_\_\_ Email: Scott221144@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Love Technology! Love Advancement! Dont like putting people out of their homes. Why not use current road ways/Rail systems > might cost more but you <sup>wont</sup> disrupt peoples lives?! Also, do we really need this system? I can see having run up/down the East coast to Florida this small trip from DC to Baltimore - How about Baltimore to Orlando or Baltimore to NYC? How much will it cost me to go from BWI to DC? Its going to be expensive!! I dont want my taxes to go to it! Please dont ~~kick~~ <sup>KICK</sup> people out of their homes!



DATE 10/16/17

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COMMENT FORM

Name: Michael Solly

Address: 2312 Snowflake Dr.

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: trainman52@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: If this proposal reaches a workable level of support & funding, I would like to suggest that the wide and open median of the BW Parkway would be by far the optimal choice for the MAGLEV alignment, at least a good portion of it. The NPS has spent ~~no~~ time or money maintaining the highway and thus should have little or no authority to decline use of the median. In fact management of 29.5 is long overdue to be transferred to another agency.

If the state can obtain this right-of-way, it will very significantly reduce the cost, environmental impact, and local disruption along a large portion of the proposed alignment.



DATE 10/16/17

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**COMMENT FORM**

Name: Kathleen Mittner

Address: 7788 Gabriel Garth Ct

City: Severna State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: ~~xxx~~ katecalben5@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This is not a cost effective solution to the transportation  
problem. Add to this displacement of home owners, this  
is a poor choice. This also doesn't benefit local  
citizens. It will only increase congestion on local roads  
near the stations.





DATE 10/16/2017

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**COMMENT FORM**

Name: Chris Bernis

Address: 2506 Country Oak Ct

City: Odenton State: MD Zip: 21113

Telephone: (410) 695-2744 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We are not in support of the Maglev Project.  
Our concerns align to possible decline in residential home values,  
increase in noise pollution, long term upkeep costs, with the  
indirect benefit to commuters outside our area, we are faced  
with being impacted with no benefit to those of us in the area.  
I would rather see the funding for this project be redirected  
towards our schools in the area.

Chris



DATE 10-16-2017

bwmaglev.info

**COMMENT FORM**

Name: Shirley R. Gaither

Address: 8224 River Park Rd

City: Bowie State: MD Zip: 20715

Telephone: 301 860 0026 Email: Srgaither2@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: There is no benefit for Maryland in this project. I have  
attended several of these meetings and the information has been given in small  
portions. Results overall - STOP This Project there is no benefit for  
Maryland in it. It is constantly stated that this project will reduce traffic on  
the roads. How when those in Maryland still will have to drive to Washington Dc  
or BWI airport?



DATE 10/16/2017

bwmaglev.info

**COMMENT FORM**

Name: Michelle Edwards  
 Address: 609 Yearling Ct  
 City: Severn State: MD Zip: 21144  
 Telephone: 443 688 6096 Email: mszrom14@yahoo.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: Although the alignments now minimize  
destruction of homes, communities and businesses, I  
still strongly oppose the MAGLEV. There will still  
be significant impact to our communities- I  
can't imagine anyone wants a train running under  
their home. How would that impact property values?  
The cost of the MAGLEV is not realistic. It  
will be incredibly expensive and I don't believe  
a large enough client base exists to support the  
train. Will taxpayers be responsible to foot the  
bill? Please don't continue with this project.  
It's too expensive and has little benefit.  
We already have a train - update Acela track  
instead!





DATE Oct 16, 2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Marisa Gower

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This will compete w/ our MARC train line  
\*Amtrak. We do not need to go to N.Y in 1hr  
This will put our train system out-of-competition  
The BWI airport is already congested + there is  
no parking for a train station.

Environmentally how bad will this be for  
our Patapsco State Park + Patapent Fesl +  
Wildlife Refuge. With our current political situation  
there's no way to evaluate an impact on the  
environment



DATE 10/16/17

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**COMMENT FORM**

Name: Amy Tardiff  
 Address: 126 Langdon Farm Circle  
 City: Odenton State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My biggest concern is that this project is not necessary - very few people are traveling from DC ↔ Baltimore. I also am interested to hear how our power grid can handle a project like this. Many in the Odenton area where this is being pitched are on well water - how will underground trains affect the aquifer?  
How will the cost of this project be paid for?  
These are temporary jobs that will create a train that will displace the communities of hardworking people already settled here.



DATE 10/16/17

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**COMMENT FORM**

Name: Jean M. Lloyd  
 Address: 1177 Delmont Rd  
 City: Severna State: md Zip: 21147  
 Telephone: 410-241-0250 Email: momracer701@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Maps are incomplete doesn't have my street on it! really in this day ~~and age~~ maps should be correct if you are presenting proposed routes.



DATE 10/16/17

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**COMMENT FORM**

Name: Ruth H. Walters

Address: 2258 Misthaven Lane

City: Gambrills State: MD Zip: 21054

Telephone: 410-695-1959 Email: drummerboy@broadstripe.net

Comments: If we have to have a Maglev, please, please build it along 295 - SO many problems would be eliminated !!!

Thank you





DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Stephanie Baker

Address: 524 JoAnn Dr

City: Odenton State: Md Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do not like this project Taking peoples  
homes & businesses. is a horrible thing  
People have lived in this community for year  
and for govt to just come in & throw them  
out of a home. Buying at market value yeah fine  
but that's not taking in to account the fact  
they have to relocate. No one can state  
for a fact that this will profit anyone except  
the builders How can the Federal govt justify  
Spending >28mil for research? Schools, Churches  
businesses, human beings are going to be  
greatly affected in a negative way



DATE 10/16/17

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**COMMENT FORM**

Name: Benjamin Dubit

Address: 7801 Locust Wood Rd

City: Severn State: MD Zip: 21144

Telephone: 301-412-9478 Email: benjamin.dubit@gmail.com

Join our mailing list?  Yes  No  
benjamin.dubit@gmail.com

Preferred method of communication?  Email  Mail

Comments: 1. Tunneling; what about homes that are  
on well water? the tunnel is 120 feet under  
ground? Our well is 300-400 deep....  
The entire Elmhurst community is on well water  
Severn MD - Sheet 5.



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

NO

DATE 10-16-17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Michael Nichols

Address: 1259 Delmont Rd.

City: SEVERN State: MD Zip: 21144

Telephone: 410-551-4106 Email: mike.nichols@Regal6ebit.com

Comments: this is not a good system for this area & is only going to be used by a small portion of the people in the area. These people already have the train for this purpose. The homes in the area will be destroyed because the property value will never be what it was before. Also, the area will become a slum.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/2017

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COMMENT FORM

Name: Patricia Mercer

Address: 814 THICKET COURT

City: OBENTON State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: Redcar58@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: a brief technical break down of what a feasibility study is & that it is step 1. Then another technical breakdown of how it (train) is constructed & that ~~there is~~ your fears can be quelled by watching construction video. But again that <sup>all of this</sup> is way out there <sup>infuture</sup> so something brief would help. fees etc. should be discussed but with the understanding that we are no where ready to discuss because construction contracts, etc have not been developed yet.



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Louise Keister  
 Address: 875 Frost Valley Lane  
 City: CATMILLS State: MD Zip: 21054  
 Telephone: (410)672-5217 Email: lkeister@msn.com

Comments: + thank you for holding this Open House.

My main concern @ this point is the environmental impact especially the electromagnetic field. MD has a very high cancer rate & ionization radiation causes leukemia + lymphoma. So want to make sure this is part of your study.

Thanks for removing the "Yellow Route" from consideration.



DATE 6/02/17

baltimorewashingtonscmaglevproject.com

COMMENT FORM

Name: B. J. Lester

Address: 575 Forest Valley Lane

City: Somerville State: MD Zip: 21054

Telephone: 410 812-2503 Email: BLESTER@MGM

2 CON

Comments: \_\_\_\_\_

I'm against this train  
coming through our County, AA.

Only Doctors, Lawyer + CEO  
will be able to afford the  
trip charges.

I don't see where that this  
is needed

Thanks  
for your attention



DATE 10-16-17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Judith M. Beall

Address: 872 Frost Valley hwy

City: Gambriells State: Md. Zip: 21054

Telephone: 301-717-0155 Email: lablady111@comcast.net

Comments: Still not too sure about it.

How Far would one of these Trains go if it  
went off the Track or hinc going over  
300 miles per hr<sup>3</sup> ~~hr~~ Need to know  
alot more infor - Not For it close to my  
house.





DATE 10/16/2017

baltimorewashingtontcmaglevproject.com

**COMMENT FORM**

Name: Robert A. Smith

Address: 503 Trotters Ridge Way

City: Severna State: MD Zip: 21144

Telephone: 443-688-6137 Email: ras067@comcast.net

Comments: Strongly object to the proposed E-1 routing  
through Severna, MD. Recommend <sup>primary</sup> consideration  
be given to the current J, J1 proposal. Also  
recommend the MD State Legislature form a Task  
Force as it did in 2003 to evaluate the development  
& construction of the MAGLEV





DATE 10/16/17

baltimorewashingtontsmaglevproject.com

**COMMENT FORM**

Name: Shari Jones

Address: 481 King Malcolm

City: Denton State: MD Zip: 21112

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: Why not let the people ~~write~~  
vote for this. It will effect  
our homes, neighborhood, taxes,  
~~deliberate~~. Why do we need  
this! It's a waste of money



DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: \_\_\_\_\_

Address: 504 Domain Ct

City: Odenton State: MD Zip: 21113

Telephone: 443-631-1831 Email: Jarsisromirez@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I will like to know how this train will benefit  
residents of Odenton that work in DC. Do residents of  
the area will have discounted tickets? Do the traffic will  
be less? Will we hear the train?



DATE \_\_\_\_\_

bwmaglev.info

COMMENT FORM

Name:

Gloria Torres

Address:

720 Chapelview Tr

City:

Odenton

State:

MD

Zip:

21113

Telephone:

410-900-4447

Email:

gtorresdrama16@gmail.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Comments:

Please stop this train. If I have to sell it will be a big hardship for me. I'm a Senior Citizen where I've decided to give my years left in my house. I have no where to go. My friends are all here.

Stop the Train please!!!



DATE 10-16-17

bwmaglev.info

**COMMENT FORM**

Name: KEN GLENDENNING

Address: 602 FAIRMOUNT RD

City: LINTHICUM State: MD Zip: 21090

Telephone: 410-859-8459 Email: KENGLENG02@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This is a system that we do NOT need. It will never  
pay for itself. The idea to destroy residential communities  
is the most ridiculous thing I have ever heard. Whether  
you build above or below ground you should not  
go through neighborhoods. When you go 80-150 deep what  
happen to residents who are on well water? Who determines  
where the vent tubes will be placed? Will they be on  
private property? What will happen to property values  
for those with the tunnel under their homes?  
Summation put the line on the BW Parkway (295).





DATE 10-16-2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Nancy Cool

Address: 1305 Jade Court

City: Odenton, ~~MD~~ State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: Do NOT want This Train. A total waste  
of money. Too disruptive to the Odenton area.  
Property values will most likely go down.  
Money could be better spent to better the  
quality of lives for so many in this state.





DATE 10-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Sandy Walker

Address: 513 Bruce

City: Odenton State: MD Zip: 21113

Telephone: 410-674- Email: M Walker - 574@msu.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  E-Mail

Comments: I have lived in Odenton all my life.  
This train will not do anything for  
our local traffic. The train is  
not going to help me in my area  
the train is not good for  
environment. Will not provide  
jobs.



DATE 12-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Maria T. Samonisky  
 Address: 7901 Tressel Ct  
 City: Severn State: md Zip: 21144  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The train will be going right under my house. Delmont St neighborhood has spring all over. What is going to happen with my house? Who is going to pay for us to move if your company decides that my house is on the way of this project?

I do not want this train in my neighborhood







DATE 10/17/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Witter

Address: \_\_\_\_\_

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: There were no charts showing cost! When asked, staff could only say 27 million is being spent on planning with no idea where the building money will come from. Residents have the right to know how this will get funded -- private, taxes... What? This money could have been spent on more worthy projects like drug addiction, better schools.

Will more federal funds go toward this. If so, a waste of taxpayers money

DATE 10/14/17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Jennifer MacQuillan  
 Address: 525 Jo Ann Drive  
 City: Odenton State: MD Zip: 21113  
 Telephone: 443-691-9319 Email: \_\_\_\_\_

Comments: At this point I'm <sup>an</sup> not totally exposed nor for this Project. This meeting today is good for awareness yet nothing has been finalized - so good public input. It would be tragic to uproot businesses + homes & only be offered fair market value. ~~State~~ Needless to say, this would also take away business from those already established here with less homes, etc.

Would the \$ spent already on the GTS be put to better use widening roads - it would still bring in jobs + add'l revenue.

Will anyone for this Project be affected like those of us who live here?



DATE 10/10/17

bwmaglev.info

**COMMENT FORM**

Name: Susan Liden  
 Address: 7913 Delmont Station Rd. (200 ft. from one route)  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-519-1985 Email: slliden@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Not Needed!

It takes my husband 30 mins. to ride train from  
BWI with 5 stops to DC. An express train would take  
20 mins. Baltimore -> DC. This is overkill! Why is  
this being considered?

Susan Liden





DATE 10/14/17

bwmaglev.info

**COMMENT FORM**

Name: ANGELA ZUBROD

Address: 523 QUEEN ANNE AVE.

City: ODENTON State: MD Zip: 21113

Telephone: 410-707-8359 Email: ZDEVILISH@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I totally DISAGREE with the fact that  
we as residents were NOT informed of this  
PRIOR to the start of plans. There are new  
houses going up & being sold & those residents  
have no idea this is happening.

We are not going to benefit from this due to  
cost & location. There isn't a "LOCAL"  
station.

GIVE the residents a VOTE!

Angela Zubrod



DATE \_\_\_\_\_

bwmaglev.info

**COMMENT FORM**

Name: Jackie Streeks

Address: 4403 Oakview Lane

City: Bowie State: MD Zip: 20715

Telephone: 3014648036 Email: jackiestreeks1944@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I don't see any real need for this train. Ridership will be very low, who would drive from Bowie, Odenton, etc to Wash DC or Baltimore to take a train to the opposite end? NO ONE only < 1% of the commuting traffic in the area travel from Wash to Balti and vice versa.

DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Michael GormanAddress: 494 Rita Dr.City: Odenton State: MD Zip: 21113Telephone: 410-551-1108 Email: \_\_\_\_\_Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: It was explained to me that this project would be for no more than 10% commuter ridership. Some 60% or more would be business + airport travel. ~~It is~~ Competitive costs of approx. \$80-\$100 for Balt. to DC (in today's dollars) vis-à-vis Acela seem foolish to me to serve airports. For one thing, if only serves one airport. The train gets nowhere near Reagan or Dulles. So the "benefit" is for wealthy or business travelers to get from Balt. to BWI in 10 minutes rather than 20, and to get to DC in 15 mins rather than 60. So the time savings to BWI is negligible and to DC airports hardly noticeable.

So... what's the point? (a part from all other issues)



DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Kenneth + Deborah Thomas

Address: 410 Forest View Rd

City: Linthicum State: MD Zip: 21090

Telephone: 410 694 0542 Email: buskdak@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concerns & Questions:

- 1) Impact of deep boring on the physical environment. Fracking has been widely opposed, how is this safer?
- 2) ~~For~~ Will dogs be extremely adversely affected by vibration during construction + when trains pass?
- 3) Will the <sup>sound</sup> vibrations adversely affect people's health regardless of whether they're perceptible to the human ear?
- 4) What would precisely be the depth of the track below Linthicum Elementary School?
- 5) If toxic explosion occurred via terrorism on the train, what would be the environmental impact & protocol?
- 6) WHO IS SUPPORTING THIS?? State PRECISELY what their income levels are, who they work for, where they live, what they expect to gain, how they'll truly benefit.



DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Alexander Michael DicksonAddress: 12208 Roundtree LaneCity: Bowie State: Maryland Zip: 20715Telephone: 240-205-1054 Email: Alex.dickson87@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I have looked at all of the displays at the open house today and there is information that has not been disclose in this study. The study has failed to disclose the Electro-magnetic radiation energy that will be needed for the train to work. I am very concern that houses near the train will be exposed to ~~the~~ large amounts of EMFs. I have not seen a single study on this subject. I think the public should know what the effects are on our health. Also, the three remaining routes are going to have 2/3 of the route underground. We need to know how this could effect our water supply. The citizens that live near the route that depend on well water will have their supply of water impacted the ~~the~~ routes. Overall, these questions/problems need to be address moving forward and need to be measured before making a final



DATE 10/16/17

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**COMMENT FORM**

Name: BARBARA HOLT  
 Address: 8352 DUBBS DR  
 City: SEVERN State: MD Zip: 21144  
 Telephone: \_\_\_\_\_ Email: bholt@mindspring.com

Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: ① Way too much money being spent on feasibility studies — Improve THE train system already in place.  
 ② This meeting yielded no new info — looking @ maps of something with too many options @ THIS POINT.  
 ③ No enlightening info!



DATE 10/28

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**COMMENT FORM**

Name: Chris Wagner  
 Address: 832 Maple Rd  
 City: Gambrills State: MD Zip: 21054  
 Telephone: \_\_\_\_\_ Email: chwagsmd@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① This project will not cost 10-15 Billion  
 ② I understand that most of the money for the project will come from gov't loans. How much capital will the private equity have at risk?  
 ③ I understand that private funding is being used to fund this study - what's to ensure that the study is performed in a non-biased manner?  
 ④ Environmentally - what will be the impact?





DATE 10/10/17

bwmaglev.info

**COMMENT FORM**

Name: Michelle Jackson  
 Address: 13107 Jordans Endeavor Drive  
 City: Bowie State: MD Zip: 20720  
 Telephone: 301-262-7893 Email: Kmjackson5@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: How will the \$10-15 Billion be funded?  
No one seems able to answer this. When will  
that question be answered? What entity will  
be charged with conducting the ridership  
study. If it is a private entity, what is  
the name of the company?

I am a MD Resident, living in the unincorporated  
portion of Bowie. Our schools are over-  
crowded, and the facilities are deplorable,  
and you want to build a \$10-15 B train - Really!?





DATE 10/16/17

bwmaglev.info

COMMENT FORM

Name: Paige Klingler  
 Address: 737 Seneca Drive Annapolis MD  
 City: State: Zip: 21113  
 Telephone: 3107099460 Email:

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I think it is silly.  
 I dont like that you want  
 to wreck down our house.  
 I dont like that idea.

From: Paige Klingler  
 age 6



DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Greg Klingler

Address: 737 Seneca Drive

City: Odenton State: MD Zip: 21113

Telephone: 818-481-3899 Email: greg.klingler@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This process was flawed from the being. The scoping process was subversive. I have no idea why this company is pushing this project. It will not help the community. It will not help the displaced home owners. It will not help the tax payers who will clearly be subsidizing this project that is clearly destined for financial ruin.

~~The only~~ It will not help the the environment.

The only Party this will benefit the construction company.

I don't see a reason to harm everybody to line someone's pockets!!!

This project is a terrible idea!!!

Regards,  
Greg Klingler *[Signature]*



DATE 10/16/2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Michael Bahr

Address: 7735 Leaside Ct

City: Honover, State: MD Zip: 21076

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: I am very concerned about people's lives and  
homes that will be destroyed/impacted by the proposed  
paths. For me I am worried about the vibration  
(I am ~ 3000 ft from proposed path underground). How  
will this effect my home and others in the area.  
We ~~are~~ already have AMTRAC & MARC for travel to  
BWI & Baltimore. This train is not worth it in  
this area.

MB



DATE 10-16-14

*bwmaglev.info*

**COMMENT FORM**

Name: MARIANA MEYER

Address: 2610 WILLOW LEAF COURT

City: ODDENTON State: MD Zip: 21113

Telephone: 410-404-4466 Email: MEYERMA@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: WHO PAYS AND AT WHAT COST  
TO THE SURROUNDING POPULATION?!!

TOTALLY AGAINST THIS  
PROJECT AND I WILL  
DO WHATEVER I CAN TO  
FIGHT IT!!





DATE 10/16/2017

*bwmaglev.info*

**COMMENT FORM**

Name: Leslie Ann NATARO

Address: 1338 Chapelview Drive

City: Odenton State: MD Zip: 21113

Telephone: 301-906-2886 Email: leslieann427@msn.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

① What is the plan if the helium shortage continues or even worsens in the coming years? Appears most helium needs to be imported.

② Who is responsible for maintaining the area under and around the maglev tracks?

③ What is the size of the vents for the underground section and distance between the vents?

DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Christine Gibson  
 Address: 1243 SCOTTS Manor CR.  
 City: Odenton State: MD Zip: 21113  
 Telephone: 443-956-1831 Email: Christine-gibson09@comcast.net  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: Please stop the Alternative E1  
proposed tunnel transition Portal.  
It will impact the Piney Orchard  
and Odenton Community immensely.  
A lot of people will lose their brand  
new homes and cannot afford to be  
relocated.  
We cannot afford to lose the Odenton  
Fire hall.  
It is a volunteer only station and  
the residents of the community  
cannot afford to rebuild.  
Please stop this unnecessary  
expensive train!!!!



DATE 10/16/17

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**COMMENT FORM**

Name: A. LEE

Address: 1245 QUEEN ANNE AVE

City: ODENTON State: MD Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: alee@howardcc.edu

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: • IMPACT ON PATUXENT WILDLIFE REFUGE

THE REFUGE IS A SANCTUARY FOR MANY SPECIES

OF WILDLIFE AND A RESEARCH FACILITY - THE ONLY

ONE THAT IS BOTH REFUGE & RESEARCH.

• WHAT BENEFIT FOR LOCAL COMMUTERS - NOT COST EFFECTIVE

• LONG TERM EFFECT OF MAGNETIC FIELD & GASES. UNKNOWN.

• TRAIN WILL DISPLACE NEIGHBORHOODS - LOSS OF

REVENUE, HOMES - EMINENT DOMAIN - PAY VALUE

LESS THAN HOME IS WORTH. AT TRUE VALUE.

• UNKNOWN EFFECTS OF MAGNETIC FIELD ON

AIR, WATER

• WE DO NOT NEED THE MAGLEV - WE NEED TO

BUILD OUR INFRASTRUCTURE WITH THE FUNDS

THAT WOULD BE USED FOR THE MAGLEV.





DATE 10/16/17

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**COMMENT FORM**

Name: Catherine R. Lash

Address: 8332 Dubles Dr.

City: Severn State: md Zip: 21144

Telephone: \_\_\_\_\_ Email: CRLASH@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Need to inform communities of proposals & up to date information of what is happening. Need a website to go to for all up to date information & new maps of Routes. More news outlets & Advertising





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/17

baltimorewashingtontcmaglevproject.com

COMMENT FORM

Name: Tom Shiffle

Address: 114 Edgerton Ave

City: Severna Park MD State: \_\_\_\_\_ Zip: 21144

Telephone: \_\_\_\_\_ Email: tshiffle@stagnet.org

Comments: Alternative E1 Tunnels - will have to  
run sound barrier with trains, IF this  
run goes thru my back yard can  
I ride for free, also will  
this drop my house value.

DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Janet DurigAddress: 2502 Amber Orchard Ct W #102City: Odenton State: MD Zip: 21113Telephone: 202 413 2722 Email: director@chpc.usJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I go into DC everyday - driving - I see no need to disrupt properties with a high speed train. At Creyone I talked with tonight wearing a "Staff" badge answered all my questions with something similar to "we won't know that answer until we get further down the road" —  
Well, Now would be the time to explain what happens to people's homes if they own one <sup>above</sup> when the tunnel would go — OR — what happens to the homes near the above ground railway?

Please do not do this.

Janet Durig  
Creative Director CHPC



DATE 10/16/17

[baltimorewashingtonscmaglevproject.com](http://baltimorewashingtonscmaglevproject.com)

**COMMENT FORM**

Name: Suzanne Boisseau

Address: 801 Blackcherry Way

City: Odenton, MD 21113 State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: I am against this project as financially irresponsible  
and negatively impactful to the residents that it  
affects most directly with decreased property values.  
I am furious with previous attempts to conduct the  
required meetings with inadequate notification to the  
residents that it affects.





DATE 16 Sept 17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: MARIA AVELLANEO

Address: 1326 AUA RD

City: SEVERN State: MD Zip: 21142

Telephone: 908-240-7792 Email: MARIAISMAGENT@AOL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I DO NOT BELIEVE COMMUNICATIONS IS  
EFFECTIVE. ABOUT TWO WEEKS SHOW A MAP  
THAT MY LAWSON THE ROUTE. IT IS IMPERATIVE  
THAT THOSE AFFECTED BY EMINENT DOMAIN.  
BE INFORMED AT THE SOONEST CONVENIENCE.  
I DO NOT SEE HOW ~~OUR~~ <sup>OUR</sup> NEIGHBORHOODS WILL  
BENEFIT IN ANY WAY. NO STOPS WITH FREE PARKING  
I SHOULD GUESS THE EXPENSIVE TICKETS WOULD  
AFFECT MORE WHO WOULD USE. THEREFORE  
TRAFFIC WILL BE AFFECTED ONLY IN CROS  
PROXIMITY TO THE SORT, ONLY A FEW  
ARE WE GOING TO RECEIVE FEEDBACK OF  
OUR INPUTS.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/2017

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COMMENT FORM

Name: Eileen T. Frazier

Address: 1012 Samantha Lane, Apt. 301

City: Odenton State: MD Zip: 21113

Telephone: 410-874-7626 Email: \_\_\_\_\_

Comments: It is difficult to see any benefit to Anne Arundel County or the City of Odenton from the proposed MAGLEV projects. However, taxpayers will foot the bill. A better use of taxpayer dollars would probably be to improve, maintain and expand existing infrastructure, such as the MARC train service and infrastructure <sup>(and roads)</sup>. Therefore, <sup>STRONGLY</sup> oppose the project.



DATE \_\_\_\_\_

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Katherine Butcher

Address: 1114 Autumn Gold Drive

City: Gambrills State: MD Zip: 21054

Telephone: 410.812.4805 Email: \_\_\_\_\_

Comments: This project is a ridiculous waste! It will not support it self. It will disrupt hundreds of lives, this area is already well supported by the MARC train for commuters to DC and Baltimore and ~~Amtrak~~ Amtrak provides service up the North east corridor. Other Maglev trains throughout the globe have dismissed this technology. ~~an~~ We do not need to replicate their mistakes, we need to learn from them and not spend any more time and money on this project that is doomed to fail.

Katherine M Butcher



DATE 10-17-17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: MICHAEL KRELL

Address: 169 ELLEN AVE

City: SEVERN State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: MSKRELL@YAHOO.COM

Comments: THIS IS A COMPLETE WASTE OF MONEY WITH NO  
WORTH WHILE RETURNS. IT DOES NOT SERVE THE PEOPLE THAT  
IT WILL INCONVENIENCE. THIS WILL NOT HELP WITH CONGESTION  
PROBLEMS ON THE ROADWAYS, SO THE WHOLE PREMISE BEHIND THIS  
PROJECT IS ALIE. YOU SHOULD PUT THE TAXPAYER MONEY INTO  
IMPROVING THE INFRASTRUCTURE THAT WE ALREADY HAVE.



DATE 10/16/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**Name: Jamie BeachAddress: 8713 Spring Brook WayCity: Odenton State: MD Zip: 21113Telephone: 443-570-1752 Email: jebeach04@yahooComments: This is a complete waste of taxpayers money.

Fix our current crumbling infrastructure! I-97 is a pothole  
jungle. Bridges in Baltimore are crumbling! Acela is introducing  
more & new faster train NEXT year! This will not improve  
commuting in this area at all - there is no need for the

Maglev. we can't keep up our current roads & rails. Also were  
will this ever go? You will destroy communities for a train  
to nowhere. There is no way you will ever get the bus right  
in New Jersey & New York. It is too congested & expensive.

This type of project was shot down out near Las Vegas  
a few years ago - where there was just desert! How can you  
justify destroying homes & communities and waste taxpayers money!

NO to the Maglev!

Jamie Beach





DATE 16 OCT 2017

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**COMMENT FORM**

Name: KAREN HOLMES

Address: 730 EMERALD WAY

City: ODDENTON State: MD Zip: 21113

Telephone: 240 723 7390 Email: karenholmes2@cs.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: THE BW ALIGNMENT SHOULD GO UP THE CENTER  
MEDIAN. RUNNING THE TRAIN UP ONE SIDE OR ANOTHER  
WOULD ENTAIL THE LOSS OF TOO MANY TREES AND WILDLIFE  
HABITAT.



DATE OCT 16, 2017

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**COMMENT FORM**

Name: MARVIN J. ROBINSON

Address: 403 EAST MAPLE ROAD

City: LINTHICUM HEIGHTS State: MD Zip: 21090

Telephone: 410 684-3343 Email: robinsonmjr@msd.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I JUST WONDER, WHY DOES A COUNTRY LIKE AMERICA PAY SUCH LITTLE REGARD TO PUBLIC TRANSPORTATION VIA RAIL? AMTRAK EQUIPMENT IS OLD. AMTRAK TRAINS MUST YEALD TO FREIGHT TRAINS. INTRODUCE A RAIL SYSTEM LIKE YOU CAN FIND IN EUROPE AND OTHER DEVELOPED COUNTRIES AND I THINK YOU WOULD BE AMAZED BY THE RESPONSE, FUND AMTRAK - FORGET MAGLEV.

LIGHT RAIL IS A DISGRACE. THE PUBLIC WOULD LIKE TO KNOW,

1. WHAT PERCENTAGE OF OPERATING EXPENSE IS COVERED BY TICKETS SOLD?
2. HOW MANY PASSENGERS ARE CAUGHT WITHOUT A TICKET ON A MONTHLY BASIS?
3. WHEN A PASSENGER IS CAUGHT WITHOUT A TICKET WHAT IS THE PENALTY? SHOP LIFTING IS A CRIME, RIDING WITHOUT PAYING IS SPEAKING - NO DIFFERENT THAN THEFT!
4. WHY IS THE MDT POLICE CUBICLE AT THE LINTHICUM STATION LEFT EMPTY. A LOT OF PUBLIC NUISANCE GOES ON THERE.
5. NOW LIGHT RAIL HAS DIRECTLY LOWERED OWN HOME VALUES IN LINTHICUM AND KILLED ALMOST ALL COMMERCIAL SUCCESS AT THE CROWWELL FIELD SHOPPING CENTER WHY WOULD I TRUST MDT TO MAKE MY LIFE BETTER BY INTRODUCING MAGLEV? FREE RIDES FOR LIFE?

I WISH I BELIEVED THESE THOUGHTS WOULD BE READ, CONSIDERED OR EVEN



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10-16-17

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COMMENT FORM

Name: Marlene Lembach

Address: 504 Bruce Ave

City: Odonton State: Md Zip: 21113

Telephone: 4106747669 Email: \_\_\_\_\_

Comments: this area is not ready for this  
don't want to lose my home this  
maybe a rich area but our pay is  
no good. With this train going so fast not  
healthy for your brain. I'm oppsing this  
put money where needed



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Rolf Hill

Address: 1009 Ice Crystal Ct.

City: Odenton State: Md Zip: 21113

Telephone: 410-672-5836 Email: RolFP4@yahoo.com

Comments: Consideration to accomodate  
COMMUTERS should be part of the study.  
I'm NOT suggesting MORE stations. Rather parking  
and ease-of-access to daily commuters....  
especially those near BWI should be  
analyzed.





DATE 10/16/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Allen Shu  
 Address: 8370A WB & A Rd, SEVERN, MD 21144  
 City: SEVERN State: MD Zip: 21144  
 Telephone: 410-865-9393 Email: allen\_shu@yahoo.com

Comments: First = Who are your customers?  
 How much you going to charge?  
 for saving 15 mins one way, who not  
 invest in existing customer and improve MARC  
 Services instead of delay every day -

I am definitely/absolutely oppose to this ~~idea~~  
 ideas! Impact on existing residence, Currently,  
 Acera takes 30 mins for one way DC - to Balt.  
 It costs \$50 (approx), Who are the riders and  
 How many?

Put out to the voter and vote in  
 November! -



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Jennifer Schuman  
 Address: 959 CITRINE WAY  
 City: ODONTON State: MD Zip: \_\_\_\_\_  
 Telephone: 914-804-6214 Email: \_\_\_\_\_

Comments: \_\_\_\_\_

I Am totally AGAINST this  
 UNNECESSARY project. There is  
 a GREAT TAX payer expense, IN ORDER  
 to SAVE 10-15 minutes time to get  
 TO DC. "WHAT will be the SARS  
 if it was BUILT??"

We will Assume great financial  
 Hardship if BUILT, As the  
 construction will devalue our  
 property.

Against this project



DATE 10/18/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: JASON SCHMORN

Address: 959 CITRUS WAY

City: ODENTON State: MD Zip: \_\_\_\_\_

Telephone: 944-230-3073 Email: \_\_\_\_\_

443-230-3073

Comments: I reside in ODENTON and I'm  
~~TOTALLY~~ AGAINST THE PROJECT.

IT'S A TOTAL WASTE OF TAXPAYER  
MONEY, THERE ARE TRAINS THAT  
GO FROM BWT TO DC IN  
20 MINUTES. THE ENORMOUS  
EXPENSE, TIME, CONSTRUCTION, THE  
AFFLICTION OF THE POPULUS, AND  
DESTRUCTION OF THE ENVIRONMENT  
OVERRIDES THE PROJECT





DATE 10/16/17

*bwmaglev.info*

**COMMENT FORM**

Name: Sam & Katina Zee

Address: 763 Seneca Drive

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: krzee64@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do not want the MAGLEV to come through the Odenton corridor. The overall project will be too disruptive to our community. Our <sup>property</sup> house values have been lowered due to this effort. People have paid too much to build their lives in this area for this disruption. We were not notified in time and we don't feel we will be given proper notification if the routes change. This effort could cause numerous problems for this community to include traffic congestions, lowered property values, even health problems. We vote NO!





DATE 10-16-17

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**COMMENT FORM**

Name: Karen Wootton

Address: 8519 Summershade Dr

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: kwoots@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I'm worried abt the cost to tax payers to create something only the wealthy would use. What would the ticket price be?

Will the environmental impact be within the guidelines of the current EPA/ this administration which are ~~to~~ not as strict as the previous administration?



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: JOSEPH S. LOVE

Address: 8720 BRIGHT MEADOW CT

City: ODENTON State: MD Zip: 21113

Telephone: 443 758 3762 Email: \_\_\_\_\_

Comments: IT IS MY DIRECT CONCERN THAT THIS PROJECT WOULD EFFECT AND  
INCREASE WHAT HAS ALREADY BECOME AN OVERLY CONGESTED TRAFFIC  
SITUATION. A STRONG CONCERN OF PLUMMETING PROPERTY VALUES IS  
AN EVEN STRONGER CONCERN. IT WOULD BE MY VOTE TO VOTE AGAINST  
THIS PROPOSAL.



DATE 10/16/17

baltimorewashingtontcmaglevproject.com

**COMMENT FORM**

Name: Jamie Stiffler

Address: 118 Ederton Ave

City: Severna State: Maryland Zip: 21144

Telephone: 410 519 4524 Email: Jamiesstiffler90@gmail.com

Comments: I think it is stupid, shitty and crap  
who needs a freaking maglev train going through  
a fucking neighborhood and wildlife and other things like  
that. you can drive take the Amtrak for all I  
care. I am pissed off about it nobody needs this  
shit. I am Jamie I am against it / maglev.

fuck you



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: STEVEN NEWCOMER

Address: 1098 COLONY RIDGE RD

City: ODENTON State: MD Zip: 21113

Telephone: 410-672-2970 Email: \_\_\_\_\_

Comments: THIS IS GOING TO BE MASSIVELY  
DESTRUCTIVE TO EVERYONE LIVING ALONG THE  
PROPOSED ROUTES.

THIS DOES NOT HELP THE AVERAGE CITIZEN  
WITH THEIR COMMUTE TO WORK.

IT ALSO IS GETTING U.S. TAXPAYER MONEY  
TO HELP FINANCE THE CONSTRUCTION AND OPERATION.

IT APPEARS TO BE A SYSTEM THAT WILL  
BENEFIT ONLY AN ELITE POPULATION THAT DOESN'T  
WANT TO FACE TRAFFIC OR CROWDED AIRPORTS.

I WILL NOT VOTE FOR ANY ELECTED OFFICIAL  
WHO IS IN SUPPORT OF THIS PROJECT!!





DATE 11/16/17

bwmaglev.info

COMMENT FORM

Name: Bob Felton  
 Address: 1525 Wintfields Lane  
 City: GAITHERSBURG State: MD Zip: 21054  
 Telephone: 410-654-4093 Email: Bobafms@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

What will be the daily ridership of the MAGLEV, will the users come from Odenton, GAITHERSBURG or Piney Orchard OR will it come from outside our area. Are we expected to pay the price for these improvements w/o any of the benefit. Additionally, when the economic benefits are estimated the properties w/in 1/2 mile of the line need to be taken into account as a reduction in value.



DATE 12-16-17

*bwmaglev.info*

**COMMENT FORM**

Name: Michele Claycomb

Address: 8160 Old Quartersfield Road

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My concerns include what happens to people wells the tunnels underground etc they gonna run under the structures or are the buildings gonna be torn down. What about noise and vibrations. Please consider these concerns.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_



DATE 10-16-17

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**COMMENT FORM**

Name: Joseph Scher

Address: 1811 Woodrail Dr

City: MILLERSVILLE State: MD Zip: 21108

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Focusing such a substantial investment on infrastructure that will  
directly benefit such a small percentage of population contrary to informed  
government planning for the good of all citizens. It is also likely the on-going  
operational costs will surpass all revenues.

How can state and federal government support such a project  
when our local and national infrastructure continues to crumble  
with unsafe bridges and roads?

Don't spend tax payers dollars for the high income portion  
of the population. Where is the pay back???



DATE 10/16/17

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## COMMENT FORM

Name: BARRY HARTMANAddress: 597 PASTURE BROOK RDCity: SEVERN State: MD Zip: 21144Telephone: 4436766215 Email: BLHARTMAN@HOTMAIL.COMJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I LIVE IN AN AREA THAT IS ALREADY SUBJECT  
TO THE BWI AIRPLANE ROUTES & TRAINS. I DON'T UNDERSTAND  
WHY WE WOULDN'T REPLACE THE EXISTING TRAIN SYSTEM, OR  
PORTIONS OF IT, WITH MAGLEV. HAVING MAGLEV INTRODUCED  
INTO NEW AREAS WOULD HAVE A MAJOR IMPACT ON HOME VALUES  
AND THE ENVIRONMENT. I DON'T SUPPORT THE PROPOSED  
ROUTES & EVEN CHALLENGE THE VALUE OF SUCH A SOLUTION.  
JUST BECAUSE MAGLEV SOLVED A PROBLEM IN ANOTHER COUNTRY  
DOESN'T MEAN IT IS A GOOD SOLUTION FOR US. I AGREE THERE  
IS A CONGESTION PROBLEM BUT WOULD ASK IF ALL ALTERNATIVES  
HAVE BEEN CONSIDERED. THE IMPACT AND CHURN THIS IS CREATING  
FAR OUTWEIGHS THE BENEFITS.





DATE \_\_\_\_\_

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Louisa Baucom

Address: 8311 Harriet Lane

City: SEVERN State: MD Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: Louisa.baucom@gmail.com

Join our mailing list?  Yes  No already joined

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Obviously our neighborhoods are opposed.  
there are no guarantees this will not  
affect property values even if homes are  
not destroyed.  
We oppose on the basis of property rights.



DATE 12-16-17

*bwmaglev.info*

**COMMENT FORM**

Name: Jenifer Hill  
 Address: 1117 Thompson Avenue  
 City: Severn State: MD Zip: 21144  
 Telephone: 443-618-0761 Email: jcmthill@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We already have transportation from Baltimore to Washington. Most people in Maryland would not even use this train. To many homes, schools, wildlife effected. What if your close, what danger is this magnetivity. I'm sure noise & vibrations will be unbearable not to mention what an eye sore to our state. How about we use this money on improving our roads and keep this train out of Maryland. Thank-you for  
(Considering) my concerns, Jenifer Hill



DATE 10-16-17

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**COMMENT FORM**

Name: VIRGIL BROADWATER

Address: 1603 KATHERINE VILLAGE SQ

City: ANNAPOLIS State: MD Zip: 21409

Telephone: 410-812-7338 Email: foby0423@OUTLOOK.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I think this should have been presented in  
auditorium. This is a fabulous idea. We need  
to get out of 19<sup>th</sup> century in transportation ideas.  
I also think 295 corridor should be used due  
to all the government buildings, the land is  
state or federal lands and won't impact  
population as much. Build this train,  
stipulation that all materials are produced  
in the USA

We can't continue to build roads and all  
the gridlock.





DATE 10-16-17

*bwmaglev.info*

**COMMENT FORM**

Name: KENNETH W. SHOSTER

Address: 422 BLAIRFIELD COURT

City: SEVERN State: MD Zip: 21144

Telephone: 410551-3243 Email: CPK82@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

I AM OPPOSED TO ALTERNATIVE E1  
EVEN THOUGH IT IS TUNNELED I AM  
CONCERNED ABOUT VIBRATION AND  
PROPERTY BEING DE-VALUED.





DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: DAN SYM

Address: 1009 ELBRIDGE WAY

City: SEVERN State: MD Zip: 21144

Telephone: 518-248-9847 Email: DANIEL.J.SYM@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The costs of the project to the citizens of Maryland greatly outweigh the benefits I can appreciate the perspective of trying to decrease congestion on the highways, but when peoples' homes & life savings & futures are in jeopardy, it doesn't make sense. The astronomical costs in money & environment are greatly detrimental to our community & to the people of central Maryland, with little to no benefit. For the trip on maglev between DC & Baltimore to save 5-10 minutes, it doesn't help overall. Stop This Train.

DATE 10-16-17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: C Baker  
 Address: Criterion Dr  
 City: Odenton State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: \_\_\_\_\_

This project + the possibility of it becoming reality is now affecting me + my neighbors. Our property values are falling!

It is an absolute nightmare to think that our homes could be destroyed.

Our homes are where we make our lives, they are the culmination of decades of working + saving + sacrificing. They are our greatest assets, + our sense of security + stability.

~~By~~ The maglev Train would "undo" decades of progress by individuals. Everything we sacrificed for would disappear.

DATE 16 Oct 2017

bwmaglev.info

## COMMENT FORM

Name: BERT L. RICEAddress: 1217 Hillcrest RoadCity: Odenton State: MD Zip: 21113-2005Telephone: (410)672-0106 Email: bert07@comcast.netJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: First, thanks for the opportunity to comment and for conducting the OpenHouse at Anundel High School today. The information provided was helpful to better understand the project.

My concerns are: ① cost to construct and to ride. ② Location of route to minimize disruption to our communities. ③ How and who will fund this project. ④ I believe the funds might better used in doing necessary repairs to the interstate network and bridges.

Finally, I believe if a concept was developed for maglev that might go from Boston to Miami it would make more sense. If this <sup>concept</sup> or something like it is being considered then this information should be shared with the citizens!! Just a few thoughts.





DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: A. D. Poulos

Address: 8109 Telegraph Rd

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Much to do about  
nothing!



DATE 10/16/17

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: C Trapp  
 Address: 208 Hawthorne Rd  
 City: Linthicum State: MD Zip: 21090  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: I am totally opposed to this project. My neighborhood will be negatively impacted as my house is very close to one of the proposed routes. My property value will decline & my overall quality of life will be affected as well. I see no need to have an additional transportation service between Baltimore & DC. Amtrak & the Metrolink train are well used. I see no value for the vast majority of citizens. Entire neighborhoods will be torn apart - senior citizens will lose their homes; how easy will it be for people in their 70s & 80s to obtain a loan to re-locate? I will do all I can to oppose MAGLEV.



DATE 10/16/19

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: SCOTT HOWE  
 Address: 1201 WINEZ ROAD  
 City: ODEMAN State: MD Zip: 21113  
 Telephone: 410-868-3676 Email: SCOTT\_SPE\_1@COMCAST.NET

Comments:

① SINGLE SOURCE VENDOR = BATH / SWITCH  
 MAINTENANCE COSTS (JAPAN)

② CREATING A CONDUIT FOR METRO CRIME  
 (DC → BALT.)

③ HOW WAS SCOPING CONDUCTED? HOW WIDE OF  
 AN AREA WAS SURVEYED? CONSULTED?  
 I HEARD NOTHING!

★ ④ I AM (100%) OPPOSED TO THE CONSTRUCTION  
 OF THE MAGLEV TRAIN! PUT THE \$\$\$ INTO  
 AMTRAK + OTHER EXISTING TECHNOLOGY/INFRASTRUCTURE!

I SPEAK FOR THE ENTIRE COMMUNITY IN  
 WHICH I LIVE! THIS IS AN IMPULSIVE  
 DECISION BY OUR GOVERNOR!



DATE 11/15/17

bwmaglev.info

**COMMENT FORM**

Name: Connie Swenson

Address: 1447 WATTS AVE.

City: SEVERN State: MD Zip: 21144

Telephone: 410-489-4533 Email: fl3ce5pl@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Geographical concerns. Tunnel would be  
90 ft below earth. Area has many different  
soil, rock, sand, etc.  
My concern is the stabilization of the earth  
below our homes. I understand the train is  
magnetic but by boring to establish a  
tunnel, Destabilization of earth layer  
Above is of concern.




**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**

 DATE 10/16/2017 ~~2017~~

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name:

Paula Tanabe

Address:

7734 Actocomia Dr.

City:

Hanover

State:

MD

Zip:

21076

Telephone:

410-551-8295

Email:

Paula.Tanabe.7734@gmail.com

Comments:

How can you build another train in this area? We have Acela, high rail and Amtrak. I know a new Acela train is coming on board ~~for~~ that goes 160 mph. ~~This is~~ The Acela would meet our needs. We are talking about a 5 minute difference between the Acela & the Maglev. Also, how can you displace so many people out of their homes that they have had <sup>them</sup> for 30 or 40 years? This is NOT a good project. ~~Why are we~~





DATE 10/14/17

bwmaglev.info

COMMENT FORM

Name: Edi Biggenstaf

Address: 9208 Greenwood Lane

City: Lanham State: MD Zip: 20706

Telephone: 301 577 8153 Email: elpenovocasa@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We already have Amtrak Acela, widening of R/W Parkway, is cutting down traffic from the airport & Balt to DC is the goal, why not schedule bus service at prescribed times in both directions in the proposed toll lanes. We don't need to saddle the state with this financial burden.

DATE Oct 16, 17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Tim Morris  
 Address: 897 Beech Tree RD  
 City: Severn State: MD Zip: 21144  
 Telephone: 443 756 9734 Email: tamorris8@gmail.com

## Comments:

1. I oppose this project as I think the existing (Amtrak/MARC) system should receive any funding to improve the efficiency of that service. For example the 27 million for the feasibility study would be better spent on the above, existing system.

2. Given my opposition -

A) Perhaps this should be voted on by MD residents

B) If it continues to move forward, make sure additional comments on alignment and eminent domain are heard.


**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/2017

baltimorewashingtongmaglevproject.com

**COMMENT FORM**Name: Cynthia & Alan ClineAddress: 11907 Proctor Ct.City: Bowie State: MD Zip: 20720Telephone: 301-262-2292 Email: acmgcline@aol.com

Comments: 1) Current Acela Express exists ~~but~~ <sup>for</sup> those commuters who need to get from DC to Balt or vice versa <sup>in 15 mins</sup>. Most commuters are not ~~so~~ willing to spend the money for a high speed commute.

2) There are no current maglev trains in the world that proves Nideship

3) Cost to MD. taxpayers will be too high to maintain once it is built

4) too much destruction to homes and businesses along ~~the~~ all of the 3 remaining lines

5) no notice was given to impacted homeowners during the SCOPING process.

6) I completely oppose this entire project. This is ridiculous and doesn't serve MD taxpayers. This is a waste of money. Improve current railways



DATE 10/16/2017

bwmaglev.info

**COMMENT FORM**Name: Kevin JonesAddress: 4902 Raemore LaneCity: Bowie State: MD Zip: 20715Telephone: \_\_\_\_\_ Email: KCVishnu@aol.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: It seems an enormous expense and a huge  
disruption for minimal return. I assume there are  
thousands upon thousands of people who live in Baltimore  
that work in D.C. and vice versa. I think this  
is a fantasy. In Japan I heard the cost overrun  
run up to 90 billion dollars and it is 1 million  
per mile of track for maintenance. Put the  
90 billion and invest it in our current MARC  
and Amtrak railroad. Someone is making  
a fortune out of this. We will follow the  
money. This makes more sense for going from  
New York to LA - Not from Washington to  
Baltimore with no stops. Also it has the potential  
of destroying some existing communities.





DATE 16 Oct 17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Roger Shanks

Address: 1530 Star Stella Dr

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concerns with excessive cost of both construction & ridership fees. Sorry but driving is much cheaper, especially with multiple passengers.

This area most effected by construction will never benefit as # we are not at the endpoints.

Public transportation will always have issues with time due to schedule and layover time.

The 15 min advertize trip will never be actuall achieved. Even MARC train around the corner take required I leave the house 2 miles away 15 mins before scheduled train, add delay of train, train trip and then pas destination travel from endpoint.

~~Sf~~ Snow plan is <sup>funny</sup> ~~humorous~~ Spraying water on snow is not a great plan.



DATE 10/16/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: Mary Ann Frymark  
 Address: 1314 Ruby Ct. Odenton, MD 21113  
 City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_  
 Telephone: 410-695-2646 Email: \_\_\_\_\_

Comments: \_\_\_\_\_

Mark Train does a good job of  
 moving people from Odenton to DC.  
 Who is going to pay for this?  
 where are the riders coming from?  
 If it is going to be built it should go  
 along the highway or existing rail tracks.  
 The secrecy of this project smells of  
 big brother hiding what is being done.  
 Only now is it becoming talked about.  
 Not Maglev!







DATE 10/16/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: William Schmore, DDS  
 Address: 1333 DONALD AVE  
 City: SEVERN State: MD Zip: 21144  
 Telephone: 410-551-5598 Email: thebes9587AOL.COM

Comments: Admittedly against this  
CONSTRUCTION for various reasons  
(1) Will affect my property Intimately  
(2) There are 3 TRAINS going  
to D.C. from BWI Region,  
And the tremendous cost of  
project of it is TOTALLY  
UNNECESSARY. EVEN IF, my  
RESIDENCE MAY NOT BE AFFECTED  
I'm Admittedly against the expense,  
The construction



DATE 10/16

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: Diana Middleton  
 Address: 2719 Hickory Knoll Ct  
 City: Odenton State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

## Comments:

I have researched + discussed the proposed train and have decided I am STRONGLY OPPOSED. Not only do I not see any benefit to Marylanders, I believe the damage done to the communities surrounding the train is a detriment to our state.

I believe the current information + studies show that this costly endeavor will only benefit select few. Please do not waste any more of our money on this project, put the money into <sup>current</sup> Maryland transportation or education of our children!



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/2017

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: CRAIG & Pat Carroll

Address: 627 CLEVELAND RO.

City: LINTHICUM State: MD Zip: 21090

Telephone: 410-859-1707 Email: COMCARROLL@YAHOO.COM

Comments: This project has no way of making money. After the novelty and tourism wears off this will be only a drug transfer between Baltimore & D.C. of course maybe some of BALTIMORES GANGS will move into WASHINGTON. This project WAS a WASTE in 2008 AND in 2017.

Thank you  
Craig & Pat



DATE 10/16/17

bwmaglev.info

**COMMENT FORM**

Name: LORETTA ROCKO

Address: 1611 Coolidge Ave

City: SEVERN State: MD Zip: 21144

Telephone: 443-274-2270 Email: LRocko1611@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: First, I never was notified of this project as was told to us. Second, the line will cross over Clack Str Road and Coolidge Ave. I live on Coolidge. I am a single older lady that was able to retire and sell my T.H and have a single home built. This is everyones dream. I am against all alignments for this project. ~~My~~ I will not be able to sell my home at the price and relocate at my age. We have means to get to DC/Baltimore - this will only help certain people. Sufficient info is not being provided to residents. Rethink the proposal - it will does benefit the community



DATE 10-16-17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**Name: Susan McCutchenAddress: 5404 Spring Rd.City: Bladensburg State: MD Zip: 20710Telephone: 301-699-9035 Email: hkdmccutch@yahoo.com

Comments: I live in Bladensburg and will be affected by the train. Two routes are right through my house. While you say because it's underground, homes will be safe. Who are you kidding? Areas will be taken by eminent domain to dig underground. If a structure stays, the digging below will affect its foundation.

This is to benefit Congress and business travelers, not for regular people. We have other "improvements" - Silver Line, Purple Line, extra lines - and need no more added for a pretty, fast, supercool train!





DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Pranita Sinha

Address: 1306 Crawfords Ct.

City: Odenton State: \_\_\_\_\_ Zip: 21113

Telephone: \_\_\_\_\_ Email: bs1ms2343@gmail

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Closeness of train to home, noise level and transportation from the building of system  
Running of Bowie State University. Cost of homes lowered from this. Schools involved will be ~~need~~ lost. Fair market value of homes will be down.



DATE 10/16/17

bwmaglev.info

COMMENT FORM

Name: Joe Stanton

Address: 1440 Misty Lake Ct

City: Hanover State: MD Zip: 21076

Telephone: \_\_\_\_\_ Email: jastanto97@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: <sup>comment</sup> This process is appreciated, but the more beneficial appropriation of funds would be a train that is parallel to the BW Parkway or even directly above the Parkway.

I would like to know more about how home owners will be compensated for the eminent domain taking of their property. There was no information provided for this. Please provide information.

There was information provided about the train but not the Environmental Impact Statement. I cannot make a full assessment w/o that information.



DATE 10/16/2017

bwmaglev.info

**COMMENT FORM**

Name: Pamela Beidle

Address: 620 Fairmount Rd

City: Linthicum State: MD Zip: 21090

Telephone: 410 841 3370 Email: Pamela.Beidle@House.State.md.us

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I support the "No Build" options  
This project affects too many homes.  
It is too expensive, will use a  
great deal of electricity and be  
to expensive for the average person  
to use.



DATE 10-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Leonard Beidle

Address: 620 Fairmount Rd

City: Linthicum State: MD Zip: 21090

Telephone: 410-859-0796 Email: lenbeidle@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Not ever enough info on the Maglev.  
No one has answers to questions!  
I do not support this project.





DATE 10/16/17

*bwmaglev.info*

**COMMENT FORM**

Name: Catie Comer

Address: Linthicum, Md 821 Concorde circle #3105

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: 21090

Telephone: 410 897 7053 Email: CatieKinlein@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Who is the target market for this train? How are home values going to be affected for communities along the route? I am a Linthicum Resident and I do NOT want this train coming through my neighborhood when it is not designed to be a commuter route. There are ~~not~~ no health studies yet staff can point us to regarding the long term impact of the magnetic and increased ~~technology~~ electrical radiation on health.

Please do not send this train through my neighborhood.



DATE 10-16-17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: JULES R. CORNET

Address: 7879 EAST DRIVE

City: SEVERN State: MD Zip: 21144

Telephone: 410-551-5525 Email: HH MACHINE @ AOL.COM

Comments: THIS TRAIN IN MY OPINION IS TOTALLY  
WORTHLESS. THIS TRAIN WILL TAKE ALL  
MY PROPERTY & MY HOUSE THAT I  
WORKED 50 YEARS TO PURCHASE AND  
PAY FOR. I AM 79 YEARS OLD AND  
I DO NOT KNOW HOW MUCH LONGER I  
HAVE TO LIVE.

DATE 10/16/17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Anita Skrenker  
Address: 307 Otto Lane  
City: Gambroils State: MD Zip: 21054  
Telephone: 410-674-2780 Email: \_\_\_\_\_

Comments: I am interested in the findings from the  
research group. We have been doing our own  
research and through discussion with neighbors  
do not see a need for this train. Most people  
I discuss this with have no interest and this is  
a waste of the MD resources. This money could be  
better spent on our out of date and  
crumbling schools and even our road system.  
Most people who work in DC know about  
the traffic and if they need to get to  
New York, we have Amtrak. Wasting our  
money could be better spent on the citizens  
of the state not a small group who  
work in DC or travel to New York.



DATE \_\_\_\_\_

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: ANN MARIE THOMAS

Address: 1357 MEYERS STATION RD

City: ODENTON State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: apntk1333@hotmail.com

Comments: Request strong consideration of the federal land, 295 route. It definitely has minimal impact on citizens and more of a direct route.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16

bwmaglev.info

COMMENT FORM

Name: N. Scott

Address: 310 Fairfield Dr.

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \* Not enough study on environmental damage.

\* No warning / or effort made to really inform affected homeowners.

\* Makes more sense to build along existing roads (BW Pkwy) - less homes - more government buildings / gov. land (like refuges)

\* Expense will cut usage.



DATE 16 OCT 2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: TODD BROWER

Address: 1912 CRUET LANE

City: SEVERN State: MD Zip: 21144

Telephone: 410 551 6769 Email: tbrower@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: THIS TRAIN WILL IMPROVE WASTE & PROPERTY  
VALUES IN OUR NEIGHBORHOOD. IT WILL NOT BE  
USED DUE TO HIGH COST OF RIDE SHIP AND OWNERSHIP.  
JOBBS BROUGHT IN BY IT WILL NOT BE WORTH  
THE ECONOMIC IMPACT TO OUR AREA (TAX DOLLARS,  
LOSS OF PROPERTY VALUE AND ANNOYANCE OF TRAIN.  
PLEASE DO NOT CONTINUE WITH THIS PROJECT

DATE 10-16-17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Dr. Clifford Thomas  
 Address: 5708 Vernon Way  
 City: Suitland State: MD Zip: 20746  
 Telephone: 202-251-1030 Email: tkdramblers@yahoo.com

Comments: This should not take place. Thousands of homes, schools, churches, apartment complexes, and senior homes will be lost due to the damage to infrastructure digging underground. The only people it will benefit is those with money and we will not benefit in Prince George's County. Show me how it reduces the traffic problem when it doesn't stop in Prince George's County. The only jobs for us will be digging a hole in the ground and you will bring in your own people. Once it's built, our workers won't have jobs.  
This is a joke - a bad one.

DATE 10/10/17

bwmaglev.info

**COMMENT FORM**Name: Jonathan SchofieldAddress: 726 Thornubod DrCity: Odenton State: MD Zip: 21113Telephone: \_\_\_\_\_ Email: jonbonbusiness@aol.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: The outreach for this was disingenuous, at best. No opportunity for public comment. There will be no compensation for people who will lose value to homes. The noise of trains coming out of tunnels will be significant.

I moved here one year ago with my young family. I have 22 years federal service, working in some of the most dangerous places in the world. We moved here as a promise to my young children and my wife that their sacrifice was not for nothing. Now, this. No contact during scoping, no consideration of home value loss, no details as yet on environmental impact.

This is shameful. Don't do it - please!





DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Patricia Kinlein

Address: 112 Lee Drive

City: Annapolis State: Md Zip: 21403

Telephone: 410-263-0048 Email: pkinklein@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: It does not appear to have  
enough notices given  
to the surrounding communities that will be impacted  
for example, one of the tracks is projected to  
go through Linthicum Heights, was there or is there  
an open house to inform the people of this  
neighborhood, why isn't one of these open  
houses being held in the Linthicum School  
cafeteria? Almost all of the routes are  
going through Linthicum, #



DATE \_\_\_\_\_

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Heidi Pena

Address: 1523 Star Stella Dr

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: hpena@Casapena.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Have studies been completed on commuters?  
I'd like to see who they anticipate will be taking  
this form of transportation for commuting  
purposes. Everyone I commute with comes into  
either Baltimore or DC from the suburbs.  
Not commuting from one city to the other.  
I'd also like to hear the approximate cost  
for commuting via maglev.



DATE 16 OCTOBER 2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: JOSEPH S. MROZ

Address: 1706 JONAFREE COURT

City: ODENTON State: MD. Zip: 21113-3961

Telephone: 410-305-0658 Email: js.mroz@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: MORE INFORMATION ABOUT THE CONSTRUCTION OF UNDER-  
GROUND TUNNELS WOULD BE VERY HELPFUL. FOR EXAMPLE,  
HOW DEEP WOULD SUCH TUNNELS BE BELOW THE SURFACE;  
HOW WOULD AIR BE PUMPED INTO THE TUNNELS; HOW WOULD  
AIR BE REMOVED FROM THE TUNNELS; WHAT IS THE TYPICAL  
DISTANCE BETWEEN AIR STATIONS ?

ON ANOTHER TOPIC, THE DIAGRAM SHOWING THE AREA COVERED BY  
THE NINE MAP SHEETS SHOULD BE POSTED ON THE PROJECT'S  
WEBSITE.

IN ADDITION, IT WOULD BE HELPFUL TO KNOW WHERE PARKING LOTS  
WILL BE LOCATED NEAR THE PROPOSED STATION LOCATIONS.



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Geraldine Brown

Address: 524 Patricia Ct

City: Odenton State: Md Zip: 21113

Telephone: 410-624-~~551~~ Email: gerryb@aol.com  
551-0965

Comments:

NOT Necessary  
We now have Trains to  
get to DC + Balto  
in 15 MINS

Will Definitely Affect  
our Country's Way of Life

NO Maglev!





DATE 10/16/17

baltimorewashingtonscmaglevproject.com

COMMENT FORM

Name: PAUL F. ZIMMERLI  
 Address: 939 WAUGH CHAPEL RD  
 City: GAMBRIILLS State: MD Zip: 21054  
 Telephone: Email:

Comments: THIS PROJECT FOR THE FORSEEABLE FUTURE,  
 SERVES ONLY THE CORE OF D.C., BWI, & THE  
 CORE OF BALT. ITS AN AIRPORT SHUTTLE.  
 THE PRECIOUS ENVIRONMENT OF THE PATUXENT  
 WILDLIFE PRESERVE WOULD BE FOREVER HARMED BY  
 THE ROUTE THRU ODENTON. IF THE PROJECT  
 WERE TO GO FORWARD, THE ROUTE ALONG THE  
 B&W PARKWAY WOULD BE THE "LESSER OF TWO  
 EVILS." "



DATE Oct 16. 17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: Bonnie Stacks

Address: 528 Michille Rd

City: Odenton State: md Zip: 21113

Telephone: (410) 672 2953 Email: \_\_\_\_\_

Comments: ~~no~~ No train  
Dont want to move sin to Old 89 yrs-

His rupe out way of life

Dont need any more trains

DATE 16 OCT 2017

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: KAREN HOLMES  
 Address: 730 EMERALD WAY  
 City: ODENTON State: MD Zip: 21113  
 Telephone: 246 723 7390 Email: Karenholmes2@cs.com

Comments: THANK YOU FOR HOSTING THIS OPEN HOUSE TO INFORM THE  
COMMUNITY ABOUT THE PROJECT. IT'S GREAT THAT THE WB&A TRAIL  
ALIGNMENTS HAVE BEEN TAKEN OFF THE TABLE AND IT IS VITAL THAT  
THEY REMAIN OFF THE TABLE FOR THE ENTIRETY OF THE STUDY PERIOD.  
THESE ALIGNMENTS CAUSED FAR TOO MUCH DAMAGE TO OUR COMMUNITIES  
AND OUR SENSITIVE ENVIRONMENTAL RESOURCES.

SIMILARLY, THE AMTRAK ALIGNMENT CAUSES SIGNIFICANT  
COMMUNITY DISRUPTION AND NOISE PROBLEMS. THE BW PARKWAY  
ALIGNMENTS MAKE FAR MORE SENSE FROM A COMMUNITY IMPACT  
STANDPOINT.

WHILE THE DRAWINGS PROVIDED BY THE PROJECT PROPONENTS ARE ALL  
VERY ATTRACTIVE & FUTURISTIC, THE REALITY IS THAT ABOVE-GROUND  
MAGLEV IN BUILT-UP AREAS WILL BECOME A MAGNET FOR LOITERING  
AND GRAFFITI.





DATE 10-16-17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Ray LEAVER  
 Address: 6240 WOODLAND RD.  
 City: LINTHICUM HILLS State: MD Zip: 21090  
 Telephone: 410-760-6687 Email: DEB N RAY I@CABLESPEED.COM

Comments: \_\_\_\_\_

WE DON'T NEED MORE TRAFFIC AROUND  
 THE AIRPORTS. WILL THEY TAKE RIDERSHIP  
 FROM MARC + AMTRAK - THEY'RE  
 ALREADY LOSING MONEY? WILL  
 THEY BE SCRAPPED? WE DON'T WANT  
 OUR COMMUNITIES UPLOADED AND  
 DESTROYED FOR A FEW POLITICIANS  
 OR BUSINESSMEN TO GET FROM  
 BALTIMORE TO DC 10 MINUTES  
 FASTER  
 STOP THIS TRAIN!!





DATE Oct. 16, 2017

baltimorewashingtontcmaglevproject.com

**COMMENT FORM**

Name: DonoThy Bell

Address: 526 Patricia Ct.

City: Odenton State: Md. Zip: 21113

Telephone: 410-614-8892 Email: \_\_\_\_\_

Comments: Stop The Train

W We don't need to be disrupted at  
the taxes expense.



Please use !!!  
I am NOT receiving anything

DATE 10/16/17

bwmaglev.info

COMMENT FORM

Name: Mary B Bauer

Address: 503 Prince Charles Ave

City: Odenton State: Md. Zip: 21113

Telephone: 410.551.3093 Email: seahag1@gmail.com

Join our mailing list?  Yes  No I have submitted info 2 times now + still receive NOTHING

Preferred method of communication?  Email  Mail

Comments: I am furious that my County has not been informed like Baltimore City & DC were.

We have 2 main systems already in use and Amtrak is re-wamping to shave 60 minutes off their travel so this MAGLEV is not necessary.

Also - the seller of the B&A RR was not the legal party so this whole project should be Null & Void.

Please stop this train!!!



DATE 10-16-17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Sam Powell

Address: 8331 DUBBS DR

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: Serannapolis@yahoo.com

Comments: Why are we not being

notified of these meeting?

I found out from a neighbor

What economic need has been

demonstrated?





DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Joy Hart

Address: 702 KRAFT CT

City: GEN BOENIE State: MD Zip: 21061

Telephone: (410) 900-9988 Email: wilsonj22@hotmail.com

Comments: \_\_\_\_\_

I do not want this train to be brought to Maryland at all. I feel that this is not the right move for the ~~residents~~ residents of MD. This train will create more economic hardships on existing homeowners. This will take homes from the elderly, 1<sup>st</sup> time homeowners, existing homeowners, school (students) businesses, churches & etc. Most will not even be able to use this train.

NO TRAIN





DATE 10-16-17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: DAVID OWEN  
 Address: 1320 PASSAIC DR  
 City: ODONTA State: MD Zip: 21113  
 Telephone: 410-612 2043 Email: \_\_\_\_\_  
410960-6434

Comments:

OPTION NOT TO BUILD. NO NEED GO OVER AMTRAK LINES  
FIX AMTRAK  
IF YOU WANT TO DEVELOP THE STATE  
BUILD THIS GOING FROM THE EASTERN SHORE  
TO THE WEST SHORE. THE EASTERN SHORE NEEDS  
JOB ACCESS TO GOOD JOBS ON WEST SIDE  
NEW HIGH SPEED TO OCEAN CITY ~~TO~~ MIGHT  
TAKE AWAY CAR TRAFFIC ON THE BAY BRIDGES.  
~~WE DO~~ THERE IS NO NEED TO GO BETWEEN  
BALT. WASHINGTON IN LESS MINS THEN AMTRAK DOES  
NOW  
BUT FAST RIDE CROSS THE BAY TO  
OCEAN CITY WOULD HELP OUT TRAFFIC JAMS  
WE HAVE NOW.

DATE 10/18/17

baltimorewashingtongmaglevproject.com

## COMMENT FORM

Name: ROGER K. STEPHENS

Address: 13143 DAIRYMAID DR. #203

City: GERMANTOWN State: MD Zip: 20874

Telephone: (301)428-0259 Email: rogerkstephens@aol.com

Comments: It is a shame that this project did not get better and more advance publicity, info. to interested public.

Mainly am concerned about amount of money to finance versus financial benefit, which most likely <sup>will</sup> be a loss.

Also, who will use it if they have to drive to either Baltimore or D.C. to get on it to travel back to the other city — REALLY!!!

The people being dislocated, or at the least highly inconvenienced, may never recover their losses while the benefits accrue to the few who can afford it and will be the few who use it. Additionally, everyone in the state will pay more in some way, whether it be in taxes (the Japanese will not pay for the whole thing).

Because of the lack of proper and pervasive advance notice, the whole thing will not get an objective hearing. Therefore, the whole thing is a travesty and will not pay for itself. Who will pay???



DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Patricia Bower

Address: 1211 Garnet Ct.

City: Odenton State: MD Zip: 21113

Telephone: 410/551-2375 Email: pattijbower@gmail.com

Comments: A much better way to provide faster transportation  
between DC + Baltimore is to add a nonstop Amtrak line.  
This would be much more cost-efficient even if an additional  
line would be needed.





DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: PATRICK KLEESPIES

Address: 7726 LOCUST WOOD RD

City: SEVERIN State: MD Zip: 21144

Telephone: 240-938-0321 Email: KLEESPIES@GMAIL.COM

Comments: I WOULD LOVE TO HAVE THE DEPTIT OF  
TUNNELS ADDRESSED, AS IT WOULD APPEAR CONSIDERABLE  
AMOUNTS OF TRACK, REGARDLESS OF PATH, WILL  
BE TUNNELED. GIVEN THE FACT THAT MANY  
HOMES IN THE PATH OF THESE PROPOSED LINES  
RELY ON WELLS FOR POTABLE WATER, HOW WILL  
AQUIFERS BE IMPACTED, AS WELL AS THE WELLS  
THEMSELVES BY THIS CONSTRUCTION?

THANK YOU FOR YOUR TIME





DATE 10/16/2017

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Milton Hogeboom

Address: 2815 5Th Ave

City: Odenton State: MD Zip: 21113

Telephone: 410 365 1383 Email:

Comments: Why are we NOT Building 100% of this train here in America?

How is it being paid for?

Do They have to demolish homes building etcetera in The pathway of the train?

DATE 10-17-17

baltimorewashingtongmaglevproject.com

## COMMENT FORM

Name: Carrie Krell  
 Address: 169 Ellen Ave  
 City: Severn State: md Zip: 21144  
 Telephone: 443-679-5480 Email: cgalaway@hotmail.com

Comments: I am happy to see the Red + Yellow lines have been removed. However this entire project is a waste of money. Why spend \$10 Billion plus dollars to save 5 minutes (potentially) on commute time? We already have the Acela line that takes 20 minutes! Use the money to fix our existing infrastructure and trains! In addition, daily commuters will not ~~be able~~ want to use this. It is too expensive to use on a daily basis and does nothing to help commuters who live in the areas between DC + Baltimore. So it will not help reduce commute times or traffic congestion. In addition those who will lose their homes and businesses due to eminent domain will most definitely lose money in the process.  
also not support this project.

Carrie Krell



DATE 10/16/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Aaron Klebanoff

Address: 8579 Summerstade Dr

City: Odenton State: MD Zip: 21113

Telephone: 410-672-6474 Email: adkleba@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My main concerns, highest first, are:

→ Cost: Who pays for this & how? Local taxes? Increased Federal &/or State Taxes?  
What will tickets cost? How competitive can the ticket cost be vs. alternatives?

↓ (\$) Impact to property: The Odenton route (Amtrak option) is above ground by  
Piney Orchard, & amazingly, straight through existing property by Hwy 175  
Why wouldn't you go underground sooner? Obviously cost is the  
issue. See other concern!





DATE 10 16 17

[baltimorewashingtonscmaglevproject.com](http://baltimorewashingtonscmaglevproject.com)

**COMMENT FORM**

Name: Joy Lawson

Address: 571 SHIPLEY RD

City: LINTHICUM HEIGHTS State: MD Zip: 21090

Telephone: 202 834 9031 Email: JOY NICOLELAWSON@GMAIL.COM

Comments: This (yellow/gold) line would go ~~to~~ straight through  
my quiet, proud, peaceful community. My family chose  
to live here because of the spirit of the community.  
This town would rip us apart. We wanted to put  
long term roots down on Shipley but the  
construction, <sup>enviro</sup> impact, + noise would force us to leave  
a place we have grown to love.

Please don't build this rail - for my 3 year  
old's future - ~~for~~ out of respect for the history  
of this community.





DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Wanda Jones

Address: 1267 Belmont Rd

City: Southon State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: Why can't we put money in the  
trains we currently have? This will  
go right thru our property. The noise  
potential safety hazard will  
decrease property values. We won't  
be able to sell the house then.  
We need to ask ~~us~~ our selves is a  
few minutes really so important  
if negatively impacts the majority  
of the people



DATE 11-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: DONALD HORNICK

Address: 1312 HALLOCK DR.

City: ODENTON State: MD Zip: 21113

Telephone: 410 674 8586 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: HOW WILL THIS IMPACT LAND OWNERS WHEN THEIR HOME AND LAND IS NEEDED FOR THIS PROJECT? THE IMPACT WILL CAUSE ALOT OF DISRUPTION IN FAMILY LIFE. WHAT WILL THE OFFER FINANCIALLY BE OFFERED TO MOVE AND PURCHASE A NEW HOME? WHAT WOULD THE TIME FRAME BE?

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DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: ARLENE BOUD

Address: 656 CHAPELVIEW DR

City: ODDENTON State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: ambrdh@verizon.net

Comments: I have been unhappy with the maglev train since I first heard about it 4-5 months ago. At that time I found out that my house would be impacted. I am opposed to this for a variety of reasons, but the biggest is the impact on so many neighborhoods. At this point it looks like my property will no longer be impacted but there are other communities that face the same upset that I had over the last few months. I am also concerned that citizens of Maryland will wind up having to contribute to maintenance of the train infrastructure. I am not convinced that there will not be environmental problems with the construction and eventual running of the trains. As far as the area to build the track if it must be built would be only feasible along the Balt.- Wash Parkway in my opinion.





DATE 10/16/2017

*bwmaglev.info*

**COMMENT FORM**

Name: Allison Dubit

Address: 7801 Locust Wood Road

City: Severn State: MD Zip: 21144

Telephone: 301 906 5503 Email: allison.dubit@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

It seems as if this project has gotten far into the approval process without notification of most of the effected residents. This is a disgrace, as you are devaluing and taking/destroying our properties. Don't you think that we should've been the first to know, not the last. My husband and I searched for years for our dream property and finally found it in Severn. We now have a daughter and the thought of either raising her over/under train tracks for a train that we can't even afford to ride is very disheartening.

Kevin Plank was quoted in 2015 saying that the SC MagLev will be benefit to Under Armor and his development of Port Covington. It has been mentioned in recent months that the MagLev will help the City of Baltimore with attracting Amazon to make their second headquarters there. How? Are they expecting employees to commute from Washington, DC to Baltimore to work?

We currently have not only Amtrak, but the MARC trains that run from Washington, DC, to BWI Airport, and onto Baltimore. Amtrak estimates the trip from DC to Baltimore to take about 21 minutes and cost anywhere from \$20 to \$80 dollars. What do you expect the MagLev ticket to cost for the savings of about 7 minutes off travel time?

Environmental Effects: We are in the Chesapeake Bay watershed in Anne Arundel County. As you know, the Chesapeake Bay is a very polluted and fragile environment. How does the SC MagLev project plan to keep the impact to the Bay to a minimum?

Magnetic Field: There are medical conditions that warrant patients to not be exposed to magnetic fields. One that I can think of off-hand is people who have pacemakers. How far will the magnetic field stretch and what is your plan for nearby residents who are affected medically?

Who will pay for this multi-billion-dollar project?



Malana Brown

Dear Sirs and Madams:

10/16/17 Please stop the Mag train

it would cause much sorrow  
even just my family. I have  
friends who's house would be  
seized, it would run right behind  
my grandparents house and  
you might have to seize  
my house which I have lived  
in since I was 2 (two not a).  
Plus I love my house dearly,  
even for a 10 year old. My house  
has just under gone 2 renovations  
one in our unfinished basement

1 and the other because a flood.

ruined the first floor and the newly finished basement. As you can see from the above stated facts it would cause my family much sorrow and it might cause you to seize my beloved house which we have put a lot of work into. If it would cause so much sorrow to 1 family imagine about all the others. If you would stop the mag. train it would be much appreciated.

Respectfully,

Malana Brown

5th Grad, age 10.



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10-16-17

baltimorewashingtonscmaglevproject.com

## COMMENT FORM

Name: Susan SteppAddress: 1009 Morgan Station DrCity: Severna Park State: Md Zip: 21144Telephone: 410 969 7540 Email: steppfamily4@live.com

Comments: I have several concerns about the Maglev -  
First, the impact it will have on my community  
and value of my home - I live in the E1 (Amtrak)  
route path -

Secondly - I have seen nothing with #'s to  
substantiate a need and the feasibility of  
use - My concern is that ridership will be  
less than estimated, how many folks will/can  
afford a one way ticket fare of \$50.<sup>00</sup> - \$75.<sup>00</sup> -  
And if this project ~~to~~ losses money it will fall  
to the tax payers to foot the bill.

Thirdly, the \$ granted to study this project is  
sickening - that amount of money would/could  
have greatly improved our existing roadway  
or rail system (Acela train should be  
studied) also the feasibility of adding  
How lanes to 295 or 95 - or ~~to~~ raised





DATE \_\_\_\_\_

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Susan Stepp

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Roadway for express travel.

Ultimately, if the ridership does not meet the potential - there will be no relief of congestion and commuter traffic - Again, why it would be much more beneficial for all to improve our existing road/rail ways with a reasonable cost to all commuters.

thirdly, the anticipated speed at which this train is promised to travel at is very scary - how is it reasonable within a 30 mile distance to accelerate to such a speed and then come to a halt (safely) and to address the issue of a possible



Comments: accident - Reports from the accident of the Maglev train in China - debris was found up to 300 yd away - and an ~~accident~~ accident would be catastrophic at that speed.

Fourth, this train has been proposed to extend to NY, but PA, NJ, NY have not show a commitment to building a Maglev extension - this "proposed project" will have a direct impact on Marylanders financially Fifth - Do we want a foreign private company to have such a hold on Maryland - Germany stopped the building of their Maglev project - China is truly still in the experimental stages of their Maglev ~~pr~~ train Why would Maryland invest in something of this magnitude.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



Comments:

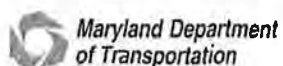
Lastly, any route chosen will have a huge impact on our wildlife, historic sites, schools, churches, communities - who is going to pay to build new schools to replace one's destroyed, church that will be lost, historical sites can not be replaced.

I am completely against such a project that has such a negative impact on ~~us~~ us and very little benefit for other than a few elite people. And it is not a good feeling that my community never received any mailings or notification of this proposal and our community would be directly affected.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/16/17

*bwmaglev.info*

**COMMENT FORM**

Name: ~~#~~ Cindy Baldwin

Address: 1335 Donald Ave.

City: Severn State: MD Zip: 21144

Telephone: 410-925-9027 Email: cindy/kbaldwin@  
yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

This project, above or underground, is NOT wanted! To begin with, who is going to pay Amtrak rates to ride this? We keep hearing this will "take cars off the road". This is comical. No one will use this line to commute to work. They cannot afford it. We need wider roads and improvements to MARC, light rail and Acela for local congestion reasons; not another way to get to DC or NY quicker. I would personally be affected by the yellow/Amtrak route but am not for any of them. What about the



Comments: gases they need to be released through  
the ground up to where our houses are?  
What about losing Ridgeway Elementary  
school which is already over capacity?  
Where will these kids go? We do not  
have enough schools in ratio to students  
anyway. The \$28 million spent for  
environmental studies could have been  
used for more important things!!!  
NO TRAIN!!!



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Baltimore, MD 21202

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DATE Oct 16 2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Elissa Weidaw  
 Address: 8612 Fluttering Leaf Tr #307  
 City: Odenton State: MD Zip: 21113-4055  
 Telephone: 410 695 0609 Email: elissa.weidaw@yahoo.com

Comments: ① I do not think the ridership will be there. I absolutely oppose the continuation of this project. You need to take into account declining ridership of public transportation due to our increasingly connected society where people can do so much more with mobile devices and are not so much willing to pay for a few minutes saved.

② The Amtrak system is great and gets me where I want to go in plenty of time. I can work on the train using mobile devices. I do not need a new system and I doubt that many enough people would be interested enough to use it.

③ E1 route goes right by my home. ~~I want to be~~ Before the project goes forward, (which I hope it does not) I want there to be an audio track available online that simulates the noise of air displacement. I do not believe it is comparable to Amtrak. I want proof. Get some recordings ~~be~~ from Japan if this is really the equivalent technology.

January 2018

④ I do not believe that <sup>we</sup> taxpayer will not be <sup>on</sup> (over)

Comments: the book when this project stalls, gets delayed,  
has cost overruns, etc,

(5) The E1 alternative takes away homes in Odenton and splits us apart. This route should be eliminated from consideration.

(6) What did Mr Bratcher mean that "we do not know who would be in charge of alignment." Seriously.

Again, I am completely opposed to this project. Our river watersheds are already under too much stress!!

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

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c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

**Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)**





DATE \_\_\_\_\_

[baltimorewashingtonscmaglevproject.com](http://baltimorewashingtonscmaglevproject.com)

**COMMENT FORM**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: \_\_\_\_\_

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Comments: unfeasible; THE COST, THE DESTRUCTION OF PROPERTY AND THE  
 DISMEMBERING OF ESTABLISHED COMMUNITIES. THERE ARE MANY MORE IMPORTANT  
 THINGS THAT MONEY SHOULD GO FOR! WIDEN THE BW RAIL. AND A CENTER  
 LANE LIKE THE CHESAPEAKE Bay - GEORGETOWN AVE LINE. IF THIS GOES  
 THROUGH I WILL WORK AGAINST EVERY POLICEMAN THAT SUPPORTS IT.

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 Hanover, MD 21076

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DATE 10/14/17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: RITA WELLSAddress: 408 HISTORICAL WAYCity: LANTHANUM State: MD Zip: 21090Telephone: 410-851-1177 Email: RNWELLS@HOTMAIL.COM

- Comments: On top of the cost of the train, the people will  
need to pay for parking (no free parking in Baltimore, BWI or Washington)  
Then when they get to where they are going they will  
have to pay for additional transportation to get to their  
final destination - NOT COST EFFECTIVE for most people
- What about the health concerns associated with the maglev  
magnets + radiation fields, also the pollution from the air  
shafts from the tunnels
  - This could also effect vegetation and green spaces, air quality
  - what about all the communities that will lose businesses  
+ homes for a train that won't be used and will cost taxpayers  
money
  - What about the lost tax revenues from all the property taxes  
from the homes + businesses that are destroyed
  - The train has failed in many locations already how about  
you study why it failed - This is a waste of time + money

- Comments: This won't take traffic off of the roads - it doesn't have stops that help most Marylanders - No one will drive from Odenton to BWI + vice versa to catch a train - Instead build + improve roads that help everyone
- There is already a MARC train + Amtrak they are much better options because they stop and pick up commuters along the way
  - No way to get up to 311 mph from Baltimore to BWI and vice versa in addition when they stop time is added - so how long will the trip really take? Take the Acela its a better option - improve what we have for railways - This won't be a big time saver
  - Too expensive - taxpayers will have to pay to help build and sustain
  - This only will have a small percentage of riders, like tourists and rich people, maybe some business people not from the area instead of helping a very small percentage of the people; why not help lots of people and improve roads
  - ~~The~~ Most people don't know about <sup>MAGLEV</sup> the ~~MARC~~ it seems like this whole project is very shady

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

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DATE 10/16/17

bwmaglev.info

**COMMENT FORM**Name: Nancy DicksonAddress: 12208 Round Tree LnCity: Bowie State: MD Zip: 20715Telephone: 202 683 0661 Email: nsorto@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This has to be the worst idea MD has ever thought of.  
How could you even consider tearing people from their homes to  
make your pockets bigger. There is already an existing train  
that gets to Baltimore in 20 minutes. The Maglev is said to get  
their in 15 minutes. Why is it so important to get back 5  
minutes of your life. Also this is not even a commuter train.  
So how is this going to help the congestion in our city. What  
is this even going to cost? No one seems to know the answer.  
Why build a train that hasn't been successful anywhere else  
in the US? It's a joke! Plus this type of energy is going to have  
health affects in the long run and building it will ruin the  
infrastructure of anyone that is 10 miles of this monster  
train. My house may not be in the path anymore but I  
still don't want to live close to this type of transportation.

Comments: In the end the cost of this train will result  
in bankruptcy just like the one in California. How  
come know one talks about the bad facts about  
this train. This will result in the pockets of our tax  
payers money. Also how is this train going to  
coexist with the existing railways?

DO NOT BUILD THIS TRAIN!!!!

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

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 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





**OPPOSED**!!!



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10-16-17

baltimorewashingtongsmaglevproject.com

COMMENT FORM

Name: Mads

Address: 1509 enterionk

City: Dodenton State: MD Zip: 2113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments:

**Benefits** to local communities =

**\* NONE \***

(we would have to commute to the stations, negating the reduction in commute time) + construction <sup>would block</sup> existing roads

**IMACTS** to the local communities =

- \* homes destroyed
- \* green spaces destroyed / Parks + <sup>smaller</sup> community parks
- \* people lose their life savings, sense of security

- \* potential health risks
- \* destroyed green spaces + recreation areas

- \* noise pollution
- \* decreased property values
- \* what happens to the elderly?
- \* waste of tax dollars
- \* loss of businesses + vibration effects
- \* electro magnetic fields
- \* disruption to commuters due to construction
- \* reduction in quality of life

Comments: \_\_\_\_\_

The study + evaluation period

greatly affects the value of  
our homes for the duration  
of the study.

We are all angry that we were ~~not~~  
informed of this project until  
recently via word of mouth ~~to~~ by  
community organizations. This has  
(already) hurt our communities +  
personal well being + has lowered  
the values of our homes.

Stop the study now!!! Discontinue it!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 10/16/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: ANNE MURRAY

Address: 1305 WICKELL RD

City: ODENTON State: MD Zip: 21113

Telephone: 410-672-0438 Email: askmurray@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am against the Maglev being constructed. We already have sufficient transportation for the area and adding this will not add any benefit except for a small percentage and only for their convenience. We have poor traffic flow for the existing residents, contributing to large back ups and this will only contribute to it more.

I do not want to be looking at elevated tracks when I moved here to enjoy the views that we have left after all the blight of over-building and shopping centers. The out-of-control building needs to stop!

The proposed build areas will destroy many of the rural areas we have left. Going through Bowie will destroy the beauty of the structured communities without adding benefit to the residents there. Having no access to the line, having

Comments: to travel to utilize it, adds no benefit to them.  
What will we be compensated, as residents, when our  
properties are de-valued? Property values are just now  
recovering and we don't want to see the loss again.  
Who, exactly, benefits from this venture? It's certainly  
not the residents who will be impacted by it.  
I am AGAINST anything that ruins our preserves  
and impacts our wildlife

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE Oct 14, 2017

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COMMENT FORM

Name: Lorraine Tishey

Address: 8335 Dubbs Dr.

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: latishe@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

~~mailing list~~

Comments: Spending billions of dollars, which ~~the~~ makes  
neither state nor federal government has, ~~is not~~  
NO sense. ~~The train~~ will never pay for itself, it  
will disrupt thousands of people's lives  
(either by taking their land or causing massive  
traffic & environmental problems), all so some  
people can take a faster ride between D.C. &  
New York. Many people whose property will  
be taken will not be able to afford the cost  
of the train and will never be reimbursed  
for their property at its proper value plus  
the other costs of finding affordable housing  
elsewhere. Please don't do this. We need  
other things more - better roads, schools, better pay



DATE 10/16

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**COMMENT FORM**Name: Robert MauroAddress: 7929 Henslowe CtCity: Pasadena State: MA Zip: 21122Telephone: (410) 903-0465 Email: r/mauro2@yahoo.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I work at a middle school in Linthicum Heights just north of the proposed BWI station. One of the three remaining options (E) runs close to Lindale Middle. Since this area is in a proposed tunnel, and 2/3's of the proposed path is in a tunnel, my biggest concern now is cost and the timetable. \$10-\$15 Billion, or more, is an awful lot to throw at a project that will be entirely hit-or-miss. With the Metro in D.C. you started with a few stations from Union Station to Metro Center. If it stopped there, you'd still be left with something functional. If this project stalls or runs over budget, which seems excessively likely given its uniqueness and the vast amount of tunnelling, the tax payers are left with nothing to show



Comments: For their taxes, time and patience. Please  
consider the history of other big tunneling projects,  
see Boston's big dig, and stay realistic on budget.  
This could easily cost \$30 - \$40 billion and not  
open for 20 years. Please be objective  
in reviewing the reports, fiscal and environmental.  
Please don't put this on taxpayers.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

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**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE Oct 16, 2017

bwmaglev.info

**COMMENT FORM**

Name: Donna Graham  
 Address: 8607 Wintergreen Ct, Unit 106  
 City: Odenton State: MD Zip: 21113  
 Telephone: (410) 695-0162 Email: donna\_graham@Verizon.net  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I am a technical person, with a Masters Degree in Computer Science. I think this is a great futuristic high tech idea, I also feel very strongly about protecting the environment and doing things for cleaner air and water. I like the fact that the SC maglev is cleaner than current trains, cars, and airplanes. I have made changes in my life to reduce my carbon emissions. I spoke to many local people today who don't see how this will benefit them, I tried to explain that it will definitely reduce traffic on 295 and locally on both ends.  
On second thought, what will people do for a



Comments: means of travel once they get to either Baltimore or Washington? Is the needed public transportation in place? Will it cause things to be more crowded? The biggest question is: will it be used enough to pay for itself? I've read that the German Maglev train project has been scrapped due to it being too expensive. But the Japanese SC maglev train seems to be doing very well. It also has a very good safety record. Regular trains are ~~more~~ unfortunately prone to accidents, which kill people. In addition some of our current trains may be getting. It's always good to move to newer technology. When this line is extended to N.Y., I think it will reduce car traffic on Rt. 95 and even airplane traffic between Balt. and New York. This would help reduce carbon emissions.

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Many people were also concerned by the cost of a ticket.

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c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10/10/17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Stephanie Handt  
 Address: 962 Patuxent RD  
 City: Odenton State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: Steph08e@aol.com

Comments: There is so much to be  
said about this project it's hard to  
know where to begin. It seems that  
the general ~~consensus~~ consensus is to improve  
the infrastructure we already have.  
The impact to the community +  
individuals would be too great. This  
line is so close to home for me as one of  
the lines would run behind my home,  
so where does that leave me and others  
like me?

Any plan that is to be considered  
should have the least impact to  
residents, like the line that would

Comments: run along 295 - anything else  
is just unreasonable.

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

**SCMAGLEV Project**  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Patricia Butler

Address: 2617 Brown Alder Ct.

City: Odenton State: MD Zip: 21113

Telephone: N/A Email: ~~patrickbutler@msn.com~~ pkj964@msn.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The train <sup>could possible</sup> go through our community  
disrupting home, environment, yet it won't be  
used by Odenton residents. Why would they  
drive to the airport - pay to park - when it  
would be easier & cheaper to drive directly to  
BALT or Wash.

Why not use the funds to fix the roads & bridges.

This train will be used by those with more money -  
not the general public. And those with more  
money do not live in the areas impacted by the  
construction of the train.

Comments: My MD STATE TAX Dollars will be used for  
Something I would not use ① based on  
close proximity of DC + BALT ② based on the  
ticket price - (lets be honest - it is not going to be  
comparable to the train!) ③ it will ~~benefit~~ benefit  
most - Kevin Plank + Amazon + Wealthier Americans  
+ Businesses

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 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10-16-17

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COMMENT FORM

Name: CAROL BAK

Address: 1507 Critenon Dr

City: Odenton State: MD Zip: 21113

Telephone: 410 695 1920 Email: bakcarol@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am adamantly opposed to the MAGLEV TRAIN FOR THE FOLLOWING REASONS:

\* Our property values are tanking as we speak. For the duration of the study, realtors must disclose that this train may level our neighborhoods & destroy our communities. As a result, many of my neighbors have already put their houses up for sale + they are not selling. Prices continue to drop. ~~The~~ Our homes are our biggest investments financially & emotionally. I just spoke with a woman who told a story of her neighbor selling a house for almost half of its value (list price 249,000 - selling price \$149,000) because of the



Comments: possibility of the train destroying our neighborhood

\* I feel that there is a significant health risk (magnetic field in highly populated areas). where are the studies? how does this affect the elderly, children, pregnant women, + everyone else?

\* This train will not benefit the local communities in any way. Our commutes will not be shorter because we would have to drive to DC or Baltimore or BWI to catch the train. Furthermore, it would be cost prohibitive.

\* This area is simply too populated to perform an experiment in transportation. It would be catastrophic if there were to be an accident

\* I adamantly oppose the use of my tax dollars

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to fund this study. we have so many greater pressing needs that would benefit a greater number of people.

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Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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do not destroy our homes + communities!! Page C-847





DATE 10/16/18

bwmaglev.info

COMMENT FORM

Name: Tyson Heck

Address: 426 Burnside St. / 1283 Ana Rd

City: Annapolis / Severn State: MD Zip:

Telephone: Email: THeck@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Property values? Well water? Congestion? These are all underlying issues affected by this project - a project with no promise of completion. A project that could cost unimaginable amounts of money. A project with environmental impacts, infrastructure impacts, and monetary impacts. Is there a time frame for completion? Is it properly being studied, despite the lack of information we've received over the last 8 months and the uninformative babble we received today? Congestion won't improve here - it will bring more people to the roads. It will lower the property values of people who have lived here well before these

Comments: was any suggestion to build a white-  
collar transportation method to and have it barrel  
through suburbia. Lets be honest with ourselves.  
IF YOU lived here, if YOUR kids went to  
school here, if YOUR families future was here,  
would eyes still be for building this train?

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6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/16/17

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## COMMENT FORM

Name: Lois McKeonAddress: 1507 Sapphire CtCity: Odenton State: MD Zip: 21113Telephone: 410-305-0425 Email: LoismcKeon1@aol.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: ① Have you considered running the maglev in the median of the BW Parkway instead of using the alternative proposal running on either side of the parkway? It certainly seems more logical & less disruptive ~~to~~ to homes and communities to put this with an already established, relatively straight line between DC & Balt. Too logical??

② Now that the BWA trail has been removed as an option, can you assure us that it will stay removed? We need to know that this will NOT re-surface at a later date.

③ I have still not heard any great explanation about how the Maglev benefits any residents that



Comments: live along any proposed route since there may only be one stop at the airport. How does this help us??

# How will this be funded? When the Japanese money & the federal money run out, who's going to pay for this?? Maryland taxpayers?? Bad idea!

B There are still too many unanswered questions to be totally supportive of any of this.

D Where is a copy of the old EIP study that was done back in 2007 (or maybe 2003) Nobody here knows how to get a copy!

E Will read-outs of the open houses be sent to those of us who signed up for e-mails? Will we be kept in the loop about future proposals, decisions, etc? We need to be informed before anything happens, not after the fact! Thanks for allowing us

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Baltimore, MD 21202

*the opportunity to  
Comment!*

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 16 Oct 2017

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**COMMENT FORM**Name: KATHLEEN YELSHINAddress: 1525 STAR STELLA DRCity: ODENTON State: MD Zip: 21113Telephone: 410 672 7191 Email: kmyelshin@yahoo.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I have some serious reservations about this project. Where is the power coming from? Ft Meade had to build an entire power plant to supply its needs - this is another major drain on the system and I am not convinced BG+E can handle it with its current capabilities. That means a new power plant - where will that go? What will drive it - renewable energy, coal, nuclear?

I am also concerned about the safety + security of locations along all routes. Ft Meade has a lot of unexploded ordnance on its property, as does the Patuxent Wildlife Refuge - it used to be a live fire training area. That will cost a lot to clear - who pays? Who pays for the fire stations

(continued)

Comments: and schools that will be in the path of  
this project? I sincerely hope it is not me -  
I get no benefit from the project, Bowie  
will get nothing except disruption.

I will be encouraging my local community  
and political representatives to oppose  
this project. I don't feel a \$30 (or much more)  
train ride between Baltimore & Washington DC  
is worth this much of the state's money.

It will destroy environmentally sensitive areas  
(Wildlife Refuge, Beltsville agricultural research  
zone) all for the sake of building a cool toy  
that may never connect anywhere else along  
the eastern seaboard. From what I understand,  
other communities stand in opposition as well.  
Is it really worth the risk to Maryland's  
taxpayers?

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DATE 10/16/2019

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**COMMENT FORM**

Name: Andre Williams  
 Address: 8239 Clark Rd  
 City: Ft Meade State: MD Zip: 20755  
 Telephone: 410-305-4000 Email: Drewilliams5@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: First, Forums like this always seem to bring out Environmentalist and the tax conscience who have more Negative to say than Positive. I think the information provided lays out a well thought out plan that seeks to minimize Environmental Impact and reduce Exposure to the tax payer. With an ever increasing population ~~and~~ widening roads will not reduce the impact to our daily commutes. Only creative solutions like ~~the~~ investment in public transportation and City planning will help our state and region. I for one support the



Comments: proposal. My only concerns are the day to day cost to the individual and the ~~amount~~ number of stops. The cost may affect ridership as \$40+ per trip over such a short distance is cost prohibitive. The lack of a fourth or fifth stop suggest the purpose of this project is only to move affluent riders from Baltimore to DC. The Maro already serves that purpose albeit on a less efficient scale.

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6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/16/17

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**COMMENT FORM**

Name: Tom Michaels

Address: 802 Seneca Drive

City: Odenton State: MD Zip: 21113

Telephone: 410-672-6820 Email: PTAS 2704@vizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This train project will ruin  
our Odenton / Severn communities and  
will do nothing to enhance the  
local economy.

Using and upgrading our current Amtrak  
Acceletrain is the way to go.

STOP This  
Train!!!

Comments: \_\_\_\_\_

STOP This  
Again Project !!!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/16/17

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**COMMENT FORM**

Name: Sandra Vogel  
 Address: 1302 Gill St  
 City: Odenton State: MD Zip: 21113  
 Telephone: 410-674-3938 Email: THEVOGEL4@MSN.COM

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This Maglev project is threatening to affect hundreds of thousands of citizens by displacing them, lowering property values, and adding to the noise and eyesore problems of large overhead tracks. The only people who would benefit from this project, besides the companies making millions of dollars, are people in Baltimore, Washington DC, and people flying into BWI. The average Marylander is not going to drive, pay parking, and then a high cost ticket to use this monstrosity. Who is going to buy a house with a train running under it? Noone, and property values will plummet. Where will all the displaced



Comments: people who's homes are demolish going to live? Maryland is law on housing as it is so where will they go. How many people are actually going to use a train that is costing billions of dollars <sup>to build</sup> and that doesn't include all the upkeep. Just because something is new and fast does not make it better. This train isn't going to help with transportation issues either as we already have many public transportation options but frankly many people don't want to use public transportation and can afford not to use it. This train is a gigantic disaster. I don't want any part of it.

*Sandhu*

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DATE 10.16.17

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**COMMENT FORM**

Name: ANN TODD

Address: 1241 DUKE LANE

City: ODENTON State: MD Zip: 2113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Why why why HAS IT TAKEN  
SO LONG FOR THIS "SCREENING" TO COME  
TO PUBLIC LIFE.

People spend THEIR LIVES BUILDING A  
home & finding A neighborhood TO RAISE THEIR,  
FUTURE GENERATIONS?

Why ARE THERE NO "BIG WIGS" here TO  
ADDRESS OUR CONCERNS?

ALL MEETINGS ARE BIGGER BACAADS MORE  
CONFUSION & NO INFO. (ONLY AIDs THAT  
CANNOT ANSWER OUR QUESTIONS.

I AM STILL NOT CONVINCED THAT MY  
HOME IS SAFE. NO CLEAR ANSWER AT ALL

Comments: meeting,

ELECTION YEAR!!

my confidence has been ERODED

Send These TRAINS TO <sup>Gov.</sup> Hogans home neighborhood.

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DATE 10/16/2017

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**COMMENT FORM**

Name: Dee Zenker

Address: 8334 Dubbs Dr

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: deez21144@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Money could be better spent on current infrastructure. No clear answer about money needed to sustain this technology. Do not like impact on established neighborhoods and building. One route would take over the volunteer fire dept which would not be moved/rebuilt by tax money - would have to be raised locally. Health concerns - noise pollution, construction issues, magnetic fields, vibrations above the underground areas. People losing houses or houses losing value cuz of proximity to train. Cost of ticket on train expensive.

Comments: no commuter stops so will not help  
the working citizen in this area.

This is a ridiculous use of tax  
payers money

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c/o Suhair Al Khatib  
Maryland Transit Administration  
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Baltimore, MD 21202

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

- Alternative #1 proposed tunnel Transition Portal

Name: Christine Gibson

Address: 1243 Scots Manor Ct

City: Odenton State: MD Zip: 21113

Telephone: 443-956-1831 Email: Christine-gibson09@comcast.net

Comments: The proposed route along 175

through the Odenton area would  
severly impact the Odenton area if the  
Odenton Fire Hall is impacted. This  
is a strictly volunteer only station.

If it has to be rebuilt, taxpayers who  
cannot afford to pay for the cost to  
re-build it would be affected. There  
is not a close enough station if an emergency  
comes up.

No commuter access to this area, a resident  
would have to drive to BWI to park.

No one deserves to loose their home  
for this costly train that has no benefit  
to the area.

Please re-consider the proposed routes  
For this train! Thank you.

Comments: This is for the Alternative  
E1 proposed tunnel Transition Portal  
These are old neighborhoods that  
many people have lived in the area  
for a long time and cannot afford  
to re-locate.  
Please stop this very costly-non-  
beneficial train to this area.  
Commuters have access to the Metro,  
the Mare Station and there are  
bus services in the area.  
Please stop this.  
No one cannot afford this on a daily  
basis.  
There are brand new townhomes in  
this area as well.  
No one deserves to lose their home!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Karen Bemis  
 Address: 2506 Country Oak Ct.  
 City: Odenton State: MD Zip: 21113  
 Telephone: 410 695 2744 Email: bemiskp@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

The proposed routes have a direct impact to my home value. I moved to Maryland in 2005, at the peak of the real estate market high, I don't want to see my largest asset ruined. If the Amtrack route is selected, that will also ruin home values - all the properties close to it will turn into rentals which will ruin the community. It is ridiculous to spend 27M for a study for a train that won't even provide relief for commuters. Seems like a waste of tax payers \$.

It could be better used for our school

Comments:

System ; horrible road conditions.  
There needs to be more transparency  
with what this rail will actually  
provide — it won't relieve traffic  
is it will be too expensive for the average  
commuter.

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Baltimore, MD 21202

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DATE 10/16/2017

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## COMMENT FORM

Name: Tom Edwards  
 Address: 609 Yearling Ct.  
 City: Severn State: MD Zip: 21144  
 Telephone: 443-688-6096 Email: tedwar133@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have several concerns about this project. I want to make sure it is safe for the environment and communities that will be located around/under/over this train. How will the high voltage electricity and radiation be contained. Will the severn run and other wetlands be impacted? I am also questioning the need for this project. Because there are only 3 stations this is clearly not a commute train that will help reduce traffic. I question the amount of people who will regularly ride this and justify the expensive cost. I also want to know how much the tickets will be? Will they be cheaper than the existing MARC Train, and if not why would

Comments: people ride this train instead when the stations are further from peoples houses? Also will the cost of buying all of the homeowners out who will have houses destroyed by this project as well as the cost of tunnels still allow this project to be profitable? I am worried by the wording that the routes are not set in stone and that the company building this project will push to build more sections overhead which will cause more people to lose their homes. Plus I know the eventual goal ~~is~~ is to extend it to New York, but what is the likely hood of that as those areas are ever more populated and congested with existing buildings and Tunnels the Maryland

Please Justify this Project!

Don't Impact the Families that live here

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Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/16/2017

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**COMMENT FORM**

Name: Christina Zimmerman

Address: 1330 Huntover Drive

City: Odenton State: MD Zip: 21113

Telephone: 410 674 7306 Email: christinazimmerman@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The current proposed orange line that goes along Amtrak in Odenton will likely go through our local fire station. We would need a new fire house or this will be a terrible idea for our Odenton community. It also still destroys a few Odenton communities. I don't like any plans that destroy communities, forests/wildlife areas, schools, etc. I want to continue to enjoy green spaces, such as the WB and A trail. I look forward to seeing the AACO side connect to the P&CO side, but with no train/tracks/tunnels, sounds, etc. I don't see how this train plan would ease commutes either. It seems that improving existing transportation lines, (i.e., roads, MARC, light rail, etc.) would be a better place to invest all this money. Who will pay for this? ->

Comments: Maryland cannot pay teachers the raises they should get according to their published pay scales, so why or how should the state take on this type of tax burden. I am very against the Maglev train, anywhere in our state.

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DATE 10/16/17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Barbara Baschenic  
 Address: 5931 Linthicum Lane  
 City: Linthicum Heights State: MD Zip: 21090  
 Telephone: \_\_\_\_\_ Email: Bergave@cable-speed.com

Comments: My husband and I are firmly, 100%,  
against the Maglev project. Most projected routes  
will impact my community of Linthicum and not  
in a positive way. I have spent my whole life in Linthicum.  
I grew up there and raised my children there. I am now  
helping to raise my grandson there. It was bad enough  
that we had the Light Rail put in our community against  
the majority's wishes. Now we deal with increased  
crime in our neighborhood as well as suspicious  
people near our elementary and middle schools. The  
Linthicum community is tired of having to accept these  
big projects that will ultimately lose money and  
be detrimental to our way of life.

Please - NO MAGLEV IN MY BACKYARD !!  
(OR ANYONE ELSE'S!)

January 2018 It will not benefit anyone in our beautiful  
 communities.

Comments:

Also, Linthicum is a community that will be greatly impacted by this project. Our delegate, Pam Beidle, has tried to set up a meeting in Linthicum, with No success.

At least I have the chance to email and write my representatives, although if history repeats itself, my messages will fall on deaf ears except for those of my LOCC1 representatives.

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Hanover, MD 21076

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DATE 10/16/2017

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**COMMENT FORM**

Name: Gwen Knaub

Address: 1337 Passage Drive

City: Odenton State: MD Zip: 21113

Telephone: 410-693-2145 Email: gwen.knaub@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am against this train in any shape or form. This will not help the Maryland people through the 40 miles of this particular proposal. We have no idea if the continuation of the additional 760 miles to new york will ever be completed. The cost spent for this 40 mile stretch could update all of the Amtrack lines to New York to make that line more effiecent and remove freight issues. I am happy the yellow & Red lines have been set aside due to the destruction of 3000 homes including mine. However the Amtrack line will remove our fire department and run through an elementary school which will force redistricting of the schools. This will not help the people who it will effect and displace. I am middle

Comments: class and have never taken the Acela or Amtrack train to New York due to cost for myself or family of 4 I have taken the mega bus and paid \$55.00 for a day to New York for myself other than that we take our car.

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## COMMENT FORM

Name: Matthew Schmit  
 Address: 829 Snow Valley LN  
 City: Odenton State: MD Zip: 21113  
 Telephone: 703-371-4958 Email: MSchmit\_2000@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: To date it is not clear to me who  
benefits from this project. I think this  
entire project would be a huge mistake.  
A few points:

1. The price point for tickets and the lack  
of local stops means commuters to  
DC and Baltimore cannot use it. So this  
will have no effect on local traffic congestion.
2. The number of people who would take  
this train each day would not justify  
the tens of billions of dollars needed for  
construction and operating costs. This  
train will not be able to operate without

Comments: significant taxpayer subsidies.

3. Construction of this train would disrupt local traffic and force many people out of their homes.

4. There are no noteworthy efforts to continue this line up to New Brk. What if Maryland builds this and then the other states don't follow suit? Maryland would be stuck with a costly white elephant.

5. Why do we need this? Amtrak has announced plans to improve Acela Service in the Northeast corridor. Why would anyone pay a premium to take Maglev? This train would just be a luxury convenience for a few rich people.

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DATE 10-16-17

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**COMMENT FORM**

Name: Kathryn G Dailey  
 Address: 2156 Commissary Cir  
 City: Odenton State: MD Zip: 21113  
 Telephone: 410-674-4127 Email: kawilson71179@gmail.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I have yet to ~~be~~ be convinced that this is  
benefiting the areas most impacted. It will not  
impact the traffic in our local area as the stop  
are not local. Even if there were going to be  
local stops, I am not convinced it's worthwhile  
in any way. The cost to travel 40 miles is  
astronomical. And with no guarantee that the  
train will go all the way to Boston, how do  
we know we're not just getting a ~~tricky~~  
huge expensive train that makes 15 min  
jaunts between Baltimore + DC? ~~I~~ I understand  
this is in the very early stages but it feels like  
information has been so scarce that something  
must be going on that residents don't know



Comments: about. I look forward to seeing upcoming  
info be more publically available.

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DATE 10/16/2017

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**COMMENT FORM**

Name: Eileen Messer

Address: 1525 Manor View Rd

City: DAVIDSONVILLE State: MD Zip: 21035

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My initial questions are about safety. There was very little information on the safety of the system. "State of the Art Safety" system doesn't mean anything to me. When a train malfunctions or derails or has a fire, how are the passengers and the people who live above the tunnels protected?

What will the expense be of a ticket to ride? Amtrak is already over-priced for most people. DC's metro is also very expensive when compared to other subway/train systems in the world, such as NYC, or Paris. The riders who can afford a train like this are not the drivers clogging up the highways. It is the average person, living in less expensive housing, commuting to

Continued from front

Comments: More affluent areas for work.

The excitement about bringing more jobs wanes when you consider the need for qualified tradesmen/people. Maryland is already hurting for qualified people to perform skilled work. BWRR Needs to be recruiting Now, in highschools like Arundel, South River, Southern for people to commit to learning a skilled trade. Otherwise, the skilled workers will come from afar and take the funds with them when the work is done - not a complimentary economic benefit!

What will be a realistic maintenance plan? DC Metro let maintenance slide for years. Now riders and taxpayers are literally paying for it. What is a truthful report of the safety and maintenance as experienced by Japan?

I would like to see the continued path to Philadelphia and NYC. <sup>there is a</sup> much greater impact on Baltimore when you look beyond map #6 as provided tonight. How much will eminent domain affect my neighbors?

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DATE \_\_\_\_\_

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**COMMENT FORM**

Name: Kristen Brown

Address: \_\_\_\_\_

City: Odenton State: MD Zip: 21113

Telephone: 410-493-7242 Email: KABROWN2007@MSN.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am a DC commuter who doesn't think that this project will benefit the community in the long run. This project will impact a largely military community who houses could be impacted. Those families will never get the true value of their houses.

I have many concerns one of which is security. As a member of the law enforcement community I can see this infrastructure as being a target. The lines will go near some of the most critical infrastructure (NSA/ Ft. Meade) in the nation. I would be more receptive of this project if you went through a less populated route (Frederick, Montgomery Co) up to New York/ Philly. Amtrak's Acela already carries passengers to Baltimore. If you take a completely different route through less populated counties the impact wouldn't be as great. ~~and you're~~ Western Anne Arundel is the fastest growing in the state. I just don't see

Comments: the point of this project <sup>through</sup> ~~is~~ this region

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**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**Name: Alan Ibele

Address: \_\_\_\_\_

City: Odenton State: MD Zip: 21113Telephone: \_\_\_\_\_ Email: cylindricalFile@hotmail.com

Comments: \_\_\_\_\_

- The public outreach/notification conducted for this project between 2016 and spring 2017 was insufficient. Extensive notification efforts should have been focused along the proposed corridor, as opposed to at the termini.

- Any build/no-build decision should be made only after considering:

- Potential economic impact to existing Amtrak services
- Whether ridership at projected price point justifies cost to tax base and impact to property owners
- Whether benefits are evenly distributed across socio-economic strata (i.e. do only those wealthy enough to afford a ticket benefit, while those having their property re-purposed belong disproportionately to a less-privileged demographic),

Comments: • ridership, reliability, and maintenance costs  
for similar projects completed elsewhere.

• I find SCMaglev's counterexample on the handout distributed at the 10/16 Anne Arundel Hs open house (under "This train is only for elites. where will SCMAGLEV riders come from") to be slightly disingenuous. No one would dispute that SCMAGLEV's travel time savings would be "highly valued". The operative question is whether it would be affordable for the average working class commuter.

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DATE 10/16/17

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**COMMENT FORM**

Name: Dr. Karina Golden

Address: 4902 Raemore Lane

City: Bowie State: MD Zip: 20715

Telephone: \_\_\_\_\_ Email: ~~XXXXXXXXXX~~ Kcivishn<sup>u</sup>@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: - We are against this train. It is too expensive and has not had any success in other parts of the world.

- Money should be spent to improve infrastructure of current rail system - MarC Train, Metro, & Amtrak - rather than disrupting neighborhoods that will not benefit from the proposed MAGLEV train.

- There will not be enough ridership to support <sup>MAGLEV</sup> and federal and state funding should not be used to support MAGLEV when repairs to current system are needed.

- We are concerned about health effects - especially long term.

Comments: - Putting the train underground  
does not help the problem! It  
will still be too disruptive.  
PLEASE STOP THIS TRAIN!

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DATE 10/16/17

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**COMMENT FORM**

Name: Carol Scott  
 Address: 310 Fairfield Dr.  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-551-6377 Email: cscott282@verizon.net

Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: <sup>①</sup> I am outraged at the proposed route through <sup>the</sup> Severn & I Route. <sup>②</sup> My home is directly involved in this route. <sup>③</sup> It doesn't make sense to severely impact already existing home where noise, vibration, & financial impacts would greatly adversely affect us & affect our health & financial viability. There are significant environmental risks. <sup>④</sup> We were <sup>NOT</sup> given an opportunity to comment on this proposal when the initial scoping began, which is terribly unfair. <sup>⑤</sup> The BWI phase is a much better idea since it goes along the highway. There are more government ~~houses~~ <sup>business</sup> buildings, which would not as severely affect home owners. <sup>⑥</sup> The expense

Comments: on home owners & those using the system doesn't make sense

\* We were NEVER given notifications for a chance to comment when this process began INITIALLY 2/14/14

Please, please don't do this noise, home values, vibration environment. I am sick about this

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DATE Oct. 16, 2017

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**COMMENT FORM**

Name: Jessica Snipes

Address: 719 Northwood Estates Dr.

City: Severn State: Md Zip: 21144

Telephone: \_\_\_\_\_ Email: wgjc709@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

I would have liked to have known about this before this evening. Our development, Northwood Estates in Severn would be directly affected by this project. Concerns - environmental impact, tunneling under will cause our property value to drop tremendously. I should have been informed a long time ago that my area was in the plan - and now to see it has continued to be an option (E1) when other areas are "off the table."

I worry about how far away will the air handlers (ventilation buildings) be from my home (if it still is in existence). I want to know more - I'm upset and appalled this is even being considered - and for who.

Comments: Where will parking be for all these people  
to park - especially since I live so close to  
BWI - Thurgood Marshall? Why were the communities  
who could or will be impacted not notified during  
the "scoping process."

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DATE 10-16-17

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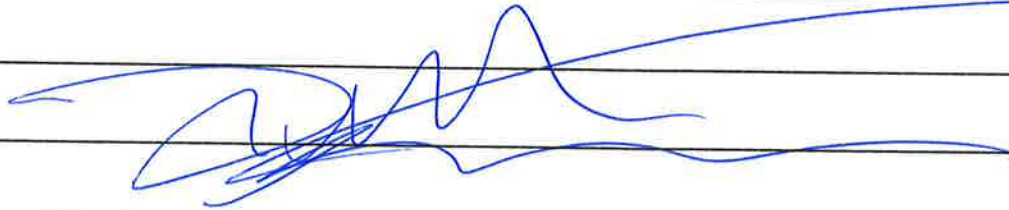
**COMMENT FORM**

Name: DEVON MEDDLETON  
 Address: 2719 HICKORY KNOLL CT  
 City: ODENTON State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Comments: I AM VERY INTERESTED IN THE FINDINGS OF THE ENVIRONMENTAL IMPACT STUDY - I AM TRYING TO KEEP AN OPEN MIND - HOWEVER - THROUGH PRIVATE RESEARCH AND DISCUSSIONS AMONGST THE COMMUNITY - IT IS APPALANT THAT THE MAGLEV TRAIN IS NOT GOOD FOR MD IT BENEFITS VERY FEW AND THE COST DOES NOT WARRANT IT'S CREATION. THE MONEY IS BETTER SPENT TO IMPROVE THE EXISTING INFRASTRUCTURE OF TRANSPORTATION. I CANNOT STRESS ENOUGH THE IMPORTANCE THAT THE RESIDENTS OF MARYLAND WILL BANNED TOGETHER TO DEFEAT THIS. WE WILL NOT SUPPORT REPRESENTATIVES THAT SUPPORTS THIS AND WILL VOTE

Comments: AGAINST ANYONE THAT SUPPORTS  
THIS IN NOVEMBER 2018

THANK YOU,



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DATE 16 OCT 2017

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**COMMENT FORM**

Name: CHRISTA HAYATI  
 Address: 1210 GRAYBURN CT  
 City: ODENTON State: MD Zip: 21113  
 Telephone: 410-507-4661 Email: LOGINNAME1970@GMAIL.COM

Comments: The main theme put forth at this open house is that the train is needed to relieve congestion. In this area, the train will do NOTHING to relieve congestion because it will not stop here. With that in mind, any route that comes near this area should go out of its way not to disturb communities that it will not serve. That devastating long-standing and newly developed neighborhoods in this area was even a consideration is unconscionable. What is needed more than a shiny new object for politicians to pat themselves on the back for is upgrades to the rail and road infrastructure already in place. Amtrak serves this community but is badly in need of upgrades. THAT is what

Comments: will relieve congestion in this area. More and better Amtrak trains would help this community get to its destinations in Baltimore and DC, unlike the MAGLEV.

The communities that will be devastated by a train running through are exactly the communities that all politicians pledge to help: the middle class. Seized properties and plummeting property values in this area will impoverish a wide swath of middle class that includes military and federal employees who call this area home.

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DATE 10-16-17

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**COMMENT FORM**

Name: Kathy Love  
 Address: 12815 Kemper Ln  
 City: Bowie State: MD Zip: 20715  
 Telephone: \_\_\_\_\_ Email: Klove268@gmail.com

Comments: I am opposed to the MAGLEV train.  
The expense is exorbitant and I can  
see no benefit to this train. Just  
make current public transportation  
better. I can only see this as  
increased costs to Marylanders. I  
feel the environmental impact  
is not beneficial to us either.  
People's homes & businesses will  
be lost and how will our  
health be affected by this  
technology ??? What about  
the impact on schools and our  
children? Please don't allow

Comments:

this to happen.

Also, why haven't we heard anything about this until just now?

I know of no one who would use this.

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DATE 16 October 2017

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## COMMENT FORM

Name: Valerie Boshane  
 Address: 7856 Dogwood Blossom Rd.  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-969-6375 Email: ValriB10@gmail.com

Comments: 1. What are projected ridership numbers? Are/will they be based on actual current Acela/Amtrak/MARC ridership?  
 2. How will this take large numbers of cars off the road between Baltimore and Washington, as most commuters ~~get~~ come from the suburbs, and do not commute between cities?  
 3. No other states have committed to building this line—how will this be self-sustaining?  
 4. Will this be ultimately paid for by taxpayers when it loses money?  
 5. The German Maglev was a bust and was abandoned. The Chinese line is subsidized by the Communist Chinese government. The Japanese SCMaglev will not begin (in part) operating until 2027, and has requested \$1 Trillion






**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**Name: JULIE MAIRAddress: 494 Williamsburg LaneCity: Odenton State: MD Zip: 21113Telephone: 410 695 5784 Email: jsmair@gmail.com

Comments: This project was (and is) ill-conceived from the beginning, especially as it impacts Odenton and surrounding neighborhoods.

It disproportionately affects Odenton residents and other <sup>residents in nearby</sup> neighborhoods. There is absolutely no benefit that accrues to Odenton - it doesn't stop here - it just runs through a community destroying the quiet, disrupting traffic flows, causing a huge unattractive eye sore, lowering property values, exposing residents to risks of health issues, not yet determined in extent.

It is obvious it is being built for the downtown communities at the expense of the suburbs.

Odenton is a middle class, working class neighborhood. We are teachers, nurses, police officers, firefighters, and military personnel. We serve our community, city, state, and nation. Our major investment is our homes. We live in our homes for decades. Building →



Comments: this huge projects completely destroys our quality of life and the future ~~man~~ in the sense of financial stability. In addition, the City of Odenton has a town plan that the route through Odenton will completely disrupt.

2) There are alternatives. There is not a need for such a high speed train. Amtrak can be expanded, metro can improved, etc, etc. Amtrak is already increasing its speed; it just needs more tracks so more trains can run.

3) destroys wild life, historic areas, and other protected spaces unnecessarily.  
In conclusion, this project which sacrifices Odenton for the possible benefit of an elite few is an unconscionable project that should receive a "no build" determination. It will not serve enough people to warrant the financial investment and continuing operating costs. Until that determination is made, the proposed route going through Odenton

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should be taken off for consideration.



4) Never notified for scoping meetings in Dec 2016 & April 2017 - very limited notification to the downtown communities, at none to those communities most affected. This had to be intentional.





DATE 10/16/17

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**COMMENT FORM**

Name: Julie Devers

Address: 950 School Lane

City: Greenbrids State: MD Zip: 21054

Telephone: 443-883-5860 Email: jbdevers@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Thank you for removing the option that goes through  
Odenton/Piney Orchard. I have two comments. The first is  
that I am concerned about the train going above ground  
through Patuxent Research Refuge. It is far from pristine  
but is one of the few green spaces in this area. The  
existing train already makes a significant amount of  
sound pollution in the refuge. An elevated railway would  
create an eyesore in the refuge. However, I understand that  
it needs to go somewhere and through the refuge is  
preferable to through people's homes.

My other comment is that I am concerned that the  
homes that are removed will disrupt communities  
that will not be served by the train. The train  
will likely be expensive (the Acela is already out of

Comments: price range for many living in the communities that will be disrupted, and I expect few in the communities will be able to afford the train. The only way to make it affordable is through subsidies that will take taxes away from schools, roads, and other much needed amenities for those living in the DC/Baltimore area. Otherwise - if no homes were disrupted and it wasn't so extremely expensive, I would support this form of public transportation.

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DATE 10/16/17

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## COMMENT FORM

Name: Linda & Marshall Johnson  
 Address: 2340 Chapel Hill Blvd  
 City: Odenton State: MD Zip: 21113  
 Telephone: 410 672 5986 Email: m.ljohns4@msn.com

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: Very frustrated by the approach for this project -  
no notice for first meetings - 2 of the routes were  
just blocks from my home - no excuse! It made  
this project look shady from the start.  
Who really benefits? #'s were provided in terms  
of projected growth, but there were no facts to  
support the projections!  
Images were shown of easing traffic congestion -  
really? How will people get to the respective  
stops? -- they will drive! What portion of the  
27 mil study is being spent on that?  
Ridership? - only people at the 3 respective  
stops - since no other stops along the way people  
who live along the route won't benefit.

Comments: I have 2 kids in NYC - They take the Megabus because Amtrak costs too much - Why aren't we investing in infrastructure to make it more efficient & affordable? This doesn't help the average person.  
Piney Orchard is a lovely community - Yes we have airplane noise & we hear the trains & the shooting range on Fort Meade. This area has wetlands & was a bird sanctuary. It has parks, schools, bike trails & truly a community feel.  
While I can appreciate the desire to advance technology, I worry about the lack of care our leaders, companies, etc seem to take in preserving communities.  
- And then you wonder why kids don't respect their communities. This project is based on a 30 mile long piece track in Japan that is still testing & has been evolving technology for 10-30 years. What's more now can we make promises of connections to NYC when PA, DE, NJ & NY aren't even close to making this a reality - Time to STOP the train & USE

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

TAXPAYER STUDY \$  
 to improve & make  
 what we have more  
 efficient & effective.

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/16/17

bwmaglev.info

COMMENT FORM

Name: Marilyn Manners

Address: 1309- Tenbrook Road

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No U.S. Mail

Preferred method of communication?  Email  Mail

Comments: I'm against the B-W Super-Conducting MAGLEV Project! The study cost of \$27.8 million is too MUCH! The construction underground will environmentally harm students at 15 schools! Where are the electromagnetic environmental studies, professional trends, historical facts, and feasibility studies to cover cost for health risks? This reminds me of the movie Silkwood. How much money will you have down the road to pay for lawsuits? Our children would be potential riders. The electro-

For a few miles.

Magnetic damages will harm their future lives. This does not make sense. Scrap the project. →

Comments: What about historical sites and Cemetarys that will be in the way? Your representatives and Scoping process was insufficient (500 cards to D.C., and Baltimore). You have a lot of money so why not spend it on notification? Apparently you know how to notify based on tonight's Open House. The Anne Arundel County Transportation Plans of 2008, pages 29-30 shows the MAGLEV. We are just now hearing about this. There's no separation of parties with BWRP and MAGLEV collecting data and overseeing the project. I don't want the Superconducting MAGLEV in my community. It's a waste of Federal Funding, ~~crucial~~ crucial to the wetlands and health

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

of our children, **SCMAGLEV Project** costly for the planned  
 short route, a hazard c/o Suhair Al Khatib Maryland Transit Administration  
 noisy and disruptive. 6 Saint Paul Street Baltimore, MD 21202 to property too  
 for our living in harmony,

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Jerome McDonald  
 Address: 1301 Berni Ruth Lane  
 City: Severn State: MD Zip: 21144  
 Telephone: 7039898507 Email: jerry.mcd1@yahoo.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: My wife and I just moved to the  
Shannon Green neighborhood. We have  
moved here 2 months ago. The proposed <sup>(yellow or orange line)</sup> underground tunnel is targeting our house  
and community. We think it is a horrible  
idea to run a tunnel underneath a well  
established community that has been around  
for 25 years. There are potential health  
risks that have not been discerned  
if we were to remain there and the tunnel  
is constructed. Also the government has  
the right of eminent domain we are aware  
and the home owner is always shorted  
when it comes to that. They should cease

Comments: and desist the construction of this tunnel  
and ultimately by the Maglev. The state does  
not need this means of transportation  
at this time.

concerned citizen  
Jerome W. McDonald

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10/16/17

bwmaglev.info

**COMMENT FORM**

Name: Damita McDonald  
 Address: 1301 Berni Ruth Lane (Shannon Green subdivision)  
 City: Severn State: MD Zip: 21144  
 Telephone: 919-623-6650 Email: damita\_chambers@yahoo.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: My husband and I recently bought our house in the neighborhood we chose for its charm and ease of commute to our jobs. It's turning out to ~~to~~ be the best choice for our family to have relocated to this beautiful community. The disruption caused by the addition of a Maglev tunnel is something I cannot imagine. A community of tight-knit neighbors in a welcoming <sup>environment</sup> ~~community~~ would be ~~be~~ destroyed. How could we considering raising our future children in a house impacted by an ~~un~~untested technology? Or maybe we would not even get to keep our house because the project would require that ~~or~~ it would be taken by eminent ~~dom~~ domain. Either way, the life we envisioned for our family would be rent apart. A home, a community, is something we all seek. My



Comments: husband and I have found that in the  
Shannon Green subdivision, please do not pursue  
the tunnel under our neighborhood.

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SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/16/2017

bwmaglev.info

**COMMENT FORM**

Name: BARBARA SHANKS

Address: 1530 STAR STELLA DR

City: ODENTON State: MD Zip: 21113

Telephone: 443-889-6689 Email: BARBARA@SHANKSETAL.COM

Join our mailing list?  Yes  No ALREADY ON IT

Preferred method of communication?  Email  Mail

Comments: WHAT WILL THE BUILDING OF THIS TRAIN COST? WHO  
WILL PAY THE BILL? JUST BECAUSE TECHNOLOGY IS THERE  
DOES NOT MEAN IT IS PRUDENT TO USE IT. THE MASSIVE EXPENSE  
OF THIS TRAIN SEEMS LIKE A HORRIBLE MISUSE OF FUNDS  
WHICH WILL NEVER BE RECOUPED.

I HAVE READ THAT THIS TRAIN IS "SAFE", BUT UNLESS  
IT IS PROVEN THAT THE ELECTROMAGNETIC RADIATION FROM THESE  
EXTREMELY HIGH POWER MAGNETS POSES NO THREAT, ~~IT IS A~~ CALLING  
IT "SAFE" IS AN UNSUBSTATIATED CLAIM. STUDIES NEED TO BE  
DONE TO ANALYZE POTENTIAL HEALTH EFFECTS BEFORE ~~BE~~ SINKING  
MORE MONEY INTO THIS PROJECT.

WHAT BASIS EXISTS TO CLAIM THAT THIS TRAIN IS EVEN  
NECESSARY? TRAFFIC ON LOCAL ROADS (I95, BWI PARKWAY) IS

Comments: OF THIS TRAIN, BECAUSE "IF YOU BUILD IT, THEY WILL COME"  
EXCEPT WITH ONLY A FEW STATIONS COMMUTERS WILL STILL BE  
ON THE ROADS.

NO BUILD IS THE ONLY SAFE OPTION.

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DATE 10-16-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Robert Fam Smith

Address: 1487 Chatham Court

City: Crofton State: Maryland Zip: 21114

Telephone: 410.721.6458 Email: robert.fam.smith@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concerned about several factor -

1) Two of the proposed Maintenance yard locations, especially the one highlighted for the E1-Modified Amtrak ~~route~~ Alternative, significantly cut into parkland of the Patuxent Wildlife Refuge. The other y proposed location, at off the J+J-1 alternatives, ~~southwest~~ along the northeast perimeter of the Refuge, running along ~~1977~~ Rt. 198, also cuts into the ~~front~~ Refuge boundaries. ~~There~~ (Also, that property was successfully approved by citizen groups to be used as industrial space ~~the~~ immediately adjacent to the Refuge.) Concern about it impact on the integrity of the Refuge + wildlife sanctuary it provides, especially with the continued diminishing contiguous green spaces for wildlife to travel in the area.

Concerned about increased development it may spur in the area.

January 2018 Page C-914  
 Would like a public presentation of the Electro Magnetic Field + Radiation that will come with the operation of the train. This cannot be limited to the



Comments: just the viaduct itself, but also, more importantly, the power generating stations that ~~would~~ must accompany the train throughout its route. Need to ~~minimum~~ establish safe minimum distance from station from any residence. There are health concerns along with residential electronic disturbances.

• Concerned about the size of the size of disturbance of infrastructure supporting the train, ~~at~~ the least of which is the 1.5 acre ~~is~~ proposed for the ventilation shafts. Understood the functional & safety needs for these shafts, but size & scale throughout the entire infrastructure to support requires lots of acreage & disturbance.

• Concerned about the tunnelling operation & its effects on the aquifers. Many of the homes in the area proposed ~~for the~~ along the underground sections are on well water & could be impacted. Also, could impact flow of aquifers throughout watershed. Would like this impact presented to public in future meetings.

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Baltimore, MD 21202

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DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: ELENA BOZHKOVA

Address: 2846 SETTLERS VIEW DR.

City: ODENTON State: MD Zip: 21113

Telephone: 410 695 1662 Email: EBOZHKOVA@

Join our mailing list?  Yes  No YAHOO.COM

Preferred method of communication?  Email  Mail

Comments: I WOULD LIKE TO REQUEST  
INFORMATION FROM BWRR ABOUT:

- NOISE

- VIBRATION

- ELECTRO MAGNETIC WAVES FOR MAGLEV TRAINS

THE INFO I WANT CAN BE GENERIC  
& BASED ON EXPERIENCE ELSEWHERE, FOR  
EXAMPLE IN JAPAN.

I AM ALSO STRONGLY OPPOSED TO THIS  
PROPOSAL FOR THE FOLLOWING REASONS:

- NO WAY THIS IS FINANCIALLY VIABLE <sup>8M</sup> <sub>DC2 Balt.</sub>

- THIS IS UNNECESSARY & WILL ONLY

CREATE NOISE, POLLUTION & NEGATIVE ENVIRON-



DATE 10/16/17

baltimorewashingtongmaglevproject.com

## COMMENT FORM

Name: Debbie Wellens  
 Address: 408 Historical way  
 City: Lithicum State: MD Zip: 21090  
 Telephone: 410-859-1177 Email: RNWELLENS@hotmail.com

- Comments: - This won't take traffic off the roads it doesn't have stops that help most Marylanders - No one will drive from Odenton to BWI to catch a train. Instead build + improve roads that help <sup>everyone</sup>
- There is already a MARC train + Amtrak - they are much better options because they stop and pick up commuters along the way
  - No way to get up to 311 mph from Baltimore → BWI and vice versa - ~~so it is~~ In addition when you make a stop that adds time - Take the Acela it a better option
  - improve what we have for railways - This won't be a large time savings
  - ~~Too expensive~~ - tax payers will have to pay to help build + sustain
  - This only will have a small percentage of riders like tourist and rich people, maybe some business men/women not from the area - Instead of helping a ~~small~~ very small percentage of the people; why not help lots of people and improve roads



- Comments: - On top of the cost of the train, the people will need to pay for parking (no free parking @ Balt, BWI or Washington)
- Then when they get to where they are going they will have to pay for additional transportation to get to their final destination, - NOT COST EFFECTIVE for most people
- ~~What about~~ what about health concerns associated with the Maglev magnets + radiation fields, also the pollution from the air shafts from the ~~tunnels~~ tunnels
  - This could also effect ~~vegetation~~ <sup>vegetation</sup> and green spaces, air quality
  - What about all the ~~lose~~ <sup>lose</sup> communities that will ~~lose~~ <sup>lose</sup> business + homes for a train that won't be used and will cost all taxpayers money? - Para
  - What about the lost tax revenues from not having all the property taxes from those homes or businesses
  - This train has failed in many locations already how about you study why it failed because this is a waste of money + time
  - It might never make it to Philadelphia, NJ, NY + Boston they probably won't want it either
  - Destroys foundations of homes + buildings

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

**SCMAGLEV Project**  
 c/o Mr. Bradley M. Smith  
 7201 Corporate Center Drive  
 Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10-16-17

baltimorewashingtonscmaglevproject.com

## COMMENT FORM

Name: Edward Mysoski  
 Address: 4804 Rocky Spring Ln  
 City: Bowie State: MD Zip: 20715  
 Telephone: 301-805-1839 Email: edthefed@verizon.net

Comments: Rail Maintenance Facility on MD197  
Given the proximity of the Maintenance Facility  
next to the Patuxent River it would be next to  
impossible to keep the Facility from polluting the  
River.

This DC to Baltimore route is based on the  
idea that extending the line to New York City  
will happen. That would require Delaware, Pennsylvania,  
New Jersey and New York buys into the idea. If not  
the line is not economically viable between Baltimore  
and D.C. The 40,000 daily MARC riders will not  
support the Maglev as only 30,000 ride between  
DC and Baltimore

Comments: MDOT Failed to notify people  
of the ~~the~~ involved countries in a timely  
manner. The Scoping Activity on December of 2016  
was a joke.

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DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Sally A Wood  
 Address: 6223 Groveland Rd  
 City: Linthicum Heights State: MD Zip: 21090  
 Telephone: 443-799-6613 Email: Sallya.wood@aol.com

Comments: This is being billed as a commuter train yet it could never be for commuters. The environmental impact is preclusive to proceeding. Existing trains already make the trip from Baltimore & BWI to DC. Acela can make the trip in 20 minutes without any additional environmental impacts. The cost of the ride will exceed that of Acela. Alternative updates to infrastructure are much more cost-effective and at minimal environmental or residential impact.

Aside from my environmental concerns, my home is along the proposed route as is the community's elementary school as well as the homes of our community, over 500, and historical sites. At a point when I am ready to remodel my home, I stopped planning because I can't put \$ into a house that I will



Comments: lose to eminent domain. That means you, with this proposal, are already taking my money out of the local economy. Inthcom fought maglev in 2002 and we won that battle. We are not standing down this time either.

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Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10-16-17

bwmaglev.info

**COMMENT FORM**

Name: Cheryl Brown

Address: 1608 Old Bay Lane

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: jacobdylensmom@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: With the new high speed Amtrak train, which uses existing infrastructure, why is yet another train necessary? We live in middle class working communities. Our homeowners cannot afford the cost of an airline ticket to go between DC & Baltimore on a commuter basis. What about the loss of homes, schools, parks, churches, etc? Can a price be put on that? For many of us, our home is our retirement nestegg. A substantial loss in value may mean that we can never retire and may have to start all over again.

The money would be much better spent fixing poor traffic patterns (I75 at 295), aging & insufficient highways, aging bridges, etc. The huge amount

Comments: of new construction in the Severn, Odenton and Annapolis Junction area stresses the need for better and adequate local roads.  
How much will our homes be worth when a tunnel causes constant rumbling/earthquake feeling? what about damage to foundations for the homes left behind?

Personally, you cannot put a cost on people's homes, work locations, schools etc for a company to make a profit on the "little", "working class" residents

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 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10.16.17

bwmaglev.info

**COMMENT FORM**

Name: Megan Davis

Address: 312 Archer Ave.

City: Linthicum State: MD Zip: 21090

Telephone: 443-956-7919 Email: megcdavis13@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① Any info you provided tonight means nothing without actual testimonials from communities whom have been impacted by this before. Unless you can find some positive ones in Japan where the Governor saw this shiny new toy, I suggest abandoning ship. You painted a pretty picture but I'm not buying it without reading the reviews first. You put a lot of effort into putting this together so I'm guessing that if positive testimonials existed to address community concerns you would have posted them too. Seeing as how there are none I think it's safe to say they don't exist.

② Before you even put anymore money into this you must first fix the vast amount of issues that exist with our current transportation systems like light rail.

③ If you like this shiny new toy so much, please play with it in your own yard. I'm tired of cleaning up the mess of what we already have. No more →



Comments: (4) I can promise that if this moves forward, kiss  
 re-election goodbye. You are not winning anyone over  
 with this, period.

Thank you & I hope to hear from  
 you soon, please me wrong.

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DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Rena Shylanski  
 Address: 8713 Capistrano Way  
 City: Odenton State: MD Zip: 21113  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am very concerned about the lack of communication regarding this project. Although my neighborhood and community (Piney Orchard) have the potential to be severely impacted, I have yet to receive any direct communication (via mail, postings in my community, etc.) regarding the project. For something so potentially impactful, this is unacceptable. It is critical that citizens be informed so that we may continue to express our great concern over the cultural, economic and environmental

Comments: impact that would result from this train. Based on the information shared, I do not find the potential benefits to outweigh the devastating impact that this project will have on the communities and the environment. I am opposed to the SCMAGLEV proposed project in Maryland.

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Baltimore, MD 21202

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DATE 10/16/2017

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## COMMENT FORM

Name: Vessela Valianitchanska. Marum  
 Address: 535 Shipley Rd.  
 City: Kinshicun State: MD Zip: 21090  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: Concerns: 1) The disruptive construction process, with ~~access~~ tunnel access shafts at barely 3 miles apart  
2) ~~At~~ Ground vibrations: recorded levels are at 46dB, which is ONLY 4dB less than humanly perceptible 50dB. Peds would certainly be affected.  
3) ~~What~~ What is projected GDP growth based on?  
4) What is projected environmental benefit based on? This is NOT a commuter train.  
5) What about underground water pollution from increased radiation levels?  
6) Have you thought about upgrading the existing Amtrak lines instead? - the French

Comments: TGV and the German ICF are good models.

7) The above-ground structures are really ugly.

8) ~~Who~~ Whom is this intended to serve? At what fare prices?? Right now people ride the bus between Baltimore + NY b/c it is affordable. How is ~~it~~ <sup>SCMAGLEV</sup> going to pay for itself?

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DATE 10/16/17

bwmaglev.info

**COMMENT FORM**

Name: Mike & Kim Jimenez

Address: 2286 Snowfall Ct

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: memail14@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① The trains magnets need helium to cool down... helium is a non-renewable resource that is currently nearing a world-wide shortage. How will this train run without helium if we run out? Also, helium is widely used in the medical field. Why would we consider taking away from the medical field to run a train that is competing with 3 other train lines in this area? (MARC, NE Regional & Accela)

② The project will create substations to help power the train & the cost of electricity will be factored into the tickets... what guarantee do we have that the cost of electricity will not be passed on to the community?

③ Kobe Steel has been in the news recently for selling

Comments: faulty steel. Are they a supplier of this project? How will you guarantee that faulty steel will not be used in this project?

④ Is the money that Japan has pledged towards this project a loan that will have to be repayed? Who will be responsible for repaying this? What if the train ridership ~~does~~ is not <sup>enough to</sup> sustain costs - then who will pay?

⑤ There are multiple maglev projects across the world that have failed - what will be done to ensure this one does not get added to that list?

⑥ For the above ground sections, <sup>especially</sup> whose money will be used to train first responders on how to assist in the event of an emergency?

⑦ We do not support this project because we do not see the value that it will bring to this area. To spend BILLIONS of dollars & destroy communities on a train that will be competing with THREE other/existing lines is completely unnecessary.

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Baltimore, MD 21202

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DATE 10-16-17

bwmaglev.info

## COMMENT FORM

Name: Janet RenoAddress: 1312 Wickell Rd.City: Odenton State: md Zip: 21113Telephone: 410-674-5927 Email: \_\_\_\_\_Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: What will happen to the commuter trains that are already in place? Wouldn't our economy benefit from upgrading the train industry we have? Do we need to buy Japanese?

Who will ride the Maglev? The fares will be far greater than the current train service.

Once we have gone into super debt to build this monster, we will be liable for upkeep and repairs.

At this point, we are hearing the same refrain that we heard every time some developer wanted to cram extra development down our throats: enlarge employment and improve the economy, broaden the tax base. We are tired of hearing these platitudes! There are so many

Comments: We no longer trust our government or the developers! So far, \$27,000,000.00 has been allocated just for a study. As tax payers, we were not consulted. Billions more will be spent on a very unpopular project. Our tax dollars could be spent on many other projects. Maglev seems to be putting plenty of money into Japanese pockets. Odenton has been abused by politicians and developers for years. There has not been enough planning for all of the development in the area, which has lowered the quality of life for residents. New construction <sup>that</sup> was approved by our politicians is now threatened by the newly proposed construction of the Maglev. The wildlife preserve was supposed to have been protected, but one of the proposed routes goes through it. Environmentally, the Maglev is a disaster.

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DATE 16 Oct 17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kelley Welch  
 Address: 8119 Mount Aventine Rd  
 City: SEVERN State: MD Zip: 21144  
 Telephone: 410 695 6085 Email: KiKatKel68@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

- I ride the MARC TRAIN and CAN attest to the low ridership from DC to Baltimore. How CAN MAGLEV support ridership that supports the cost to people and the environment.
- I want my state - local leadership to focus on improving the existing rail lines.
- MAGLEV will grossly & negatively impact citizen's homes, schools, churches & businesses in the name of progress ~~that~~ <sup>AND</sup> will not benefit from this ~~development~~ development. ~~I~~ Without stops this only benefits the elite or specialized traveler.
- I do not support foreign investment or loans to build a solution ~~to~~ to a problem that won't be solved with a train that doesn't serve the local

Comments:

- I Request a reopening of the Scoping process.
- I request to clear rationale for this development.
- Amtrak is a solution that should be considered above all else.

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 6 Saint Paul Street  
 Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10/16/17

baltimorewashingtonscmaglevproject.com

## COMMENT FORM

Name: PAULA DARRAH  
 Address: 206 HAWTHORNE RD  
 City: Linthicum State: MD Zip: 21090  
 Telephone: 410 375 5139 Email: RJDARRAH2015@gmail.com

Comments: As a 20 plus year resident of Anne Arundel County, I oppose the MAGLEV train. The routes through Linthicum will devastate a middle class tax paying historic community established in the 1800s. Gone would be our Blue Ribbon school; Church (St. John's) and our library. We are adequately served with Light Rail; the airport; Amtrak; the Marc Train Lines. People before \$\$, Community before 5 minutes off of a current 20 minute commute to DC or 15 to Baltimore (Light Rail). Stop this train. Invest in our schools - don't destroy them for a \$100 train ride for others who neither live nor work in our community. Invest in Amtrak - upgrade your light rail. STOP STOP STOP this MAGLEV project.



Comments:

The politics and dollars of this calamity are obvious to those of us who will suffer. My home would be non saleable while this hangs over my head. I will petition for a reduction in my property taxes when the impact of this pending threat looms over me for the foreseeable future. SAD.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Mr. Bradley M. Smith  
 7201 Corporate Center Drive  
 Hanover, MD 21076

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DATE 10/16/17

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## COMMENT FORM

Name: Donna VanDine  
Address: 8235 Quarterfield Rd.  
City: Severn State: MD Zip: 21144  
Telephone: \_\_\_\_\_ Email: dlvandine@gmail.com

Comments: \_\_\_\_\_

I see no need for an additional train in our area. We already experience increased air traffic noise and Amtrak noise. While currently my property is not in the direct path, many of my friends are/will be personally affected. I also don't think this will improve my property value. I've lived in Severn for more than 20 years, now retired and this project could have an adverse effect on my property's future. There is already public transportation to DC and NY. We don't need another train! Invest in the transportation (train, plane) we already have available. We don't need

Comments:

to disrupt more lives so we can have the first Maglev train in the U.S. I saw the devastation when people were uprooted because the airport was expanding. Don't do this to us again.

Stop this train!!!

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DATE 10/16/17

bwmaglev.info

## COMMENT FORM

Name: Benjamin DubitAddress: 7801 Locust Wood RdCity: Severn State: MD Zip: 21144Telephone: 301 412 9478 Email: benjamin.dubit@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: 1. If I live in the area of study, how come I have never received anything on this? Letters, literature, fliers, etc.... only on facebook!?

2. If my house is in the path of construction, how do you determine fair market value? Now? Once construction has started and you already made my property value go down?

2B. If my house is not destroyed, but my property value goes down because the track is a few hundred feet away, what type of reimbursement, incentive, etc is available from you?

3. What is the anticipated tax payer increase?

4. I keep hearing "construction starts in 2019", when is the route plan finalization date?

5. If my home is in the path & will be bought, what type of timeframe do I have?



Comments: 6. If there are no stops between DC & Balt, what incentive is there for us local people?

7. Have you communicated to local home builders that are in the path? (current construction)

8. What is planned with the land you waste? Will you give back to the community? Parks, schools, etc.

Multiple horizontal lines for additional handwritten comments.

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Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/16/17

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: PATRICIA NICHOLS

Address: 1259 Delmont Rd

City: SEVERN State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: pnic@aol.com

Comments: This is not something that will help the communities of Severn, Odonton, etc. This will service the people living in Baltimore City and Washington, D.C. This train is slated to run through many existing communities in A.A. City including recently built developments. This will impact people's homes, businesses, schools and their everyday way of life. I have live on the same road for over 60 years! A.A. City has continued to expand and allow more homes to be built over the last 20 years and now this train line will go right through some of the! Our areas will deteriorate and become slum areas because of this train.

This is outrageous that the communities

Comments: have not been notified prior to this!!  
typical government trying to hide what  
they are planning that affects the  
residents!

STOP THIS MAGLEV

Project

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DATE 10/16/2017

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**COMMENT FORM**Name: Brian Boudreau

Address: \_\_\_\_\_

City: Odenton State: MD Zip: 21113

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I have very serious concerns about the  
~~the~~ economic benefits of this project. ~~The~~ While I can  
appreciate the desire to have a quick route between  
Washington and New York, I fail to see how this  
project - with only three stops in the Baltimore-  
Washington region - actually does anything to  
alleviate the overcrowding and congestion on the  
local highways. Moreover, Maryland should not  
have to incur the cost of billions of dollars to  
build a trainline that will, in reality, have little  
economic impact to our local area, not to mention  
the potentially thousands of displaced families in  
this area that ~~lacks~~ already lacks enough  
affordable housing.



Comments: \_\_\_\_\_

This is simply too expensive for the benefits. The state of Maryland would see greater return on investment if it improved the already existing MARC/AMTRAK lines. Local solutions will actually benefit the citizens.

Additionally, I've found some of the staff unable to answer the more contentious questions and would recommend allowing people to submit questions online for general audiences.

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Baltimore, MD 21202

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DATE 10/15/2017

bwmaglev.info

**COMMENT FORM**Name: Ann RinkerAddress: 601 Willow Tree DriveCity: Glen Burnie State: MD Zip: 21060Telephone: 410-360-5474 Email: aprinker@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I am very concerned about this project and suspect that as taxpayers,  
We will eventually be required to foot the bill, despite the fact that it is doubtful if  
we will benefit from it. Although it supposedly will be privately funded, I doubt  
that will come to fruition without being subsidized.

I am also concerned about the impact of going through residential areas, regardless of  
whether it is elevated or tunneled. Tunnels concern me due to the possibility of  
adversely affecting the foundations of housing, & other buildings. How will it affect  
water tables, wells, septic systems, etc.?

As far as the financial feasibility, I cannot see that it will work, especially because  
it does not appear to be a viable means of going from Baltimore to Washington DC. for  
commuters, <sup>especially since there are no stops between the two cities.</sup> Who is it aimed at benefitting? How much will it cost to ride?

It is already adversely affecting property values, so if a homeowner's home is taken through  
eminent domain, it certainly won't provide them with a true market value.

Comments: are for too old to be uprooted from their homes they know and love. Can you imagine  
being in your mid-80s to 90s and being forced out of your home against your will?  
My husband's Uncle & Aunt are very close to the proposed yellow line, so that could  
very well happen to them. I know it would break their hearts!

Please, please make sure that this project is being fair to residents,  
whether they will lose their homes or will be forced to pay for this project,  
despite not being of benefit to them.

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**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 16 OCT 2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: STEVE DAUVALI  
 Address: 8115 QUARTERFIELD FARMS DR.  
 City: SEVERN State: MD Zip: 21144  
 Telephone: (410)551-0411 Email: \_\_\_\_\_

Comments: DID NOT SEE WHERE THE IMPACT  
TO PROPERTIES VALUES, PARKING LOT  
LOCATIONS AT THE FEW STATIONS,  
OR EVEN THE PROPOSED IMPACT  
TO PROPERTIES THAT THE AUXILIARY  
SUPPORT BUILDINGS WOULD CAUSE,  
THERE HAS BEEN NO ANALYSIS THAT  
I COULD FIND <sup>DATA</sup> THAT SHOWS A DIRECT  
RELATIONSHIP TO RIGOROUS VS. IMPACTED  
RESIDENTS. I AM ABSOLUTELY  
OPPOSED TO THE CONSTRUCTION OF  
THIS TRAIN PROJECT. YOU DON'T  
EVEN SPEND MONEY ON EXISTING  
TRAIN STATIONS. THESE ARE OLD  
IN DISREPAIR, DIRTY, AND  
UNDERUTILIZED. YOU DO NOT  
ADDRESS WHERE ALL THOSE "RIDERS"

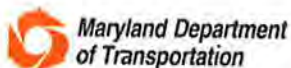


Comments: ARE GOING TO PARK AT THOSE FEW STATIONS? IF THE NUMBERS ARE WHAT YOU PREDICT, THE NUMBER OF CAR PARKING SPACES WILL BE MANY THOUSANDS. IF THE COST OF A TICKET IS ABOUT \$100/RIDE THEN PARKING IS ANOTHER \$25, WHO CAN AFFORD TO RIDE IT? WHY NOT LET THE TAXPAYERS VOTE FOR OR AGAINST THE EFFORT? MONEY COULD BE BETTER SPENT ON EXISTING RAIL LINES & TECHNOLOGY UPGRADES. IN THE END IT'S OUR MONEY, OUR HOMES, OUR NEIGHBORHOODS!

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**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/16/17

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name:

Rebecca Methugh(continued)

Address:

1282 Ava Road

City:

Severn

State:

MD

Zip:

21144

Telephone:

443 799 6694

Email:

rmchugh@aacps.org

Comments:

This project is a bad idea. The places that have had this have scrapped them. ~~Thousands~~ - Thousands of Marylanders will lose their homes or their property value due to this thing running through or under their neighborhood. 373 people in my town of Severn alone. How many people are these investors & the BWRR willing to sacrifice?! Peoples homes - businesses - green spaces all destroyed w/ no real idea of the long term environmental / health repercussions since there is no long term system to study.



Comments: all this project boils down to is the wealthy elite getting a "toy" to make MD look "progressive." I argue that destroying homes & environment is the exact opposite of progressive.

The argument is that this<sup>1</sup> will create more jobs - false - local folks won't be building this or running it. Argument 2 - It will reduce congestion - False - people love their cars & if only 3 stops - no local congestion will be relieved.<sup>3</sup> Its good for the economy - False we (Marylanders) will be left on the hook for Billions whilst the investors write it off when it goes belly up just like it has everywhere else. INVEST THIS \$ INTO OUR CURRENT INFRASTRUCTURE - THIS IS NOT a gamble MD ~~should~~ should take. It is not worth the financial - Environmental - OR Human risks!

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DATE 10/16/17

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## COMMENT FORM

Name: Rebecca Methugh  
 Address: 1282 Ava Road  
 City: Severn State: MD Zip: 21144  
 Telephone: 443 799 6694 Email: rmchugh@aacps.org

## Comments:

#1 No notice was given to AACo residents - I am in the direct "hit" area - no post card - no notice of anykind to me or any of my neighbors only 1 flyer posted in the worst area - Meade Village - REOPEN PHASE 1 !! This was hidden from us in AACo.

#2 The cost is becoming exponentially greater w/ the tunneling - Who is going to fund this <sup>all</sup>?

#3 There is no working Maglev anywhere in the world. - Why are we <sup>going to be</sup> on the hook for billions? Germany Scrapped theirs - California's project has been called "a crime" by one of its former proponents - Rhode Island & Connecticut? Have Scrapped their



Saying it was just common sense to scrap it

Comments: Project! How do we know anyone North of us will hook in? We don't.

#4 The ridership is not there. This is designed for the wealthy elite. The cost will be on par w/ the Accela (or greater) regular folks cannot afford daily commuter fees w/ this

#5 No one who is impacted in my area will benefit from any of this - This will not reduce congestion in our area & will increase it near BWI.

#6 Environmental impact - This is drilling beneath my neighborhood - My child's elem. School - what if there is a catastrophe below her School? What about my home w/ my well being bored under? My Property value <sup>water</sup> will decrease - Can you promise the Safety & Structure of my home during

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Construction?  
Who will reimburse me?

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DATE 10/16/17

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: Sharon Stewart  
 Address: 1333 Donald Ave  
 City: Severn State: MD Zip: 21144  
 Telephone: 443-591-7171 Email: \_\_\_\_\_

Comments: I AM AGAINST THIS PROJECT.

(1) How will this effect human life, a powerful magnet  
 so close to where we live, passing by us how many  
 times a day.

(2) What about the noise level? we already have the  
 other trains & planes.

(3) Damage to our houses.

(4) The value of our property will drop

(5) Construction will make traffic unbearable.

(6) Why do we need another train? isn't there already  
 a train that gets to DC in 25 mins. what difference  
 is 15 mins?

(7) Use the money to make the current roads better  
 wider safer.

Comments:

- (8) Who is pay for all this
- (9) who could afford to ride this Train surely Not the Common People!
- (10) I don't feel we need another disturbance in our area.
- (11) This is not benefiting anyone.
- (12) Not enough studies have been done on the cost to our lives when expose to these high powered magnets.
- (13) would you want to live over, under or near this thing?
- (14) what happens when we find out how the exposure affects our children, our farms, our water our environment. It will be too late.
- (15) Stop this now use the money wisely
- (16) God gave us one Earth. Take Care of it!

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DATE 10-16-17

baltimorewashingtontscmaglevproject.com

## COMMENT FORM

Name: Irene Hildebrandt  
Address: 1303 Jade Ct.  
City: Odenton State: MD Zip: 21113  
Telephone: 410 519-3442 Email: ihilde@comcast.net

Comments: \_\_\_\_\_

I have long voiced that we need to address improvements in our transit/transportation systems in the USA. Having travelled in several countries ~~and~~ <sup>now</sup> seen what is available, we are not competitive! That said, I am ~~not~~ inclined to support any of these options. I am glad to see that the route going thru Pine Orchard has been deleted. It is still not clear to me how any person could possibly think that there is an ounce of sense in that route. BTW I do not live in Pine Orchard but that is a no brainer

Comments: <sup>5</sup> I think that we spend 27 million on planning & we have no funding identified to construct makes me question this whole process.

Perhaps the survey that is needed is how do citizens rank their needs. Undoubtedly Health, Education, Senior Care, even Park Lands would come out above this train. Address what is needed for people to live, eat, stay healthy and other survival needs before we spend any more money on this.

I know Governor Hogan is very much in favor of this. I supported him in his election, but pushing this down our throats will change my mind next time.

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DATE 16 Oct 2017

baltimorewashingtongcmaglevproject.com

COMMENT FORM

Name: Christy Daddario  
 Address: 8115 Quarterfield Farms Dr.  
 City: Severn State: MD Zip: 21144  
 Telephone: 410-551-0411 Email: Christyd@cableispd.com

Comments: This is the worst thing that can  
happen to the Washington/Baltimore Area.  
nobody has considered cause & effect.  
we are overwhelmed - this ~~isn't~~ DOES NOT  
help stop building on every existing  
piece of land in DC County & we wouldn't  
have this issue. All this does is benefit  
the builder & gives the Governor a resume  
check-off. ~~The plan~~ <sup>the plan</sup> provides no  
direction for how & where parking is provided  
once you're at station. If I leave my car  
at BWI & go to DC... what transportation  
is available once I get to DC? & vice versa.  
BWI is only an airport - not a destination.

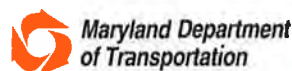


Comments: I am absolutely opposed to this project & will do everything I can to see it stopped. all it does for A.A.C.D. is disrupt our lives.

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DATE 10/16/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Tina Ladabouche

Address: 1425 Flagstone Ct

City: Severn State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: ladabouche@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

- Concerns regarding the actual cost of fares - can citizens in this area afford the cost or continue to commute via cars (thus not meeting one of the objectives of building MAGLEV). High end cost to travel.
- IS MAGLEV the best solution to ~~combat~~ or resolve the increasing traffic concerns in the area.
- The line (yellow) <sup>Amtrak</sup> running thru Severn ~~is~~ creates displacement of citizens - why not go along BW ~~to~~ Parkway.
- How many people will travel Balto to DC for larger fare than driving car?
- Faster doesn't mean better.
- Does the volume of traffic in this metro area really warrant an investment of this magnitude?



Comments: magnititude?

- Impact homes/communities and school systems.

- It seems that those that will benefit are the higher paid citizens wanting to travel from Balto-Dc.

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Baltimore, MD 21202

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DATE 10/18/2017

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## COMMENT FORM

Name: Harriet Caroline HasslerAddress: 8740 Bright Meadow Ct.City: Odenton State: MD Zip: 21113Telephone: 240-291-4869 Email: hchassler@yahoo.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: Extremely skeptical that this has viability. Alternatives are MARC, Amtrak, and Amtrak Acela. People will not be willing to pay premium required to support this technology to shorten a trip that is already < 1 hour. Tunnel route will not be appealing and above ground is an environmental and community nightmare. The real rationale for this is to compete on DC to NY and DC to Boston routes. Don't start this here b/n DC and Baltimore. Why not try in a corridor that does not already have one single commuter rail connection - Philly to NY for example. I believe the rationale of beginning here was to pick a somewhat less residentially settled area, a soft target. This will never be built in more congested part of

Comments: The corridor and this small piece that would be built will be a total boardoggle.

Regarding community disruption, please address how rights to tunnel under private property would be acquired.

Regarding reducing congestion on surface roads, this project does nothing to benefit commuters with only one stop at BWI. I would greatly prefer to see FRA and MDOT support improving existing Amtrak and MARC rail service. I ride MARC Penn Line daily. I see no benefit to those using MARC to this project

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE <sup>18</sup> 10/17/2017

bwmaglev.info

COMMENT FORM

Name: J. Price

Address:

City: State: Zip:

Telephone: Email: Jeff. Price @ dot.gov

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments:

How do interact with existing rail such as Amtrak? Can I transfer at stations to get to destinations east?

I use Uber / Lyft and transit to get around, will these stations have adequate drop-off and pickup for Transportation Network Companies (TNC)? Will you charge these companies extra fee to service your stations?



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/18/17

[bwmaglev.info](http://bwmaglev.info)

COMMENT FORM

Name: ERIC JENKINS

Address: 341 O ST. SW

City: WASH State: DC Zip: 20024

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Excellent initiative —  
about time we moved into  
the future.



DATE 10-18-17

bwmaglev.info

**COMMENT FORM**

Name: Enjoli Gonzalez

Address: 5200 Quincy Street, #202

City: Bladensburg State: MD Zip: 20710

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Please start the meetings at 6-most people  
get off of work @ 5 pm.

My concerns:

① What will the cost of living increase look like  
for residents in these areas?

② How will the electromagnetic feedback  
impact people, wildlife, environment, etc.?

③ While it will allow Access to Anacostia Park,  
why was it not placed in the SE part of  
DC (southernmost) which needs major  
cultural and economic development? The  
same goes for the further southern parts of  
PG (ie. near Fed Ex field).

January 2018 ④ Will taxes be increased? If so, how much? Why? Page 6-968



Comments: it will increase jobs and is privately funded,  
all companies look to make money?

⑤ How will this impact ~~the~~ the subway of DC?

It seems as though it is catering to the same  
population, which will take money from the subway,  
impacting the community at large.

⑥ Looking at the stops in Baltimore, the train is  
giving access to people in the inner harbor. How  
can we make it more accessible to people in  
Baltimore and/or DC who aren't near a metro  
or subway?

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DATE 10/19/17

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**COMMENT FORM**

Name: Margaret Matijasevic

Address: 3900 52nd St. Bladensburg

City: Bladensburg State: MD Zip: 20710

Telephone: \_\_\_\_\_ Email: margaretmatic@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Though this project study has been underway for quite some time, no communication with residents/homeowners has occurred until the last few weeks. This demonstrates poor collaboration/communication.

The routes all pose issues of concerns, but those directly affecting my home & neighborhood show an invasive plan to people's homes and our Bladensburg community (including school & parks.) This is of great concern to me personally.

On a larger scale, I find concern in building over BW Pkwy, potentially destroying the natural forestry. All lines, though traveling over

Comments: government/public property show at a superficial level a high impact on the natural habitats/environment. This is a great concern.

The positive perspective of high speed travel, limiting traffic/pollution and gas use seem positive contributions, but the expense seems to great at this point in the study.

As a homeowner/landowner on/near most of the proposed routes, I would appreciate more transparent communication moving forward that actually provides what you expressed in this meeting/project proposal.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10-18-17

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**COMMENT FORM**

Name: Kurt Pluntke

Address: 3304 40th Ave

City: Colmar Manor State: MD Zip: 20722

Telephone: 202-246-0690 Email: kplunt@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concern that that the NEPA process may not be able address adverse effects such as ~~the~~ the FRA Train Horn Rule requiring mandatory signaling at all at-grade crossings 24/7/365. The implementation of sensible Quiet Zones is onerous and nearly impossible to implement for something as simple as not signaling ~~at~~ in the middle of the night or installing flashing lights. I support sensible technology if implemented with due regard to health and well-being of those living in surrounding communities.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/18/17

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COMMENT FORM

Name: J. Price

Address: 1200 New Jersey Ave,

City: Washington State: DC Zip: 20985

Telephone: 202-493-0830 Email: Jeff.Price@dof.gov

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments:

Will the maglev train be operated  
in Metro in km/hr?

What will the costs be to ride  
from DC - to - Balt?

How much tunnelling will occur under  
the streets?



DATE 10/18/17

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**COMMENT FORM**

Name: John O'Donnell

Address: 625 Monroe St NE ~~NE~~

City: Washington DC State: DC Zip: 20017

Telephone: 516-404-1349 Email: John.od228@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: What will the noise level be for people  
outside the train? Will the noise harm any wildlife  
in the area? How deep do the tunnels have to be to  
not affect anything above it?



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COMMENT FORM

Name: FRANK ALEXANDER, III

Address: 2 LARCH COURT

City: FISHKILL State: NY Zip: 12524

Telephone: 845-220-7830 Email: alexanderiii@cwa.edu

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I PERSONALLY FEEL HAVING THE STATION <sup>STATION</sup> ~~STATION~~  
~~STATION~~ LOCATED AT THE COVENTON PORT WOULD BE THE  
MOST EFFICIENT ROUTE. IT CAN CROSS OVER THE  
WATER (BY A CERTAIN STRUCTURE OR TUNNEL) TO ALLOW LESS  
OF AN IMPACT ON HOMES OR COMMERCIAL AREAS.



DATE 10-18-2017

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**COMMENT FORM**

Name: Christopher Martin

Address: 629 Monroe St, NE

City: Washington State: DC Zip: 20017

Telephone: 978 304 2312 Email: celo.martin@cva.edu

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I think this has immense potential. Acquiring space and funding may be a major concern. However the benefits will outweigh these adversities.





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COMMENT FORM

Name:

Samuel Yates Wilburn

Address:

2509 10th St. NE

City:

Washington State: DC Zip: 20018

Telephone:

\_\_\_\_\_ Email: syateswilburn@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: From what I've heard, read, and seen here,  
there should be no reason to fund this project with  
taxpayer money. Every piece of literature screams  
to the heavens that this will be a profitable  
venture, with private investment taking the  
risks. However, if that were the case, Federal  
dollars should not be necessary. It would be  
a simple matter of security a bank loan or further  
investment. This paradox was answered by one of  
your reps, who responded to the question "If  
this is going to be as profitable as you claim, you should  
have no problem getting bank loans, like all other business.  
Why seek Federal money?" with "Why get private  
loans at 15% when you can get Federal loans at 2%?"

Comments: It is that arrogance that irks me. It is not the taxpayer's responsibility to subsidize your business venture. Risk your own money, not the taxpayers.

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DATE 10/18/17

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COMMENT FORM

Name: Sofya Leonora

Address: 201 Q St NE, #3141

City: Washington State: DC Zip: 20002

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Just general comments that environmental impacts of potential construction is a very important consideration + I hope the folks working on this will get buy-in from park/environmental stakeholders before proceeding.

Secondly I'm still not convinced the economics of this project can really work out. It's tough to imagine this really fully recovering costs from fares. I think the public will need more evidence (and solid, independent analysis) on feasibility/economic sustainability of this proposal without public subsidy.



DATE OCT 18, 2017

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**COMMENT FORM**

Name: JOE HARRIS

Address: 5999 EMERSON ST # 705

City: BLADENSBURG State: MD Zip: 20710

Telephone: 240.381.8001 Email: JSHARRIS312@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ROUTE E1 WORK BEST FOR ME AND MY SENIOR NEIGHBORS,



DATE 10-24-17

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**COMMENT FORM**Name: Jennifer WardAddress: 10114 Snowden Rd.City: Laurel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I am strongly opposed to this project. I see several problems/issues with it. First, it is extremely expensive - ~~and~~ both to build the system in the first place, but it is likely to be too expensive for many people to use. Secondly, I see three different projects being talked about that would cover approximately the same route - this, the hyperloop, and the proposed additional lanes on the BW Parkway. This seems to clearly be the least appealing option for many reasons. The hyperloop is privately funded, this will come from our tax money. Additional highway lanes are much less expensive to build than this project, and are useful to a much bigger portion of the population. If this project were built with a station in the Laurel area, it would add more

Comments: traffic to our roads as people try to get to the stations, possibly requiring additional highway lanes even if the Maglev is built. I also feel like this project is trying to meet the same needs as the Marc train. ~~SCMagLEV~~ I have tried to use the Marc train to get into Baltimore several times, and always arrived at the Bowie station to find the train so delayed that I would miss the shuttle connection I needed at BWI, resulting in me turning around and driving to my original destination. If the Maglev is as poorly run as Marc, it would be useless and not attract many riders. I heard the Maglev would only run about 1x/hour, which seems like it would not take a significant # of cars off the road. I also live 1/2 a mile from one of the suggested routes (just west of the BW Pkwy) & I DO NOT WANT THIS TRAIN IN MY NEIGHBORHOOD!

I think it would be wrong to take peoples' homes, wrong to take land from the agricultural research center, and

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wrong to take land from the Patuxent Wildlife Refuge. I would much rather see an extra lane added in each direction on the BW Parkway, rather than see a new project started. This seems like an idiotic waste of money that would only benefit

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Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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wealthy people or out-of-towners flying into BWI airport. Get rid of this project!



DATE 10/24/17

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**COMMENT FORM**

Name: Louise Piper  
 Address: 27 Little River Rd.  
 City: Laurel State: MD Zip: 20724  
 Telephone: 301-498-6469 Email: \_\_\_\_\_

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This project seems unnecessary. The inter-county connector is not used by the number of people projected. Who is going to pay to make this economically feasible? The path along the parkway will remove the trees that make the parkway beautiful. This will also result in more noise to the communities along the parkway. This is a huge negative visual impact!  
What about the deer and other animals along the parkway? Where do they go? They have adjusted to cars, not this!

Comments:

I have lived in Rossett for almost 25 years. We fought a Redskins stadium, a NASCAR track, & the county wants to build a water tower adjacent to our homes. When does this end? Do we have to fight against a SCMaglev too? This project does not make sense!

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DATE 10/24/2017

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**COMMENT FORM**Name: Veronica BulvitchAddress: 10409 Balsamwood CtCity: Laurel State: MD Zip: 20708Telephone: 301-498-5758 Email: wdbulvitch@hotmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I am not for the Maglev Project going through Laurel. Oneither side of the parkway. Environmental Impact with the construction alone would be ~~horrible~~ horrible. This project in no way benefits Laurel except to make it a place for someone not to move to. While you may not be taking my house, ~~I would~~ I ~~would~~ would be right next to the train. The impact on the value of my home would be costly. Who wants to live next to this train. As a recent widow, how do I convince someone to buy my home. This needs to be stopped. The cost along for maintenance and upkeep would be a tax burden on us all. I do not wish to pay for something that does not benefit my community. We already hear the traffic ~~and the~~ both land and air. What happens if something goes wrong? What will be the cost

Comments: and impact to the community during  
construction. This community is already a poor  
region in a lot of areas. We are just getting to  
be the point where our community is showing  
growth. This would set us back.

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DATE 10-24-17

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## COMMENT FORM

Name: Katharine Webster  
 Address: 8323 Snowden Oaks A  
 City: Laurel State: MD Zip: 20708  
 Telephone: 3018060650 Email: blk855smc@gmail.com

Join our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This project does not seem to benefit Laurel at all. The cost of building & maintenance will far exceed any profit. With no stops from Wash to Balt no one can use the train. Nor would it be financially possible to ride it at \$150 a ticket. The <sup>BW</sup> parkway plan will severely impact previously protected park & wetlands. - It only takes 30 minutes to drive to either city. The 15 minute ride ~~also~~ won't be proven unless the train reaches full speed. Is there enough space? The BW Parkway already serves as a corridor between the 2 cities. Who is



Comments: going to pay to maintain this  
track

I disagree with all plan to build  
the Maglev along our Parkway

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DATE 10-20-2017

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## COMMENT FORM

Name: Ellen Moss

Address: 7503 Gink Middle Lane

City: Jessup State: MD Zip: 20784

Telephone: 410-960-8338 Email: elandm@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: BWRR rep said that the economic benefits on the display board was done by a Japanese company and may be completely wrong. However, the display board is being portrayed as truth... unless you ask. This sort of deceptive practice undermines every piece of information presented here.

There is no information (even questionable) about how many people would ride this train.

I am concerned about peoples' properties being impacted. Any property above or adjacent to a track must be fully compensated.

The idea of maglev is great, ~~but~~ however, the technology is not quite ready. Tunneling has come a long way, but is still so expensive. I would hope taxpayer \$\$ would be put to better use tunneling before undertaking this project. The magnetic

Comments: The current magnets must be cooled to near absolute zero,  
There are a variety of higher temp (still cold) in work,  
plus other ways to create strong magnetic fields.  
But not ready yet.

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DATE 10/24/17

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**COMMENT FORM**

Name: Alison Hector

Address: 12206 Blue Moon Court

City: Laurel State: MD Zip: 20708

Telephone: 301 317 5883 Email: alisonhector@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I'm very concerned about the potential impact of the J1 proposed route on the homes in the Montpelier Hills development. It appears that Hermosa Drive and Apache Tears Circle will take a direct hit, but so, it seems, will the houses on neighboring streets, such as Blue Moon, Ivory Fashion, and Sea Pearl.

1. What provisions are being made to mitigate the effects of Route J1 on the Montpelier Hills community?

2. What environmental concerns (especially noise pollution) have been taken into consideration?



Comments:

3. In what specific ways will this MAG-LEU project benefit the citizens of the counties that will be affected (PG, Ann Arundel, Howard)?
4. How many jobs (long-term) will result from the implementation of MAG-LEU - jobs in the affected communities?
5. Why is it feasible to build a MAG-LEU from DC to BW when Metro, which serves more specific local stops in the DMV, needs to be beefed up?
6. How likely is it that citizens of MD affected by MAG-LEU will support it, seeing that it most likely won't be using the MAG-LEU if it's only stopping in DC/BW/Baltimore?

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DATE 10/24/2017

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**COMMENT FORM**

Name: Charlene Kalinich

Address: 9100 Sumner Grove Drive

City: Laurel State: MD Zip: 20708

Telephone: 443-618-1081 Email: ckalinich13@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① Sumner Grove community did not receive notice of this project until word of mouth this week

② The J1 option runs ~~through~~ <sup>above</sup> existing houses  
a- what will happen to those homes?  
b- what construction and operational impact to remaining homes?

c- what noise abatement will be offered  
③ <sup>J1</sup> J1 are ~~not~~ viable options. They disrupt unique wildlife habitats; displace existing home owners (high cost housing with big annual tax payments) Does not have a big picture solution for east coast travel; Does not tie into existing transportation hubs

Comments:

④ What health & ~~an~~ environmental ~~may~~ impact does high magnetic forces have on people, animals and plant life?

⑤ ~~Does~~ Does it provide a joint solution by all transportation efforts under way (Maglev vs Hyperloop vs high speed rail)

⑥ The \$15-\$30 ~~to~~ price tag is outrageous. There are many other traffic relief options that can be achieved for far less (hot lanes, HOV, toll roads)

⑦ ~~Rail Dept~~ Latest rail usage studies indicate rail travel is down. This option doesn't relieve road travel since it only makes 3 stops.

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DATE 10/24/17

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COMMENT FORM

Name: Donna Haselton

Address: 10802 Forestgate Place

City: Glenn Dale State: MD Zip: 20769

Telephone: 301-805-2152 Email: dhaselton@msn.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Overall this project seems to be massively expensive, with significant costs to the local communities with minimal benefit to those communities. Funding for this project could do much to improve transportation in this area without the negative impacts maglev would have.

More specifically, the E1 route as currently drawn runs directly thru St George's Episcopal Church, Glenn Dale ~~MD~~ property. St George's has an historic chapel + cemetery which ~~is~~ would be directly impacted - the cemetery + chapel would probably need to be relocated if that route were selected. Section 4(f) of the DOT Act should

apply here, + if this project must go forward, it

Comments: Should be along one of the alternate routes  
identified, not the route thru Glenndale.

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**COMMENT FORM**

Name: Jacqueline Taylor  
 Address: 4601 69th Ave  
 City: Hyattsville State: MD Zip: 20784  
 Telephone: 301/322-2513 Email: jackie-taylor@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: As a PG county resident, it is very offensive that our county (homes, schools, etc) will be the most effected with absolutely no benefit to us, not even one stop. To be honest I do not feel this is necessary, nor does it warrant enough need in my opinion for us to be guinea pigs, being this would be the first one outside of Japan (as far as I know). My neighborhood & child's school are of more importance to me than saving someone an hour commute. I lived in DC most of my life & it never took more than an hour to get to Baltimore from where I lived so I am having a hard time understanding the need. The safety, money, & disruption that this would cause to PG residents is totally unreasonable to ask for such a short

Comments: Commute, especially considering it takes longer for people to commute ~~from~~ DC to Virginia (Woodbridge).

Multiple horizontal lines for additional comments.

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DATE 10/24/17

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COMMENT FORM

Name: Howard ALDAG  
 Address: 11919 LANHAM SEVERN ROAD  
 City: Bowie State: MARYLAND Zip: 20720  
 Telephone: 240 688 7584 Email: TAALDAG@gmail.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: Please do not consider the PENN-Line / AMTRAK Alternative. This is the worst of the three Alternatives. Putting the MAGLEV through Historic Bowie AND Environmentally Sensitive Patuxent wildlife Refuge Does not make sense.

The Best Alternative is to go along the Baltimore Washington Parkway. This Land is already owned by the Government and would have least impact on the immediate area. Since this train will only have a few stops, there is no need to have it travel through a populated neighborhood. There would be - No condemnations, No Amtrak easements (which are hard to get), no noise or health complaints. In fact construction along the BWP could go 24 hours a day saving millions in construction delays. This project should

Comments: Be coordinated with Elon musks High Speed train Project. Using his Equipment to put this underground would be the best if it was an alternative.

Also there is the Existence of FA Fiber optic tunnel Along the PENN-Line/Amtrak tracks that would require construction outside of the Amtrak Easement and making the need for wholesale condemnation of most of the properties Along Lantans Seven Road.

Thank you For Listening,

*[Handwritten Signature]*

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DATE 10/24/18

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COMMENT FORM

Name:

Winstone Paulette Gunter

Address:

9009 Summer Grove DR

City:

Dauver

State:

MD

Zip:

20708

Telephone:

301-467-0595

Email:

paulette\_gunter@yahoo.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Comments:

my biggest concern would be the noise  
 facture of 51 is the selected plan of the value  
 of the remaining homes in the neighborhood.  
 Summer Grove is already built on west land & I'm  
 afraid the vibration would create sink holes  
 b/c cause my home to shift I already have  
 sink holes in my backyard & I'm afraid this  
 maglev train would make it worst. I'm on  
 Summer Grove DR on the line of the impact  
 and I'm afraid already with the underground  
 train that's already coming under the PKwy  
 will cause say structure damage to my  
 home. I'm afraid proper notice will not  
 be given to us home owners or that

Comments: I hope compensation would not  
be given also. This is very nerve  
racking for my family.

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DATE 10/24/17

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**COMMENT FORM**

Name: Sheila Edmonds

Address: 9250 Greenwood Lane

City: Lanham State: MD Zip: 20706

Telephone: \_\_\_\_\_ Email: sedmonds123@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Why, why must we always be in a rush to get to where we are going? Why is there a need to reach Baltimore in 15 mins when Amtrak and the Marc train take care of the traffic. Historic Prince Georges County should not have to suffer the construction, noise, disruption, congestion for a 15 min trip. I live with the noise of the Amtrak and Marc trains. I live with the congestion of traffic for people to catch these trains. ~~they~~ ~~would~~ this would make it much worse with ~~the~~ the building of a station at my back door.

P.S. I have missed so much because I live in a community that verbalized, televised, mailed, emailed to those who fell under this plan.

Comments: Why must we lose our homes. Why is this County willing to fund this project when we have a hard time receiving current services without multiply calls. What do we, who may live in the impact area and possibly lose our homes and businesses get for a 15 min trip to Baltimore. I suppose, there are so many things, good things we can do with this money to improve the County. To maintain its historic value. Why must we always destroy to improve. There is always another way without taking our homes and businesses

All for a 15 min trip to Baltimore!  
NO!!

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DATE 10/24/17

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**COMMENT FORM**

Name: Jessica Cabness

Address: 12204 BLUE MOON COURT

City: LAUREL, MD State: MD Zip: 20708

MOBILE Telephone: (727) 215-5305 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My concern is that light-rail transit representatives, including engineers and project managers, respond to consumer concerns in a way that satisfies consumers, and this may be done quite in earnest. However, I fear that the consumer concerns will not be factored into project outcomes. My community is very concerned about decreased property values (who wants to live next to a railroad track?); environmental pollution (i.e., noise); and disrupted traffic routes.

RECOMMENDATIONS / SUGGESTIONS!

(1) CONSTRUCT <sup>PERMANENT</sup> SUPER-TALL CONCRETE BARRIERS (OF THE TYPE THAT ABUT RT 95-SOUTH AROUND FREDERICKSBURG). TO PROVIDE NOISE ABATEMENT.

(2) PROVIDE SHUTTLE BUSES TO GREENBELT METRO DURING THE ENTIRE CONSTRUCTION PERIOD, TO TRANSPORT RESIDENTS TO / FROM LOCAL (OVER) ENTIRE

Comments: TRANSIT LIVES.

*[Handwritten signature]*

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BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 1/24/17

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COMMENT FORM

Name: Debi Szordlin

Address: 4229 Edmonston Rd

City: Bladensburg State: MD Zip: 20710

Telephone: 301-927-8648 Email: dszordlin@bladensburg.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Three of the six proposed routes go through the Town of Bladensburg. Bladensburg is only 1.1 sq. miles. Routing the proposed train through our Town will destroy the economic viability of the community. Whether the train is above or below ground, homes and businesses along the proposed routes will be drastically impacted. Additionally ~~several~~ several of the seven schools in the Town's boundaries will be impacted as well. There is not a demand to get from DC to Baltimore in 15 minutes, or from Baltimore to DC. This project is not in the best interest of our community and Bladensburg needs to be

Comments: removed ~~as~~ from consideration.

Please take the Town of Bladensburg  
off the proposed routes.

Best Regards

Debi Swartz  
Town Administrator  
Town of Bladensburg

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
c/o Suhair Al Khatib  
Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: Martha Hunter

Address: 6811 Gurnan Pkwy.

City: Riverdale State: MD Zip: 20737

Telephone: 301/459-2394 Email: mhunter1123@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I vehemently oppose to the routes by  
the developers which run through my  
community, Beacon Heights. This project  
is very costly and would place an  
unfair burden on Beacon Heights citizens.  
It does not benefit the community  
since it does not stop at any point  
between Washington and Baltimore with  
the exception of BWI.  
Vibrations from the train will cause  
damage to <sup>the</sup> my <sup>founder of</sup> home, as well as  
depreciation of property value.  
Not a good idea since we <sup>already</sup> have  
the Hyattsville + Purple lines. Don't  
make sense to disrupt the community (over)

Comments: for such a seamless endeavor,  
the environment is also a concern.

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6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: CARLA CLEMONS

Address: 7717 ORN CT

City: GREENBELT State: MD Zip: 20770

Telephone: 301 474-2939 Email: CLEMONS@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This seems like a huge expense for very little result. To make a 35 mile trip into 15 minutes is not worth this expense or effort.

One of the proposed routes will tunnel under my house (Orn CT in Greenbelt). How does this effect the value of my home (if it doesn't have to be taken down)?

It seems that this is only a benefit for ... actually I don't know who would really benefit. It wouldn't benefit PG County in any way. Unless it's my expense to ride (which I doubt) most folks would take alternative options. You can already take the Acela or Amtrak for a pretty quick ride, so not sure of the point of this

Comments: \_\_\_\_\_

What is the point of a 300 mph train for a 35 mile trip?

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6 Saint Paul Street  
Baltimore, MD 21202

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/24/17

bwmaglev.info

**COMMENT FORM**Name: IMRAN SYEDAddress: 3516 RIPPLING WAYCity: LAUREL State: MD Zip: 20724Telephone: (240)4560384 Email: ImranSyed9@aol.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: 1. During the construction of the MD197 bridge at BW Parkway, there were a lot of subsidence issues and aggregate piles were constructed to remedy the problems. A tunnel construction will have to address that.

2. The BWI Marshall Airport Station can be a part of a grand development scheme.

3. The disruption to public will be horrendous and needs to be addressed.

4. Major utilities in the vicinity of the development off MD198 and BW Parkway will be impacted.

5. Ameliorating the impacts will significantly increase the cost of the project.

6. The noise and vibration issues are a major problem.

A detailed study needs to be performed to characterize the

Comments: Subgrade soils and their propensity to transmit vibrations to the surrounding infrastructure.

7. Large amounts of stray currents will be discharged into the ground accelerating the corrosion of the infrastructure.

8. Location of Access Shafts will have significant impacts.

9. Union Station, Washington DC and areas of Cloverly, Chewahy and Bowes in Maryland have weak soils - Coastal Plain deposits.

Fast moving train can have significant negative impacts.

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Maryland Transit Administration  
6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/24/17

bwmaglev.info

**COMMENT FORM**Name: Kelli HigleyAddress: 1436 Virginia Ave.City: Severn State: MD Zip: 21144Telephone: \_\_\_\_\_ Email: kelli.higley@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I can see the purpose of a high speed (MAGLEV) train to get quickly to places far away. But I can't see how this will solve problems of local congestion. ~~And~~ It may add though as we drive to ~~near~~ the maglev stations.

~~How~~ And I worry about a tunnel under my local elementary school. Or my neighbor's yard. Will people even ride this? It will cost so much money! Surely we could use that money to actually reduce congestion. Or improve our schools. Or feed the hungry.

Finally, I am frustrated that this has been in the planning/~~also~~ discovery phase for over a year, and I am just learning about it. ~~From~~ Word of mouth was not acceptable. Two or three routes ran within a mile of my house. Someone should have told me directly.



Comments: \_\_\_\_\_

But, your people were very nice tonight. And some of them even knew what they were talking about.

Multiple horizontal lines for additional comments.

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Maryland Transit Administration
6 Saint Paul Street
Baltimore, MD 21202

Comments also can be submitted via e-mail: info@BaltimoreWashingtonSCMaglevProject.com





DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name:

Regina Halper

Address:

10414 44th Ave

City:

Beltsville

State:

MD

Zip:

20705

Telephone:

301937-9136

Email:

r.halper2@verizon.netJoin our mailing list?  Yes NoPreferred method of communication?  Email Mail

Comments:

I am very concerned about two areas: the train yard in BARC East along Powder Mill Rd right near sensitive wetland areas and on OpenSpace (Reserved) land.

at NASA Goddard and in Laurel there are numerous dinosaur sites. Extreme care should be taken to preserve the finds. At Goddard an amateur paleontologist found the tracks after his lunch at the staff cafeteria. It contained nodosaur, pterodactyl and mammal prints in the same track. You must expect that between NASA Goddard & Blue Ponds that there are additional dinosaur finds.

I am also very concerned about the lack of →



DATE 10/24/17

bwmaglev.info

**COMMENT FORM**Name: CINDY TrappAddress: 208 Hawthorne RdCity: Linthicum State: MD Zip: 21090Telephone: \_\_\_\_\_ Email: Ctrapp208@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I strongly oppose the MAGLEV. I have yet to hear one convincing reason why this project is remotely necessary. I am a Linthicum resident - our community will suffer greatly while reaping absolutely no benefit from MAGLEV - neither will the majority of people in AA County or PG County. If the main goal is to transport people for from Baltimore & DC in 15 minutes, what is the purpose if we already have efficient trains ie Marc, Amtrak & the Acela line? I can leave the BWI Amtrak station & arrive at Union Station in 22 minutes. My ticket will be \$7.00. I have heard a ticket for the MAGLEV will be \$50 - \$70<sup>00</sup> and for what? To shave 10 minutes off the ride?? I see no reason to support a train with unproven technology, unknown



Comments: ridership, shaky financial backing to name  
just a few reasons. The opposition to this monstrosity  
is growing. The destruction to our homes, schools,  
green spaces, and neighborhoods is reason alone to  
oppose MAGLEV. Coupled with all the other  
negative impacts this project will bring to thousands  
of residents & voting citizens, is why I strongly  
oppose MAGLEV.

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

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 c/o Suhair Al Khatib  
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 6 Saint Paul Street  
 Baltimore, MD 21202

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DATE 10-24-2017

bwmaglev.info

**COMMENT FORM**

Name: WENDY COLLINS

Address: 11414 Elmshorn Way

City: Laurel State: MD Zip: 20708

Telephone: 301 317-8573 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ① I think it is appalling that each home owner was not directly contacted about the project & meetings.

② With Amtrak, Marc, BW Parkway, route 1, as well as, several bus routes, I still do not understand the need for this additional service ~~which~~ which seems to benefit only those in DC & Baltimore, & ~~BWI Airport~~

③ Jobs created from construction are not permanent & most likely will not employ the average person who will loose their homes. Certainly myself & my neighbors can not perform construction work.

④ If there are no stops, or just one stop between DC & Baltimore, where is the economic benefit ~~for~~ for those of us living between the two cities? Who in Laurel is going to DC to catch a train to Baltimore. By

Comments: The time you get to DC, you could have gotten to Baltimore w/o the Superconductor!

⑤ For those families who will ~~be~~ have to sacrifice for the comfort & convenience of a relative few - how will the compensation be determined? What besides FMV will be considered. Will cost of recent work done in the home, cost of moving, cost of a comparable home in another neighborhood w/ significantly higher prices ~~be~~ all be considered?

⑥ Why hasn't the meeting regarding this project not been widely advertised. Where ~~are~~ are the public notice signs?

⑦ This project IS NOT NEEDED!  
I am all for STOP THE TRAIN

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Baltimore, MD 21202

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DATE 10.24.17

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**COMMENT FORM**

Name: Anonymous

Address: Bowie, MD

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

*Thanks to Bowie citizens coming out*

Comments: Understands Bowie path has been dropped -  
This project \$28m, even though federal funds to study -  
Could have been used to do a study in region -  
less concentrated than MD - a region where folks  
stuck in ~~at~~ places who need to get to/commute to  
larger work centers - like those who benefit by  
working low paying jobs to higher paying jobs in  
larger work centers - ie. folk in WV to other  
states ~~where~~ who need low skilled workers + whose  
pay scale is higher - OR between other states  
that would benefit from getting folk from  
distribute circuits to larger work centers - ~~states~~ workers  
whose cost of living less ~~than~~ ~~cost~~ would benefit  
as well as larger work centers who need them

(How many people contribute to the project? what is driving the urgency for a project of this magnitude)

Comments: This region would benefit greatly from a rapid metro extension + maintenance, more light rail options using existing tracks; ~~however~~ more incentives to NOT DRIVE - To build a system which excludes those who will be impacted the most by its development ES schools like our local ~~one~~ that has not been given pre-liminary analysis - ~~without~~ <sup>before</sup> spending \$28m -

MD citizens need more than ~~one~~ construction jobs for its <sup>less skilled</sup> workers - construction jobs do not LAST and there are few paths to other work to sustain these workers after the "big one" is over -

I would like to see Maryland take the high road - stop now - try to get remaining of \$28m committed to something that would produce better results for the counties impacted by this project + the DC-MD connection as a whole

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6 Saint Paul Street  
Baltimore, MD 21202

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DATE 10/24/17

bwmaglev.info

## COMMENT FORM

Name: Lucy + Loren Carter

Address: 11205 Laurel Grove Ct.

City: Laurel State: MD Zip: 20708

Telephone: 301 210 3373 Email: lucylaurel@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We do not see the need for this massively disruptive project. We personally would have the train in our neighborhood, within a quarter mile of our home. We personally would never use the train, as we do Metro subway to D.C. and drive to Baltimore.

The train would cut through the Patuxent Wildlife Refuge, where we hike, and where my students go on field trips to see a bit of forest and wildlife. It would cut through BARC, where important agricultural research is ongoing.

What's wrong with MARC? Is it at full ridership? Couldn't more trains be added?

Comments: Really, how many people NEED to get from D.C. to Baltimore in 15 minutes on a consistent basis? We were told 4 trains per hour, with about 700 passengers on a 12-car train. Do 3,000 people really need to go that route ~~in 15 minutes so often~~ every hour?

We're all moving too fast, anyway!!  
 Look at all the empty space in the industrial/research/development park around Route 200. Jobs can come there so why do people need to travel to Washington and Baltimore. If they work in either of those urban areas, they should live in those urban areas.

This project seems spurred by a private entity who wants to make \$ at the expense of Maryland residents who like the suburbs because they're quiet, w/o high speed trains in their back yard. We will need WAY MORE

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

proof of necessity  
 support this!!

SCMAGLEV Project  
 c/o Suhair Al Khatib  
 Maryland Transit Administration  
 6 Saint Paul Street  
 Baltimore, MD 21202

before we'd ever

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)







DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Rodger Granger Brackton - Pat  
 Address: 3509 St. Marys View RD Alcookeek, MD  
 City: Alcookeek State: Maryland Zip: 20607  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am in total agreement of what you  
are planning. 311 MPH is really fast. I hope  
I will get to ride this train. I admire how you  
care about how many houses you're knocking down.  
Some businesses don't.







DATE 10-24-17

baltimorewashingtongmaglevproject.com

COMMENT FORM

Name: DONNA K. HARDING

Address: 10300 BALSAMWOOD CT.

City: LAUREL State: MD Zip: 20708

Telephone: 301-543-7343 Email: BUDSYBOOPER@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I HAVE CONCERNS ABOUT THE ABOVE GROUND PORTION OF THE MAGLEV.

I AM IN MONTPELIER WOODS AND THE ABOVE GROUND PORTION WOULD IMPACT MY NEIGHBORHOOD.

WHAT IS THE NOISE IMPACT?

WHAT ABOUT AIR VENTS IN THE UNDERGROUND PORTION?

WHAT WILL THE COST BE TO RIDE THIS TRAIN?

WILL IT EVEN BE AFFORDABLE AFTER ALL IS SAID AND DONE?

IF NOT, WHAT IS THE POINT?



DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: Kenneth Howell, Jr.  
 Address: 2004 Chadwick Terrace  
 City: Temple Hills State: MD Zip: 20748  
 Telephone: 202-459-3875 Email: kennethhowell495@gmail.com

Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: No to this train. Prince George's County communities do not need this train going through their neighborhoods costing taxpayers money to upkeep a train that serves absolutely no benefit to. This train will hurt property values, harm parkland, and there is "hope" the train will go to NY but no one knows, I say NO!



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Bismarck E. M. III

Address: 35 B St. Mary's View RD ACCOKEEK, MD 20607

City: ACCOKEEK State: Maryland Zip: 20607

Telephone: 301-283-9145 Email: JessicaSharese@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am in total agreement with what you are doing.  
311 MPH is very fast. I hope that I will be able to ride this train.  
I admire that you care about the houses and want to  
destroy as little as possible. Some people and  
businesses don't care.

Keep up the good work.

Sincerely,

Bismarck.

P.S. Don't mind my friend Redgor, he is a real  
prankster.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 23 Oct 2017

bwmaglev.info

COMMENT FORM

Name: Ronald Diggs

Address: 11400 Elmstorkn Way

City: Laurel State: MD Zip: 20708

Telephone: 301-919-7847 Email: rdiggs448@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The Baltimore Washing ~~Pass~~ West Route  
is supposed to go right through my Home.  
Can you please meet with my Community  
Members to keep us abreast of any  
decisions that might impact us.  
Also what will be the environmental impact  
on my Community, How many jobs can  
my Community expect. and what  
percentage of the Home value for those  
Houses affected.



DATE 10/24/2017

*bwmaglev.info*

**COMMENT FORM**

Name: Nickesha Brown

Address: 12243 Apache Tears Circle

City: Jumel State: MD Zip: 20708

Telephone: 908422 2121 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I reviewed the propose route and my house  
is in the direct route and would have to be  
demolished. I own the end unit on Apache Tears  
Circle and refuse/disagree with project.



DATE October 24, 2017

bwmaglev.info

COMMENT FORM

Name: Juanita J. Watts

Address: 6817 Purman Parkway

City: Lensdale State: md Zip: 20737

Telephone: ~~XXXXXXXXXX~~ Email: JUANITAWATTS1947@Yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I feel that the Maglev project would not be beneficial to the residents of Prince Georges County, especially the seniors who actually have home ownership. Also the seniors and residents would be displaced if homes are demolished. The property value of home in community would decrease. Also the residents definitely would not want to hear the noise or feel the vibrator as the train pass. What would be the Environment effect of the maglev after 5-10 years in regards to possible "Cancer" to residents. The cost effective is definitely too expensive and would not benefit Prince Georges County residents.

Foundation of our homes deteriorating, "Cracking, Shaking"





DATE 10/24/2017

bwmaglev.info

**COMMENT FORM**

Name: Howard L. Campbell II

Address: 11202 Hickory Grove Court

City: Laurel State: MD Zip: 20708

Telephone: 301-210-3723 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Our neighborhood is very close to one of the propose routes. We have a small very quiet neighborhood. I would hate to see it ruined by this project.

I think this project should follow already existing railroad lines (Amtrack) to minimize damage and hardships to existing neighborhoods. I would think it would be cheaper to build on existing right-of-ways also.

I hope the P.G. County Government is able to stop this project.



DATE 24 October 2017

bwmaglev.info

**COMMENT FORM**

Name: SABRINA B. Cobb.  
 Address: 9222 ISPAHAN LOOP.  
 City: Laurel, State: MD Zip: 20708  
 Telephone: 240.342.1242 Email: bvice824@gmail.com

Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I oppose the construction of the train going on -  
285 and 144. My neighborhood is off of murkirk rd.  
apache terrace. This of course will reduce the overall  
market and value of my home. I just purchased this home  
three years ago and presently the value is going up.  
I not only am looking at cost, but also the environment.  
The need is not there in the area, because no assistance  
to Prince Georges County. Again I want to reiterate  
that I AM NOT supporting this project (Maglev).  
If you need more information from me please contact  
me by email.  
Thank you for your time and I hope this project  
is discontinued.

S.B. Cobb



DATE 10/24/2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Gary Battel

Address: 12102 Aspenwood Lane

City: Laurel State: MD Zip: 20708-3104

Telephone: 301-776-7222 Email: g55battel@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I think that the MAGLEV is a good thing for  
this area as well as our nation. However, I'm not thrilled that  
the option of the MAGLEV being above ground within 0.3 miles  
of my house. I think it would be wonderful if it were  
below ground in the areas where the train is so close to  
houses





DATE 10/24/17

baltimorewashingtongcmaglevproject.com

COMMENT FORM

Name: Michelle Woods Long

Address: 8817 Sumner Grove Dr

City: Laurel State: MD Zip: 20708

Telephone: (240)-508-0623 Email: long1963@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I'm against the construction of this train because the J1 line will run <sup>above ground</sup> directly behind my home on Sumner Grove Drive. I'm concerned with the amount of radiation that will be given off as well as vibration and noise. I don't see how it would benefit my neighborhood. It will decrease the value of my home <sup>just</sup> to increase revenue for the state. I can't imagine looking out my backyard to see a cement wall instead of the beautiful trees that are there now. I'm totally against this project to disrupt people homes to make money for the state



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Mary Canning

Address: 9223 Oregon Ct

City: Laurel State: MD Zip: 20708

Telephone: 301-498-5672 Email: goldencanning@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: totally unneeded to knock 15 minutes off  
a trip to Baltimore or airport. Park lands vital  
to our environment, regardless of Trump's troops  
feelings). If at all acceptable run it along the  
Amtrak line - only acceptable option.  
Our neighbor hood will be devastated. We'll  
have a train running alongside my home.  
VETO THIS PROJECT!



DATE 10-25-17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Denise Johnson  
 Address: 8028 Patuxent Landing Loop  
 City: Laurel State: MD Zip: 20724  
 Telephone: \_\_\_\_\_ Email: Tama6703@live.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Maglev technology is 20 years old  
and I would like interested in  
seeing the analysis of downtime and  
maintenance. Other northern states  
have said no to this project. Why is  
MD saying yes.

This train is so close to Russett Md  
that it is set slated to be underground  
at the intersection of Route 198.





BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/2017

bwmaglev.info

COMMENT FORM

Name: Kathleen Todd

Address: 2313 Apricot Arbor Pl.

City: Denton State: MD Zip: 21113

Telephone: 443-205-4123 Email: Todd.Kotodd@gmail.com  
Todd.Kotodd@gmail.com

Join our mailing list? <sup>Email</sup>  Yes  No

Preferred method of communication?  Email  Mail

Comments: Please continue the possibility study. I'm pleased w/ what I've seen thus far!

fewer Carson roads  
to Hutton.  
Faster Time Travel.  
Quicker!

Thank you!  
K.T.

DATE OCT 29 2017

bwmaglev.info

## COMMENT FORM

Name: AL GRAYAddress: 11618 CLOCKTOWER LANECity: LAUREL State: MD Zip: 20708Telephone: 301 210 1551 Email: JPUTW2@AOL.COMJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I have concern about the extra noise impacting my community (WOODBRIDGE CROSSING) as well as potential encroachment to the existing and planned National Cryptologic Museum at Fort Meade, where the National Cryptologic Museum Foundation is sponsoring the building of a multi-million dollar ~~new~~ new museum next to Route 295/BW Parkway

I'm also puzzled about Gov. Hogan's intent to add new lanes on the BW Parkway thereby absorbing new level of noise and congestion.

There also is the question of how does MAGLEV duplicate plans for Elon Musk's Hyper Speed train from DC via Baltimore to New York, construction which I understand has already begun in Jessup.

AL Gray





DATE 10/24/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Samuel J. Parker, Jr

Address: 5601 57th Ave

City: Riverdale State: MD Zip: 20737

Telephone: 301-742-1972 Email: sjparker2@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: There needs to be more consideration of direct economic benefit for Prince George's Co. This is a regional economic stimulant as well as a transit mover. Another convenient stop in the county would better position the county as a regional economic engine for the regional economy in the future.

There should be more attention to underground tunnels for the lines in the County. More above ground in the County than other areas.





DATE 10/24

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: John Woo (DEAF)

Address: 2313 Apricot Arbor Pl

City: OPENTON State: MD Zip: 2113

Telephone: 410-940-9228 Email: skyskydive@outlook.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Definitely support this project.  
Reduce Heavy traffic on BW Pk. Rt 50.

John Woo

DATE 24 Oct 17

bwmaglev.info

**COMMENT FORM**

Name: Mark Andersen

Address: 8601 tabb ct

City: Bowie MD State: MD Zip: 20720

Telephone: 703 731 0222 Email: mozart@ix.netcom.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

1. Your public outreach is not working. I've been talking to people in my development, which is less than 1/2 mile from one of the routes, and they have never heard of it. Considering you've been working on it for 2 years, that's a fail. I recommend purchasing local TV time.
2. It is unclear why we would need both the Maglev and the hyperloop, which has already started construction.
3. Most, if not all of the people affected by construction ~~with~~ <sup>are</sup> not commuters that would benefit, and likely cannot afford the tickets.
4. Limiting Postcard distribution to 500 ft from proposed routes is too restrictive. Recommend 5 miles.

I know 3 people personally who have died of cancer in 1 year. All are under 50 y old.



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Lavenia Dorsey  
 Address: 11200 Hickory Grove Ct.  
 City: Laurel State: MD Zip: 20708  
 Telephone: 240-832-1113 Email: LD14625@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The IJ1 line runs directly in our neighborhood. We are against the MAGLEV for the following reasons: EMF which will give off radiation and cause cancer. We will loose the tree line which is a barrier for noise on BW Pkwy. I currently do not and will not use public transportation because I like the convenience of my car. My children and I take field trips to D.C. and Baltimore via car and we live 5 minutes from the Met Kirk MARC train. You did not ask the opinion of P.G. and MD residents before starting.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

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COMMENT FORM

Name: RAY SZYPERSKI

Address: 332 MARGANZA SOUTH

City: LAUREL State: MD. Zip: 20724

Telephone: 301-725-5904 Email: RSZYPERS@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: BELIEVE THAT MANY OF THE  
PROPOSED ROUTES COME TOO CLOSE OR  
EVEN THRU RESIDENTIAL AREAS, ESPECIALLY  
IN THE MARYLAND CITY AREA OR  
EVEN TOO CLOSE TO NSA.  
BELIEVE "HYBERLOOP" WOULD BE  
BETTER.



DATE 24 OCT 2017

*bwmaglev.info*

**COMMENT FORM**

Name: JOSEPH BLEES

Address: 422 YELLOWSTONE SQ

City: LANING State: MD Zip: \_\_\_\_\_

Telephone: 301-490-6769 Email: GYRINGJOLEP@OL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I AM NOT CONVINCED THE MAGLEV PROJECT IS SOUND. THERE ARE  
TWO MANY QUESTIONS ABOUT WHO - FEDERAL - STATE - COUNTY - PRIVATE - PAYS FOR THIS. HOW DOES  
THE HYPER LOOP PROPOSAL AFFECT THE MAGLEV PROJECT  
I DO NOT SUPPORT ANY FORM OF TAX OR LOAN PAYING FOR THE  
MAGLEV PROJECT

RESPECTFULLY SUBMITTED

*Joseph E. Bles*



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name:

Ms. Valerie Schiele

Address:

7818 Patterson Way

City:

Hanover State: MD Zip: 21076

Telephone:

Email: valerie.schiele@comcast.net

Join our mailing list?  Yes

No

Preferred method of communication?  Email

Mail

Comments:

The proposed plans calls for potentially creating tunnels under my brand new townhome community in Hanover. Please contact me with how these plan will work and disruption of a new community being built as I write?





DATE 10-24-17

bwmaglev.info

**COMMENT FORM**

Name: KENNETH J. KUHN

Address: 9206 MONTPELIER DR

City: LAUREL State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: KUHN.K1@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: IT SEEM TO ME THAT MONEY WOULD BE BETTER SPENT ON  
FIXING EXISTING ROADS. THE EGRESS ROUTE OUT OF DEERFIELD RUN  
ELEMENTARY SCHOOL HAS HOLES DEEP ENOUGH TO BREAK AN AXLE.

THIS WILL SERVE ONLY THE PEOPLE WHO COMMUTE BETWEEN  
BALTIMORE + WASHINGTON ON A REGULAR BASIS. HAVE YOU  
DETERMINED THE NUMBER OF PEOPLE THAT WOULD USE THIS (CONSIDERING  
THEY WILL HAVE NO CAR AVAILABLE AT THE END TERMINUS)? IT'S HARDLY  
WORTH THE BILLIONS TO SERVE SUCH A SMALL PERCENTAGE OF THE  
POPULATION.



DATE 24 OCT

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COMMENT FORM

Name: STEGALL, Glenn

Address: 7711 BARNSEDE COURT

City: SEVERN State: MD Zip: 21144

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: WHAT IS THE TRADITIONAL COMPENSATION FOR IMMINENT DOMAIN PROPERTY?  
 - OWNER PURCHASE PRICE - LOCAL RECTOR ESTIMATES  
 - LOCAL PROPERTY TAX ASSESSMENT - PURCHASE FROM BANK @ EQUITY TO OWNER.

IF THE PATH IS TUNNELED UNDER YOUR PROPERTY, <sup>does</sup> ~~will~~ ~~does~~ THE LAND ABOVE HAVE TO BE CONFISCATED?

~~What~~ What is the EXPECTED SOUND AND VIBRATION <sup>EMIGRATION</sup> ~~REGISTRATION~~ DISTANCE.

~~Does~~ MY PROPERTY HAS A WATER SOURCE UNDERGROUND, <sup>How</sup> ~~will~~ ~~Does~~ THIS IMPACT TUNNEL BORING, ARE THE UNDERGROUND RESERVOIRS Re-routed?

~~which~~  
~~what~~ ~~can~~ ~~visit~~ ~~road~~ ~~→~~



DATE \_\_\_\_\_

bwmaglev.info

**COMMENT FORM**

Name: Marcia & Ken Kuhn

Address: 9206 Montpelier Dr.

City: Lanham State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: KUHN.KI@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We Do NOT need this!!!  
Use the money to repair our roads / bridges  
~~Instead of~~ We should not use this money  
to help China pay for research for their Maglev train!!  
Tell Hogan & his cronies we are Voters & we  
DO NOT WANT OR NEED THIS!!!!





DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: David E. Goldenberg Ph.D.  
 Address: 12209 Brittany Pl.  
 City: Laurel State: MD Zip: 20708  
 Telephone: 301-953-9253 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This project does not do anything positive for me or my neighbors. It is an unnecessary boondoggle + waste of public \$.

There are no stops anywhere near my neighborhood. Living half-way between Balt + Wash DC, it would not be worth my time of \$ to use this service.

Elevated portion would be ugly + lower my property values. Noise would be a major problem.

NTI MRC



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: John Costigan

Address: 1310 Sewell Farm Drive

City: Hanover State: md Zip: 21076

Telephone: 804 687-1789 Email: john.costigan@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The J path not only goes straight down Sewell Farm Drive and my house it also goes directly under Hebron Harmon Elementary School and the Burley-Cook-Sims Cemetary, if residences schools & sematary are your concern this is a poor path.

I am viamently opposed to <sup>this project due to</sup> A environmental impact of this or similar projects as well as grave concerns about the potential financial impact to the county in the long run.

I strongly recommend a switch to the E1 path or the J1 Alternative path if it goes anywhere

DATE 10/24/17

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## COMMENT FORM

Name: Thomas Casey

Address: 5473 Stream Bank Ln

City: Greenbelt State: MD Zip: 20770

Telephone: 617-894-0166 Email: tcasey1995@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I generally support the MAGLEV project because it provides a much needed alternative to car travel. I recognize that having improved transit systems encourages economic growth, reduces carbon emissions due to traffic congestion, and creates competition among transportation industries that may bring prices down for consumers.

However, I do not support routes that severely impact Maryland's environmental and historical landmarks. I hope that the EIS will properly weigh the environmental and EJ impacts of each of the 3 proposed routes before moving forward.





DATE 10/24/17

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COMMENT FORM

Name: VAL MOHABIR

Address: 1314 SEWELL FARM DR.

City: HANOVER State: MD Zip: 21076

Telephone: 410-595-6486 Email: VAL.MOHABIR@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: SCMAGLEV ALIGNMENT PATHWAY "I" GOES THROUGH  
OUR NEIGHBORHOOD, BURLEY CROOK SIMMS CEMETERY & E.S.

MY CONCERNS ARE:

- (1) THIS MAY DESTROY OUR COMMUNITY.
- (2) LOWER OUR PROPERTY VALUES.
- (3) IF EMANATE DOMAIN BECOMES A PART OF THIS, WHAT IS  
FAIR MARKET VALUE.



DATE 10/24/17

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**COMMENT FORM**

Name: Susan Walker

Address: 35 RIDGE RD UNIT E

City: Greenbelt State: MD Zip: 20770

Telephone: 301-345-3597 Email: SWA.MSW.LICSW@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am President of the Greenbelt Community Development Corporation. I see that the MAGLEV might be good for economic development. But you need to give all of us a lot more information. I was really disappointed at how unknowledgeable your staff is - they didn't even know the information on your board!

I will be following closely and getting a lot of people politically involved to be sure this doesn't negatively impact Greenbelt. Also following info

SANAL





DATE 24 Oct 17

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**COMMENT FORM**

Name: Alfreda Lewis  
 Address: 12232 Apache Tears Circle  
 City: Laurel State: MD Zip: 20708  
 Telephone: (301)213-8107 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I Opposed to the idea of running any  
transit system through Laurel, MD  
because it doesn't benefit residents  
if there's no stopping or jobs for residents  
of PG County. Thank-you, Ms Lewis





DATE 10/24/2017

*bwmaglev.info*

**COMMENT FORM**

Name: Nickesha Brown

Address: 12243 Apache Tears Circle

City: Lanard State: MD Zip: 20708

Telephone: 908 422 2121 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I reviewed the propose route which goes directly over my house. I own the end unit on Apache Tears Circle. and oppose to this project I refuse to allow my ~~ow~~ home of one year. to be demolished or my home life to be affect.

\_\_\_\_\_

\_\_\_\_\_

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\_\_\_\_\_

\_\_\_\_\_



DATE 10/24/2017

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**COMMENT FORM**

Name: Djuna Craig  
 Address: 9225 Fairlane Place  
 City: Laurel State: MD Zip: 20708  
 Telephone: \_\_\_\_\_ Email: djuna\_craig@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: After reviewing the remaining alternatives for the project, I am unfortunately not surprised that the alternatives impact low and middle income property owners. While I appreciate all attempts to improve transportation options, would it be too much to ask to invest in the existing <sup>rail</sup> system to improve reliability, ~~and~~ access and availability? I am frustrated that these projects seem to always fall on the citizens ~~in~~ with the least ability to rebound financially from the impact. It shouldn't be impossible to consider all citizens and not just those who have the ability to organize and prepare opposition more efficiently. No one here was able to answer ~~the~~ the question of usage, cost per use or whether the ~~forward~~ expenditure would be cost productive to the use.





DATE 10-23-17

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**COMMENT FORM**

Name: Jon Berle  
 Address: 3746 Chateau Ridge Dr.  
 City: Ellicott City State: MD Zip: 21042  
 Telephone: (410) 720-2003 Email: jonberle@yahoo.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: My strength is utilizing the written and printed word - at some point I believe a quarterly MAGLEV newsletter will be warranted, mailed directly by carrier route pre-sort to the zipcodes affected - retention value for printed material, according to a study at TSU, is 3x-4x greater than electronic or online.  
I see that MAGLEV's "magnetic personality" has so far failed to attract the masses but I believe public perception can be changed if we present actual scientific facts about the technology, and this will dispel the dogma people have - they basically will fear or be suspicious of new technology & things they don't





DATE 10-24-2017

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**COMMENT FORM**

Name: Elaine M. Skell

Address: 8501 Montpelier Dr.

City: Laurel State: MD Zip: 20708

Telephone: 301 776-4271 Email: elaine@greville.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This is an opportunity to prove an unnecessary technology. It makes no sense in terms of need between these 2 points. Most BW Pkwy traffic starts S. of BWI and ends before the center of DC. There are many intermediate destinations and this traffic will not be available for this product. Do not pretend that most all users of the Pkwy. are heading either near downtown DC or near BWI. Even if money is available this effort should not happen.

January 2018 DISRUPTIVE, Tremendous IMPACT to BW Parkway health's health just not a valuable product Page C1.062



DATE Oct 24, 2017

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**COMMENT FORM**

Name: T. Benedik  
 Address: 7826 Hanover Pkwy #101  
 City: Greenbelt State: MD Zip: 20770  
 Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: So far I have not seen how this will help with traffic issues. With only limited stations, there will still be traffic to/from suburbs to ride. There is no benefit for users that actually live on the route. Right now there is a WMATA bus from Greenbelt to BWI but that route is not heavily traveled so there have been recent reductions in service. I am curious how you think there is enough ridership to/from Baltimore and Washington.

The speedy commute sounds nice but unless there is a way for users to take advantage of the service from all over the area, I don't see how this will succeed.







DATE 10/24/17

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**COMMENT FORM**

Name: Kadijah Hughes  
 Address: 12210 Blue Moon Ct.  
 City: Laurel State: MD Zip: 20708  
 Telephone: 301-498-8236 Email: Kizhughes@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I don't want this Project. I am a  
home owner and I have a business in my home. I'm  
loosing all if this Project comes to my area. This  
train will also effect the lives of individuals with  
health issues like pace makers. Why can't they  
Choose a undeveloped area away from people,  
homes, business or schools. The purchase of homes  
is to live in that community. No Train IN  
Laurel. I love my community my business has  
been here for 17 years.



DATE 10-24-17

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**COMMENT FORM**

Name: AMY PERGOSKY  
 Address: 13505 KEYSTONE CT.  
 City: LAUREL State: MD Zip: 20708  
 Telephone: \_\_\_\_\_ Email: APERGOSKY@YAHOO.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: MY HUSBAND AND I LIVE TO THE WEST OF YOUR POSSIBLE ROUTE, ON THE WEST SIDE OF 295. WE DO NOT SUPPORT THIS PROJECT. IT WILL UNDOUBTEDLY REDUCE OUR PROPERTY VALUE. WE'LL NEVER USE IT. USE THAT MONEY TO WIDEN 295 AND IMPROVE EXISTING LOCAL ROADS. THE CONSTRUCTION NOISE AND TRAFFIC DELAYS WILL BE UNBEARABLE. I'M NOT COMFORTABLE BEING THE FIRST ONE IN THE COUNTRY TO HAVE THIS. WITH HEIGHTENED SECURITY CONCERNS IN THIS COUNTRY I THINK IT'S FOOLISH TO CONNECT SOMETHING LIKE THIS TO DC! AND IT'S APPALLING THAT YOU MAY USE EMINENT DOMAIN TO TAKE PEOPLE'S PROPERTIES!! I WILL

CONTINUE TO FIGHT THIS!  
Amy M Pergosky



DATE 10/24/17

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**COMMENT FORM**

Name: Fetima + Marcus Benjamin

Address: 8919 Summer Grove Dr.

City: Laurel State: MD Zip: 20708

Telephone: 301/213/2286 Email: MFsquad113@gmail.com or time-worthy@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This project will run directly over or  
~~the~~ / through my house. What step should  
my family prepare for come 2018.  
How can we fight for the property?

This is very stressful!...





DATE 10/27/13

bwmaglev.info

COMMENT FORM

Name: JACQUELYN L. DEBAR  
 Address: 1377 BECKNEL AVE - 527 BRUCE AVE - 1301 JUNE DR  
 City: ODENTON State: MD Zip: 21113  
 Telephone: 301-466-4421 Email: LADJLD@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I own three homes in Odenton & we are already over powered w/ cars from new homes, Apts, Business, we need more schools & roads to be widened not the maglev project. If we are to be tried on this in some way, were & who will pay for this Md, Pa, Va, N.Y. I only want to see it as underground projects. Linthicum will be impacted by this and there is no meeting set up yet & they need one. If Port Covington is looking to profit from this, they should be the one to pay. I vote no for this project.

Thanks  
Jacquelyn L. Debar



DATE 10/24/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: D YUAN

Address: 10405 BALSAMWOOD CT

City: LAUREL State: MD Zip: 20708

Telephone: 301-257-0725 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ~~Not a comment~~

I am strongly opposed to the Maglev project.  
To read in the Washington Post that "Hyperloop" has already  
been approved for tunneling raises the question of proper  
State planning, as the two multi-billion projects have the  
same purpose.

It is stunning that neither State nor Federal officials at the  
October 24 meeting would speak with me about Hyperloop.



DATE 10/24/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: D YUAN

Address: 10405 BALSAMWOOD CT

City: LAUREL State: MD Zip: 20708

Telephone: 301-257-0725 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am strongly opposed to the Maglev project.

To hear from the Federal representative - Oct 24 - that no  
Impact statement has been submitted yet, let alone available  
for public review, is IRRESPONSIBLE of the State DOT.

- Ridership

- Fares

- Reliability

- Safety to riders

- Environmental impact





DATE 10/24/17

*bwmaglev.info*

**COMMENT FORM**

Name: Carlton Rice

Address: 12500 Silverbirch Ln

City: Laurel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: carice@stmp.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: As a resident of South Laurel, I would  
certainly hope that the investment would be  
made to bury the Maglev in our area, just  
as it has been buried from Baltimore to  
route 175. For a project of this magnitude,  
it would seem pennywise and pound foolish to  
scimp on this.





DATE 10/24/17

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**COMMENT FORM**

Name: JOSEPH LESTER

Address: 12412 POPLAR VIEW DR. BELTZ, MD. 20720

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: 301) 789-8882 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: NO EI. THE WHOLE IDEA IS FOOLHARDY.  
TO EXPENSIVE. NO WAY RIDERSHIP IS WORTH ALL OF  
THIS. UNNECESSARY. GOVERNOR HOGAN'S VANITY PROJECT.  
IT WILL BE A REAL NEGATIVE FOR OUR LOCAL COMMUNITIES  
AND AFFECT OUR QUALITY OF LIFE.





DATE 10/24/17

baltimorewashingtontscmaglevproject.com

COMMENT FORM

Name: Fitroy Dany

Address: 11409 Hermosa Dr

City: Lanham State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: fitroyd@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am very uncomfortable with the fact that the line will be coming above ground immediately in front my home my home is on Hermosa Drive and I do not understand why the line cannot go under ground. The speed, construction and maintenance of train & rail will have a significant impact on my home. I believe the structure will deteriorate along with the value. The noise and sounds will also have a significant impact on me and my family. I believe it is not in the best interest of the people currently living on Hermosa Drive to have this expensive train system that will repeatedly affect taxpayers in the future.

DATE 10/24/17

bwmaglev.info

**COMMENT FORM**Name: Sterling SpanglerAddress: 3404 Bitterwood Place T-303City: Laurel State: MD Zip: 20724Telephone: 301-490-0775 Email: sterling@sterlsnet.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: This project would be too disruptive and not useful for the majority of the population. The area around the proposed rail yard off of Soil Conservation Road is already a traffic mess, especially around the Blw Parkway, which is horrendous 7 days a week. This will not alleviate traffic and provide little benefit. Such a huge investment and permanent negative impact for so little benefit to the community through which it passes. I can picture a concrete monstrosity running next to the Parkway. What if you run out of money? Then there will be an unfinished monstrosity forever, unless the taxpayers bail them out. This will increase traffic along the Parkway horribly.



DATE 10/24/2017

bwmaglev.info

COMMENT FORM

Name: CARL CARINGTON

Address: 12200 APACHE TERRAS CIRCLE

City: LAUREL State: MD Zip: 20708

Telephone: 3015372251 Email: Cmyreactor@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

QUESTIONS!

1) EMINENT DOMAIN? WHO ACTIVATES THIS PROCESS? WHEN WOULD SOMETHING LIKE THIS GO INTO EFFECT?

2) SOLE AGENT - How is it handled

3) ELECTRO MAGNETIC FIELDS - THE LITERATURE SAYS THAT IT WILL NEED A SUB-STATION THAT IS 1.5 ACRES. WHAT TYPE OF EMF WILL THAT CREATE?

4) CONSTRUCTION IMPACT TO NEIGHBORHOODS - ~~WORDS~~ OF THE REPRESENTATIVE SAYS THAT PROPERTIES THAT ARE 72 FEET FROM THE CONSTRUCTION WILL BE MONITORED FOR ISSUES. IS THERE ANY THOUGHT OF EXTENDING THAT RANGE?

2



DATE 10/24/17

bwmaglev.info

## COMMENT FORM

Name: Kelly UmeAddress: 12107 Ivory Fashion CourtCity: Laurel State: MD Zip: 20708Telephone: \_\_\_\_\_ Email: Kelly.ume@hotmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I moved from the busy city of New York to Laurel MD four years ago to experience a sense of "Burburbia." My hope is that this train project will not go through. Not because I don't support the idea of it, but because it will destroy the suburban neighborhood. If I wanted to live in the city, I would live in D.C. The noise and congestion it will cause is disheartening.

Please have a heart, would you like someone to build a train by your home?? Just think about that and if yes, move to the city.

Thanks.



DATE 10-24-17

bwmaglev.info

COMMENT FORM

Name: Alice Tracy

Address: 8300 Cypress St

City: Laurel State: MD Zip: 20707

Telephone: 301-490-5639 Email: AliceTracy169@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1) The environmental areas, especially where the maintenance yard(s) are to be located, are an increasingly rare resource. To site the train through those areas will be detrimental to the protection of the environment.

2) No citizens in the alignment route areas will be served by the train. It does nothing for us. We are not driving to BWI to get on the train.

3) Place the maintenance yard in a failing strip mall. That way you won't have no future.



DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: Nadine Raley, Esquire

Address: 13000 Bay Hill Dr.

City: Beltsville State: MD Zip: 20705

Telephone: 301 502 0908 Email: nmo76@hotmail

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

*Keep website up to date!*

Comments: \_\_\_\_\_

Any reduction in emissions or reduction in I95 road traffic will be so minimal! The residents of PG/AA counties will truly see No benefits - they will have to drive to Baltimore or DC, pay to park & ride a very expensive train, so they won't save any time or money - will still deal with local traffic. So the area residents are bearing the burden for the benefit of the non-local commuter (North of Baltimore, DC & NY).

- what a terrible waste of <sup>any</sup> local resources!!

Where is the Hyperloop going to go? Are we having both?

Where? What a wasteful shame. Just b/c Hogan saw a cool idea doesn't mean he should shove it on anyone.

Fix the traffic on the beltway!!

I sure feel really bad for the people whose homes are being taken.



DATE 10/24/17

bwmaglev.info

## COMMENT FORM

Name: Robert Walker

Address: 543 Bruce Ave.

City: Baltimore State: MD Zip: 21113

Telephone: 410-672-1050 Email: rw.towers@fistmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: We would like to see the proposed routing to be underground in any areas that this would affect residential properties. We have worked to hard for most of our lives to have our homes destroyed by so called progress. These neighborhoods affected will not see any benefits of this train. Traffic from Washington to Baltimore may be eased but the people affected will not be using this train with no stops to board, I don't think we're going to drive to DC to catch a ride to Balto. Even if privately funded I don't see much benefit to those it will disrupt the most. The cost to tunnel underground should just be the cost of this progress rather than destroying homes and lives as a cheaper alternative



DATE 10/24/17

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**COMMENT FORM**

Name: Jamar Staples

Address: 9915 Mallard Dr

City: Laurel State: MD Zip: 20708

Telephone: 301-404-1816 Email: Jamarstaples8582@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Please mail me a hard copy of all info ~~shown~~  
shown today



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Roger McClain

Address: 13114 North Potomac Ln

City: Laurel State: MD Zip: 20708

Telephone: 301-776-5545 Email: mickeley4@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This project serves no useful purpose for residents  
of South Laurel. The removal of homes along the route  
will cause major disruption of lifestyles for those  
displaced and lower the value of homes near the route  
which will change the demographics of the area and  
increase crime and lower the standard of living.





DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: JUDY McCLAIN

Address: 13114 NORTH POINT LN.

City: LAUREL State: MD Zip: 20708

Telephone: 301-776-5545 Email: RAMCCLA@AOL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This does not benefit those of us in South Laurel.  
Residents in the area where it is to be built will lose  
their homes. It will lower our property values which will  
then change the demographics for our area which possibly  
would increase crime. We do not want this in S. Laurel.



DATE 10-24-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Joan Goldenberg

Address: 12209 Brittany Pl.

City: Laurel State: MD Zip: 20708

Telephone: 301-953-9253 Email: locusjg@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1- This is disruptive to my community with no benefits.

2 - destroys house values

3 - noise levels moving higher destroys ambiance and increases ill health

4 - What does it cost to even use this? It isn't like there aren't other rail options.

5 - Why not use I-95 if it must be built. How about the CSX tracks.

6 - This seems to be a boondoggle that only benefits the construction industry.



DATE Oct 24 2017

bwmaglev.info

**COMMENT FORM**

Name: Joyce A Loney

Address: 501 LaClaire Ave

City: Linthicum State: MD Zip: 21090-1925

Telephone: 410-859-1397 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Gov Hogan: Please consider this project carefully. Do we really need this train when we have Amtrak? Please do not approve any route that slices through Linthicum, destroying homes and peace of this beautiful community. I was against maglev 15 years ago and I still oppose any train that runs through Linthicum.

Sincerely - Joyce Loney





DATE 10/24/17

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**COMMENT FORM**

Name: Kimberly Sreed  
 Address: 12200 Blue Moon Ct  
 City: Laurel State: Maryland Zip: 20708  
 Telephone: 240-204-1065 Email: Laurellady@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I've lived in South Laurel's Montpelier Hills for over 20 years. If one of the routes is approved it will demolish the home I sacrificed to purchase. This is a community of businesses, schools and residences. We have already endured the housing mortgage crisis in the county. Now our community is on the block for a project that does not benefit our community. I find it interesting your office is looking at residential areas with predominately African-American homeowners to make yet another sacrifice. Totally unacceptable. I am



DATE 10/24/2017

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**COMMENT FORM**

Name: Leslie Curtis

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: LeslieCurtis57@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: While the technology seems ready for the future it doesn't seem like it will serve the greater good of homeowners, taxpayers, educators, families, or the environment. Also, the SCMAGLEV will disrupt neighborhoods, causing many to leave their homes, ruining their quality of life & likely ability to find & buy a new place to live.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_





bwmaglev.info

DATE 10/24/2011

COMMENT FORM

Name: Patricia Roges  
 Address: 14 Northway  
 City: Greenbelt State: MD Zip: 20770  
 Telephone: \_\_\_\_\_ Email: Tissaroges@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: With no stations in MD suburbs, train would  
be of no use to Greenbelt residents  
Potential long term expansion to New York  
might be a possibility for benefit

would worry about wildlife that live in woods  
between Greenbelt houses and B/W Parkway. Already  
woods are inhabited by deer, foxes, owls and other  
wildlife. Running a train so close to them  
might frighten them away.





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DATE 10/24/17

COMMENT FORM

Name: Howard Campbell  
 Address: 11202 Hickory Grove Court  
 City: Lanrel State: MD Zip: 20708  
 Telephone: 301-210-3723 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: If this project is built, please put it underground so it doesn't impact mine and others neighborhood.

I think any money spent on this would be better spent fixing existing roads and bridges. Seems like a big waste of time and money for an ~~air~~ airport shuttle. I can't imagine a very big ridership for this.



DATE 10/25/17

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COMMENT FORM

Name: SEAN BEAVER

Address: 9201 PLEASANT CT.

City: LAUREL State: MD Zip: 20708

Telephone: 301.490.2113 Email: Sean.beaver@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments:

Question 1 - Was the I-95 corridor considered, and if so, what caused it to fall off the list?

Question 2 - Why is more of the track raised as opposed to underground/tunneled?

Question 3 - How to scale are the current maps regarding placement of the rail path? - On MAP 8, near South Laurel/Montpelier, the path crosses over Muirkirk Rd. Is this accurate?

Question 4 - I am hoping that, if built, there would only be 1 Maintenance yard and not the potential for both that are shown on the map?



DATE 10/24/17

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## COMMENT FORM

Name: Brian & Yvette SMITH  
 Address: 12125 Ivory FASHION CT  
 City: Laurel State: MD Zip: 20708  
 Telephone: 301 498 2999 Email: Yvette.SMITH@EDUSERC.ORG  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I am very upset & disturbed that the  
superconductor train is proposed to destroy  
and demolish ~~my~~ <sup>our</sup> neighborhood. ~~We~~ have  
lived in the Montpelier Hills neighborhood  
for more than 20 years. We have a tight knit  
neighborhood & community. ~~We~~ have  
children that go to school in the  
neighborhood and destroying our  
homes for a train that will not  
benefit the residents of Laurel is  
sad and a disserve to Laurel.  
We are proponents of change and  
technological advances but not at  
the expense of an entire community.





DATE 10-24-2017

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**COMMENT FORM**

Name: GARY LACKEY

Address: 6219 GROVELAND RD.

City: LINTHICUM HEIGHTS State: MD. Zip: 21090

Telephone: (443) 716-6016 Email: GARYLACKEY@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: TOTALLY OPPOSE THE ALTERNATIVE E1 TUNNELED  
ROUTE. THIS ROUTE IS 1 BLOCK AWAY (WOODLAND RD) FROM  
THE STREET (GROVELAND RD) WHERE MY HOME IS LOCATED.



DATE 24 OCT 17

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**COMMENT FORM**

Name: Darryl Robinson

Address: \_\_\_\_\_

City: Laurel State: MD Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: robinson200@msn.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: TOTALLY AGAINST THIS PROJECT!!!

This rail does not benefit the residents that are impacted the most, Prince George's residents. For Prince George's county residents, there are only take aways: land, environmental, Property, and peace of mind. As A alternative, the rail should be completely underground, that way it will have very little impact on residences and the environment.



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

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COMMENT FORM

Name: THOMAS G. CANNING

Address: 9223 OREGON COURT

City: LAUREL State: MD Zip: 20708

Telephone: 301 498-5672 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: 1. Don't believe any effort or expenditure is worth  
improving travel time from 40 to 45 minutes to 15 minutes  
between D.C and Baltimore. However beyond that I  
do see major benefits in many different areas to a  
high speed travel time from 4 1/2 hours to 1 hour  
between D.C and New York.

2. From my point of view, the E alternative would be  
the most desirable.





DATE 10/24/2017

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**COMMENT FORM**

Name: Nickesha Brown

Address: 12243 Apache Tears

City: Laurel State: MD Zip: 20708

Telephone: 908 422 2121 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I reviewed the propose route which goes  
directly over my house. I own the end unit  
on Apache Tears Circle and oppose to this project  
I refuse to allow my <sup>home</sup> own of one year to  
be demolished or my home life to be affected.  
This would also lower my property value.



DATE 10/24/17

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**COMMENT FORM**

Name: Ms. GERRY WASSMANN

Address: 503 LA CLAIR AVE

City: LINTHICUM HEIGHTS State: MD Zip: 21090

Telephone: \_\_\_\_\_ Email: WASSMANN5@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: GOV. HOGAN - I THINK YOU ARE DOING A GREAT JOB!  
THANKS - BUT, I AM NOT IN FAVOR OF THIS MAGLEV TRAIN.  
TOO DISRUPTIVE FOR TOO MANY PEOPLE. HOPE IT IS NOT  
APPROVED!



DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: DAVID CLINE

Address: 12305 SW Pearl Ct

City: Laurel Md 20708 State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I live in South Laurel in the Montpelier Hills community. One of the proposed alignments it appeared would cut through the area beside my home that borders and protects me from visual and noise traffic from 295 parkway and 197. During an upgrade to 295 and 197 a considerable portion of this tree border was lost. Any plan on the west side of 295 in the area would leave our homes with more noise, worse view of traffic, and would probably lower property value. As a retiree looking to downsize in the future, property value is a concern. Also as a local resident it is hard to see the benefit for my community.





DATE 10/24/2017

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COMMENT FORM

Name: Franklin Bueno

Address: 9115 Summer Grove Drive

City: Laurel State: MD Zip: 20708

Telephone: 240-264-1507 Email: franklbueno@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Oppose to the BWI Parkway  
installation of MAGLEV.

It will bring misery to our community  
at Summer Grove. More feasibility  
study is needed. This is a quick  
meeting called in. We were not  
fully briefed. ~~for~~



DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: Tatjana Aldag  
 Address: 12001 Lanham Severn Rd  
 City: Bowie MD State: MD Zip: 20720  
 Telephone: 301-352 3642 Email: taaldag@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Who got this idea?

- High speed train through Historic Bowie is not very comparable with the town of Bowie.
- Every resident on the street is organizing to fight this Penn-Line Amtrak alternative
- It would close several businesses, including my business Bowie Assisted Living, Inc.
- Many trains have accidents, and in this populated area it would be tragic.
- Residents are <sup>worried</sup> ~~worried~~ about the strong magnetic fields and personnel health issues.
- The neighborhood is hiring a lawyers to be ready to fight the Penn-Line Amtrak alternative. if it develops.

Thank you for Tatjana A. Aldag  
 opportunity to object it project.



DATE 10/24/17

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**COMMENT FORM**

Name: Salisa Lamb

Address: 9236 Espanan Loop

City: Land State: MD Zip: 20708

Telephone: 240-593-2711 Email: SalisaLamb@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am concerned how this will effect  
the value of my home.

The noise level deciduous growth  
is destroying the trees and undergrowth &  
wild animals with construction.





DATE 10/24/17

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**COMMENT FORM**

Name: Linda Roberts-Antino

Address: 7713 Riky Road

City: Severn State: MD Zip: 21444

Telephone: 410-944-4440 Email: lindarobertsgallery@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: No ONE in our family, NO friend, neighbor  
other relatives would benefit from the  
SCMAGLEV project! I feel this is true for  
the majority of Maryland families. It doesn't  
seem necessary or prudent.

We are against the project & will be  
participating in the "Stop the MAGLEV train"  
efforts!



DATE 10-24-17

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**COMMENT FORM**

Name: DANIEL SHEFFIELD

Address: 7100 GUYEN LA

City: LAUREL State: MD Zip: 20707

Telephone: 301 497 7655 Email: FOPDAN@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I SUPPORT THE IDEA OF THIS TYPE  
OF TECHNOLOGY, ESPECIALLY THE IDEA OF  
IMPROVING OUR TRANSPORTATION OPTIONS AND  
DEVELOPMENT OF THE FUTURE. HOWEVER  
I WOULD APPRECIATE MORE OF THE TRACK  
BEING UNDERGROUND AND LESS DISRUPTION  
OF OUR HOMES IN THE ~~BE~~ IMPACTED AREA.  
THANK YOU.



DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: Mrs. Sierra Leone Nixon

Address: 12604 Cremona Court

City: Bowie State: md Zip: 20715

Telephone: (443) 398-0954 Email: Sierra/leone439@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This transportation system will disrupt my community and life as I know it. The view shed will be significantly compromised and this is not something the community asked for or even needs. The benefits to Bowie residents are miniscule, so I would like to see this whole idea scrapped. I am positive that this plan is a horrible idea. I regret that this meeting did not "let our voices be heard" and I believe this was by design. A microphone should have been available so that we could all express our desire to get rid of this train:





DATE 10/24

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**COMMENT FORM**

Name: Xavier Smith

Address: 1275 Ivory Pasture Ct

City: Laurel State: MD Zip: 70708

Telephone: 301-448-7222 Email: xavier.smith@icloud.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I think this is a waste of time <sup>3</sup> money.  
Just improve the subway system or get a Q-Line  
like in Detroit. That's simple. Don't overcomplicate things.



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

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**COMMENT FORM**

Name: Rolf Hill

Address: 1009 Ice Crystal, Odenton

City: Odenton State: md Zip: 21113

Telephone: 410-672-5836 Email: RolfPA@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Not commuter friendly --- but maybe that's not the intent. I understand, more stops means longer times. Also may compete with MARC commuter trains. Consider ONE additional stop -- to be used only during rush hour. It could be in the Agricultural area in Beltsville.

Good into at meetings. Not sure the guy who lives within 500 feet will ever jump on the bandwagon.



DATE Oct 24, 2017

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**COMMENT FORM**

Name: J D Perkins  
 Address: 5303 BREWER RD  
 City: BELTSVILLE State: MD Zip: 20705  
 Telephone: \_\_\_\_\_ Email: Perk1508@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: To lessen the environmental impact,  
why not elevate the line over the  
existing railway tracks?





DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: VITO TINELLI - TREASURER FOR BLADENSBURG

Address: 4229 EDMONSTON RD

City: BLADENSBURG State: MD Zip: 20710

Telephone: 301-927-7048 Email: VTINELLI@BLADENSBURG.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

① CAN WE GET A FULL SIZE MAP OF SHEET 7 TO HANG IN OUR TOWN HALL FOR OUR RESIDENTS, MAYOR, & COUNCIL TO USE.

② UNDERGROUND -

① HOW DEEP UNDER RESIDENTIAL AREAS?

② WILL BUILDINGS NEED TO BE DEMOLISHED AT/NEAR AREAS ITS UNDERGROUND

③ VIBRATION ANALYSIS - IMPACT ON STRUCTURES, FOUNDATIONS, ETC. AND HOW WIDE OF AN IMPACT AREA

THANK YOU IN ADVANCE!



DATE 10-24-17

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**COMMENT FORM**

Name: Leonard Bers  
 Address: 10405 SNOWDEN RD  
 City: LAUREL State: MD Zip: 20708  
 Telephone: 301-254-2377 Email: Lenny B @ Prodigy.net

Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: ① Huge # of unknowns  
- sound  
- foot print + 'eye' print  
- placement of supporting structures  
electrical  
- lighting + light spread  
- the economics - how will  
it support itself  
- the need of this project  
other than an attraction  
② There is no evidence of community  
benefit of a high speed train that  
races thru community but does not  
provide any transportation benif's





DATE 10/24/2017.

bwmaglev.info

**COMMENT FORM**

Name: MARY E. Diskend

Address: 8206 White bark Lane

City: Severn State: MD Zip: 21144

Telephone: (410)519-8220 Email: MARY.DISKEND@HOTMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Stop the train. Our tax  
dollars should not be used for this  
ill-conceived mode of transportation  
I do not want a high speed train  
Running under my house or above  
me. There has not been a well  
done study of human exposure to EMF  
pose barriers. This train will create  
billions of dollars of debt for the tax  
payer and will result in the destruction  
of homes a decline in property values  
& Anne Arundel County will lose 4

Schools alone.  
Stop the train





DATE 10/24/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Angelita Bueno

Address: 9115 Summer Grove Drive

City: Cannet State: MD Zip: 20708

Telephone: 240-264-1507 Email: N/A

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Oppose to the project 100%,  
please do not run it  
to BWI parkway.



DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: Wayne E. Falls  
 Address: 6913 Emerson St.  
 City: Hyattsville State: MD Zip: 20784  
 Telephone: 301-322-1918 Email: Wayne.falls@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

Stop TRAIN Project, Two in Process Hyperloop + Maglev  
Families + Communities will be lost. Taxes raised and for what  
15mins. No! Vibration, Health, Environment lost. We already have  
Amtrak, MARC. Repair Highways Better way to spend millions  
Run Metro Bus to Baltimore, So. No! No!



baltimorewashingtontscmaglevproject.com

DATE \_\_\_\_\_

**COMMENT FORM**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: this would <sup>add</sup> ~~add~~ debt for years, the environmental effects would be irreversible, this would greatly impact the local Laurel housing market in a negative way the likelihood of accidents could be devastating to many I am totally against this project. The cons outweigh the pros if money isn't the motivating factor

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BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE 10-24-77

bwmaglev.info

COMMENT FORM

Name: EDWARD M. ANDERSON  
 Address: 12101 IVORY FASHION COURT  
 City: LAUREL, MD State: MD Zip: 20708  
 Telephone: 202-271-0422 Email: edwardanderson@vaq.com.com  
 Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am strongly  
 AGAINST the MAGLEV  
 Project because it  
 will disrupt residential  
 neighborhoods.



DATE 10/24/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Warren Leister

Address: 332 11th St. Apt. 8

City: Lauvel State: Md Zip: 20707

Telephone: 757-407-1291 Email: thetfordboy@rocketmail.com  
thetfordboy@rocketmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Excellent open house! Love the  
technology! I am a local leader in the  
Odenton/Ft. Meade area. I also have an  
extensive background in building <sup>maintaining</sup> Navy  
ships and Commercial/Military aircraft.  
I work for OSD at the Pentagon. I had  
a long enjoyable talk with Furqan Siddiqi.  
He seemed very knowledgeable and enthusiastic.  
He answered all my questions in detail. I was  
very impressed with him and the project!  
I wish the project every success!

Cheers,  
Warren Leister





DATE 10-24-17

bwmaglev.info

COMMENT FORM

mailing address:  
 P.O. Box 3694  
 Laurel, MD 20709

Name: Michelle Y. Walker

Address: 12228 Apache Tears Circle

City: Laurel State: MD Zip: 20708

Telephone: (301) 257-7544 Email: mywalker3694@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: My community/residential location is directly impacted by one of the proposed routes. I am adamantly opposed to this transportation proposal since it has no economic benefit to our community - It fails to address the ongoing traffic congestion on the Baltimore-Washington Parkway, particularly at the 197 (Laurel-Bowie) junction. Furthermore, it does create any new jobs for the ~~se~~ local area residents. Implementation of this transportation alternative will also adversely impact our property values in a substantial way. Even by mailing, I do not believe we could be satisfactorily compensated for <sup>estimated</sup> ~~proposed~~ future home values.





DATE 10/24/17

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**COMMENT FORM**

Name: Reggie Allen

Address: 9202 Sumner Grove Drive

City: Larrel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: rfallen1425@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do not like the idea  
of this project being built  
through my neighborhood, Sumner Grove,  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
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DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Gail G Brownlee  
 Address: 9013 WIPKEY CT  
 City: Bowie State: MD Zip: 20720  
 Telephone: (301) 529-5858 Email: mrs.gbrownlee3@aol.com  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: I am a PG county resident residing in an area that will directly be affected by the proposed E1 line. I am located just a few houses from this proposed site and am fearful of the impact it will have on my home & children. Not only am I fearful of the future of my family but my neighbors that my children have grown up with for the past 13 years. Also unfortunately the fate of our residence in particular considering the housing market or the fact that we have no equity (upside down).



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kim Hall  
 Address: 9104 Wipkey Ct  
 City: Bowie State: MD Zip: 20720  
 Telephone: \_\_\_\_\_ Email: Kimtnapier@hotmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am a resident of ~~DC~~ within Prince George's County and E1 runs directly through my neighborhood, and more specifically, my actual street. I am adamantly opposed to this train and the proposed E1 line. This train could potentially cause me to lose my home under the worst-case scenario or cause me to lose my right to quiet enjoyment under a best-case scenario. I have heavily invested in my county & community. This proposed high-speed train does not benefit me or the residents of Prince George's County.




**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/24/17

baltimorewashingtongmaglevproject.com

**COMMENT FORM**Name: Herman GebrehiwetAddress: 1302 Ave RoadCity: Severn State: MD Zip: 21144Telephone: 202-469-2434 Email: gebrehwet@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I am writing this to voice my concern about the impact it will have on my ~~live~~ lively hood. I move to my residence with my husband a year ago to the neighborhood. We picked it for the peace & quiet & for its serene location. ~~Be~~ Because I work from home, and I am student. However, this impact my work ~~at~~ because I would not be able to concentrate, Despit the claim there is no vibration will not be a factor, I think it will be a factor & it will affect the value of ~~of~~ our house aswell.



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: The Rev. Connie Reinhardt  
 Address: ~~1111~~ St George's Episcopal Church 7010 Glenn Dale Rd  
PO Box 392  
 City: Glenn Dale State: MD Zip: 20769  
 Telephone: 301-262-3285 Email: rector@stgeo.comcastbiz.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: The E1 Route is currently planned to go  
directly through St. George's church property,  
including St George's historic cemetery. Our  
cemetery is still in use and has been since  
the 19th century. We have an historic chapel  
which would be directly affected, as is our  
current church building at 7010 Glenn Dale  
Road. If this route continues to be in play,  
I and the Episcopal Diocese of Washington, which  
includes Prince George's County, would like to  
know how it would seem a good idea to you  
to tear down our church and disrupt those  
who have gone to their eternal rest - or those



DATE 10/24/2017

bwmaglev.info

**COMMENT FORM**

Name: TIMOTHY BROWN

Address: 1120B HICKORY GROVE CT

City: LAUREL State: MD Zip: 20708

Telephone: 301 633 4252 Email: Starduster72@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am opposed to alternative J1. It seems that it would inconvenience many more people than J, but J would require cooperation from Secret Service and BARC. I hope our govt puts its citizens first.

In addition, I believe this project is unnecessary and taxpayer dollars should not be used. Let the private sector pay and take the risk.





DATE 10-24-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Carol Brown

Address: 12242 Apache Tears Circle

City: Laurel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: Carol.brown927@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am opposed to this project that shows no benefit to the Laurel Community. It will tare down homes, decrease property value, and have a <sup>Negative</sup> impact on school, churches and businesses. Inspite of the claim that all communities along the route will reap economic benefits, there is no proof or guarantee that this would happen. This project is an in sult to the neighborhoods affected.



BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV PROJECT

DATE Tuesday Oct 24, 2017

baltimorewashingtongmaglevproject.com

COMMENT FORM

Name: Rudy Matthews

Address: 11412 Elmshorn way

City: Laurel State: MD Zip: 20708

Telephone: 240-472-9162 Email: Rudy7000@Verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have lived and worked in laurel for over 13 years. I recently bought my home on Elmshorn way in sept of 2016. I love this community and I love my home and I do not wish to see a Train destroy this and all I have come to as Home!



DATE 10/24/17

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: London Terry

Address: 1302 Ava Rd

City: Severn State: Md Zip: 21144

Telephone: 443-525-8173 Email: Lterry21207@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I have a lot of concerns for the yellow line proposal. This line passess through my property and I have concerns about sink-holes and vibrations and noise. ~~What~~ What is the cost for riding this train? How can you determine ridership when there is only 3 stops?





DATE 10-24-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kim CRAMER

Address: 7717 ORA CT

City: GREENBELT State: MD Zip: 20770

Telephone: 301 474-2939 Email: KA.CRAMER@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: ISNT IS MORE FEASIBLE TO EXPAND THE MARC TRAIN  
OR TO ADD AN ADDITIONAL LANE TO THE BW PKWY? YOU ARE  
CONSTRUCTING A 300 MPH TRAIN TO GO 30 MILES. REALLY? WILL IT  
EVEN REACH 300 MPH IN THAT SHORT DISTANCE?

IS THIS JUST A PROJECT FOR POLITICIANS TO TRAVEL BETWEEN THE  
TWO CITIES OR FOR THE POLITICIANS TO GET A QUICK RIDE TO THE  
BWI AIRPORT?

LIVING IN GREENBELT, ISNT THE HISTORICAL SIGNIFICANCE EVEN  
JUST A LITTLE BIT IMPORTANT? OR DO WE JUST NOT GIVE ONE  
THOUGHT TO THAT.



BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: George Brownlee

Address: 9013 Wipkey Ct

City: Bowie State: MD Zip: 20720

Telephone: 301 502 4195 Email: MRMRSG63@AOL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: This will be a severe inconvenience to my family if E1 is involved. I ask you consider removing E1 as an option.



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Jose Galdamez

Address: 5005 69th Ave

City: Riverdale State: MD Zip: 20737

Telephone: 240-640-5660 Email: galdamez@maci.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I happen to live on a street where underground tunnels  
~~are~~ J1-BWP and J2-BWP will cross. Fortunately, I live atop a  
large and tall hill. I'm not concerned if those are the routes chosen.  
I think the tunnel will be deep enough that any structural impacts  
will be negligible if not non-existent. A lot of my neighbors are  
really concerned, however. I came here to get some basic questions  
answered which onsite staff did in a friendly and courteous  
manner.





DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: Patricia Behenna

Address: 12615 Cedarbrook Lane

City: Laurel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: behenna@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concerns - disruption of neighborhoods for no  
benefits to residents.

~~Has~~ - we don't need this!

- disruption to environment - will want to  
see those studies

- Some folks will lose their homes

- noise



DATE 10/24/17

baltimorewashingtongmaglevproject.com

COMMENT FORM

Name: Rachel Alexander

Address: 106 Greenhill Rd.

City: Greenbelt State: MD Zip: 20770

Telephone: \_\_\_\_\_ Email: rtalexander@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: NOT SUPPORTIVE. I noticed that one plan has  
the Maglev tunnelling under Eleanor Roosevelt H.S.  
 I remember that the school's foundation started to  
sink after the school was built. they had to  
put up scaffolding to support the school in summer  
of 1982 because there were cracks in the  
walls especially in the auditorium because  
they built the school on top of an underground  
spring. I don't know if tunnelling 80 feet under the  
school would cause major foundation problems again.  
 I'm also NOT Supportive because I live in  
Greenbelt and do not want this train to ruin  
my community <sup>just</sup> so someone can go from Baltimore to D.C.!!



DATE 10/24/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Nickesha Brown

Address: 12243 Apache Tears Circle

City: Laurel State: MD Zip: 20709

Telephone: 908422 2121 Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I reviewed the propose route which goes directly over my house. I own the end unit on Apache Tears Circle and oppose to this project. I refuse to allow my home that I recently purchase to be demolished or <sup>my</sup> home life to be affected.

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DATE 10/24/2017

baltimorewashingtontscmaglevproject.com

**COMMENT FORM**

Name: Nickesha Brown  
 Address: 12243 Apache Tears Circle  
 City: Laners State: MD Zip: 20709  
 Telephone: 909 422 2121 Email: nicky16jme@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I own the end unit townhome and w very concerned with the disruption ~~the~~ in home life this project would cause. This would also affect my property value. I completely oppose to this ~~propoal~~ project



DATE 10/24/2017

baltimorewashingtongcmaglevproject.com

COMMENT FORM

Name: Julian J. Mitchell

Address: 12310 Sea Pearl Court

City: Laurel State: MD Zip: 20708

Telephone: 301-498-1243 Email: jimitchell@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: According to your information, this costly mode of transportation has not been proven to be viable in ANY part of the world. Therefore, I feel that it is not for this area. Many of the occupants of these homes are senior citizens, new-comers, etc. At this late date in our age, we (seniors) have no place to go, and certainly not interested in being uprooted at this late date in their lives. Please take into consideration that many of us have spent enormous amounts of money to upgrade our homes to make them more comfortable. To do this is unthinkable. If you have another option to construct this monstrosity, then please do so. Near Bowie State U. would be perfect. Trees don't mind being uprooted.



DATE \_\_\_\_\_

*bwmaglev.info*

**COMMENT FORM**

Name: James Behenna

Address: 12615 Cederbrook Lane

City: Laurel State: MD Zip: 20708

Telephone: \_\_\_\_\_ Email: jebenna@verizon.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Rte 295 is a Park way; I strongly oppose any route that will affect its beauty.





DATE 10-24-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Jessica Macauley

Address: 3513 St. Mary's View Rd.

City: Accokeek State: MD Zip: 20607

Telephone: 301-283-7194 Email: jessicasharese@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Hello, I would like to help you review the comments. I am available to review email comments at anytime. I can help categorize them, and hold them for response. I can also read comments in person on evenings and weekends.

My personal comment is: Please review the plans to see if there is an option to add on a continuous bike/foot path from DC to Baltimore. It could follow the path of the train. For safety reasons, there may be extra considerations that make it difficult, but there are this would be worth it.



DATE 10/24/17

bwmaglev.info

COMMENT FORM

Name: Ruth Anthony  
 Address: 9206 Isphahan loop  
 City: h central State: MD Zip: 20708  
 Telephone: \_\_\_\_\_ Email: RuthLANthony@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I AM NOT IN FAVOR OF THIS. I  
DO NOT SEE HOW THIS WILL AFFECT MY  
COMMUNITY AND MY COUNTY (PRINCE  
GEORGES) IN A POSITIVE WAY.

HOW WILL WE BENEFIT ECONOMICALLY?  
WHAT ABOUT THE ENVIRONMENT.  
OUR HOMES WILL BE AFFECTED!



DATE 10/24/17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kay Kane

Address: \_\_\_\_\_

City: Bowie State: MD Zip: 20720

Telephone: \_\_\_\_\_ Email: Kanekw@aol.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Questions...

Will build it if only Balt/DC route? What is point if not guaranteed other legs are in place to NY?

Why not invest in expanding existing infrastructure like Marc & Metro?

Will be an eye sore driving out of my neighborhood to see miles of elevated track in a residential area (live along proposed E1-Amtrak Modified route)

What will be cost to ride. Will it be affordable to anyone?





DATE \_\_\_\_\_

*bwmaglev.info*

**COMMENT FORM**

Name: Tiana Millard

Address: 3509 St. Mary's View RD

City: Accokeek State: Maryland Zip: 20607

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Hello, I do not think you should build the train, it can  
kill many people. I would rather keep my life than get somewhere  
in 15 minutes. And not only that but relatives live in these  
areas. This idea is smart but please think of a better idea  
that's much safer.



DATE 24 Oct 2017

baltimorewashingtongmaglevproject.com

**COMMENT FORM**

Name: Carol Plotnick

Address: 604 Park Ave

City: Laurel State: MD Zip: 20707

Telephone: \_\_\_\_\_ Email: cplot49@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am not in favor of this project. Few jobs, if any, will benefit local residents. There is a high impact potential to Nat'l. Park Service, Patuxent Refuge, and wet lands areas. Property values will be affected negatively.

I do not see why one project has one purpose - transport to and from airports. This should provide service for multiple purposes. The way it's designed this won't happen.



DATE 10-24-17

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Andre' L. Jones II  
 Address: 11213 Petworth Lane  
 City: Glenn Dale State: MD Zip: 20769  
 Telephone: 240 568 9998 Email: LHJONESMPH@aol.com  
301 538 3076  
 Join our mailing list?  Yes  No  
 Preferred method of communication?  Email  Mail

Comments: The E1 line is not practical. How does it benefit the residents of the neighborhoods the suspended line run through? The three stops (DC, BWI & Baltimore) are only beneficial to those that live in DC or Baltimore because that where the access points are located. I would still have to drive to DC, Baltimore & BWI for access. MARC

Just run more Camden Line & Penn Line trains with greater frequency. It can't be any more costly than this plan. Run more through trains without stops & run more trains with local stops. If people won't ride the





DATE 10/24/2017

[bwmaglev.info](http://bwmaglev.info)

**COMMENT FORM**

Name: Kenneth Parker

Address: 5520 Ruppel Dr. Se

City: Lanham State: MD Zip: 20706

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Maintenance yard - Will there be in  
Community-Residential or in Commercial zones?

How wide will the construction zone be  
if the Balt/Wash Parkway route is chosen?



DATE 10/24/17

bwmaglev.info

**COMMENT FORM**

Name: Valerie Anderson

Address: 6813 Beacon Place

City: Riverdale State: MD Zip: 20737-1701

Telephone: \_\_\_\_\_ Email: VJCASTLEBURY24@VERIZON.NET

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Concerns; Noise & vibration, magnetic concerns  
to for residents health (cancer etc), property values,  
foundation of our homes deteriorating (cracking, shaking)

DATE 10-25-17

baltimorewashingtonscmaglevproject.com

## COMMENT FORM

Name: Deindre Suddeth

Address: 465 Aventura Court

City: Glen Burnie State: MD Zip: 21061

Telephone: ~~410-381-1234~~ Email: deindre.suddeth@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I am for the NO BUILD Option. EM Radiation has not been well studied enough for my being comfortable exposing such a dense population as the Balt/DC corridor to massive amounts of EMR. 50 feet down or not. Have you looked at health studies of the population around the Japanese trains? Have such studies even been done? There is too much EMR ~~going~~ pollution already. Have you considered the studies showing the interference with Migratory birds? I wonder if any studies have been done looking into EMR interference with Hive/Colony collapse with bees. We don't understand how to interpret their dances. It may be EMR related. No Pollinators, expect famine.





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 10/25/2017

baltimorewashingtonscmaglevproject.com

COMMENT FORM

Name: Barbara Harper

Address: \_\_\_\_\_

City: Baltimore State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: \_\_\_\_\_

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: Baltimore ~~is~~ should be one of the stops on the Express line.

The Baltimore stop should be in downtown Baltimore and connect to MARC or AMTRAK.



DATE 10/25/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: Laura Kistler

Address: 6222 Woodland Rd

City: Linthicum Heights State: md Zip: 21090

Telephone: N/A Email: laurakistler@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: If this project were to come to pass how will Linthicum directly be affected - via construction & traffic. Our small town already experiences heavy wear & tear on its infrastructure? How will the maglev affect our property values and create more transience than the existing train line already does? What date do you see this project becoming operational? Although more jobs will be created - will any of these jobs benefit our community directly or will they be jobs only made available to ~~many~~ highly skilled & trained individuals. Lastly, what kind of safety risk does the maglev line pose for our small community?



DATE 10/25/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: Paul Kowzan

Address: 520 South Broadway

City: Baltimore State: MD Zip: ~~21098~~ 21231

Telephone: \_\_\_\_\_ Email: PKowza1@gmail.com

Join our mailing list?  Yes <sup>E-mail</sup> <sub>Please</sub>  No

Preferred method of communication?  Email  Mail

Comments: I think this project is  
transformative. I hope people understand  
the importance of better connecting  
our cities. Please Build it.





DATE 10-25-17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: MARY O'keeffe

Address: 804 S. Sharp St.

City: Baltimore State: MD Zip: 21230

Telephone: 240-602-0327 Email: mfo804@yahoo.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: move E1 tunnel route away from  
Sharp Street. The area is heavily residential &  
historic. Impractical to build a tunnel - would  
require numerous arrangements/agreements with  
residents.

DATE 10/25

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**Name: Jose DoraAddress: 2222 Sidney AveCity: Baltimore State: MD Zip: 21230Telephone: 301 802 6075 Email: josedora@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: I live in the Westport Zone along the Sidney Ave alignment I am interested in getting the latest updates on this since I feel as a community, we will be directly impacted. I suggest putting a station along the Westport Waterfront where Sagamore Development has recently purchased. This would encourage/push Sagamore to develop this beautiful site into a mixed use transit-oriented development. The existing housing stock in Westport would need to be rehabilitated, and taken by eminent domain.



DATE 10/25

baltimorewashingtongmaglevproject.com

COMMENT FORM

Name: Kyle B. Leggs  
 Address: BALTIMORE CITY PLANNING 417. E. Fayette St. BALD, MD 21201  
 City: BALT State: MD Zip: 21201  
 Telephone: (410) 396-4135 Email: Kyle.Leggs@baltimorecity.gov

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: With several proposed station AREAS in Baltimore, please be sure to notify Community groups in these areas. It would be good to notify Community leaders in Cherry Hill, Westport + Mt. Winans, Federal Hill, Otterbun, Sharp Leaden Hall, Candle Camden, etc. and Port Covington Area Reps. Please contact the Department of Planning for a list of Groups, and or visit our on-line Community Assoc. Directory (CAD).

Thanks.





DATE 10/25/17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: ADAM G.

Address: \_\_\_\_\_

City: \_\_\_\_\_ State: \_\_\_\_\_ Zip: \_\_\_\_\_

Telephone: \_\_\_\_\_ Email: ALG@BJJ@GMAIL.COM

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: NEEDS TO BE A PUBLIC MEETING WITH AGENDA IN ENGLISH

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DATE 10.25.17

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**

Name: KLAVS PHILIPSEN

Address: 429 N. EUTAW ST

City: BALTIMORE State: MD Zip: 21201

Telephone: 410-685-2002 Email: kphilip@archplan.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: \_\_\_\_\_

STATION AREA EVALUATION IN BALTIMORE  
NEEDS TO CONSIDER ~~TRIP~~ ACCESS FOR TRIPS TO DC  
AND FUTURE EXTENSION NORTH AS WELL AS  
TRANSFER FROM SC-MAGLEV TO NEC TO NYC/  
BOSTON. ISOLATING THE 1. PHASE MAKES NO  
SENSE WHEN IT COMES TO THE STATION IN BALTIMORE



DATE 10/25/17

baltimorewashingtonscmaglevproject.com

COMMENT FORM

Name: JAMES KNIGHTON

Address: 3811 CANTERBURY ROAD, APT. 207

City: BALTIMORE State: MD Zip: 21218

Telephone: 410-235-3045 Email: james1tk@comcast.net

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I do not support this project in general, as I think it is a poor use of taxpayer funds. However, if the project proceeds I would hope that the study team will spell out clearly and in detail how the proposed Baltimore station location will connect to existing MTA transit service, including local bus service, ~~which is also in the EIS~~

~~to~~ I have also noticed that alignments J and J1 will likely be affected by the Governor's proposed plan to add lanes to the Baltimore-Washington Parkway, which raises a question for me as to whether alignment E is the only feasible alignment. I hope the EIS will address this issue also.




**BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT**
DATE 10/25/2017

baltimorewashingtonscmaglevproject.com

**COMMENT FORM**Name: Brian C DyerAddress: 1223 W Cross StreetCity: Baltimore State: MD Zip: 21230Telephone: 410 830 1480 Email: badye87@gmail.comJoin our mailing list?  Yes  NoPreferred method of communication?  Email  Mail

Comments: Having something with ease of  
access <sup>like this</sup> is fantastic for Baltimore  
travelers and is a great replacement  
to air travel.

1. - I propose a inner harbor downtown  
location for a station as an  
unofficial representative for the city.  
Essentially all ~~transit~~ public transit  
modes are accessible.

2. Alternatively something further  
northeast would provide better access  
to Northern Baltimore area residents.

Comments: 3 Long Term a station at Dulles would make many lives easier for Baltimoreans taking long haul International Flights

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

**SCMAGLEV Project**  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





DATE 10/25/17

baltimorewashingtonscmaglevproject.com

COMMENT FORM

Name: Rene P S Bane

Address: 12108 Quick Fox Lane

City: Bowie State: MD Zip: 20720

Telephone: 290-976-8153 Email: \_\_\_\_\_

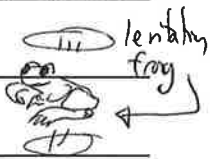
Join our mailing list?  Yes  No NOOOO - too many emails!!!

Preferred method of communication?  Email  Mail

Comments: I love the idea of maglev and was very excited to hear that it might be coming to this area, even though I personally would not benefit (other than possible reduced car traffic) (and if successful, more projects like this one).



But honestly, the "video" was super boring. Yes, it's good to know the process and to assure people that disruption, noise, and environmental destruction will be minimized.



More interesting would be snippets of animation showing how it worked, show the design itself (no engine noise, and since the trains are levitated, there is



Comments: NO noise from wheels on metal. The main noise would be air being pushed out of the way, ~~so~~ so talking about how the design of the trains (the long stubbed out "nose") and the track and walks will minimized that kind of noise.

you can also address safety designs too

Just sprinkle the important information among the interest-catching stuff. What good is an informational video if ~~no one~~ no one wants to watch it?



ok, yes, there are some people who will watch to the end. But imagine it on the web -- many people would click, but I feel few would watch it past the first few minutes. this can be fixed!

PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:

SCMAGLEV Project  
 c/o Mr. Bradley M. Smith  
 7201 Corporate Center Drive  
 Hanover, MD 21076

Comments also can be submitted via e-mail: [info@BaltimoreWashingtonSCMaglevProject.com](mailto:info@BaltimoreWashingtonSCMaglevProject.com)





BALTIMORE-WASHINGTON  
SUPERCONDUCTING MAGLEV PROJECT

DATE 25 Oct 17

baltimorewashingtontcmaglevproject.com

COMMENT FORM

Name:

Douglas Wise

Address:

2254 Sidney Ave

City:

Baltimore (Westport)

State:

MD

Zip:

21230

Telephone:

202-384-2383

Email:

douginbaltimore@gmail.com

Join our mailing list?

Yes

No

Preferred method of communication?

Email

Mail

Comments: I live in Westport and am psyched about the whole project. I'm already thinking of station locations. There is a push to eliminate the Wheelabrator Incinerator where Annapolis Rd meets Russell St. Close to Greyhound and stadiums, but close to water. Vulcan has a materials processing space on Annapolis Rd east of Manokin St. Across from that is an area of sparse housing. Around Cedley St and Manokin St is some unused CSX land and a block of mostly abandoned house. BGE tore down part of their power station between Annapolis Rd and Kloman St near Clare St. The 2300 block of Annapolis Rd North of the street has many houses in need of demolition and a recent fire. Manokin St near Tacoma St has a fair sized metal building for sale. If you're willing to go North of 295 look east of Maisel St. Between Norfolk St and Westport St is mostly city vehicle parking.







DATE 10/25/17

[baltimorewashingtonscmaglevproject.com](http://baltimorewashingtonscmaglevproject.com)

**COMMENT FORM**

Name: Diana Velasquez Kolnik

Address: 2261 Sidney Ave

City: Baltimore State: MD Zip: 21230

Telephone: 240 855 4784 Email: dianakolnik@gmail.com

Join our mailing list?  Yes  No

Preferred method of communication?  Email  Mail

Comments: I'm a resident of Westport and have some locations for track alignment I'd like to recommend in Westport.

① Previously CSX owned lot on the intersection of Cedley st and Manokin st. There is a block of vacant/boarded up properties on Cedley adjacent to lot. Also this property is only one block from Westport light rail station, which will help connect inner harbor/city residents. As well as those coming from Anne Arundel county.

② Location of Incinerator - although privately owned. There has been a local environmental push to get rid of the plant. The location of the incinerator is also a great alternative. Adjacent to incinerator is the local middle branch trail - which will help bike riders get to the location, also a Greyhound bus terminal, and a 24/7 casino

Comments: \_\_\_\_\_

which can help also with alleviating traffic congestion on the weekday / weekend. The incinerator is a good midpoint too between Federal Hill / Westport / inner harbor.

③ Industrial property - Manekin st / Tacoma st. - This industrial zoned property is currently for sale, there are a couple of rowhomes adjacent to property that were demolished. Location is central to 295 / 95 interstate highways.

④ Vulcan - PS - concrete - Annapolis Rd. They use space for concrete fabrication #2300 block of Annapolis Rd.

⑤ if Westport is considered as track alt. alignment is ~~at~~ considered after winter 2018, can we have a representative present at our Westport Neighborhood Association (WNA)?

**PLEASE PUT YOUR COMPLETED FORM IN ONE OF THE COMMENT BOXES. YOU ALSO MAY MAIL IT TO:**

**SCMAGLEV Project**  
c/o Mr. Bradley M. Smith  
7201 Corporate Center Drive  
Hanover, MD 21076

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SENT VIA EMAIL

June 1, 2017

JANET CAMPBELL-LORENC  
Director, Business Development

Corporate Planning

Mr. Bradley M. Smith  
Director of the Office of Freight and Multi-modalism  
Maryland Department of Transportation  
7201 Corporate Center Drive  
Hanover, Maryland 21076

**RE: PURPOSE AND NEED AND ALTERNATIVES SCREENING COMMENTS:  
BALTIMORE-WASHINGTON SUPERCONDUCTING MAGLEV ENVIRONMENTAL IMPACT STATEMENT**

Dear Mr. Smith:

Amtrak submits the following comments on the Federal Rail Administration's (FRA) and Maryland Department of Transportation's (MDOT) draft purpose and need statement and preliminary alternatives screening for the Baltimore-Washington Superconducting Maglev (SCMAGLEV) Environmental Impact Statement (EIS).

In summary, Amtrak has serious substantive and procedural concerns with the SCMAGLEV EIS, even based on the very limited publicly available documents. Amtrak urges that FRA and MDOT slow the pace of the EIS and take the time necessary to ensure that the EIS process does not get ahead of basic obligations such as clarifying the proposed project's Purpose and Need, informing the public and other stakeholders about how alternatives are being developed and how alternatives screening criteria are being applied, as well as clarifying how the proposed SCMAGLEV will be considered in light of other already planned and committed rail investment actions in the Northeast Corridor. The information that Amtrak has been able to review to date does not indicate that these issues, and others, have been addressed in a manner consistent with NEPA obligations.

1. The maglev Purpose and Need statement must be finalized prior to the development and screening of alternatives for the EIS.

The Purpose and Need statement is the first crucial decision-making milestone in an EIS. It is critical to the subsequent development, screening and analyses of project alternatives. The few documents that have been shared with the public to date make clear that the FRA has not approved the Purpose and Need statement for the Baltimore-Washington SCMAGLEV project.



Maryland Department of Transportation  
June 1, 2017  
Page 2 of 5



Despite this procedural omission, publicly available materials indicate that two rounds of alternatives development and screening have already been completed. The Purpose and Need of any EIS is absolutely crucial for guiding the development of project alternatives, and MDOT and FRA should fully vet and confirm the Purpose and Need statement prior to any alternatives development or screening work. Amtrak understands that, at this point, the Purpose and Need has not been finalized. Yet, it appears that other important analytical steps have been taken without completing this fundamental procedural decision. As a result, it is likely that updates to the SCMAGLEV Purpose and Need will require re-analysis of alternatives development and screening criteria, as well as additional public and stakeholder involvement.

- *Clarifying and resolving the Purpose and Need statement must be accomplished before any additional analysis is conducted.*

## 2. The maglev project EIS must fully demonstrate independent utility.

The project as scoped indicates to the public and other stakeholders that the EIS will analyze alternatives for and impacts of only the first segment of a SCMAGLEV line between Baltimore and Washington, D.C. However, public statements from the MAGLEV proponents clearly indicate that their intention for the project would extend this line from Baltimore to Boston, Massachusetts to the north, and from Washington D.C. extending to Charlotte, North Carolina to the south. There is no indication that the short Baltimore-Washington, D.C. segment demonstrates independent utility absent any commitment (planning, funding, or otherwise) for the remainder of the proposed action.

CEQ's NEPA regulations and case law interpreting those regulations prohibits a project sponsor from artificially separating out smaller segments of a project in the applicable environmental review, as segmentation leads to a NEPA document that does not consider potential environmental impacts on a sufficiently broad scope to fully inform the agency and the public about the true scope of impacts and additional and costly mitigation to address those impacts. In the transportation context, improper segmentation also likely restricts the consideration of alternatives for possible corridors/alignments, as well as other transportation improvements.

- *Amtrak is very skeptical of the true independent utility of a project that is characterized as the first stage of a much larger network, as the Baltimore-Washington SCMAGLEV has been publicly described.*



3. Revisions to the Purpose and Need statement must explicitly acknowledge that a multi-year passenger rail investment program, the Northeast Corridor Commission Capital Investment Plan, has already been collaboratively developed and adopted by the agency stakeholders, and that substantial funding has been committed by the FRA, the states and the NEC rail agencies for implementation of the Northeast Corridor Commission Capital Investment Plan.

Through leadership by the NEC Commission, the Baltimore-Washington corridor stakeholder agencies have agreed upon a multi-year NEC rail investment program. Amtrak collaborates with the Northeast Corridor Commission, the FRA, Maryland, MTA MARC, VRE, DDOT, WMATA, Virginia and the freight railroads to plan projects and invest in passenger rail between Baltimore and Washington, D.C. The most recent version of the rail investment program, the Northeast Corridor Commission Capital Investment Plan: Fiscal Years 2018-2022 has recently been approved. Each year, the NEC agencies also commit funding to an annual Spend Plan to carry out the investment plan. In fiscal year 2017, the NEC stakeholder agencies worked together to make an annual funding commitment of \$1.5 billion.

- *It is essential that the SCMAGLEV Purpose and Need explicitly acknowledge how the proposed action will coordinate with and/or conflict with the fully vetted and approved rail investment program, the Northeast Corridor Commission Capital Investment Plan.*
4. The maglev Purpose and Need statement must acknowledge that FRA's NEC FUTURE FEIS provides the comprehensive assessment of passenger rail transportation between Baltimore and Washington, DC.

NEC FUTURE is a landmark achievement and key affirmation of Amtrak's long-held view that rebuilding and expanding the Northeast Corridor is essential for the growth and prosperity of the entire region. After four years of study by the FRA, which involved the use of significant financial and human resources, and the engagement of all NEC stakeholders – the Federal government, state, cities, the railroads and the public – the recently-published Final EIS for NEC FUTURE recommends a comprehensive and coordinated planning and investment approach to address the NEC's current and future passenger rail needs.

- *NEC FUTURE must remain the prevailing guide for outlining the pathway of passenger rail for the entire length of the NEC – from Washington, D.C. to Boston. The Purpose and Need must clearly state how the maglev project fits into NEC FUTURE's plans for a comprehensive and connected passenger rail network serving the Northeast Corridor.*



Maryland Department of Transportation  
June 1, 2017  
Page 4 of 5



5. The maglev Purpose and Need statement must incorporate an assessment of the project's economic feasibility.

Amtrak is gravely concerned that there is no mention of economic feasibility of the proposed maglev project in any material provided to the public thus far. The Purpose and Need statement would be deficient without this criterion, as the proposed SCMAGLEV project would necessarily include the construction of an entirely new, very expensive, separate maglev network requiring substantial property acquisition, new elevated guideway and tunnel guideway, new trains, new stations, new parking lots, new propulsion power systems and facilities, new operational control and crew base facilities, and new train maintenance facilities.

Although the original SCMAGLEV concept was touted as a primarily private venture, at this point the project sponsors are very explicit that project funding from a mix of public and private sources is being sought, including an assumption of substantial federal funding. The cost-effectiveness of maglev must be assessed. In addition, the economic feasibility evaluation of a parallel maglev system must address the maglev effect on the existing NEC railroad. Given the assumption of substantial investment of public monies in the maglev project, the project sponsors must be required to demonstrate that the maglev system will not harm the investment in the existing NEC, and additionally provide a positive return on investment.

- *The Purpose and Need must incorporate a thorough assessment of economic feasibility.*

6. The alternatives screening criteria and the assessment underlying the application of these criteria must be made available for public comment.

The presentation materials provided at the April open houses briefly summarized the criteria used to develop, and then screen, an initial list of alternatives. However, the alternatives themselves, the thinking behind the criteria and the application of the criteria to the alternatives were never made available to the public for their feedback prior to the screening of alternatives by the project team. Two rounds of alternative screening by the project team have already occurred and resulted in alternatives that directly impact Amtrak operations, right-of-way, and financial viability without an opportunity for Amtrak or the public to comment.

At present, there is not enough detail provided in the EIS public documents for Amtrak to analyze and comment sufficiently on the development of alternatives, the development of screening criteria, or the application of screening criteria. Although Amtrak does not have all the information necessary to submit meaningful comments on this topic, we are gravely concerned that certain criteria have been elevated in importance above other perhaps equally important criteria to improperly narrow the scope of proposed alignments. All details concerning the alternative development and screening process need to be provided to Amtrak and members of the public to ensure the development of reasonable alternatives.



Maryland Department of Transportation  
June 1, 2017  
Page 5 of 5



- All materials bearing on the development of alternatives, the development of screening criteria, and the application of screening criteria to the selection of alternatives must be shared with the public.

We look forward to addressing these concerns with MDOT and FRA.

Sincerely,

A handwritten signature in blue ink that reads "Janet Campbell-Lorenc". The signature is fluid and cursive, with the first name "Janet" being the most prominent.

Janet Campbell-Lorenc, AICP  
Director, Business Development

cc: Stephen Gardner  
Byron Comati  
Michael Stern  
Mark Yachmetz  
Thomas Moritz  
Karen Gelman  
Jeff Gerlach  
Mitch Warren, NEC Commission  
Brandon Bratcher, FRA

Appendix D. Baltimore-Washington SCMAGLEV Technical  
Memorandum: Speed as a Screening Criterion.



# Baltimore-Washington SCMAGLEV

**Deliverable Name: Technical Memorandum:  
Speed As A Screening Criterion**



**EXECUTIVE SUMMARY** The purpose of this technical memorandum is to provide support for the operational and safety metrics detailed in the design criteria that are required to achieve the optimal speed of the SCMAGLEV technology and avoid the need for reduction in speed other than that imposed by the 1) normal acceleration and braking into/out of curves and 2) normal acceleration and braking into/out of stations. The SCMAGLEV project purpose is to evaluate and ultimately construct and operate a safe revenue producing, high-speed ground transportation system that achieves the optimal operating speed of the SCMAGLEV technology. The optimal speed of 500km/h will be used as a screening criterion in the evaluation of proposed alignments within the project study area.

### **Highest Optimal/Practical Operating Speed of 500km/h**

The SCMAGLEV technology is capable of achieving a maximum speed of 600km/h. A number of factors impact the determination of the optimal speed under the circumstances found in the 40-mile corridor between Washington, DC and Baltimore, MD with 3 station locations.

1. **Acceleration/Deceleration:** SCMAGLEV can accelerate to 500km/h in two minutes at a rate of 0.1g with no adverse impact on passenger comfort. With approximately 49 km between the yet to be proposed DC terminal station and Baltimore Washington International Airport (BWI) station, the train will travel about 4.1 minutes at the optimum speed after accelerating, and before deceleration is required to stop at the next station. On the BWI-Baltimore segment, the quick acceleration rate results in a travel time of 3.9 minutes; the train will not attain the cruising speed of 500km/h before having to decelerate.
2. **Energy Consumption:** Acceleration consumes the largest amount of energy. Achieving an optimal cruising speed of 500km/h within two minutes provides the most efficient, consistent run speed per megawatt of power over the distance between stations.
3. **Geometry:** Horizontal and vertical geometry has been developed and operationally established by the Central Japan Railway Company (JRC) through extensive research and testing over a 50-year period, resulting in minimum alignment radii of 8,000m horizontal and 40,000m vertical to permit the optimal operating speed. Any suboptimal geometry will require speed reductions, resulting in additional time and energy consumption for deceleration and acceleration, to ensure safe and comfortable operation.
4. **Passenger Comfort:** JRC confirmed through extensive testing on a 42.8km initial segment that the minimum geometry specified above resulted in a comfortable passenger experience at the optimal speed of 500km/h.
5. **Aerodynamics:** Factors such as air-friction between passing trains and tunnel pressure have been factored into the operational and safety metrics of the project achieve to an optimal speed of 500km/h.

The entire SCMAGLEV system – guideway geometry, tunnel configuration, power requirements, vehicle design, train control system, etc. – has been designed to operate at a maximum practical speed of 500km/h.

## Economics

This is a privately sponsored project intended to meet the challenge under the Maglev Deployment Program (MDP) and Notice of Funding Availability (NOFA) to deliver, through SCMAGLEV technology, a transportation system with the highest practical speed that results in dramatically improved runtimes between stations. Any suboptimal conditions that negatively impact a 500km/h cruising speed will adversely affect the economic viability of the project. Falling short of producing the optimal capacity of the SCMAGLEV system would result in the private project sponsor's (Baltimore Washington Rapid Rail (BWRR)) withdrawal from the project.

**Background** Over the past 25 years, the Federal Railroad Administration (FRA) and the State of Maryland have been studying a maglev service between Baltimore, MD and Washington, DC and advancing it into successive phases of development from feasibility assessment, to a demonstration project, and to the current development of a commercial system.

The National Maglev Initiative was established under the Intermodal Surface Transportation Efficiency Act of 1991. In 1994, a report entitled "Baltimore-Washington Corridor Maglev Feasibility Study" was prepared by the Maryland Transit Administration (MTA). The goal of the study was to assess the feasibility of maglev between Baltimore and Washington, DC. The criteria used required the alignment to allow a top speed of 483 km/h (300 mph). The study concluded that a maglev system connecting Baltimore and Washington, DC was feasible, and alignment and station options for such a system were identified.

Subsequently, the Transportation Equity Act for the 21<sup>st</sup> Century of 1998 established the Maglev Deployment Program (MDP) with the purpose of demonstrating the use of maglev. Through a nationwide competition, FRA selected seven states (including Maryland) to receive grants for pre-construction planning. The projects proposed by the seven states were considered the action alternative in the Programmatic Environmental Impact Statement (PEIS) for the MDP. In support of FRA's PEIS for the MDP, on behalf of Maryland, MTA undertook a second study to analyze alternatives between the two cities. The criteria employed by MTA for this demonstration project included a requirement of achieving a minimum top speed of 386 km/h (240 mph), and the alternatives not meeting the horizontal curvature requirement to attain such speed were dismissed. The three alternatives retained for the DEIS all met the speed criteria. The Maryland project was selected for continued evaluation and initial project development, including engineering design and analysis. In 2001, FRA published a Record of Decision (ROD) following completion of the PEIS. The purpose of this action was to demonstrate maglev technology by identifying a viable Maglev project in the US, and assisting a public/private partnership with the planning, financing, construction, and operation of a project. As published in the ROD, FRA concluded that Maglev was an appropriate technology for use as a new transportation option in Maryland and should be further studied at the project level.

Following the selection of Maryland in the MDP PEIS, in coordination with the MTA, FRA prepared and circulated a Draft Environmental Impact Statement

(DEIS) in 2003, for a demonstration project linking downtown Baltimore, BWI Marshall Airport, and Union Station in Washington, DC. The criteria used by MTA for this project included attaining a minimum top speed of 386 km/h (240 mph). The DEIS documented project needs, including transportation demand, regional economic growth, and reducing corridor congestion. The DEIS also documented feasible mitigation measures for the environmental impacts as well as the benefits of the project alternatives. In 2007, FRA prepared a Final Environmental Impact Statement (FEIS); however, the FEIS was not finalized.

In regard to the Baltimore-Washington SCMAGLEV Project, as was the case with the MDP PEIS, 2003 DEIS, and California High Speed Rail (CAHSR) environmental impact statements, speed has been used by FRA as an alternatives screening criterion in preparing NEPA documents. In fact, CAHSR NEPA documents cite the need to attain and maintain maximum operating cruising speeds of high speed rail between stations.

### **Why 500 km/h (311 mph)?**

The system will utilize SCMAGLEV technology as developed by JRC. SCMAGLEV technology allows for much faster speeds than the maglev technology existing at the time of the FRA MDP PEIS and the 2003 DEIS.<sup>1</sup>

SCMAGLEV service and runtimes need to be substantially better than other travel options in order to provide the mobility improvement at a level that will attract riders. At a cruising speed of 500 km/h, SCMAGLEV will be capable of 15-minute travel times between Baltimore and Washington, which is approximately two times faster than fastest intercity passenger rail travel time between the cities, approximately three times faster than the regional intercity passenger rail service, and approximately four times faster than commuter rail service between the cities.

Consistent with plans and goals of the Maryland Aviation Administration for BWI, and to adequately serve the mobility need that would be addressed by SCMAGLEV, the BWI station needs to be directly beneath the airport's central terminal area. Because not all SCMAGLEV trains will stop at BWI, the alignment needs to have express through-train geometry.

The SCMAGLEV system is able to achieve trip time goals in large part due to extremely quick acceleration of 0.01g, achieving full speed of 500 km/h in two minutes. By offering a substantially faster travel option between the cities, as compared with other options, SCMAGLEV will also address a key project need: reduce congestion on the roadways between the cities.

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<sup>1</sup> SCMAGLEV uses the principle of magnetic repulsion between sets of magnetic coils on the side of the u-shaped guideway and the vehicle for levitation, guidance, and propulsion. Maglev, as developed by Transrapid and considered in the 2003 DEIS, applies the principle of magnetic attraction between sets of coils on the guideway and the vehicle for levitation and with propulsion via a linear traveling electric field in the guideway on which vehicle rides, similar to a monorail. Due to technological limitations, Maglev can attain a maximum operating speed of approximately 431 km/h (268 mph). The SCMAGLEV system is able to attain higher speeds than Maglev because superconducting magnetism is much stronger than ordinary normal conducting electromagnets. Additionally, SCMAGLEV uses inductive magnetic reactions with no active control and rides in a U-shaped guideway; whereas, the German Transrapid system uses attractive reactions that need active controls and rides in a T-shaped guideway.



**Development and Testing of SCMAGLEV** JRC and its predecessor, Japan National Railways (JNR), have been researching and developing SCMAGLEV technology for over 50 years. JRC's 18.4 km (11.4 miles) Yamanashi test line, the most recent SCMAGLEV testing facility in Japan, opened in 1997. JRC constructed the Yamanashi line to be able to better test SCMAGLEV with tunnels, steeper gradients, and curves. MLX01 trains were tested on the Yamanashi test line from 1997 to 2011. The Yamanashi line was extended to 42.8 km (26.6 miles) and upgraded to commercial specifications, and re-opened in 2013 to test the L0 train, the vehicle planned for use when the 286 km Tokyo-Nagoya SCMAGLEV (Chuo Shinkansen) line commences revenue service in 2027. Since November 2014, JRC has operated passenger service on the Yamanashi line to preview SCMAGLEV with the public as the initial leg of Chou Shinkansen.

The decades of testing and empirical research have enabled JRC to develop commercial specifications (maximum operating speed and corresponding design criteria) for optimal performance of SCMAGLEV technology accounting for such considerations as aerodynamics, energy consumption, and passenger comfort. Based on the testing and research, which has been reviewed and accepted by the Maglev Technological Practicality Evaluation committee (MTPEC) under the Japanese Ministry of Land, Infrastructure, Transport and Tourism (MLIT), JRC has found that a speed of 500 km/h is the maximum operating speed for optimal performance. This speed corresponds with required minimum radii of 8,000 meters horizontal and 40,000 meters vertical.

**Geometry Requirements** The horizontal and vertical geometry of the SCMAGLEV technology developed by JRC correspond to an alignment that allows the optimum speed of 500km/h for SCMAGLEV technology and avoids the need for reduction in speed other than that imposed by the normal acceleration and braking curves into and out of stations. As has been operationally established through extensive research and testing of the SCMAGLEV technology, shifting direction at top speed must be safely and comfortably accommodated over a long and gradual geometry. This is one of the key factors in determining whether a future alignment is suitable for safe operation. If tighter curvature were to be introduced in order to avoid a specific impact, not only would speed be negatively impacted, it would increase maintenance requirements due to the enhanced geometric requirements of the guideway itself. For example, the length of the sidewall panels is reduced by about half for an 8000m curve compared to a tangent section. That means twice as many panels, which adds to both capital and maintenance costs. The panels contain the propulsion and levitation coils, which represent a significant cost component of the entire system.

**Passenger Comfort** Rider comfort is a critical factor underlying the geometric requirements. Passengers are sensitive to geometry changes including suboptimal banks, shifts in direction or changes in acceleration. As such, these have been minimized to the extent possible. The maximum allowable cant (super elevation) at high speed on a curve is 10 degrees. If the train has to slow down in a canted section, the motion of this shift will be perceptible and uncomfortable for passengers. An optimal system will not introduce substandard conditions because it is not commercially acceptable to have passengers discomforted by the effects of speed.

The proposed alignments selected for future alternatives analysis in the current NEPA process all meet minimum geometry requirements.

**Design Criteria** The following design criteria were established to guide the geometry of the alignments and are based on established minimum geometric and operating standards developed by JRC including such factors as passenger comfort and aerodynamics.

| Element                                                                                                 | Criteria                  |
|---------------------------------------------------------------------------------------------------------|---------------------------|
| Minimum radius <sup>2</sup>                                                                             | 16,000m (10 mi)           |
| Minimum radius for top speed operation <sup>3</sup>                                                     | 8000m (5mi)               |
| Minimum radius for slow speeds                                                                          | 800m (2600 ft)            |
| Minimum tangent section length at stations                                                              | 1000m (3300 ft)           |
| Maximum grade                                                                                           | 4%                        |
| Minimum vertical curve radius for top speed operation <sup>4</sup>                                      | 40,000m (25 mi)           |
| Minimum vertical curve radius at slow speeds                                                            | 3000m (1.9 mi)            |
| Maximum super elevation                                                                                 | 10 degrees                |
| Center-to-center spacing of guideways                                                                   | 5.8m (19 ft)              |
| Out-to-out dimension of elevated guideway (approx.)                                                     | 14m (46 ft)               |
| Internal tunnel diameter for two guideways (approx.)                                                    | 13m (43 ft)               |
| Minimum internal tunnel cross-sectional area above the guideways (governed by aerodynamic requirements) | 74m <sup>2</sup> (800 sf) |
| ROW limits for elevated structure                                                                       | 22m (72 ft)               |

**Conclusion** The ability of SCMAGLEV to achieve mobility, revenue-generation, and other objectives of the purpose and need is linked directly or indirectly to speed. The project depends on operating SCMAGLEV at a continuous cruising speed of 500 km/h accounting for such factors as power consumption, aerodynamics, and human comfort. The 15-minute runtime associated with 500 km/h provides travelers in the Baltimore-Washington region an option that is approximately two times faster than the current fastest option for travel between the cities and with a service frequency to provide substantial new travel capacity. In addition, the substantially short time may divert a segment of travelers in the Baltimore Washington region from travelling by automobile, and is expected to help reduce congestion on the region's roadways.

<sup>2</sup> SCMAGLEV uses circular curves with parabolic transition curves for horizontal geometry. A horizontal curve radius exceeding 16,000m provides an efficient operation.

<sup>3</sup> The minimum horizontal radius of 8000m requires 10% super elevation and shorter sidewall panels on the guideway resulting in higher capital and maintenance costs. The transition curve from an 8000m radius curve to a tangent section is 1100m long

<sup>4</sup> SCMAGLEV uses circular curves for vertical geometry. Vertical curves with radius of less than 30,000m require parabolic transition curves. No transition curves are needed for vertical curves with 40,000m radius. A 40,000m vertical curve that transitions from 0% to 4.0% grade is 1600m long.